Development Permit & Development Variance Permit

DP23-0135 DVP25-0101

This permit relates to land in the City of Kelowna municipally known as

211 Kneller Road

and legally known as

Lot A Section 27 Township 26 ODYD Plan EPP137707

and permits the land to be used for the following development:

Apartment Housing

ATTACHMENT A
This forms part of application
DP23-0135 DVP25-0101
City of Kelowna DEVELOPMENT PLANNING
Planner Initials MT

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

Date of Council Approval:	**June 17, 2025**
Development Permit Area:	Form & Character
Existing Zone:	CA1r – Core Area Mixed Use Rental Only
Future Land Use Designation:	C-NHD – Core Area Neighbourhood

This Development Permit is valid for two (2) years from the date of approval, with no opportunity to extend.

This is NOT a Building Permit.

In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

NOTICE

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner:	Provincial Rental Housing Corporation, Inc.No. BC0052129
Applicant:	Station One Architects

_____

Nola Kilmartin
Development Planning Department Manager
Planning & Development Services

_____

Date of Issuance

1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

2. CONDITIONS OF APPROVAL

THAT Council authorizes the issuance of Development Permit No. DP23-0135 and Development Variance Permit No. DVP25-0101 for Lot A Section 27 Township 26 ODYD Plan EPP137707 located at 211 Kneller Road, Kelowna, BC, subject to the following:

a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";

b) The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";

c) Landscaping to be provided on the land be in accordance with Schedule "C";

d) The applicant be required to post with the City a Landscape Performance Security deposit in the amount of 125% of the estimated value of the Landscape Plan, as determined by a Registered Landscape Architect;

AND THAT variances to the following sections of Zoning Bylaw No. 12375 be granted:

Table 8.3 – Required Residential Off-Street Parking Requirements
To vary the minimum number of off-street parking spaces from 39 stalls required to 33 stalls proposed.

Section 14.11 – Commercial and Urban Centre Zone Development Regulations
To vary the minimum front yard setback from 4.5 m permitted to 3.02 m proposed.

Table 14.11 – Commercial and Urban Centre Zone Development Regulations
To vary the minimum rear yard setback for an accessory building from 4.5 m permitted to 1.64 m proposed.

AND THAT the applicant be required to complete the above noted conditions of Council's approval of the Development Permit application in order for the permits to be issued;

AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

3. PERFORMANCE SECURITY

As a condition of the issuance of this Permit, Council is holding the security set out below to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Developer and be paid to the Developer or his or her designate if the security is returned. The condition of the posting of the security is that should the Developer fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the Municipality may use enter into an agreement with the property owner of the day to have the work carried out, and any surplus shall be paid over to the property owner of the day. Should the Developer carry out the development as per the conditions of this permit, the security shall be returned to the Developer or his or her designate following proof of Substantial Compliance as defined in Bylaw No. 12310. There is filed accordingly:

a) An Irrevocable Letter of Credit **OR** certified cheque **OR** a Surety Bond in the amount of **$593,356.25**

Before any bond or security required under this Permit is reduced or released, the Developer will provide the City with a statutory declaration certifying that all labour, material, workers' compensation and other taxes and costs have been paid.

4. INDEMNIFICATION

Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:

a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

The PERMIT HOLDER is the CURRENT LAND OWNER.
Security shall ONLY be returned to the signatory of the Landscape Agreement or their designates.

SITE SYMBOL LEGEND

GRID REFERENCE — 1

ROOM MARKER
- Room name ← ROOM NAME
- 101 ← ROOM NUMBER

SECTION MARKER
- 10 ← SECTION NUMBER
- A101 ← SHEET NUMBER

DETAIL MARKER
- 00 SIM ← DETAIL NUMBER
- A101 ← SHEET NUMBER

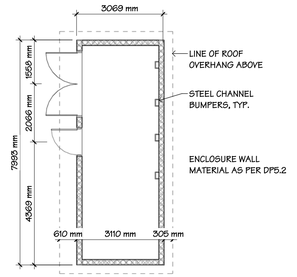
PROPERTY LINE

SETBACK LINE

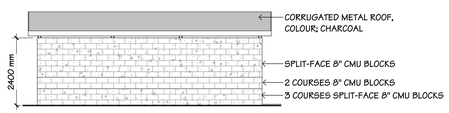
MISC. SYMBOLS
- HB — HOSE BIBB
- RWL — RAIN WATER LEADER
- GB — GAS BIBB
- FDC — FIRE DEPARTMENT CONNECTION
- F.H. — FIRE HYDRANT
- C.B. — CATCH BASIN
- L.D. — LAWN DRAIN
- LT — LIGHT FIXTURE

SITE HATCH LEGEND

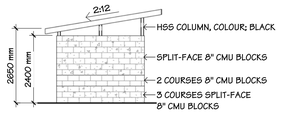
- PROPOSED BUILDING
- LANDSCAPING SPACE
- SIDEWALK
- GRAVEL
- CONCRETE
- BRICK PAVERS
- SOD
- RUBBER PLAY SURFACE



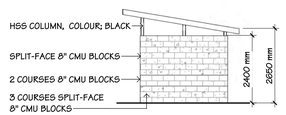
2 EXTERIOR STORAGE ENCLOSURE PLAN
1 : 100



3 ENCLOSURE EAST ELEVATION
1 : 100



4 ENCLOSURE NORTH ELEVATION
1 : 100



5 ENCLOSURE SOUTH ELEVATION
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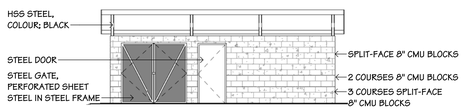
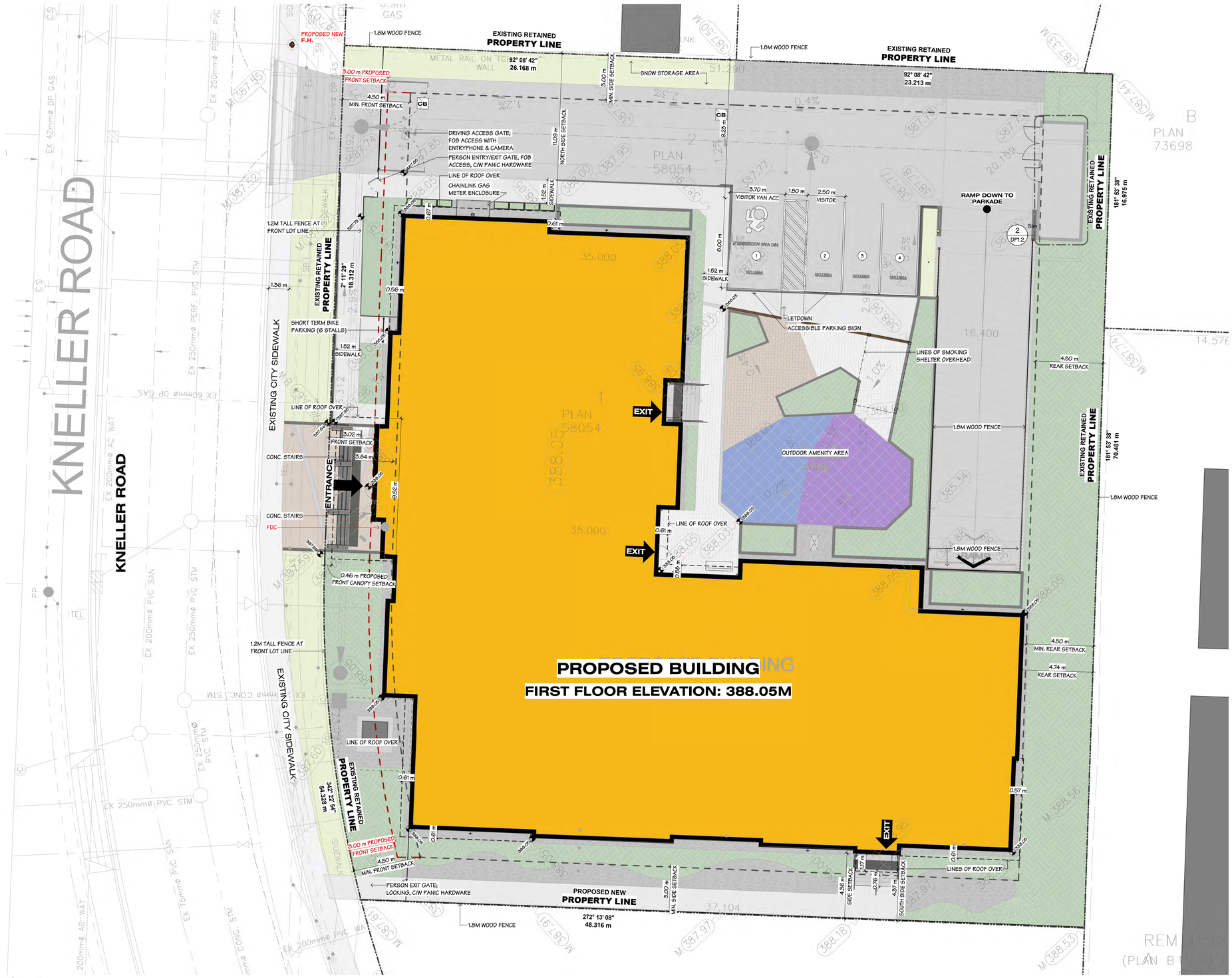


6 ENCLOSURE WEST ELEVATION
1 : 100



1 OVERALL SITE PLAN
1 : 125

NORTH

OVERALL SITE PLAN

KWS HOUSING

KELOWNA, B.C.

SCHEDULE A
This forms part of application
DP23-0135 DVP25-0101
City of Kelowna DEVELOPMENT PLANNING
Planner Initials MT

SCALE As indicated
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN S. D. EK PROVINCE OF BRITISH COLUMBIA
2025.05.12
2025-05-12

604 793 9445
soarchitects.com

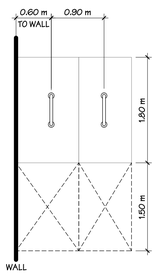
Chilliwack
2 - 9360 Mill St
V2P 4N2

5	2024.07.10	ISSUED FOR DEVELOPMENT PERMIT AMENDMENT
10	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

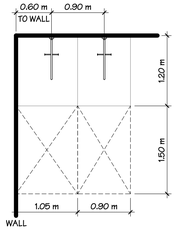
DP1.2

station one architects

FLOOR MOUNTED STALLS



WALL MOUNTED STALLS

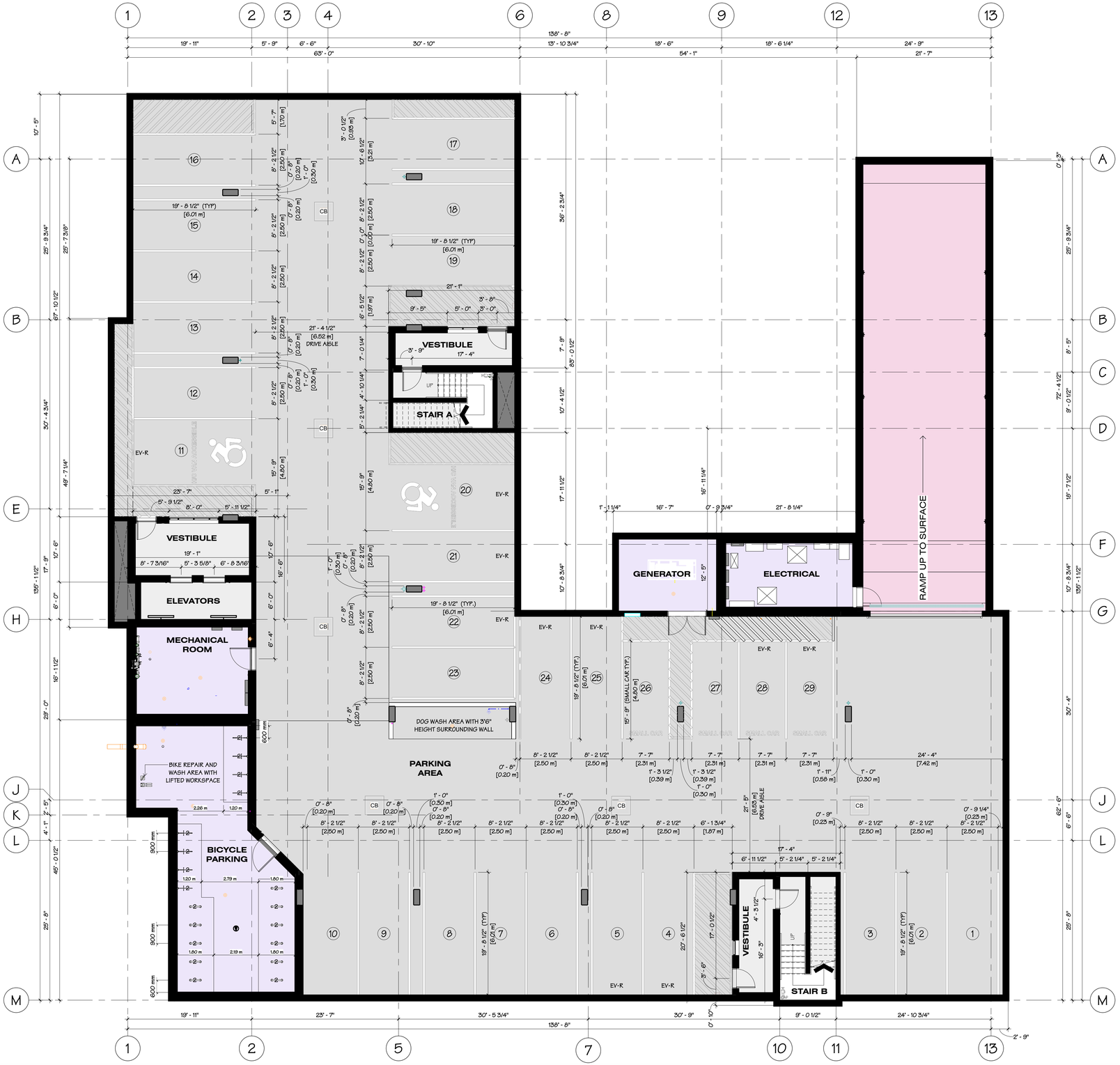


TYPICAL BIKE PARKING

1/4" = 1'-0"

ROOM TYPE LEGEND

- AMENITY
- CIRCULATION
- PARKING
- SERVICE



PARKING QUANTITIES

	REQUIRED	PROVIDED
VEHICLE	**31**	**29**
REGULAR	29	23
SMALL CAR	<15	4
ACCESSIBLE (VAN ACCESSIBLE)	2	2
VEHICLE VISITOR	**7**	**4**
BICYCLE	**37**	**38**
WALL MOUNT	<18	16
FLOOR MOUNT	>19	22

- VEHICLE PARKING REQUIREMENT INCLUDES BOTH RESIDENTS AND STAFF, ASSUMES 3 STAFF
- VISITOR ACCESSIBLE PARKING NOT FACTORED INTO REQUIRED ACCESSIBLE PARKING STALL COUNT
- ALL ACCESSIBLE PARKING SPACES SIZED TO FIT ACCESSIBLE VANS
- UTILIZES 10% PARKING REDUCTION PER 8.2.11(A)

STALL SIZING

AS PER TABLE 8.2.7.a:
DIMENSIONS OF PARKING SPACES AND DRIVE AISLES

STANDARD STALLS

CLEAR HEIGHT (MIN.)	2.0 M
WIDTH (MIN.)	2.5 M
LENGTH (MIN.)	6.0 M

SMALL CAR STALLS

CLEAR HEIGHT (MIN.)	2.0 M
WIDTH (MIN.)	2.3 M
LENGTH (MIN.)	4.8 M

ACCESSIBLE STALLS

ACCESSIBLE

CLEAR HEIGHT (MIN.)	2.3 M
WIDTH (MIN.)	3.7 M
LENGTH (MIN.)	6.0 M

VAN ACCESSIBLE

CLEAR HEIGHT (MIN.)	2.3 M
WIDTH (MIN.)	4.8 M
LENGTH (MIN.)	6.0 M

WIDTH ADJUSTMENTS

ABUTS 1 OBSTRUCTION	+ 0.2 M
ABUTS 2 OBSTRUCTIONS	+ 0.5 M
ABUTS DOOR	+ 0.8 M

DRIVE AISLE

ALL TWO-WAY DRIVE AISLES
SERVING 90 DEGREES PARKING
(E.G. PARKING LOT, PARKADE, GARAGE)

CLEAR WIDTH	6.5 M
CLEAR HEIGHT	2.0 M

BIKE PARKING

GENERAL

AS PER SECTIONS 8.5.6 & 8.5.7, AND TABLES 8.1 & 8.5.1

- A MIN. OF 50% OF THE REQ'D LONG-TERM BICYCLE PARKING SHALL BE GROUND-ANCHORED
- A MIN. OF 75% OF THE LONG-TERM BICYCLE PARKING SPACES SHALL BE LOCATED AT-GRADE OR WITHIN ONE STOREY OF FINISHED GRADE.

END-OF-TRIP FACILITIES

BICYCLE REPAIR & WASH STATION

- A BICYCLE REPAIR AND WASH STATION IS REQUIRED AFTER 20 LONG-TERM RESIDENTIAL BICYCLE PARKING STALLS
- THIS STATION INCLUDES
  1. TOOLS FOR BICYCLE REPAIR
  2. COMMERCIAL GRADE PUMP
  3. ACCESS TO WATER, AND
  4. A WAY TO RAISE A BIKE UP TO PERFORM SIMPLE MAINTENANCE

STALL DIMENSIONS

	GROUND	WALL
MIN. SPACE DEPTH	1.8 M	1.2 M
MIN. AISLE WIDTH	1.5 M	1.5 M
MIN. BETWEEN RACKS	0.9 M	0.9 M
MIN. BETWEEN RACK & DOOR/WALL	0.6 M	0.6 M

PARKADE PLAN

KWS HOUSING

KELOWNA, B.C.

SCHEDULE A
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Planner Initials MT

SCALE As indicated
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN R. DEA PROVINCE OF BRITISH COLUMBIA
2025-05-12

604 793 9445
soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

5	2024.07.10	ISSUED FOR DEVELOPMENT PERMIT AMENDMENT
13	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

DP2.0

station one architects

UNIT QUANTITIES

LEVELS	UNIT TYPES					
	STUDIO	1 BED ADAPTABLE	2 BED ADAPTABLE	2 BED ACCESSIBLE	3 BED	4 BED ADAPTABLE
LEVEL 1	0	3	3	1	2	1
LEVEL 2	2	3	4	1	2	1
LEVEL 3	2	3	4	1	2	1
LEVEL 4	2	3	4	1	2	1
TOTAL	6	12	15	4	8	4
OVERALL TOTAL UNITS						49

FIRST FLOOR PLAN

KWS HOUSING

KELOWNA, B.C.

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DP2.1

station one architects

ROOM TYPE LEGEND

- STUDIO
- 1 BEDROOM ADAPTABLE
- 2 BEDROOM ACCESSIBLE
- 2 BEDROOM ADAPTABLE
- 3 BEDROOM
- 4 BEDROOM ADAPTABLE
- AMENITY
- CIRCULATION
- SERVICE
- STAFF

UNIT QUANTITIES

LEVELS	UNIT TYPES					
	STUDIO	1 BED ADAPTABLE	2 BED ADAPTABLE	2 BED ACCESSIBLE	3 BED	4 BED ADAPTABLE
LEVEL 1	0	3	3	1	2	1
LEVEL 2	2	3	4	1	2	1
LEVEL 3	2	3	4	1	2	1
LEVEL 4	2	3	4	1	2	1
TOTAL	6	12	15	4	8	4
OVERALL TOTAL UNITS						49

SECOND FLOOR PLAN

KWS HOUSING

KELOWNA, B.C.

SCHEDULE A
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City of Kelowna DEVELOPMENT PLANNING

SCALE As indicated
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN R. DEA PROVINCE OF BRITISH COLUMBIA
2025-05-12

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soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

5	2024.07.10	ISSUED FOR DEVELOPMENT PERMIT AMENDMENT
13	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

DP2.2

station one architects

ROOM TYPE LEGEND

- STUDIO
- 1 BEDROOM ADAPTABLE
- 2 BEDROOM ACCESSIBLE
- 2 BEDROOM ADAPTABLE
- 3 BEDROOM
- 4 BEDROOM ADAPTABLE
- AMENITY
- CIRCULATION
- SERVICE
- STAFF

UNIT QUANTITIES

LEVELS	UNIT TYPES					
	STUDIO	1 BED ADAPTABLE	2 BED ADAPTABLE	2 BED ACCESSIBLE	3 BED	4 BED ADAPTABLE
LEVEL 1	0	3	3	1	2	1
LEVEL 2	2	3	4	1	2	1
LEVEL 3	2	3	4	1	2	1
LEVEL 4	2	3	4	1	2	1
TOTAL	6	12	15	4	8	4
OVERALL TOTAL UNITS						49

THIRD FLOOR PLAN

KWS HOUSING

KELOWNA, B.C.

SCHEDULE A
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DP23-0135 DVP25-0101
Planner Initials MT
City of Kelowna DEVELOPMENT PLANNING

SCALE As indicated
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN R. DEA PROVINCE OF BRITISH COLUMBIA
2025-05-12

604 793 9445
soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

5	2024.07.10	ISSUED FOR DEVELOPMENT PERMIT AMENDMENT
13	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

DP2.3

station one architects

UNIT QUANTITIES

LEVELS	UNIT TYPES					
	STUDIO	1 BED ADAPTABLE	2 BED ADAPTABLE	2 BED ACCESSIBLE	3 BED	4 BED ADAPTABLE
LEVEL 1	0	3	3	1	2	1
LEVEL 2	2	3	4	1	2	1
LEVEL 3	2	3	4	1	2	1
LEVEL 4	2	3	4	1	2	1
TOTAL	6	12	15	4	8	4
OVERALL TOTAL UNITS						49

FOURTH FLOOR PLAN

KWS HOUSING

KELOWNA, B.C.

SCHEDULE A
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Planner Initials MT

SCALE As indicated
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REGISTERED ARCHITECT JUSTIN R. DICK PROVINCE OF BRITISH COLUMBIA
2025.05.12
2025-05-12

604 793 9445
soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

5	2024.07.10	ISSUED FOR DEVELOPMENT PERMIT AMENDMENT
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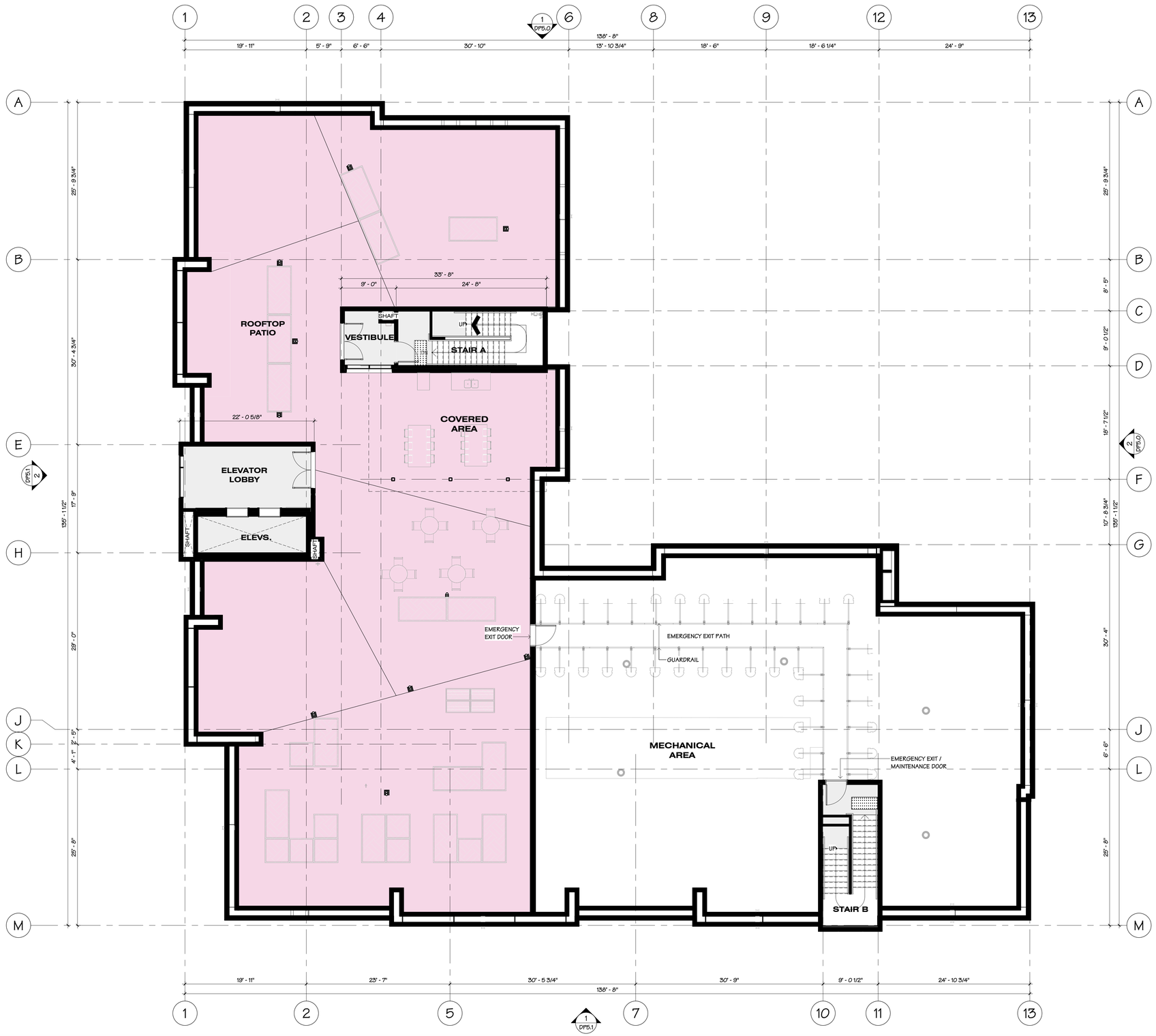
DP2.4

station one architects



ROOM TYPE LEGEND

- AMENITY
- CIRCULATION

ROOFTOP PATIO PLAN

KWS HOUSING

KELOWNA, B.C.

SCHEDULE A
This forms part of application
DP23-0135 DVP25-0101
City of Kelowna DEVELOPMENT PLANNING
Planner Initials MT

SCALE 1/8" = 1'-0"
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN R. DYCK PROVINCE OF BRITISH COLUMBIA
2025-05-12

604 793 9445
soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

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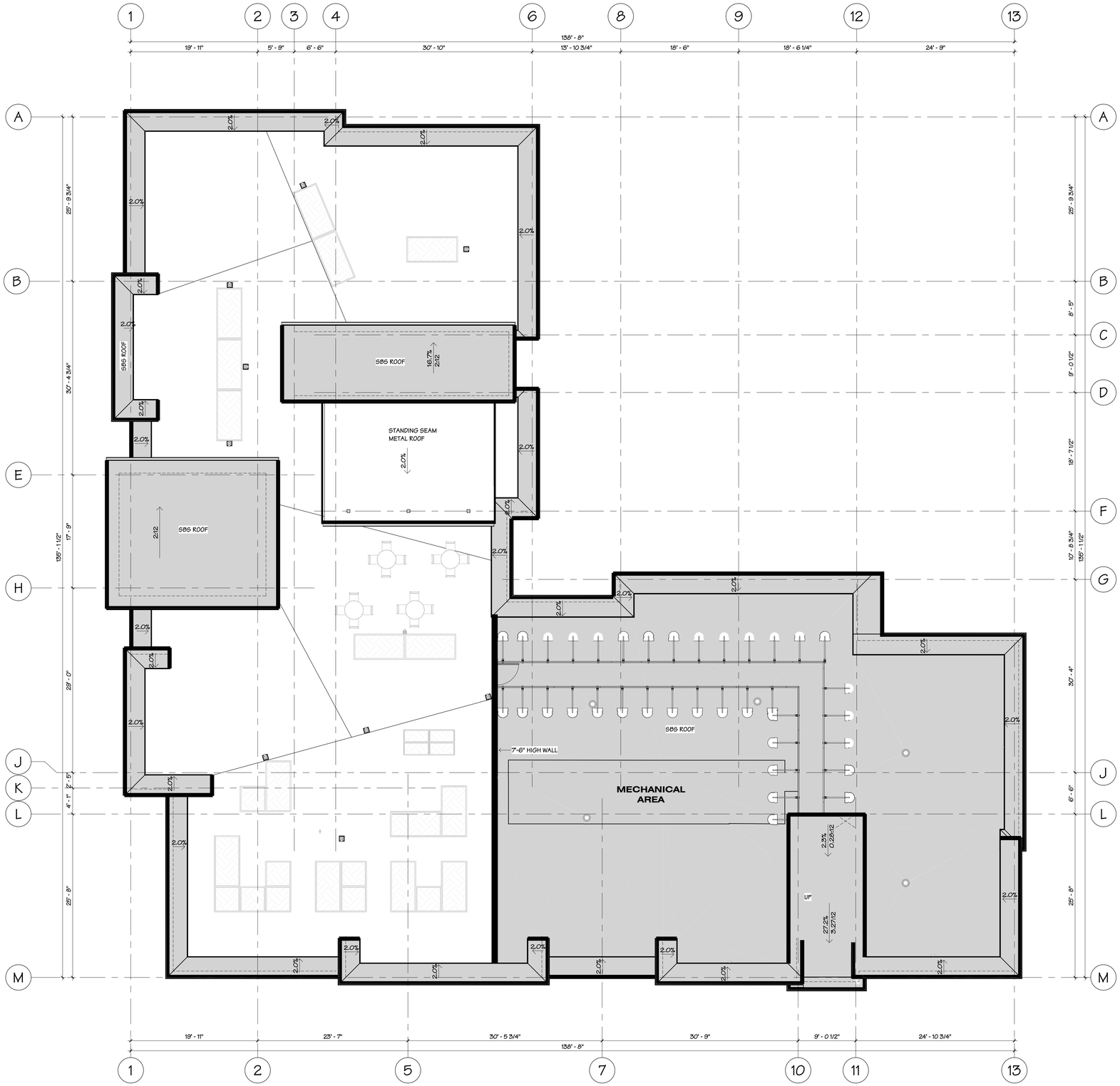
DP2.5

station one architects



ROOF PLAN

KWS HOUSING

KELOWNA, B.C.

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SCALE 1/8" = 1'-0"
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2025.05.12

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V2P 4N2

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DP2.6

station one architects

NORTH ELEVATION
1/8" = 1'-0"

EAST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISH SCHEDULE

1 FIBRE CEMENT SMOOTH PANEL I JAMES HARDIE
BENJAMIN MOORE "SILVER CHAIN" 1472

2 FIBER CEMENT SMOOTH PANEL I JAMES HARDIE
BENJAMIN MOORE "BLUE DANUBE" 2062-30

3 FIBER CEMENT SMOOTH PANEL I JAMES HARDIE
BENJAMIN MOORE "GREYSTONE" 1475

4 FIBER CEMENT CEDARMILL LAP SIDING I JAMES HARDIE
FISHER COATINGS TRUEGRAIN SERIES: CEDARTONE

5 NATURAL STONE VENEER I MUTUAL MATERIALS
ROCKY MOUNTAIN GRANITE LEDGESTONE I DRYSTACKED

6 TRIM I FASCIA I RAIN WATER LEADERS
TO MATCH VICWEST DEEP GREY

7 WOOD TIMBER BEAMS

8 RAILINGS
BLACK

9 WINDOW FRAMES
WHITE

10 RAISED SEAM ROOF
COLOUR TO MATCH VICWEST DEEP GREY

11 SBS TORCH ON ROOF
DARK CHARCOAL

12 SOFFIT I LONGBOARD
WOODGRAIN LIGHT CHERRY

13 COMPOSITE ALUMINUM PANELS
COLOUR TO MATCH CLEAR ANODIZED MULLIONS

14a SPLIT FACE CMU BLOCK
"EBONY"

14b CMU BLOCK
"EBONY"

ELEVATIONS

KWS HOUSING

KELOWNA, B.C.

SCHEDULE B
This forms part of application
DP23-0135 DVP25-010
Planner Initials MT
City of Kelowna DEVELOPMENT PLANNING

SCALE As indicated
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN R. DICK PROVINCE OF BRITISH COLUMBIA
2025.05.08
2025-05-12

604 793 9445
soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

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13	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

DP5.0

station one architects

SOUTH ELEVATION
1/8" = 1'-0"

WEST ELEVATION
1/8" = 1'-0"

EXTERIOR FINISH SCHEDULE

1 FIBRE CEMENT SMOOTH PANEL | JAMES HARDIE
BENJAMIN MOORE "SILVER CHAIN" 1472

2 FIBER CEMENT SMOOTH PANEL | JAMES HARDIE
BENJAMIN MOORE "BLUE DANUBE" 2062-30

3 FIBER CEMENT SMOOTH PANEL | JAMES HARDIE
BENJAMIN MOORE "GREYSTONE" 1475

4 FIBER CEMENT CEDARMILL LAP SIDING | JAMES HARDIE
FISHER COATINGS TRUEGRAIN SERIES; CEDARTONE

5 NATURAL STONE VENEER | MUTUAL MATERIALS
ROCKY MOUNTAIN GRANITE LEDGESTONE | DRYSTACKED

6 TRIM | FASCIA | RAIN WATER LEADERS
TO MATCH VICWEST DEEP GREY

7 WOOD TIMBER BEAMS

8 RAILINGS
BLACK

9 WINDOW FRAMES
WHITE

10 RAISED SEAM ROOF
COLOUR TO MATCH VICWEST DEEP GREY

11 SBS TORCH ON ROOF
DARK CHARCOAL

12 SOFFIT | LONGBOARD
WOODGRAIN LIGHT CHERRY

13 COMPOSITE ALUMINUM PANELS
COLOUR TO MATCH CLEAR ANODIZED MULLIONS

14a SPLIT FACE CMU BLOCK
"EBONY"

14b CMU BLOCK
"EBONY"

ELEVATIONS

KWS HOUSING

KELOWNA, B.C.

SCHEDULE B
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V2P 4N2

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13	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

DP5.1

station one architects

1 **WALL**
FIBRE CEMENT SMOOTH PANEL
JAMES HARDIE
COLOUR: BENJAMIN MOORE "SILVER CHAIN"

2 **WALL**
FIBER CEMENT SMOOTH PANEL
JAMES HARDIE
COLOUR: BENJAMIN MOORE "BLUE DANUBE"

3 **WALL**
FIBRE CEMENT SMOOTH PANEL
JAMES HARDIE
COLOUR: BENJAMIN MOORE "GREYSTONE"

4 **WALL**
FIBER CEMENT CEDARMILL LAPSIDING
FISHER COATINGS
COLOUR: TRUEGRAIN SERIES "CEDARTONE"

5 **WALL**
NATURAL STONE VENEER
MUTUAL MATERIALS
COLOUR: ROCKY MOUNTAIN GRANITE LEDGESTONE

6 **TRIM | FASCIA**
COLOUR TO MATCH VICWEST DEEP GREY

7 **WOOD TIMBER BEAMS**

8 **RAILINGS**
BLACK

9 **WINDOW FRAMES**
WHITE VINYL | CLEAR ANODIZED ALUMINUM

10 **RAISED SEAM ROOF**
COLOUR TO MATCH VICWEST DEEP GREY

11 **FLASHING**
COLOUR TO MATCH ADJACENT CLADDING

12 **EXTERIOR DOORS**
COLOUR: BENJAMIN MOORE "ASPHALT"

13 **EXTERIOR STORAGE ENCLOSURE**
8" SPLIT FACE CMU BLOCK
BASALITE
COLOUR: "EBONY"

14 **EXTERIOR STORAGE ENCLOSURE**
8" CMU BLOCK
BASALITE
COLOUR: "EBONY"

EXTERIOR FINISHES

KWS HOUSING

KELOWNA, B.C.

SCHEDULE B
This forms part of application
DP23-0135 DVP25-010
Planner Initials MT
City of Kelowna DEVELOPMENT PLANNING

SCALE 12" = 1'-0"
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN R. DYCK PROVINCE OF BRITISH COLUMBIA
2025-05-12

604 793 9445
soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

5	2024.07.10	ISSUED FOR DEVELOPMENT PERMIT AMENDMENT
13	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

station one architects

SCHEDULE C

This forms part of application
DP23-0135 DVP25-0101

Planner Initials MT

City of Kelowna
DEVELOPMENT PLANNING

LANDSCAPE LEGEND

KEY	REF.	DESCRIPTION
	4 LD-02	CONCRETE PAVING
Natural Tan	1,2 LD-02	CONCRETE UNIT PAVER
		LAWN
Blue Purple	6,7 LD-01	RUBBER SURFACING
	3 LD-03	DRIP STRIP
	4 LD-03	SEAT WALL
	1,2 LD-01	PROPOSED TREE Refer to tree schedule
	3 LD-01	SHRUB PLANTING
	5 LD-04	BIKE RACKS
	6 LD-04	FEATURE PEBBLE SEATING
	5 LD-05	BIRD BOX
	4 LD-04	BENCH
	8 LD-04	BOULDER AND LOGS

LIGHTING & FENCING

KEY	REF.	DESCRIPTION
	3 LD-05	RECESSED WALL LIGHT
	4 LD-05	BOLLARD LIGHT
	6 LD-02	PERIMETER FENCE 1.8M
	7 LD-02	FRONT YARD FENCE 0.9M

PLANT SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME	CONT	QTY
TREES			
	Acer saccharum / Sugar Maple	B&B, 5cm cal	2
	Cercidiphyllum japonicum / Katsura Tree	B&B, 5cm cal	1
	Cornus kousa / Kousa Dogwood	B&B, 3cm cal	3
	Picea pungens / Colorado Blue Spruce	B&B, 2.5m Ht	6
	Sophora japonica / Japanese Pagoda Tree	B&B, 5cm cal	1
STREET TREE			
	STREET TREE	B&B, 7cm cal	2

1:150 0 1.5m 3 4.5 6 7.5 9 10.5 12 13.5 15 16.5 18 19.5 21 22.5

VDZ+A
LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY
FORT LANGLEY STUDIO | 101-9181 Church St | Fort Langley, BC | V1M 1A0
MOUNT PLEASANT STUDIO | 102-355 Kingsway | Vancouver, BC | V5T 3J7
www.vdz.ca 604-882-0024

No.	By:	Description	Date
1	SZ	Issued for DP	Mar 04, 2024

REVISIONS TABLE FOR DRAWINGS

No.	By:	Description	Date

REVISIONS TABLE FOR SHEET

Project:
KWS HOUSING

Location:
Kneller Road,
Kelowna, BC

Drawn: SZ

Checked: KM

Approved: SH

Scale: 1:150

Stamp: BRITISH COLUMBIA SOCIETY OF LANDSCAPE ARCHITECTS REGISTERED MEMBER STEPHEN HELLER 546
2024-03-06

Original Sheet Size: 24"x36"

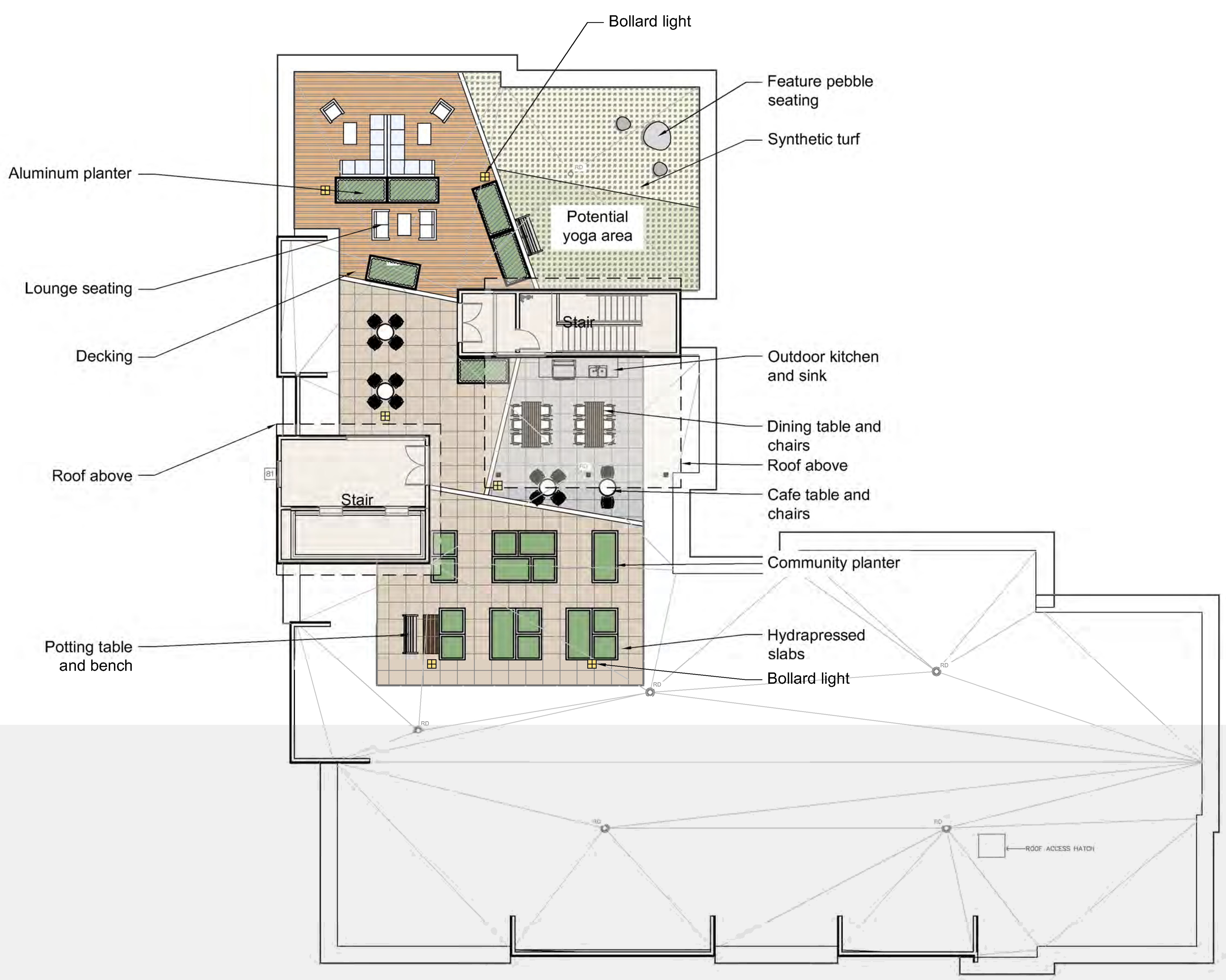
CONTRACTOR SHALL CHECK ALL DIMENSIONS ON THE WORK AND REPORT ANY DISCREPANCY TO THE CONSULTANT BEFORE PROCEEDING. ALL DRAWINGS AND SPECIFICATIONS ARE THE EXCLUSIVE PROPERTY OF THE OWNER AND MUST BE RETURNED AT THE COMPLETION OF THE WORK. ALL REZONING/DP/PPA/HA/BP DRAWINGS MUST NOT BE PRICED FOR CONSTRUCTION UNLESS LABELED ISSUED FOR TENDER/CONSTRUCTION.

Drawing Title: LANDSCAPE L1 SITE PLAN

NORTH

VDZ Project #: DP2023-28

Drawing #: L-01A



LANDSCAPE LEGEND

KEY	REF.	DESCRIPTION
Natural / Tan	3 LD-02	HYDRAPRESSED SLAB
	8 LD-01	DECKING
	2 LD-03	SYNTHETIC TURF
	2 LD-04	LOUNGE SEATING
	1 LD-04	CAFE TABLE AND CHAIRS
	5 LD-03	ALUMINUM PLANTERS
	6 LD-04	FEATURE SEATING PEBBLE

LIGHTING

KEY	REF.	DESCRIPTION
	4 LD-05	BOLLARD LIGHT

SCHEDULE C
This forms part of application
DP23-0135 DVP25-0101
Planner Initials MT
City of Kelowna
DEVELOPMENT PLANNING

1:150 0 1.5m 3 4.5 6 7.5 9 10.5 12 13.5 15 16.5 18 19.5 21 22.5

VDZ+A
LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

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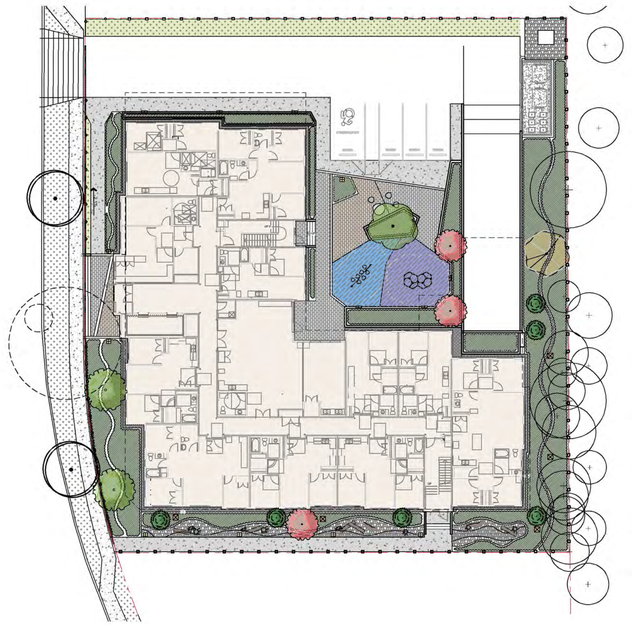
Drawing Title:
LANDSCAPE ROOF PLAN

NORTH

VDZ Project #:
DP2023-28

Drawing #:
L-02



1 LANDSCAPE SITE PLAN
Scale 1:300

PLANTING SCHEDULE

SHRUBS	BOTANICAL / COMMON NAME
Tb	Taxus baccata
Co	Corylopsis pauciflora
Ma	Mahonia aquifolium / Oregon Grape Holly
Po	Physocarpus opulifolius 'Tiny Wine' / "Tiny Wine" Ninebark
Ro	Rosmarinus lavandulaceus
Sj	Spiraea japonica `Magic Carpet` / Magic Carpet Spirea
Rh	Rhus typhina bailtiger
DWARF CONIFERS	**BOTANICAL / COMMON NAME**
Bm	Buxus microphylla japonica 'Winter Gem' / Winter Gem Japanese
Pm	Pinus mugo `Pumilio` / Mugo Pine
Js	Juniperus sabina 'Monna' / Calgary Carpet Juniper
GRASSES	**BOTANICAL / COMMON NAME**
Cm	Carex morrowii `Ice Dance` / Ice Dance Japanese Sedge
Ic	Imperata cylindrica 'Rubra' / Japanese Blood Grass
Pe	Pennisetum orientale `Karley Rose` / Karley Rose Fountain Gras
St	Stipa tenuissima
PERENNIALS	**BOTANICAL / COMMON NAME**
Nf	Nepeta x faassenii 'Walker's Low' / Walker's Low Catmint

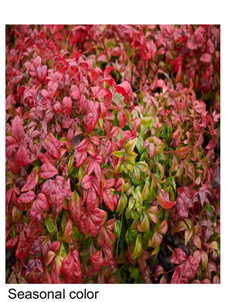
TREE SCHEDULE

TREES

	Acer saccharum / Sugar Maple	B&B, 5cm cal	2
	Cercidiphyllum japonicum / Katsura Tree	B&B, 5cm cal	1
	Cornus kousa / Kousa Dogwood	B&B, 3cm cal	3
	Picea pungens / Colorado Blue Spruce	B&B, 2.5m Ht	6
	Sophora japonica / Japanese Pagoda Tree	B&B, 5cm cal	1

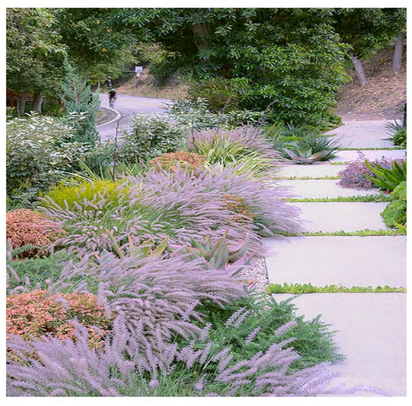
Bird Friendly Tree Species

Low Maintenance



Seasonal color

PRECEDENT IMAGES



COLOUR PALETTE
Chartreuse I Pinks I Yellows

VDZ+A
LANDSCAPE ARCHITECTURE | CIVIL ENGINEERING | URBAN FORESTRY

FORT LANGLEY STUDIO
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Key Map (NTS)

SCHEDULE C
This forms part of application
DP23-0135 DVP25-0101
Planner Initials MT
City of Kelowna DEVELOPMENT PLANNING

No.	By:	Description	Date
1	SZ	Issued for DP	Mar 04, 2024

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Approved: SH

Scale: NTS

Stamp: BRITISH COLUMBIA SOCIETY OF LANDSCAPE ARCHITECTS REGISTERED MEMBER STEPHEN HELLER 546 2024-03-06

Original Sheet Size: 24"x36"

Drawing Title: PLANTING PALETTE

NORTH

VDZ Project #: DP2023-28

Drawing #: PL-01

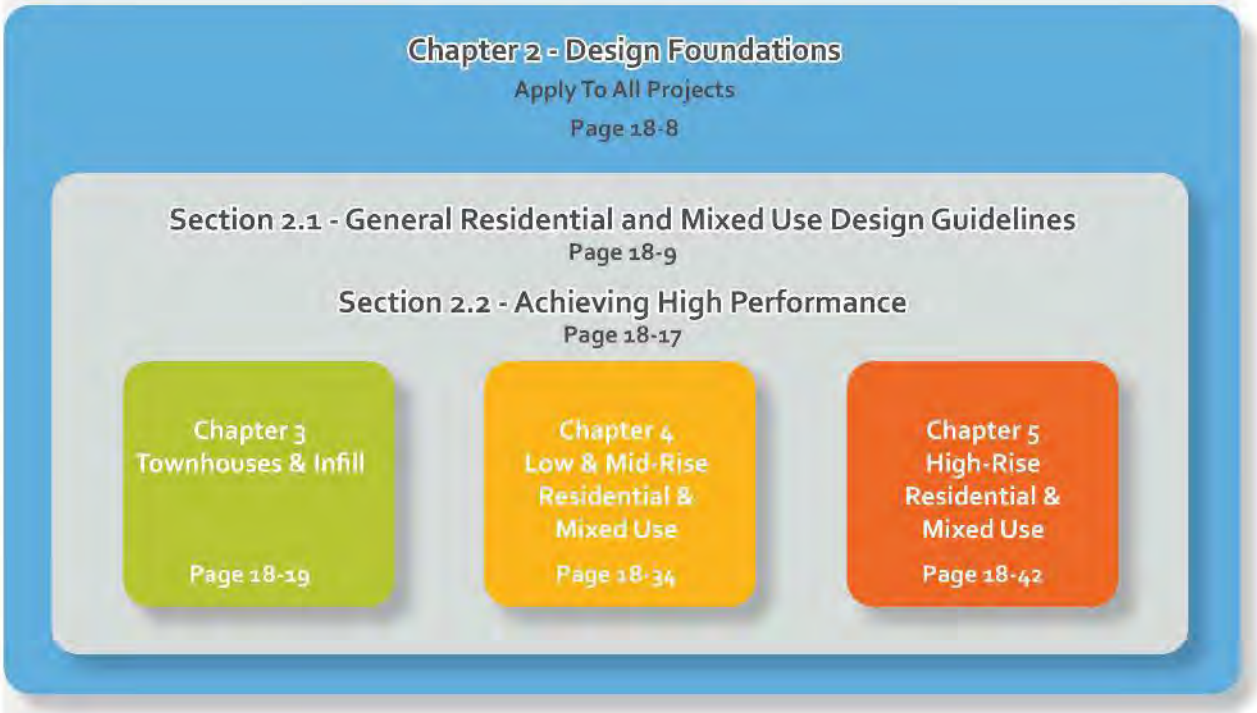
FORM & CHARACTER – DEVELOPMENT PERMIT GUIDELINES

Chapter 2 - The Design Foundations : apply to all projects and provide the overarching principles for supporting creativity, innovation and design excellence in Kelowna.

- Facilitate Active Mobility
- Use Placemaking to Strengthen Neighbourhood Identity
- Create Lively and Attractive Streets & Public Spaces
- Design Buildings to the Human Scale
- Strive for Design Excellence

The General Residential and Mixed Use Guidelines : provide the key guidelines that all residential and mixed use projects should strive to achieve to support the Design Foundations.

- The General Guidelines are supplement by typology-specific guidelines (e.g., Townhouses & Infill on page 18-19, High-Rise Residential and Mixed-Use on page 18-42), which provide additional guidance about form and character.



*Note: Refer to the Design Foundations and the Guidelines associated with the specific building typology.

Consideration has been given to the following guidelines as identified in Chapter 18 of the City of Kelowna 2040 Official Community Plan:

SECTION 2.0: GENERAL RESIDENTIAL AND MIXED USE						
RATE PROPOSALS COMPLIANCE TO PERTINENT GUIDELINE *(1 is least complying & 5 is highly complying)*	**N/A**	**1**	**2**	**3**	**4**	**5**
2.1 General residential & mixed use guidelines						
2.1.1 Relationship to the Street	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Orient primary building facades and entries to the fronting street or open space to create street edge definition and activity.						✓
b. On corner sites, orient building facades and entries to both fronting streets.	✓					
c. Minimize the distance between the building and the sidewalk to create street definition and a sense of enclosure.						✓
d. Locate and design windows, balconies, and street-level uses to create active frontages and 'eyes on the street', with additional glazing and articulation on primary building facades.						✓
e. Ensure main building entries are clearly visible with direct sight lines from the fronting street.						✓
f. Avoid blank, windowless walls along streets or other public open spaces.						✓
g. Avoid the use of roll down panels and/or window bars on retail and commercial frontages that face streets or other public open spaces.	✓					
h. In general, establish a street wall along public street frontages to create a building height to street width ration of 1:2, with a minimum ration of 11:3 and a maximum ration of 1:1.75. • Wider streets (e.g. transit corridors) can support greater streetwall heights compared to narrower streets (e.g. local streets); • The street wall does not include upper storeys that are setback from the primary frontage; and • A 1:1 building height to street width ration is appropriate for a lane of mid-block connection condition provided the street wall height is no greater than 3 storeys.						✓
2.1.2 Scale and Massing	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Provide a transition in building height from taller to shorter buildings both within and adjacent to the site with consideration for future land use direction.						✓
b. Break up the perceived mass of large buildings by incorporating visual breaks in facades.						✓
c. Step back the upper storeys of buildings and arrange the massing and siting of buildings to: • Minimize the shadowing on adjacent buildings as well as public and open spaces such as sidewalks, plazas, and courtyards; and • Allow for sunlight onto outdoor spaces of the majority of ground floor units during the winter solstice.	✓					

2.1.3 Site Planning	N/A	1	2	3	4	5
a. Site and design buildings to respond to unique site conditions and opportunities, such as oddly shaped lots, location at prominent intersections, framing of important open spaces, corner lots, sites with buildings that terminate a street end view, and views of natural features.						
b. Use Crime Prevention through Environmental Design (CPTED) principles to better ensure public safety through the use of appropriate lighting, visible entrances, opportunities for natural surveillance, and clear sight lines for pedestrians.						✓
c. Limit the maximum grades on development sites to 30% (3:1)	✓					
d. Design buildings for 'up-slope' and 'down-slope' conditions relative to the street by using strategies such as: • Stepping buildings along the slope, and locating building entrances at each step and away from parking access where possible; • Incorporating terracing to create usable open spaces around the building • Using the slope for under-building parking and to screen service and utility areas; • Design buildings to access key views; and • Minimizing large retaining walls (retaining walls higher than 1 m should be stepped and landscaped).	✓					
e. Design internal circulation patterns (street, sidewalks, pathways) to be integrated with and connected to the existing and planed future public street, bicycle, and/or pedestrian network.	✓					
f. Incorporate easy-to-maintain traffic calming features, such as on-street parking bays and curb extensions, textured materials, and crosswalks.	✓					
g. Apply universal accessibility principles to primary building entries, sidewalks, plazas, mid-block connections, lanes, and courtyards through appropriate selection of materials, stairs, and ramps as necessary, and the provision of wayfinding and lighting elements.						✓
2.1.4 Site Servicing, Access, and Parking	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Locate off-street parking and other 'back-of-house' uses (such as loading, garbage collection, utilities, and parking access) away from public view.						✓
b. Ensure utility areas are clearly identified at the development permit stage and are located to not unnecessarily impact public or common open spaces.						✓
c. Avoid locating off-street parking between the front façade of a building and the fronting public street.						✓
d. In general, accommodate off-street parking in one of the following ways, in order of preference: • Underground (where the high water table allows) • Parking in a half-storey (where it is able to be accommodated to not negatively impact the street frontage);						✓

• Garages or at-grade parking integrated into the building (located at the rear of the building); and • Surface parking at the rear, with access from the lane or secondary street wherever possible.						
e. Design parking areas to maximize rainwater infiltration through the use of permeable materials such as paving blocks, permeable concrete, or driveway planting strips.	✓					
f. In cases where publicly visible parking is unavoidable, screen using strategies such as: • Landscaping; • Trellises; • Grillwork with climbing vines; or • Other attractive screening with some visual permeability.	✓					
g. Provide bicycle parking at accessible locations on site, including: • Covered short-term parking in highly visible locations, such as near primary building entrances; and • Secure long-term parking within the building or vehicular parking area.						✓
h. Provide clear lines of site at access points to parking, site servicing, and utility areas to enable casual surveillance and safety.						✓
i. Consolidate driveway and laneway access points to minimize curb cuts and impacts on the pedestrian realm or common open spaces.						✓
j. Minimize negative impacts of parking ramps and entrances through treatments such as enclosure, screening, high quality finishes, sensitive lighting and landscaping.						✓
2.1.5 Streetscapes, Landscapes, and Public Realm Design	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Site buildings to protect mature trees, significant vegetation, and ecological features.	✓					
b. Locate underground parkades, infrastructure, and other services to maximize soil volumes for in-ground plantings.						✓
c. Site trees, shrubs, and other landscaping appropriately to maintain sight lines and circulation.						✓
d. Design attractive, engaging, and functional on-site open spaces with high quality, durable, and contemporary materials, colors, lighting, furniture, and signage.						✓
e. Ensure site planning and design achieves favourable microclimate outcomes through strategies such as: • Locating outdoor spaces where they will receive ample sunlight throughout the year; • Using materials and colors that minimize heat absorption; • Planting both evergreen and deciduous trees to provide a balance of shading in the summer and solar access in the winter; and • Using building mass, trees and planting to buffer wind.						✓
f. Use landscaping materials that soften development and enhance the public realm.						✓

g.	Plant native and/or drought tolerant trees and plants suitable for the local climate.						✓
h.	Select trees for long-term durability, climate and soil suitability, and compatibility with the site's specific urban conditions.						✓
i.	Employ on-site wayfinding strategies that create attractive and appropriate signage for pedestrians, cyclists, and motorists using a 'family' of similar elements.	✓					
2.1.6 Building Articulation, Features and Materials		**N/A**	**1**	**2**	**3**	**4**	**5**
a.	Express a unified architectural concept that incorporates variation in façade treatments. Strategies for achieving this include: • Articulating facades by stepping back or extending forward a portion of the façade to create a series of intervals or breaks; • Repeating window patterns on each step-back and extension interval; • Providing a porch, patio, or deck, covered entry, balcony and/or bay window for each interval; and • Changing the roof line by alternating dormers, stepped roofs, gables, or other roof elements to reinforce each interval.						✓
b.	Incorporate a range of architectural features and details into building facades to create visual interest, especially when approached by pedestrians. Include architectural features such as: bay windows and balconies; corner feature accents, such as turrets or cupolas; variations in roof height, shape and detailing; building entries; and canopies and overhangs. Include architectural details such as: Masonry such as tiles, brick, and stone; siding including score lines and varied materials to distinguish between floors; articulation of columns and pilasters; ornamental features and art work; architectural lighting; grills and railings; substantial trim details and moldings / cornices; and trellises, pergolas, and arbors.						✓
c.	Design buildings to ensure that adjacent residential properties have sufficient visual privacy (e.g. by locating windows to minimize overlook and direct sight lines into adjacent units), as well as protection from light trespass and noise.						✓
d.	Design buildings such that their form and architectural character reflect the buildings internal function and use.						✓
e.	Incorporate substantial, natural building materials such as masonry, stone, and wood into building facades.						✓
f.	Provide weather protection such as awnings and canopies at primary building entries.						✓
g.	Place weather protection to reflect the building's architecture.						✓
h.	Limit signage in number, location, and size to reduce visual clutter and make individual signs easier to see.	✓					
i.	Provide visible signage identifying building addresses at all entrances.						✓

SECTION 4.0: LOW & MID-RISE RESIDENTIAL MIXED USE						
RATE PROPOSALS COMPLIANCE TO PERTINENT GUIDELINE *(1 is least complying & 5 is highly complying)*	**N/A**	**1**	**2**	**3**	**4**	**5**
4.1 Low & mid-rise residential & mixed use guidelines						
4.1.1 Relationship to the Street	**N/A**	**1**	**2**	**3**	**4**	**5**
i. Ensure lobbies and main building entries are clearly visible from the fronting street.						✓
j. Avoid blank walls at grade wherever possible by: • Locating enclosed parking garages away from street frontages or public open spaces; • Using ground-oriented units or glazing to avoid creating dead frontages; and • When unavoidable, screen blank walls with landscaping or incorporate a patio café or special materials to make them more visually interesting.						✓
Residential & Mixed Use Buildings						
k. Set back residential buildings on the ground floor between 3-5 m from the property line to create a semi-private entry or transition zone to individual units and to allow for an elevated front entryway or raised patio. • A maximum 1.2 m height (e.g. 5-6 steps) is desired for front entryways. • Exceptions can be made in cases where the water table requires this to be higher. In these cases, provide a larger patio and screen parking with ramps, stairs and landscaping.				✓		
l. Incorporate individual entrances to ground floor units accessible from the fronting street or public open spaces.				✓		
m. Site and orient buildings so that windows and balconies overlook public streets, parks, walkways, and shared amenity spaces while minimizing views into private residences.						✓
4.1.2 Scale and Massing	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Residential building facades should have a maximum length of 60 m. A length of 40 m is preferred.						✓
b. Residential buildings should have a maximum width of 24 m.						✓
c. Buildings over 40 m in length should incorporate a significant horizontal and vertical break in the façade.	✓					
d. For commercial facades, incorporate a significant break at intervals of approximately 35 m.	✓					
4.1.3 Site Planning	**N/A**	**1**	**2**	**3**	**4**	**5**
a. On sloping sites, floor levels should step to follow natural grade and avoid the creation of blank walls.	✓					
b. Site buildings to be parallel to the street and to have a distinct front-to-back orientation to public street and open spaces and to rear yards, parking, and/or interior court yards:						✓

• Building sides that interface with streets, mid-block connections and other open spaces and should positively frame and activate streets and open spaces and support pedestrian activity; and • Building sides that are located away from open spaces (building backs) should be designed for private/shared outdoor spaces and vehicle access.						
c. Break up large buildings with mid-block connections which should be publicly-accessible wherever possible.	✓					
d. Ground floors adjacent to mid-block connections should have entrances and windows facing the mid-block connection.	✓					
4.1.4 Site Servicing, Access and Parking	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Vehicular access should be from the lane. Where there is no lane, and where the re-introduction of a lane is difficult or not possible, access may be provided from the street, provided: • Access is from a secondary street, where possible, or from the long face of the block; • Impacts on pedestrians and the streetscape is minimised; and • There is no more than one curb cut per property.						✓
b. Above grade structure parking should only be provided in instances where the site or high water table does not allow for other parking forms and should be screened from public view with active retail uses, active residential uses, architectural or landscaped screening elements.	✓					
c. Buildings with ground floor residential may integrate half-storey underground parking to a maximum of 1.2 m above grade, with the following considerations: • Semi-private spaces should be located above to soften the edge and be at a comfortable distance from street activity; and • Where conditions such as the high water table do not allow for this condition, up to 2 m is permitted, provided that entryways, stairs, landscaped terraces, and patios are integrated and that blank walls and barriers to accessibility are minimized.						✓
4.1.5 Publicly-Accessible and Private Open Spaces	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Integrate publicly accessible private spaces (e.g. private courtyards accessible and available to the public) with public open areas to create seamless, contiguous spaces.	✓					
b. Locate semi-private open spaces to maximize sunlight penetration, minimize noise disruptions, and minimize 'overlook' from adjacent units.						✓
Outdoor amenity areas						
c. Design plazas and urban parks to: • Contain 'three edges' (e.g. building frontage on three sides) where possible and be sized to accommodate a variety of activites; • Be animated with active uses at the ground level; and • Be located in sunny, south facing areas.	✓					
d. Design internal courtyards to:						✓

• Provide amenities such as play areas, barbecues, and outdoor seating where appropriate. • Provide a balance of hardscape and softscape areas to meet the specific needs of surrounding residents and/or users.						
e. Design mid-block connections to include active frontages, seating and landscaping.	✓					
Rooftop Amenity Spaces						
f. Design shared rooftop amenity spaces (such as outdoor recreation space and rooftop gardens on the top of a parkade) to be accessible to residents and to ensure a balance of amenity and privacy by: • Limiting sight lines from overlooking residential units to outdoor amenity space areas through the use of pergolas or covered areas where privacy is desired; and • Controlling sight lines from the outdoor amenity space into adjacent or nearby residential units by using fencing, landscaping, or architectural screening.						✓
g. Reduce the heat island affect by including plants or designing a green roof, with the following considerations: • Secure trees and tall shrubs to the roof deck; and • Ensure soil depths and types are appropriate for proposed plants and ensure drainage is accommodated.					✓	
4.1.6 Building Articulation, Features, and Materials	**N/A**	**1**	**2**	**3**	**4**	**5**
a. Articulate building facades into intervals that are a maximum of 15 m wide for mixed-use buildings and 20 m wide for residential buildings. Strategies for articulating buildings should consider the potential impacts on energy performance and include: • Façade Modulation – stepping back or extending forward a portion of the façade to create a series of intervals in the façade; • Repeating window pattern intervals that correspond to extensions and step backs (articulation) in the building façade; • Providing a porch, patio, deck, or covered entry for each interval; • Providing a bay window or balcony for each interval, while balancing the significant potential for heat loss through thermal bridge connections which could impact energy performance; • Changing the roof line by alternating dormers, stepped roofs, gables, or other roof elements to reinforce the modulation or articulation interval; • Changing the materials with the change in building plane; and • Provide a lighting fixture, trellis, tree or other landscape feature within each interval.						✓
b. Break up the building mass by incorporating elements that define a building's base, middle and top.						✓
c. Use an integrated, consistent range of materials and colors and provide variety, by for example, using accent colors.						✓
d. Articulate the façade using design elements that are inherent to the buildings as opposed to being decorative. For example, create						✓

	depth in building facades by recessing window frames or partially recessing balconies to allow shadows to add detail and variety as a byproduct of massing.						
e.	Incorporate distinct architectural treatments for corner sites and highly visible buildings such as varying the roofline, articulating the façade, adding pedestrian space, increasing the number and size of windows, and adding awnings or canopies.	✓					
f.	Provide weather protection (e.g. awnings, canopies, overhangs, etc.) along all commercial streets and plazas with particular attention to the following locations: • Primary building entrances;, • Adjacent to bus zones and street corners where people wait for traffic lights; • Over store fronts and display windows; and • Any other areas where significant waiting or browsing by people occurs.	✓					
g.	Architecturally-integrate awnings, canopies, and overhangs to the building and incorporate architectural design features of buildings from which they are supported.						✓
h.	Place and locate awnings and canopies to reflect the building's architecture and fenestration pattern.						✓
i.	Place awnings and canopies to balance weather protection with daylight penetration. Avoid continuous opaque canopies that run the full length of facades.						✓
j.	Provide attractive signage on commercial buildings that identifies uses and shops clearly but which is scaled to the pedestrian rather than the motorist. Some exceptions can be made for buildings located on highways and/or major arterials in alignment with the City's Sign Bylaw.	✓					
k.	Avoid the following types of signage: • Internally lit plastic box signs; • Pylon (stand alone) signs; and • Rooftop signs.	✓					
l.	Uniquely branded or colored signs are encouraged to help establish a special character to different neighbourhoods.	✓					

FRONT ENTRANCE - LOOKING NORTH/EAST

FRONT ENTRANCE - LOOKING SOUTH/EAST

RENDERINGS

KWS HOUSING

KELOWNA, B.C.

ATTACHMENT C
This forms part of application
DP23-0135 DVP25-0101
Planner Initials MT
City of Kelowna
DEVELOPMENT PLANNING

SCALE
JOB NO. 22052

REGISTERED ARCHITECT JUSTIN R. DECK PROVINCE OF BRITISH COLUMBIA
2023.03.10
2025-05-12

604 793 9445
soarchitects.com

Chilliwack
2 - 9360 Mill St
V2P 4N2

5	2024.07.10	ISSUED FOR DEVELOPMENT PERMIT AMENDMENT
13	2025.05.08	DEVELOPMENT PERMIT AMENDMENT

DP6.0

station one architects

OUTDOOR AMENITY - AERIAL VIEW

OUTDOOR AMENITY & PARKADE ENTRANCE - LOOKING SOUTH/WEST

RENDERINGS

KWS HOUSING

KELOWNA, B.C.

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DP6.1

station one architects

OUTDOOR AMENITY & PARKING - LOOKING SOUTH

OUTDOOR AMENITY - LOOKING NORTH/WEST

RENDERINGS

KWS HOUSING

KELOWNA, B.C.

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DP6.2

station one architects

BC HOUSING

Provincial Rental Housing Corporation (PRHC)



Affordable Housing Project for Women and Children

Kneller Road Development Permit
Parking Rationale

May 02, 2025

Report Compiled by:

TERRA
Social Purpose Real Estate

Background

The Provincial Rental Housing Corporation (PRHC) intends to develop and own an affordable housing project for women and their dependent children at the Kneller Road site. PRHC will engage an experienced housing operator from the region to operate the housing project.

There is a strong need in Kelowna for affordable housing with services for women and children, particularly those who have experienced or are at risk of experiencing domestic violence. According to Statistics Canada, Kelowna saw an average increase in rents for one and two-bedroom apartments of over 20% between October 2017 and October 2020, putting them out of reach for many women-led families. The new Kneller Road project will provide 49 new safe, affordable homes for women and children.

"Women and children are…unable to secure safe, affordable housing in the community"

Site

Four identified properties have been consolidated into one for the development: 175, 235, 239 Kneller Road & 1161 Kneller Court, Kelowna. Current zoning applicable to the site is MF-3R Apartment Housing Rental Only. Under the current Zoning Bylaw, according to Section 8.2.11(a), the project is allowed for a 10% parking reduction, therefore requiring a total of 39 parking stalls.

The project team is proposing 26 parking stalls based on the following:

Table 1- Parking Requirement Zoning Bylaw Sections 8.3 and 8.4

	Current Bylaw Requirement for Supportive Housing	Proposed # of Stalls	Variance Requested
Parking Stall for Supportive Units	30	26	4
EV Charging Stalls	12	10	2
Parking Stalls for Staff	3	3	0
Visitor Parking	6	4	2
Van Accesable Stalls	2	2	0
Bike Parking	37	38	0

PRHC is requesting a variance to reduce the parking requirement for the Kneller Road development from 39 to 31 stalls. The bylaw standard significantly exceeds anticipated demand, and full compliance would require substantial site redesigning, reduced buildable residential area, and result in the loss of supportive housing units, compromising both project viability and affordability objectives.

The following rationale presents a clear justification for the requested parking variance, based on available transportation options, reduced parking demand for supportive housing, alignment with City policy, and the secured housing agreement and covenant on title.

Transportation Amenities Provided

Proximity to Amenities

The new women and children housing project is conveniently located with many stores, restaurants, and services available at the nearby Kelowna Crossing Shopping Centre.

Table 2: Amenities

Community Amenity/ Facility	Distance from Site (meters)	Public Transportation Time
Pharmacy	600m	10 mins
Religious Services	150m	0 mins
Food/Restaurants	300m	0 mins
Daycare	750m	10 mins
Shopping	150m	8 mins

Public Transit Options

The Project proposed properties are currently vacant land and will add reasonable density around existing transit corridors, therefore, encouraging increased ridership and improving the overall reliability and frequency of the current bus services. The Project is well located with a nearby bus stop for both Bus Route 8 and Route 10.

Route 8 is the University/ OK College line. It connects the project to the east of the City of Kelowna from the Kelowna-Rock Creek Highway. It brings the tenants north to the University of British Columbia Okanagan and south to Okanagan College, encouraging the potential for some of the clients to access education opportunities. Route 10 is the North Rutland line. It connects the project to downtown Kelowna and the Queensway Exchange as well as to Rutland. Both buses pass through the Orchard Park bus loop, offering the many services and shops available in the mall as well as many more transit route options.

Decreased Parking Demands for Supportive/ Affordable Housing

Women and children fleeing violence typically have limited access to private vehicles. Many arrive under urgent circumstances with few possessions and rely on walking, transit, cycling, or transportation coordinated through support services. This demographic trend significantly reduces the demand for on-site vehicle parking.

Based on existing BC Housing projects of similar scale and location, it is anticipated that the demand for parking by the future tenants of the Project will be significantly lower than for a market rental project. Please see the data below for comparable projects:

Table 3: Comparable Projects' Parking Ratios

	Location	Shelter (Beds)	Transitional (Units)	2nd Stage (Units)	Affordable (Units)	Total Units/ Beds	Parking Provided	Ratio
Raincity Women and Children Housing	**Surrey**	30	100			130	30	.23
Cythera Women and Children Housing	**Maple Ridge**		14	22		34	11	.32
Tamitik Women of Status and Children Housing	**Kitimat**		16	10	20	46	30	.54

As you can see, the average Parking Ratio in those suburban areas is about 0.33, which aligns with the previous Parking Bylaw that we have adhered to and exceeded.

Several completed studies echo this statement, including reports completed by the City of Kamloops, City of Vancouver, and City of Victoria. The City of Kamloops

Affordable Housing Developer Package states that, based on parking demand trends, social housing parking requirements are 0.25 spots per unit.

Further, a comparison of the parking uptakes of similar affordable housing projects (scale and location) in Kelowna and other locations in the Okanagan (Penticton) was carried out to establish reasonable parking uptake expectations in affordable housing developments. The tables below show the differences between the number of units and the number of stalls in use for Valley housing projects.

Table 4: Housing Projects Parking Uptake in the City of Kelowna

City	Location	# of Units	# of Stalls in Use	% of Stalls in Use
Kelowna	**Okanagan College (KLO Rd)**	36 (41 tenants)	21	58%
Kelowna	**Near Bernard Avenue (Downtown Core)**	43 (45 tenants)	10	4%
Kelowna	**Nissen Crossing (Next to the proposed site)**	78 (88 tenants)	21	32%
Total		157 (174 Tenants)	52	31%

Table 5: Housing Project Parking Uptake in Other Cities in the Interior

City	Building Name	# of Units	# of Stalls in Use	% of Stalls in Use
Penticton	**Creekside Terrace**	34	28	82%
Penticton	**Reeds Corner**	20	6	30%
Penticton	**Tower Apartments**	124	36	29%
Total		178	70	39%

Existing Kelowna Policy

The Healthy Housing Strategy (HHS) was adopted by the Mayor and Council in June 2018 and includes information linking health and housing, and policy priorities to encourage the development of healthy housing. While the proposed project positively relates to all four healthy housing links identified in the report, one of expressed importance is Community and Location; by providing housing that is well situated in a walkable neighbourhood with strong transit (including active transit) connections, this project is the type identified in this strategy as providing healthy housing.

The strategy also identifies key directions and actions that benefit the provision of healthy housing. One direction given is to improve housing affordability and reduce barriers to affordable housing. Parking costs are one of the greatest barriers to the provision of affordable housing. The report indicates that "by setting minimum parking requirements, the City is driving up housing costs and disincentivizing sustainable modes of transportation." A key policy direction provided in the HHS is to reduce parking requirements for infill and affordable housing, and Appendix E-VI provides specific direction to identify opportunities for off-street parking reductions and/or parking elimination.

Housing Agreement and Covenant on Title

PRHC is committed to the long-term provision of affordable housing with the Project. Therefore, PRHC is prepared to work with an experienced Housing Operator from the region to secure an extended housing tenure through a housing agreement with a covenant registered on title to ensure the ongoing affordability of the proposed units. This will guarantee that the Project is operated and maintained as affordable housing in a manner that is consistent with protecting long-term affordability.

Climate Action

As the City of Kelowna's own Climate Action webpage states, "Kelowna has experienced the havoc that climate change can cause to a community, with back-to-

back years of record spring precipitation causing historic flooding, followed by hot, dry summers that contributed to droughts and wildfires (2017 and 2018)".

The Climate Projection for the Okanagan Region report (February 2020) shows that as greenhouse gas (GHG) emissions continue to grow, changes like these will become more common in the next three decades with hotter, drier summers; warmer winters; increased precipitation in all seasons except summer; and a shifting of the seasons. The wildfires in the Okanagan and across BC in the summer of 2021 demonstrate the need for clear action to address extreme weather events caused by climate change. Reduced single occupant vehicle use and greater uptake of transit, walking, cycling, and other environmentally sustainable options must be embraced by developers of housing projects such as the one PRHC is proposing.

Conclusion

The Kneller Road development is envisioned as a safe, secure refuge for women and their children, supported by an experienced housing operator committed to providing the necessary resources for long-term stability. The project will deliver high-quality, affordable housing connected to the broader community, supported by multiple transit options and secured through a housing agreement and covenant on title.

Affordability is critical to the success of residents, enabling access to essential supports such as education, healthcare, food security, and social inclusion. Parking requirements are a significant barrier to affordability, particularly for this population, which has low vehicle ownership. Similar parking reductions have been supported across the province, and we respectfully request that the City of Kelowna approve a variance to allow the proposed 26 parking stalls.