

# Development Permit

DP23-0071



This permit relates to land in the City of Kelowna municipally known as

**2727 Highway 97 N**

and legally known as

**Lot B District Lot 124 ODYD Plan 39765**

and permits the land to be used for the following development:

**Commercial – Automotive & Equipment**

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

**Date of Council Approval: August 14, 2023**

Development Permit Area: Form and Character

Existing Zone: C2 – Vehicle Oriented Commercial

Future Land Use Designation: RCOM – Regional Commercial

**This Development Permit is valid for two (2) years from the date of approval, with no opportunity to extend.**

## **This is NOT a Building Permit.**

In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

## **NOTICE**

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner: 389481 B.C. Ltd., Inc. No. BC1340661

Applicant: Dialog BC Architectural Engineering Interior Design Planning Inc

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Terry Barton  
Development Planning Department Manager  
Planning & Development Services

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Date of Issuance

## 1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

## 2. CONDITIONS OF APPROVAL

THAT Council authorizes the issuance of Development Permit No. DP23-0071 for Lot B District Lot 124 ODYD Plan 39765 located at 2727 Highway 97 N, Kelowna, BC, subject to the following:

- a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";
- b) The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";
- c) Landscaping to be provided on the land be in accordance with Schedule "C";
- d) The applicant be required to post with the City a Landscape Performance Security deposit in the amount of 125% of the estimated value of the Landscape Plan, as determined by a Registered Landscape Architect;

AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Manager approval, with no opportunity to extend.

## 3. PERFORMANCE SECURITY

As a condition of the issuance of this Permit, Council is holding the security set out below to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Developer and be paid to the Developer or his or her designate if the security is returned. The condition of the posting of the security is that should the Developer fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the Municipality may use enter into an agreement with the property owner of the day to have the work carried out, and any surplus shall be paid over to the property owner of the day. Should the Developer carry out the development as per the conditions of this permit, the security shall be returned to the Developer or his or her designate following proof of Substantial Compliance as defined in Bylaw No. 12310. There is filed accordingly:

- a) An Irrevocable Letter of Credit **OR** certified cheque **OR** a Surety Bond in the amount of **\$143,617.50**

Before any bond or security required under this Permit is reduced or released, the Developer will provide the City with a statutory declaration certifying that all labour, material, workers' compensation and other taxes and costs have been paid.

## 4. INDEMNIFICATION

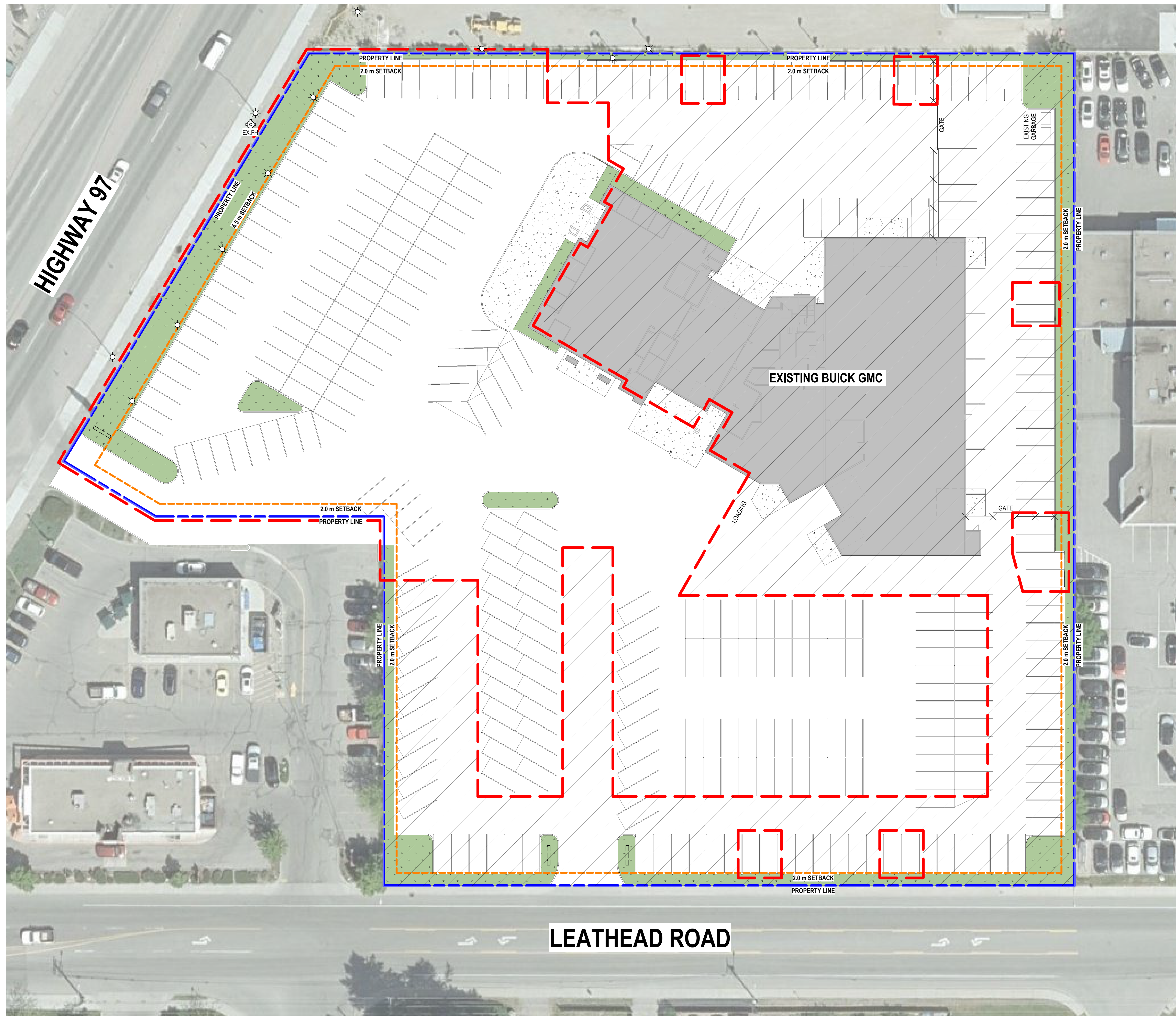
Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:

- a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

**The PERMIT HOLDER is the CURRENT LAND OWNER.  
Security shall ONLY be returned to the signatory of the  
Landscape Agreement or their designates.**





**EXISTING SITE PLAN LEGEND**

|  |                                  |
|--|----------------------------------|
|  | SCOPE EXTENT                     |
|  | PROPOSED PROPERTY LINE           |
|  | EXISTING PROPERTY LINE           |
|  | SETBACK LINE                     |
|  | EXISTING BUILDING                |
|  | EXISTING CONCRETE SIDEWALK & PAD |
|  | EXISTING LANDSCAPE AREA          |
|  | EXISTING FIRE HYDRANT            |
|  | EXISTING LIGHT POLE              |

**SCHEDULE A**

This forms part of application # DP23-0071

Planner Initials **AF**

ISSUED FOR

1 2022-03-21 DEVELOPMENT PERMIT

**METRIC**

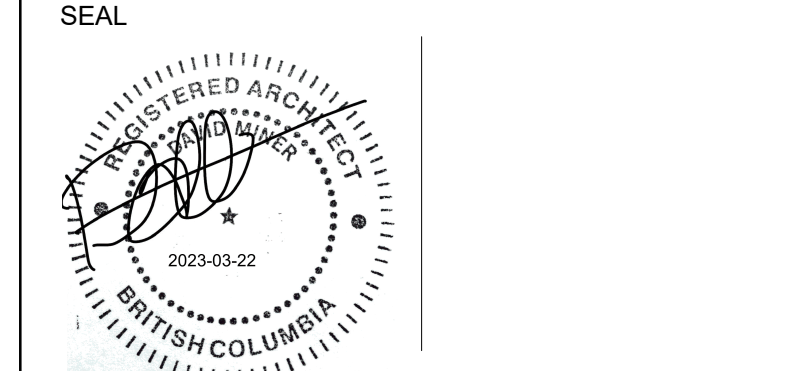
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Tel: 604-255-1169
- STRUCTURAL ENGINEER**
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- ENERGY CONSULTANT**
- REVOLVE ENGINEERING INC.  
9529 72 AVENUE NW  
EDMONTON, AB T6E 0Y5  
Tel: 780-619-2270



**KELOWNA CADILLAC**  
### Kelowna, BC

**OVERALL EXISTING SITE PLAN**

DRAWN: RE CHECKED: IC  
PLOT DATE: 3/22/2023 5:26:20 PM

**DP1.02**

BC100304 - 2006333

TRUE NORTH PROJECT NORTH

1 SITE PLAN  
DP1.02 SCALE: 1:300





**SITE PLAN LEGEND**

|  |                                  |
|--|----------------------------------|
|  | SCOPE EXTENT                     |
|  | PROPOSED PROPERTY LINE           |
|  | EXISTING PROPERTY LINE           |
|  | SETBACK LINE                     |
|  | PROPOSED BUILDING                |
|  | EXISTING BUILDING                |
|  | CONCRETE SIDEWALK & PAD          |
|  | EXISTING CONCRETE SIDEWALK & PAD |
|  | LANDSCAPE AREA                   |
|  | EXISTING LANDSCAPE AREA          |
|  | PRINCIPAL ENTRANCE LOCATION      |
|  | OVERHEAD GRADE DOOR LOCATION     |
|  | BUILDING EXIT LOCATION           |
|  | FIRE HYDRANT                     |
|  | NEW FH                           |
|  | EXISTING FIRE HYDRANT            |
|  | EXISTING LIGHT POLE              |

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City of Kelowna  
COMMUNITY PLANNING

**METRIC**

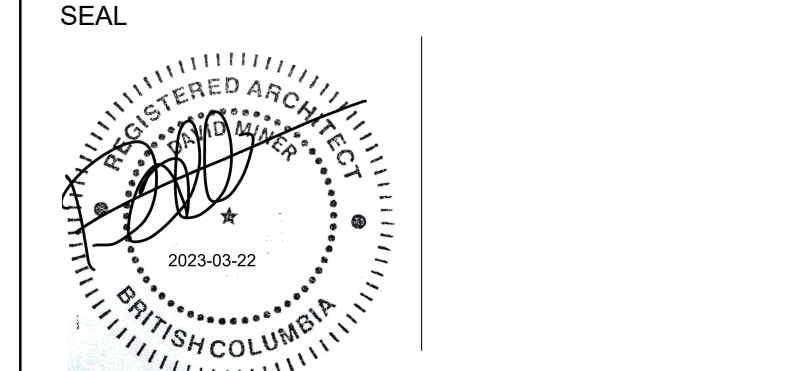
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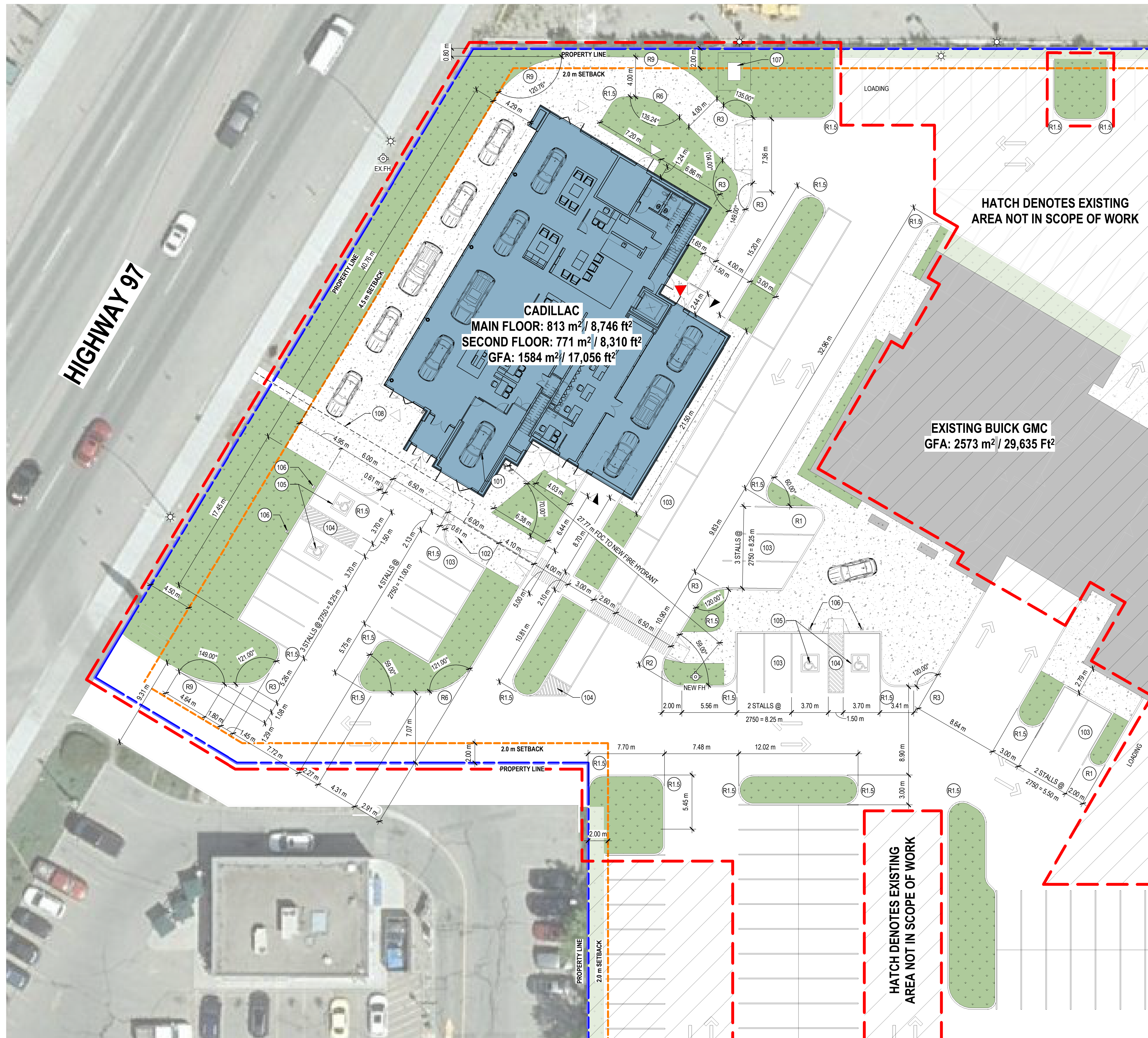


**KELOWNA CADILLAC**  
### Kelowna, BC

**OVERALL SITE PLAN**

DRAWN: RE CHECKED: IC  
PLOT DATE: 3/22/2023 5:26:24 PM





**SITE DATA**

|                                                          |                                                                                                                                 |                                                                                                  |
|----------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------|
| LEGAL DESCRIPTION:                                       | LOT B DISTRICT LOT 124 PLAN KAP39765                                                                                            |                                                                                                  |
| MUNICIPAL ADDRESS:                                       | 2727 HIGHWAY 97 N                                                                                                               |                                                                                                  |
| ZONING:                                                  | C2 (VEHICLE ORIENTED COMMERCIAL)                                                                                                |                                                                                                  |
| SETBACKS:                                                | FRONT YARD: 2 m<br>FLANKING SIDE YARD: 2 m<br>SIDE YARD: 0 m<br>REAR YARD: 0 m                                                  |                                                                                                  |
| BUILDING HEIGHT:                                         | MAXIMUM BUILDING HEIGHT                                                                                                         | 3 STOREYS / 12 m                                                                                 |
| SITE AREA:                                               | 16,192.76 m <sup>2</sup> (1.62 ha / 4.00 ac)                                                                                    |                                                                                                  |
| SITE IN SCOPE AREA:                                      | 8,029.15 m <sup>2</sup> (0.80 ha / 1.98 ac)                                                                                     |                                                                                                  |
| BUILDING FLOOR AREA:                                     | 1,573.41 m <sup>2</sup>                                                                                                         |                                                                                                  |
| IMPERMEABLE SURFACE AREA:                                | 13,761.32 m <sup>2</sup> / 16,192.76 m <sup>2</sup>                                                                             | 85.0%                                                                                            |
| LANDSCAPE AREA:                                          | 2,431.44 m <sup>2</sup> / 16,192.76 m <sup>2</sup>                                                                              | 15.0%                                                                                            |
| FAR:                                                     | 1,584.55 m <sup>2</sup> (NEW) + 2,753.18 m <sup>2</sup> (EXISTING) = 4,337.73 m <sup>2</sup> (TOTAL) / 16,192.76 m <sup>2</sup> | 26.8%                                                                                            |
| SITE COVERAGE:                                           | 812.56 m <sup>2</sup> (NEW) + 2,218.07 m <sup>2</sup> (EXISTING) = 3,030.63 m <sup>2</sup> (TOTAL) / 16,192.76 m <sup>2</sup>   | 18.7%                                                                                            |
| PARKING REQUIRED:                                        |                                                                                                                                 |                                                                                                  |
| BUILDING:                                                | CADILLAC (NEW)                                                                                                                  | BUICK GMC (EXISTING)                                                                             |
| REQUIRED PARKING:                                        | 1,584.55 m <sup>2</sup> x 2.5 STALLS / 100 m <sup>2</sup> (ZONING BYLAW TABLE 8.3.2) = 40 STALLS                                | 2,753.18 m <sup>2</sup> x 3.0 STALLS / 100 m <sup>2</sup> (ZONING BYLAW TABLE 8.3.2) = 83 STALLS |
| PROVIDED PARKING:                                        | 16 STALLS<br>CUSTOMER SERVICE: 22 STALLS<br>INVENTORY: 84 STALLS<br>TOTAL: 122 STALLS                                           | 13 STALLS<br>CUSTOMER SERVICE: 44 STALLS<br>INVENTORY: 100 STALLS<br>TOTAL: 157 STALLS           |
| PARKING RATIO:                                           | 84 STALLS / 1,584.55 m <sup>2</sup> = 5.30 STALLS / 100 m <sup>2</sup>                                                          | 157 STALLS / 2,753.18 m <sup>2</sup> = 5.70 STALLS / 100 m <sup>2</sup>                          |
| BARRIER FREE PARKING REQUIRED: (AS PER CUSTOMER PARKING) | 1 (ZONING BYLAW TABLE 8.2.17)                                                                                                   | 1 (ZONING BYLAW TABLE 8.2.17)                                                                    |
| BARRIER FREE PARKING PROVIDED:                           | 2                                                                                                                               | 2                                                                                                |
| LOADING SPACE REQUIRED:                                  | 1,584.55 m <sup>2</sup> (NEW) / 1,900 m <sup>2</sup> (ZONING BYLAW TABLE 8.4) = 1 STALL                                         | 2,753.18 m <sup>2</sup> / 1,900 m <sup>2</sup> (ZONING BYLAW TABLE 8.4) = 1 STALL                |
| LOADING SPACE PROVIDED:                                  | 1                                                                                                                               | 1                                                                                                |
| EV CHARGING PARKING PROVIDED:                            | 2 STALLS (FUTURE)                                                                                                               | N/A                                                                                              |
| BICYCLE PARKING REQUIRED:                                |                                                                                                                                 |                                                                                                  |
| LONG TERM:                                               | 1,584.55 m <sup>2</sup> x 0.2 / 100 m <sup>2</sup> (ZONING BYLAW TABLE 8.5) = 3 STALLS                                          | N/A                                                                                              |
| SHORT TERM:                                              | 2 STALLS / ENTRANCE                                                                                                             |                                                                                                  |
| TOTAL:                                                   | 5 STALLS                                                                                                                        |                                                                                                  |
| BICYCLE PARKING PROVIDED:                                |                                                                                                                                 |                                                                                                  |
| LONG TERM:                                               | 3 STALLS                                                                                                                        | NA                                                                                               |
| SHORT TERM:                                              | 2 STALLS                                                                                                                        |                                                                                                  |
| TOTAL:                                                   | 5 STALLS                                                                                                                        |                                                                                                  |

**SITE PLAN LEGEND**

|  |                                  |
|--|----------------------------------|
|  | SCOPE EXTENT                     |
|  | PROPOSED PROPERTY LINE           |
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|  | BUILDING EXIT LOCATION           |
|  | NEW FH                           |
|  | EXISTING FIRE HYDRANT            |
|  | EXISTING LIGHT POLE              |

**SITE PLAN KEYNOTES**

|     |                                                                                             |
|-----|---------------------------------------------------------------------------------------------|
| 101 | FIRE DEPARTMENT CONNECTION                                                                  |
| 102 | SURFACE MOUNTED BICYCLE RACKS                                                               |
| 103 | TYPICAL PAINTED PARKING STALL LINES, ASTM E1360 WHITE                                       |
| 104 | NO PARKING AREA, 45° 100mm ASTM E1360 WHITE PAINTED STRIPS @300mm O.C.                      |
| 105 | BARRIER-FREE DESIGNATION SYMBOL, PAINTED, CENTERED IN STALL                                 |
| 106 | BARRIER-FREE PARKING SIGN                                                                   |
| 107 | ELECTRICAL TRANSFORMER, ADJUST GUARDRAIL AS REQUIRED TO MEET FORTIS CLEARANCE REQUIREMENTS. |
| 108 | PEDESTRIAN ACCESS ROUTE                                                                     |

**SCHEDULE A**

This forms part of application  
# DP23-0071  
City of Kelowna  
COMMUNITY PLANNING

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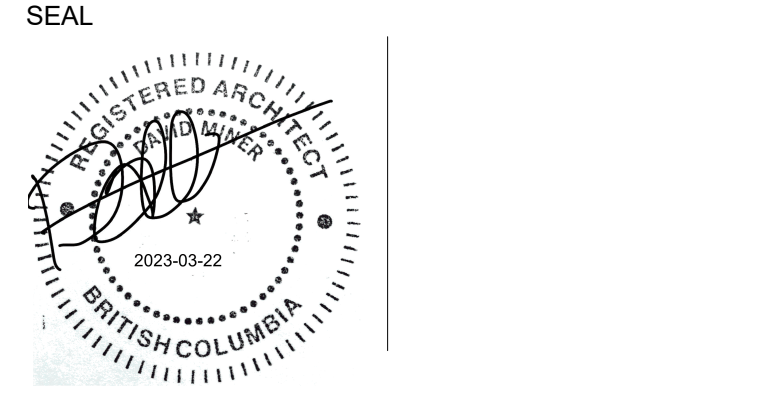
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Tel: 780-619-2270



**KELOWNA CADILLAC**  
### Kelowna, BC

**ENLARGED SITE PLAN**

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CHECKED: IC

**DP1.04**

BC100304 - 2006333

**SITE PLAN RADII**

|      |      |
|------|------|
| R1   | 1000 |
| R1.5 | 1500 |
| R2   | 2000 |
| R3   | 3000 |
| R6   | 6000 |
| R9   | 9000 |



**SCHEDULE A**  
 This forms part of application  
 # DP23-0071

Planner Initials **AF**

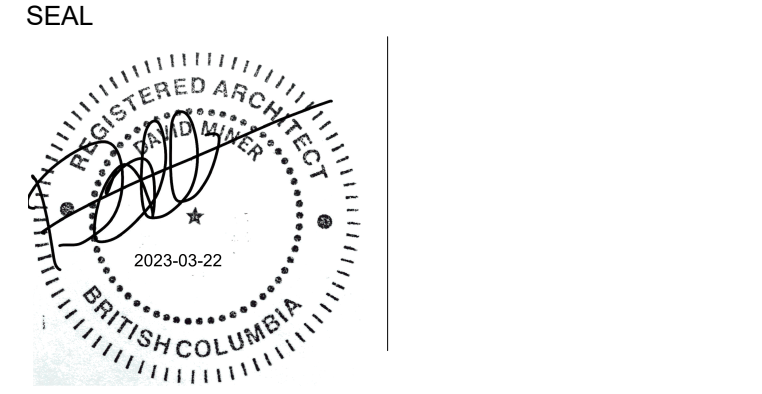
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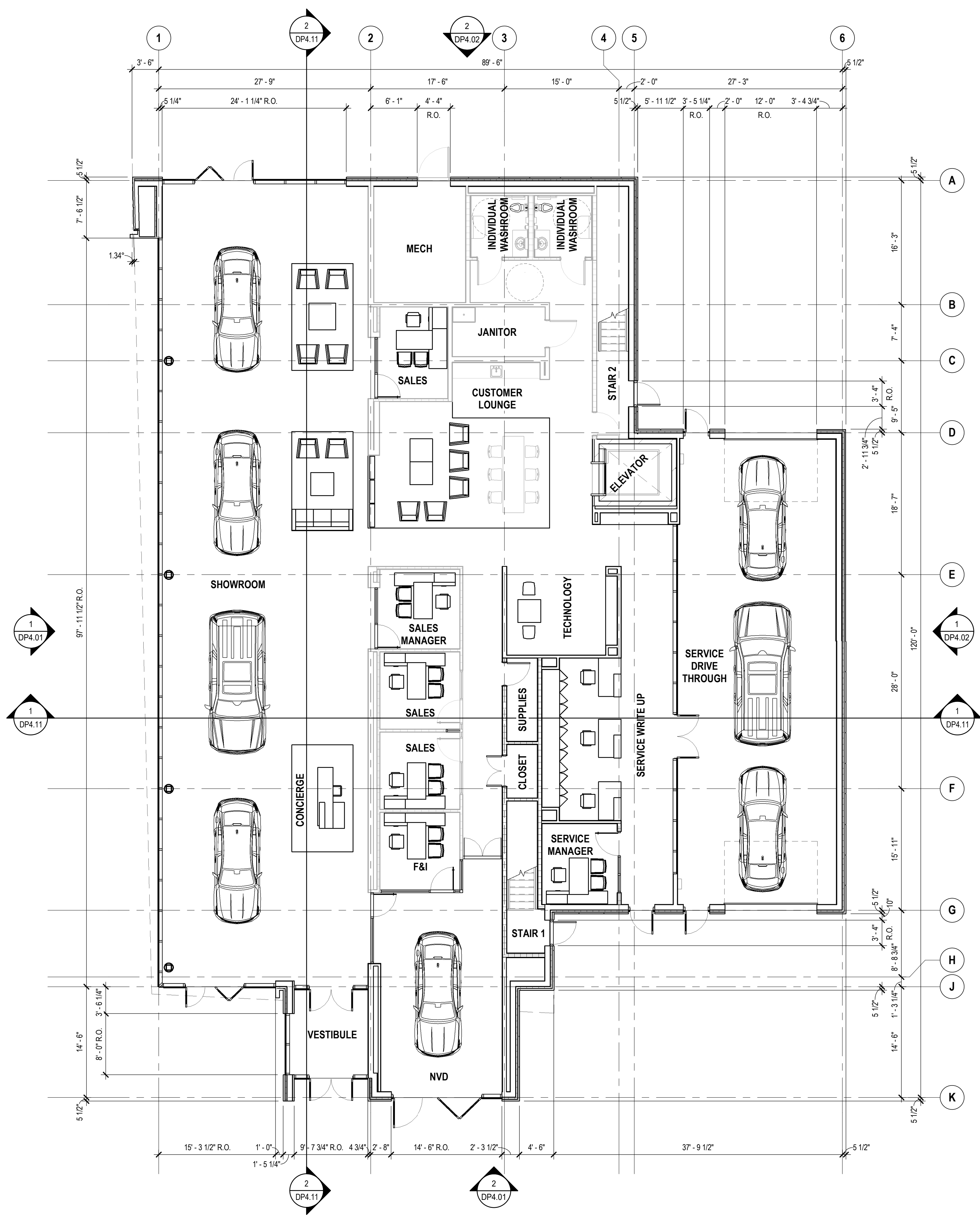
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**KELOWNA CADILLAC**  
 ### Kelowna, BC  
**MAIN FLOOR PLAN**

DRAWN: RE CHECKED: IC  
 PLOT DATE: 3/22/2023 5:26:34 PM

**DP2.01**  
 BC100304 - 2006333



**1 MAIN FLOOR PLAN**  
 DP2.01 SCALE: 1/8" = 1'-0"



**SCHEDULE A**  
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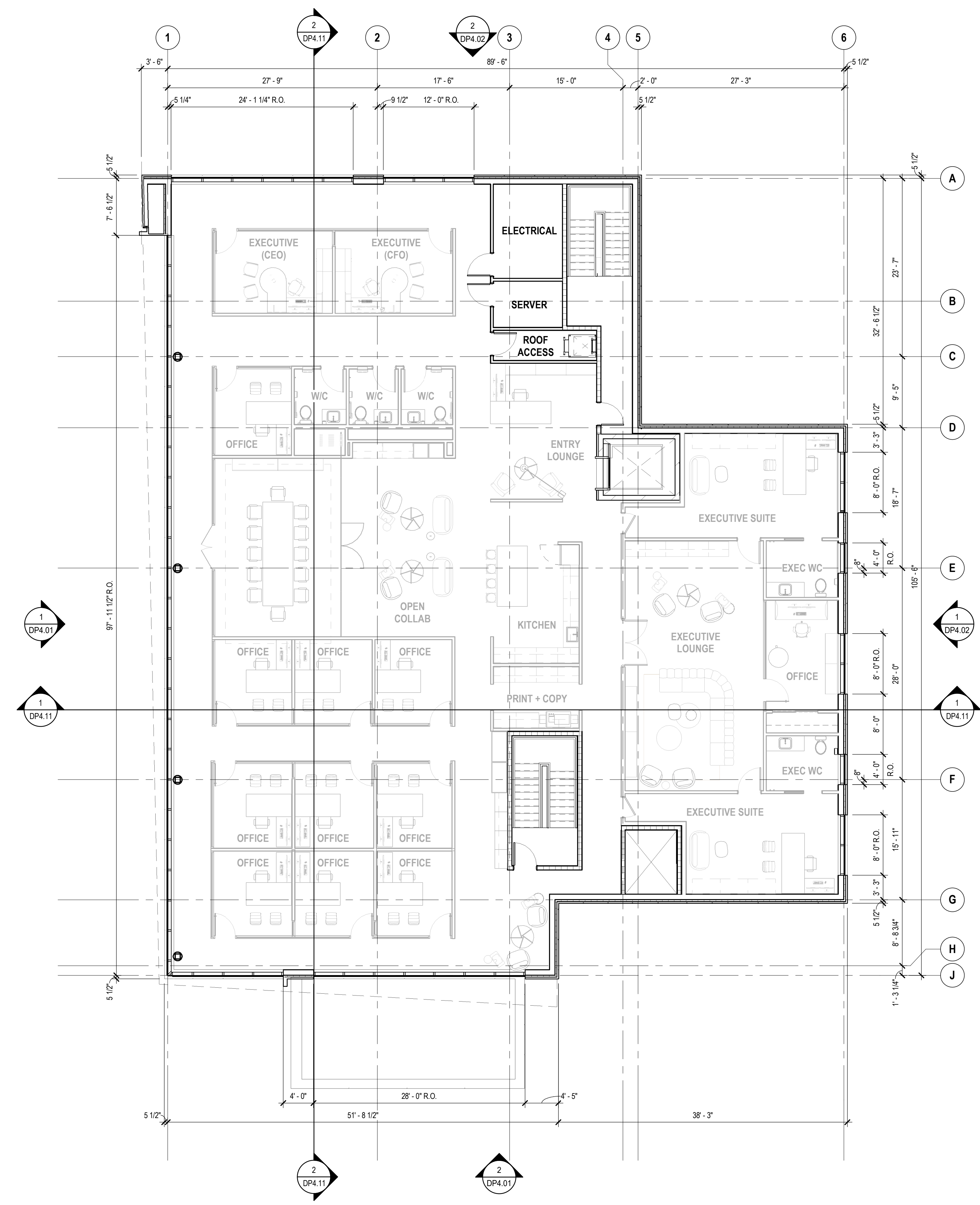
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 REVOLVE ENGINEERING INC.  
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**1** **2**  
 DP2.02  
**SECOND FLOOR PLAN**  
 SCALE: 1/8" = 1'-0"



**KELOWNA CADILLAC**  
 ### Kelowna, BC  
**SECOND FLOOR BASE BUILDING PLAN**

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**DP2.02**  
 BC100304 - 2006333



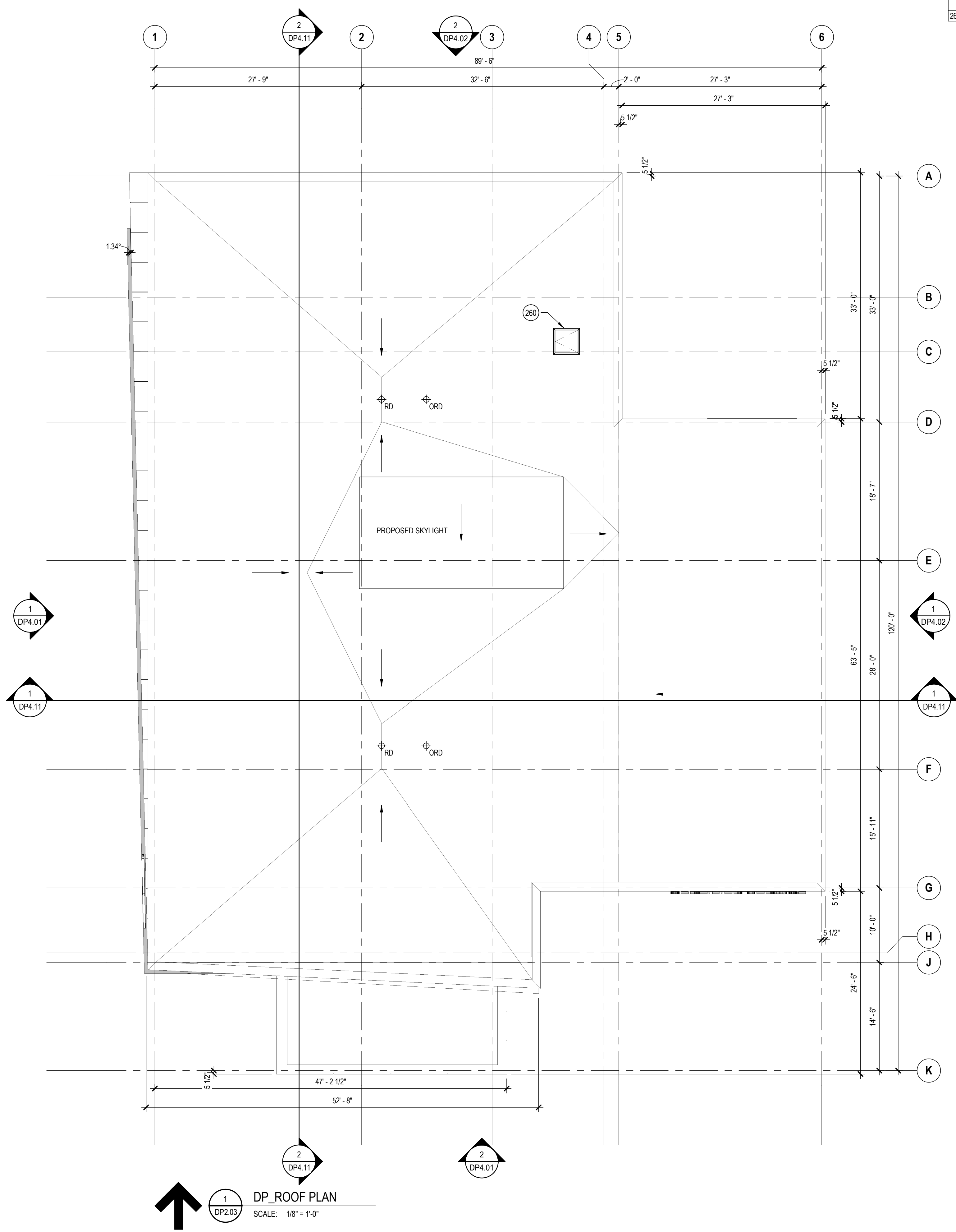
| ROOF LEGEND         |  |
|---------------------|--|
| ROOF DRAIN          |  |
| OVERFLOW ROOF DRAIN |  |

**ROOF PLAN KEYNOTES**  
260 ROOF ACCESS HATCH

**SCHEDULE A**  
This forms part of application # DP23-0071

Planner Initials **AF**

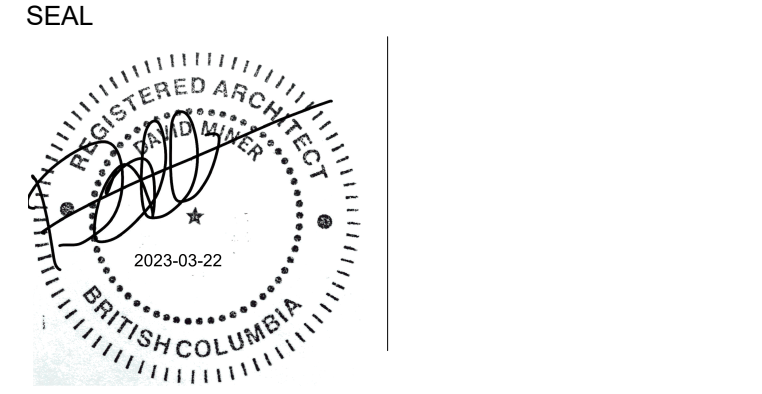
City of Kelowna  
COMMUNITY PLANNING



**DP\_ROOF PLAN**  
SCALE: 1/8" = 1'-0"

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**KELOWNA CADILLAC**  
### Kelowna, BC

**ROOF PLAN**

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**DP2.03**



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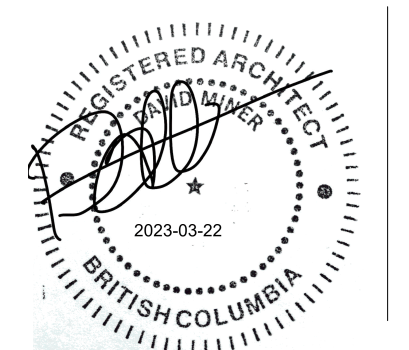
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SEAL



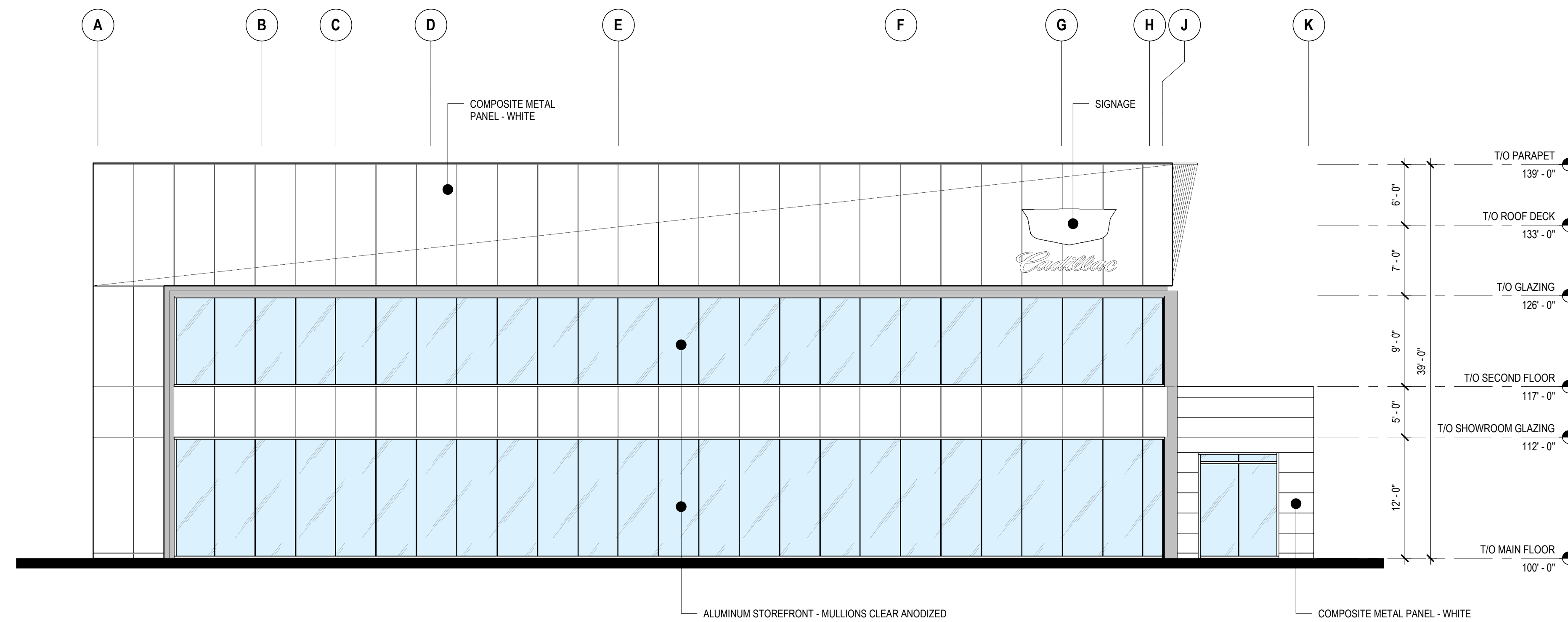
**KELOWNA CADILLAC**  
 ### Kelowna, BC

**EXTERIOR ELEVATIONS**

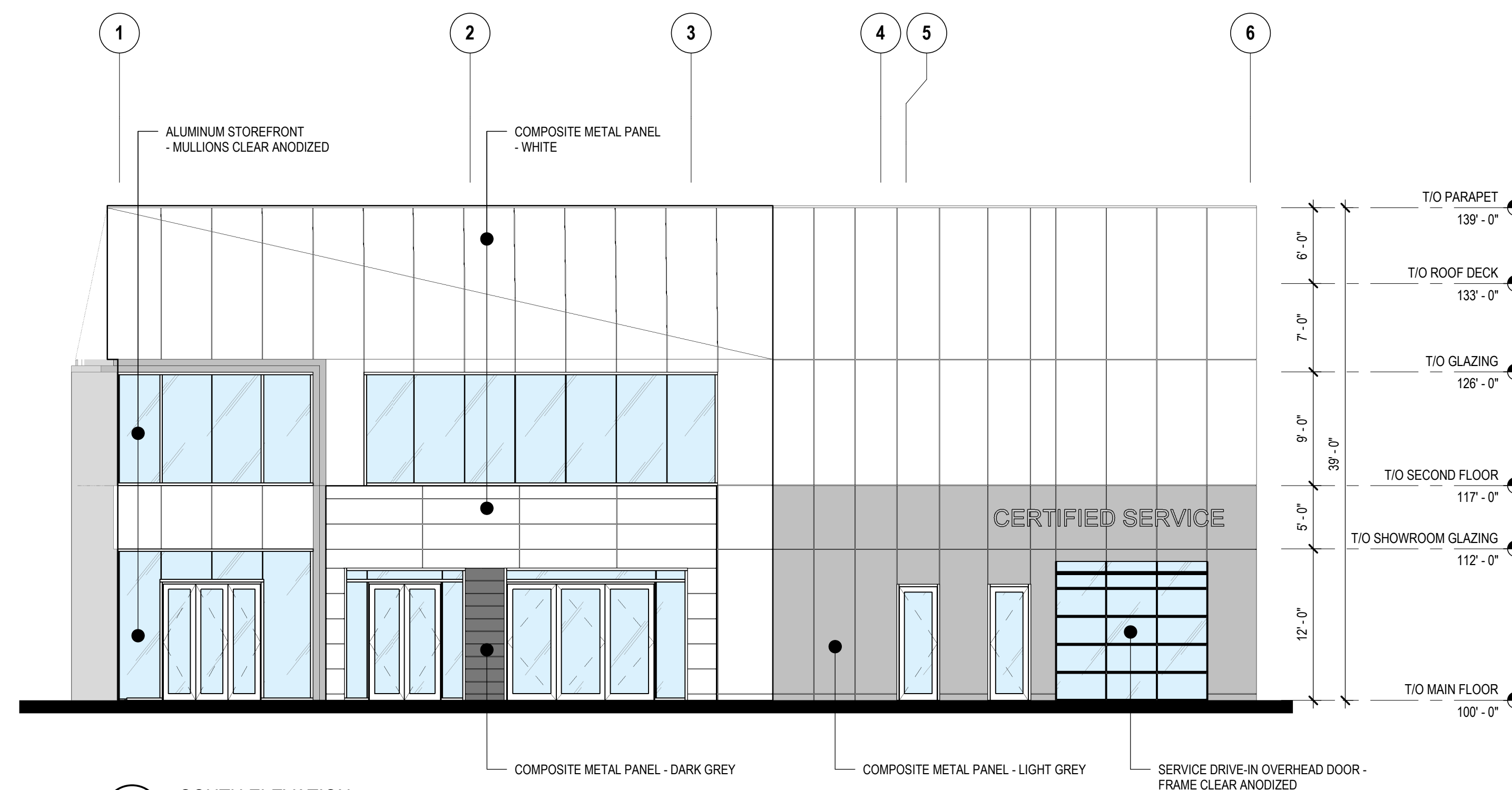
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**DP4.01**

BC100304 - 2006333

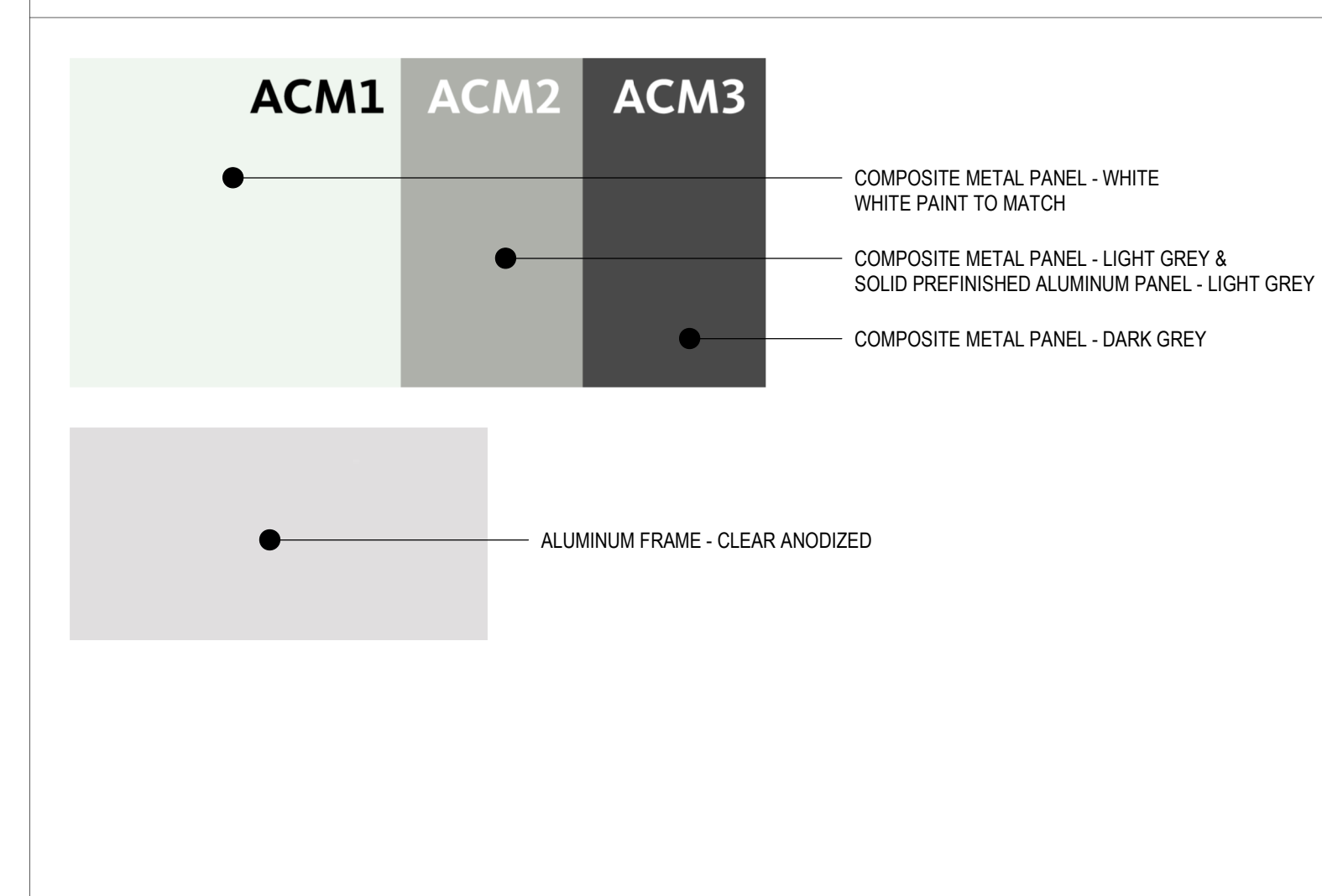


1 WEST ELEVATION  
 DP4.01 SCALE: 1/8" = 1'-0"



2 SOUTH ELEVATION  
 DP4.01 SCALE: 1/8" = 1'-0"

**COLOUR AND MATERIAL LEGEND**





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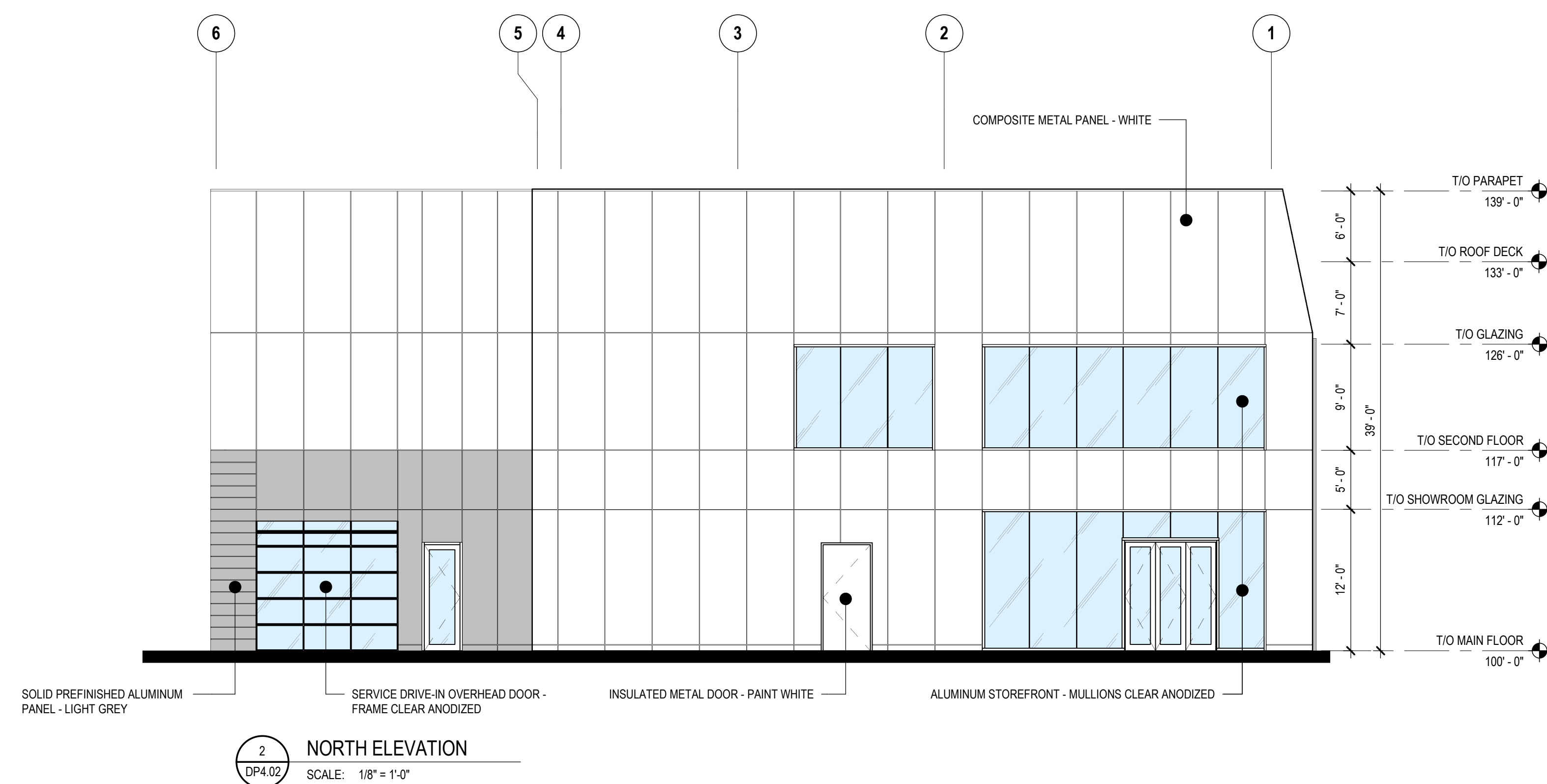
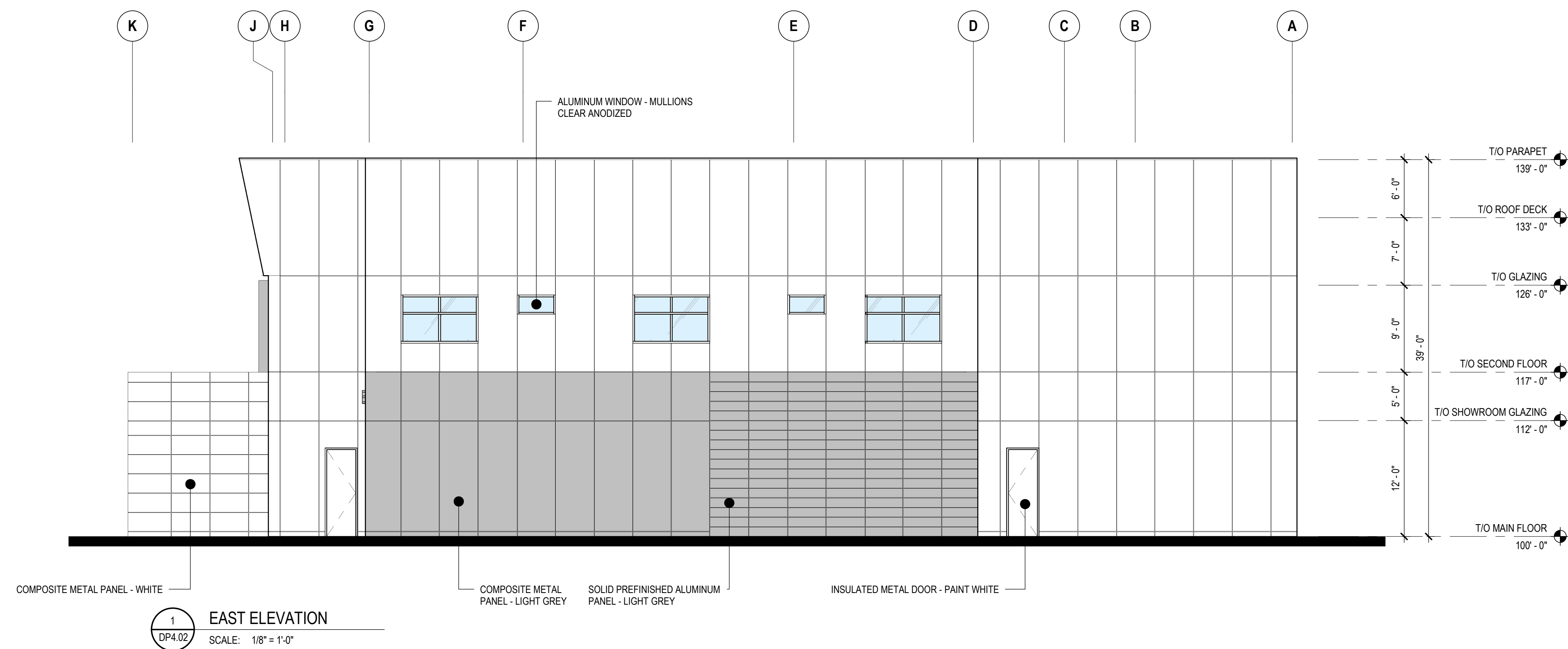
**KELOWNA CADILLAC**  
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**EXTERIOR ELEVATIONS**

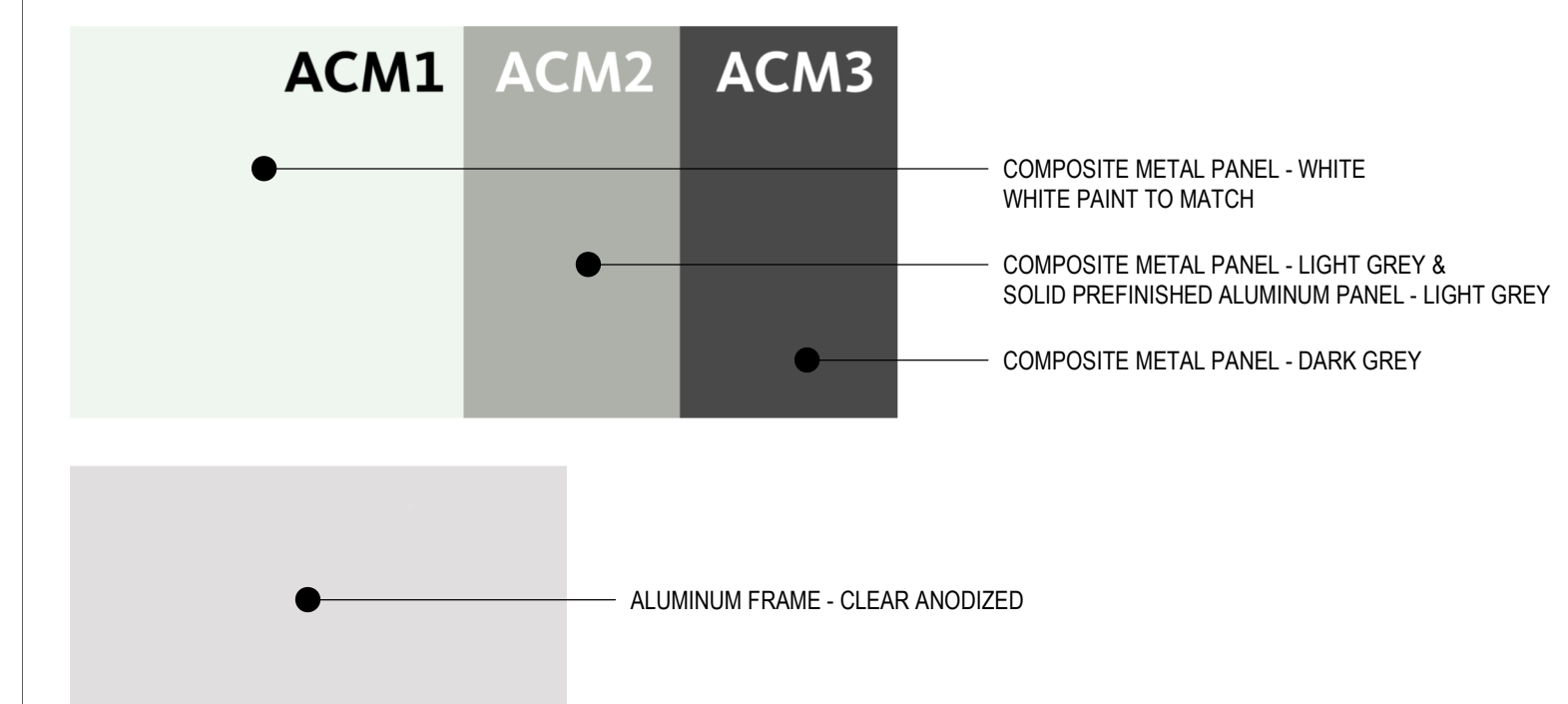
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**DP4.02**

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**COLOUR AND MATERIAL LEGEND**

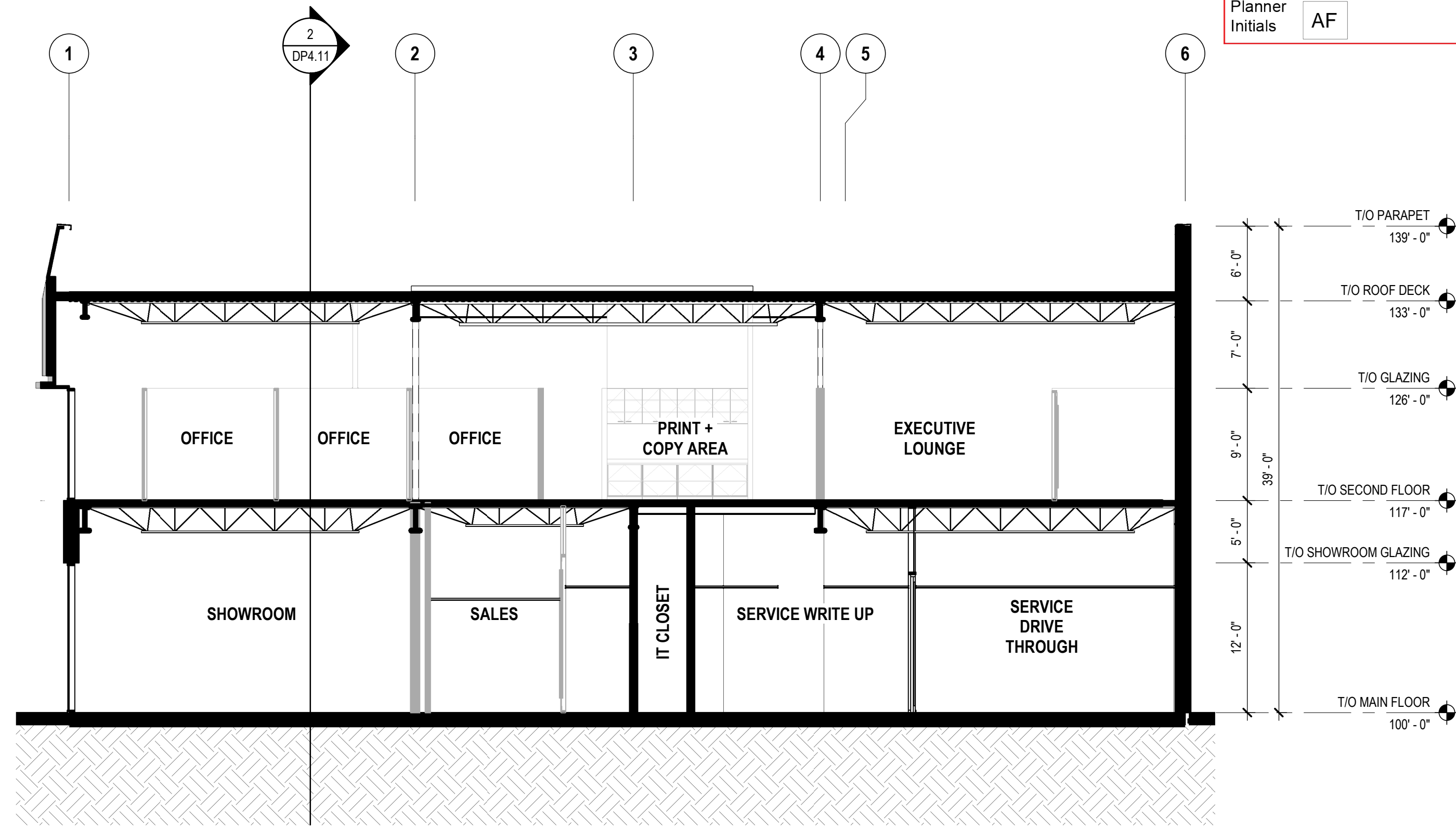




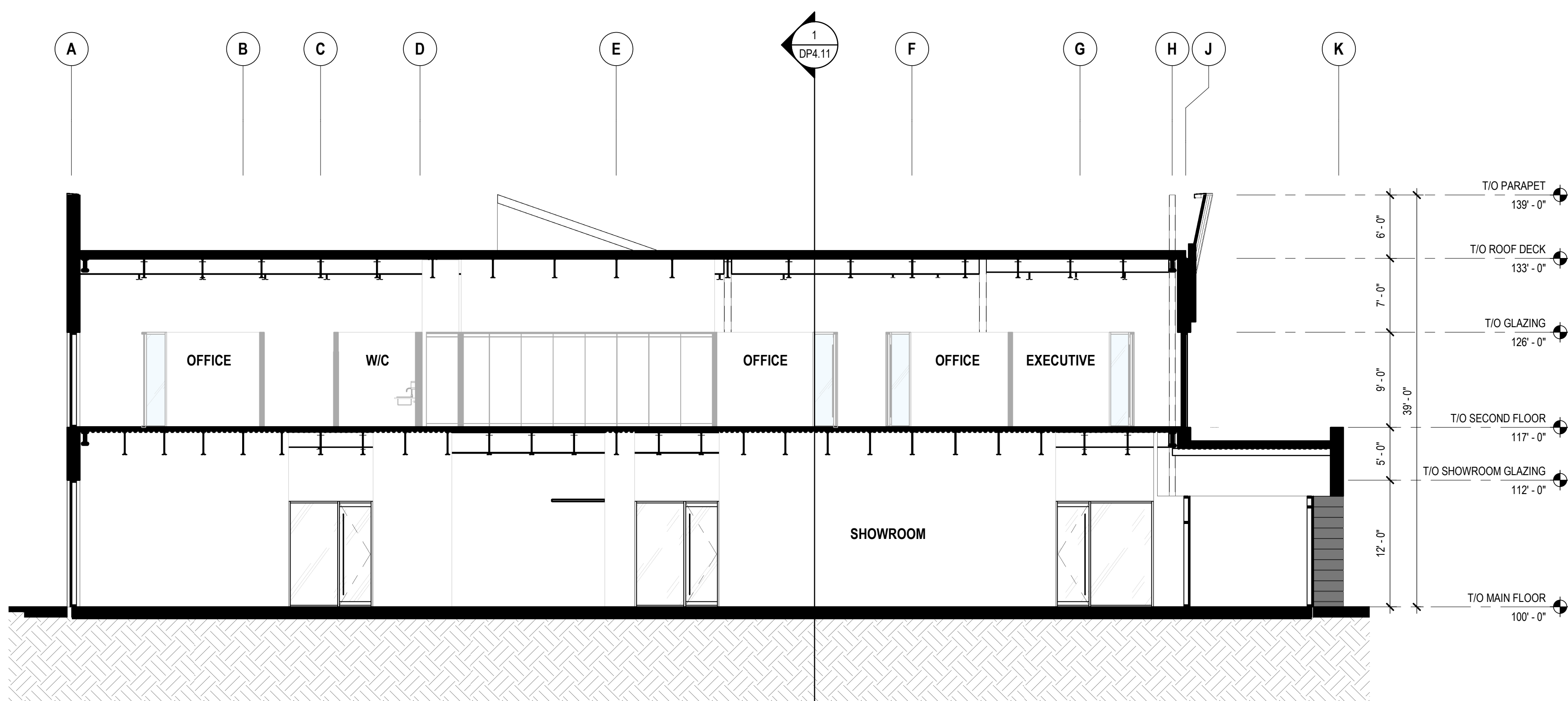
**SCHEDULE B**  
This forms part of application  
# DP23-0071

Planner Initials **AF**

City of Kelowna  
COMMUNITY PLANNING



**1** BUILDING SECTION  
SCALE: 1/8" = 1'-0"



**2** BUILDING SECTION  
SCALE: 1/8" = 1'-0"

ISSUED FOR

1 2022-03-21 DEVELOPMENT PERMIT

**METRIC**

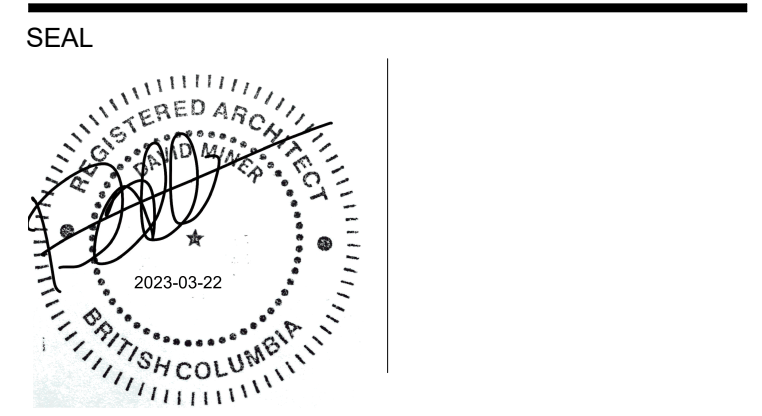
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**KELOWNA CADILLAC**  
### Kelowna, BC

**BUILDING SECTIONS**

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**DP4.11**  
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# KELOWNA CADILLAC

2727 HIGHWAY 97 N, KELOWNA, BRITISH COLUMBIA  
2023-03-21 DEVELOPMENT PERMIT

**SCHEDULE B**  
This forms part of application  
# DP23-0071

Planner Initials **AF**

City of Kelowna  
COMMUNITY PLANNING

**BANNISTER**  
AUTOMOTIVE GROUP



**DIALOG**

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2022-03-21 DEVELOPMENT PERMIT



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## DEVELOPMENT PERMIT - DRAWING LIST

### ARCHITECTURAL

- DP0.00 COVER SHEET
- DP1.01 LEGAL SURVEY
- DP1.02 OVERALL EXISTING SITE PLAN
- DP1.03 OVERALL SITE PLAN
- DP1.04 ENLARGED SITE PLAN
- DP2.01 MAIN FLOOR PLAN
- DP2.02 SECOND FLOOR BASE BUILDING PLAN
- DP2.03 ROOF PLAN
- DP4.01 EXTERIOR ELEVATIONS
- DP4.02 EXTERIOR ELEVATIONS
- DP4.11 BUILDING SECTIONS

### ELECTRICAL

- E7.01 LIGHTING CALCULATION

### CIVIL

- 23009-100 REMOVALS PLAN
- 23009-101 COMPOSITE UTILITY PLAN
- 23009-102 SITE GRADING AND STORM WATER PLAN
- 23009-201 PLAN AND PROFILES
- 23009-202 PLAN AND PROFILES
- 23009-203 PLAN AND PROFILES
- 23009-CO COVER PAGE
- V
- 23009-P01 COMPOSITE UTILITY PLAN
- 23009-P01 COMPOSITE UTILITY PLAN
- 23009-P02 SITE GRADING AND STORM WATER PLAN
- 23009-P02 SITE GRADING AND STORM WATER PLAN

### LANDSCAPE

- ONSITE
- L12 CONCEPTUAL LANDSCAPE PLAN
- L22 WATER CONSERVATION / IRRIGATION PLAN
- OFFSITE
- L12 OFFSITE LANDSCAPE PLAN
- L22 OFFSITE IRRIGATION PLAN

SEAL

**KELOWNA CADILLAC**  
2727 HIGHWAY 97 N, KELOWNA, BC

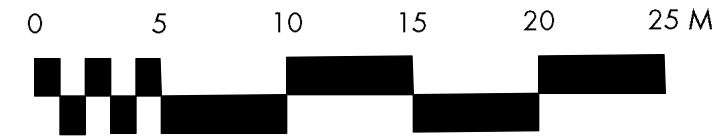
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**DP0.00**

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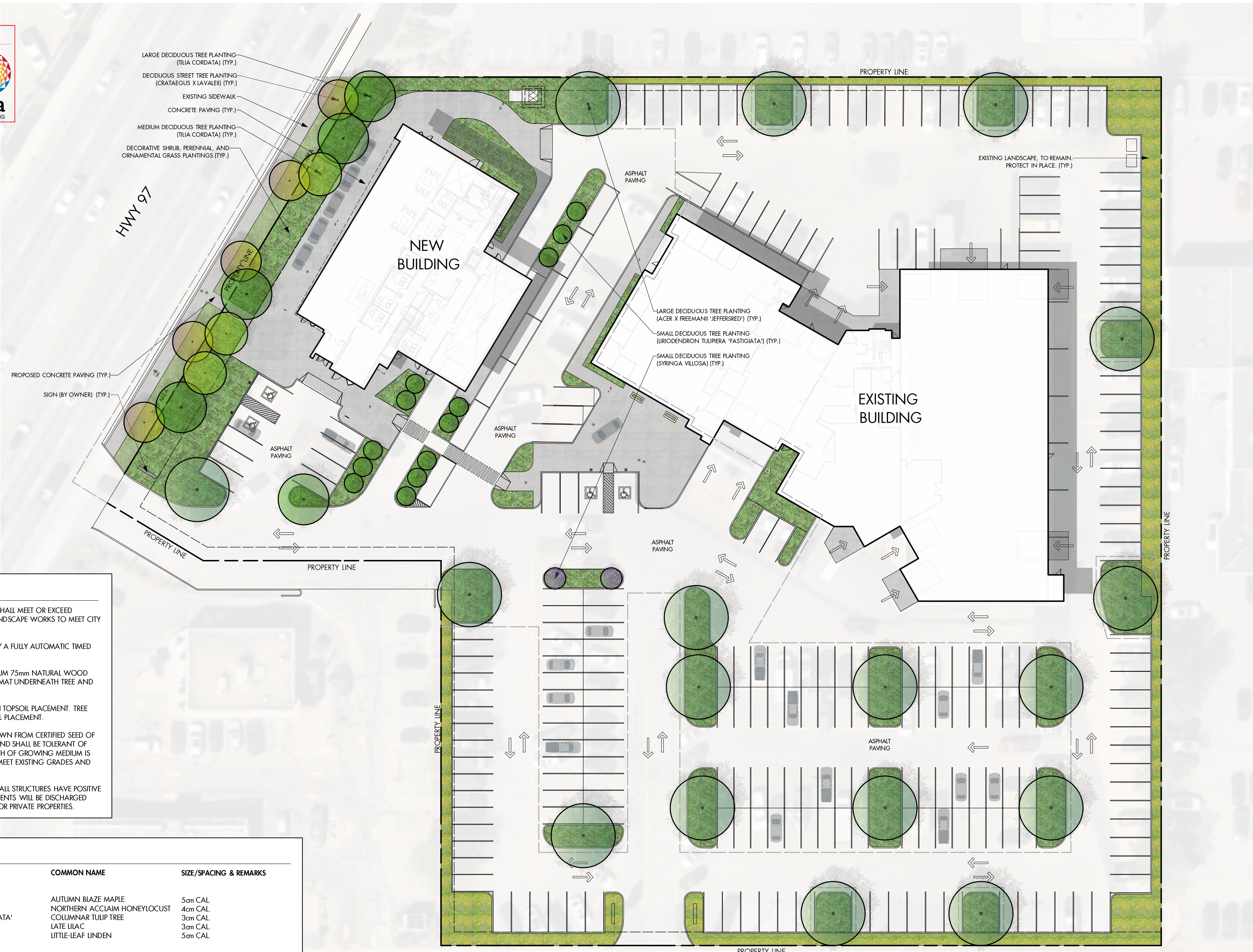




**SCHEDULE C**

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# DP23-0071

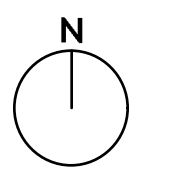
Planner Initials **AF**



- NOTES**
1. PLANT MATERIAL AND CONSTRUCTION METHODS SHALL MEET OR EXCEED CANADIAN LANDSCAPE STANDARDS. ALL OFFSITE LANDSCAPE WORKS TO MEET CITY OF KELOWNA BYLAW 12375 STANDARDS.
  2. ALL SOFT LANDSCAPE AREAS SHALL BE WATERED BY A FULLY AUTOMATIC TIMED UNDERGROUND IRRIGATION SYSTEM.
  3. TREE AND SHRUB BEDS TO BE DRESSED IN A MINIMUM 75mm NATURAL WOOD MULCH AS SHOWN IN PLANS. DO NOT PLACE WEED MAT UNDERNEATH TREE AND SHRUB BEDS.
  4. SHRUB BEDS TO RECEIVE A MINIMUM 300mm DEPTH TOPSOIL PLACEMENT. TREE BEDS TO RECEIVE A MINIMUM 1000mm DEPTH TOPSOIL PLACEMENT.
  5. TURF AREA FROM SOD SHALL BE NO. 1 GRADE GROWN FROM CERTIFIED SEED OF IMPROVED CULTIVARS REGISTERED FOR SALE IN B.C. AND SHALL BE TOLERANT OF DROUGHT CONDITIONS. A MINIMUM OF 150mm DEPTH OF GROWING MEDIUM IS REQUIRED BENEATH TURF AREAS. TURF AREAS SHALL MEET EXISTING GRADES AND HARD SURFACES FLUSH.
  6. SITE GRADING AND DRAINAGE WILL ENSURE THAT ALL STRUCTURES HAVE POSITIVE DRAINAGE AND THAT NO WATER OR LOOSE IMPEDIMENTS WILL BE DISCHARGED FROM THE LOT ONTO ADJACENT PUBLIC, COMMON, OR PRIVATE PROPERTIES.

**PLANT LIST**

| KEY                             | QTY | BOTANICAL NAME                           | COMMON NAME                      | SIZE/SPACING & REMARKS       |
|---------------------------------|-----|------------------------------------------|----------------------------------|------------------------------|
| <b>TREES</b>                    |     |                                          |                                  |                              |
| AFR                             | 17  | ACER X FREEMANII 'JEFFERSRED'            | AUTUMN BLAZE MAPLE               | 5cm CAL                      |
| CTR                             | 3   | GLEDITSIA TRIACANTHOS 'HARVE'            | NORTHERN ACCLAIM HONEYLOCUST     | 4cm CAL                      |
| LTU                             | 13  | LIRIODENDRON TULIPIFERA 'FASTIGIATA'     | COLUMNAR TULIP TREE              | 3cm CAL                      |
| SVI                             | 2   | SYRINGA VILLOSA                          | LATE LILAC                       | 3cm CAL                      |
| TCO                             | 5   | TILIA CORDATA                            | LITTLE-LEAF LINDEN               | 5cm CAL                      |
| <b>SHRUBS</b>                   |     |                                          |                                  |                              |
| BE                              | 32  | BERBERIS THUNBERGII 'MONOMB'             | CHERRY BOMB BARBERRY             | #02 CONT. /2.0M O.C. SPACING |
| CS                              | 19  | CORNUS SERICEA                           | RED OSIER DOGWOOD                | #02 CONT. /3.0M O.C. SPACING |
| PO                              | 47  | PHYSOCARPOS OPULIFOLIUS 'MONLO'          | DIABOLO NINEBARK                 | #02 CONT. /3.0M O.C. SPACING |
| RA                              | 99  | RHUS AROMATICA 'GRO LO'                  | GROW LOW FRAGRAN SUMAC           | #02 CONT. /2.0M O.C. SPACING |
| SB                              | 92  | SPIRAEA BUMALDA 'ANTHONY WATERER'        | ANTHONY WATERER SPIREA           | #02 CONT. /2.0M O.C. SPACING |
| <b>PERENNIALS &amp; GRASSES</b> |     |                                          |                                  |                              |
| CA                              | 167 | CALAMAGROSTIS ACUTIFLORA 'KARL FOERSTER' | KARL FOERSTER FEATHER REED GRASS | #01 CONT. /1.2M O.C. SPACING |
| EM                              | 51  | ECHINACEA PURPUREA 'MAGNIUS'             | MAGNIUS CONEFLOWER               | #01 CONT. /1.0M O.C. SPACING |
| RF                              | 77  | RUDBECKIA FULGIDA 'GOLDSTURM'            | GOLDSTURM CONEFLOWER             | #01 CONT. /1.0M O.C. SPACING |
| SN                              | 169 | SALVIA NEMEROSA 'MAYNIGHT'               | MAYNIGHT MEADOW SAGE             | #01 CONT. /1.0M O.C. SPACING |
| SS                              | 88  | SEDUM SPECTABILE 'AUTUMN JOY'            | AUTUMN JOY STONECROP             | #01 CONT. /1.0M O.C. SPACING |



PROJECT TITLE  
**KELOWNA CADILLAC**

Kelowna, BC

DRAWING TITLE  
**CONCEPTUAL LANDSCAPE PLAN**

ISSUED FOR / REVISION

| NO. | DATE     | DESCRIPTION   |
|-----|----------|---------------|
| 1   | 23.02.24 | Issued for DP |
| 2   | 23.03.17 | Issued for DP |
| 3   | 23.03.21 | Issued for DP |
| 4   | 23.06.20 | Issued for DP |
| 5   |          |               |

PROJECT NO: 23-0199

DESIGN BY: NM

DRAWN BY: LA

CHECKED BY: FB

DATE: JUNE 20, 2023

SCALE: 1:300

PAGE SIZE: 24x36"



DRAWING NUMBER  
**L1/2**

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**FORM & CHARACTER – DEVELOPMENT PERMIT GUIDELINES**

Consideration has been given to the following guidelines as identified in Chapter 18 of the City of Kelowna 2040 Official Community Plan:

| SECTION 6.0: RETAIL, COMMERCIAL AND INDUSTRIAL                                                                                                                                                                                                                                                                                                                                                                                                                                                                                         |     |   |   |   |   |   |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----|---|---|---|---|---|
| RATE PROPOSALS COMPLIANCE TO PERTINENT GUIDELINE<br><i>(1 is least complying &amp; 5 is highly complying)</i>                                                                                                                                                                                                                                                                                                                                                                                                                          | N/A | 1 | 2 | 3 | 4 | 5 |
| <b>6.1 General Guidelines</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          |     |   |   |   |   |   |
| <b>6.1.1 Relationship to the Street</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | N/A | 1 | 2 | 3 | 4 | 5 |
| a. Orient the long side of each building to be parallel to the public street.                                                                                                                                                                                                                                                                                                                                                                                                                                                          |     |   |   |   |   | ✓ |
| b. Locate entries to be visible and directly accessible from the public street.                                                                                                                                                                                                                                                                                                                                                                                                                                                        |     |   |   |   |   | ✓ |
| c. For buildings fronting highways, entries can be located away from the street, as long as there is a direct pedestrian connection to the site.                                                                                                                                                                                                                                                                                                                                                                                       |     |   |   |   |   | ✓ |
| d. Avoid blank walls adjacent to the highway, streets, walkways, parks, or other amenity spaces.                                                                                                                                                                                                                                                                                                                                                                                                                                       |     |   |   |   |   | ✓ |
| <b>6.1.2 Site Planning and Landscaping</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | N/A | 1 | 2 | 3 | 4 | 5 |
| a. Locate buildings to ensure good sight lines for vehicular and pedestrian traffic.                                                                                                                                                                                                                                                                                                                                                                                                                                                   |     |   |   |   |   | ✓ |
| b. Provide direct, safe, continuous, and clearly defined pedestrian access from public sidewalks, parking areas, and transit stops to building entrances.                                                                                                                                                                                                                                                                                                                                                                              |     |   |   |   |   | ✓ |
| c. Use large canopy trees to define the public realm (e.g. at the sidewalk and property edge facing the street)                                                                                                                                                                                                                                                                                                                                                                                                                        |     |   |   |   |   | ✓ |
| d. Distribute trees and landscaping throughout the site in order to: <ul style="list-style-type: none"> <li>• Soften property edges facing the street;</li> <li>• Define internal roads, pedestrian routes, and open spaces;</li> <li>• Create pleasant pedestrian conditions;</li> <li>• Screen parking, loading, service, and utility areas;</li> <li>• Manage stormwater on-site; and</li> <li>• Break up large rows of parking by substituting a parking stall with a canopy tree in planter every 8-10 parking stalls;</li> </ul> |     |   |   |   |   | ✓ |
| e. Provide on-site bio-retention facilities (e.g. bioswales, rain gardens) to collect, store and filter stormwater from parking areas.                                                                                                                                                                                                                                                                                                                                                                                                 |     | ✓ |   |   |   |   |
| f. Use permeable materials such as paving blocks or permeable concrete in parking areas to maximize rainwater infiltration.                                                                                                                                                                                                                                                                                                                                                                                                            |     | ✓ |   |   |   |   |
| g. Pedestrian pathways should provide clear sight lines and connect the following: <ul style="list-style-type: none"> <li>• Parking areas to building entrances;</li> <li>• Main building entrances to public sidewalks (where applicable);</li> <li>• Main building entrances to transit stopes (where applicable);</li> <li>• Between buildings on adjacent lots.</li> </ul>                                                                                                                                                         |     |   |   |   |   | ✓ |



|                                                                                                                                                                                                                                                                                             |            |          |          |          |          |          |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------|----------|----------|----------|----------|----------|
| h. Provide separation between vehicular routes (especially truck access/loading) and pedestrian routes on-site to avoid conflict and distinguish pedestrian routes from driving surfaces by using varied paving treatments and/or raising walkways to curb level.                           |            |          |          |          |          | ✓        |
| i. Base new development on an internal circulation pattern that allows logical movement throughout the site and that will accommodate, and not preclude, intensification over time.                                                                                                         |            |          |          |          |          | ✓        |
| <b>6.1.3 Site Servicing, Access, and Parking</b>                                                                                                                                                                                                                                            | <b>N/A</b> | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> |
| a. Design site accesses to provide the potential for future shared access with neighbours and to minimize curb cuts.                                                                                                                                                                        |            |          |          |          |          | ✓        |
| b. Where practical, link access drives and parking lots of adjacent properties in order to allow for circulation of vehicles between sites.                                                                                                                                                 |            |          |          |          |          | ✓        |
| c. The preferred location for main parking areas is at the rear and/or side of the building. Avoid locating large parking areas between the building and the street.                                                                                                                        |            |          |          |          | ✓        |          |
| d. Where parking areas are visible from the street, screen them using strategies such as tree planting, berming, low walls, decorative fencing and/or hedging.                                                                                                                              |            |          |          |          |          | ✓        |
| e. Break parking areas into smaller blocks defined by landscaping in order to minimize the amount of paved areas.                                                                                                                                                                           |            |          |          |          |          | ✓        |
| f. Locate loading, utilities, mechanical equipment and garbage collection areas away from public view by: <ul style="list-style-type: none"> <li>Integrating these facilities into the footprint of the building; or</li> <li>Screening using fencing, walls, and/or landscaping</li> </ul> |            |          |          |          |          | ✓        |
| g. Provide areas for temporary snow storage that do not conflict with site circulation, landscaping, and access to utility boxes. For example, by providing access via a lane away from public view.                                                                                        |            |          |          |          | ✓        |          |
| <b>6.1.4 Building Articulation, Features, and Materials</b>                                                                                                                                                                                                                                 | <b>N/A</b> | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> |
| a. Avoid facing unarticulated facades to the street and use projections, recesses, arcades, awnings, color, and texture to improve the pedestrian experience                                                                                                                                |            |          |          |          |          | ✓        |
| b. Design primary entrances to face the street, exhibit design emphasis, and provide weather protection by means of canopy or recessed entry.                                                                                                                                               |            |          |          |          |          | ✓        |
| c. Design buildings such that their form and architectural character reflect the building's internal function and use (e.g. an industrial building, a large format retail mall).                                                                                                            |            |          |          |          |          | ✓        |
| d. Design signage as an integral element of the building's façade and to be compatible in scale and design with the design, color and material of the building.                                                                                                                             |            |          |          |          |          | ✓        |
| e. Allow for brand identification where there are multiple buildings and uses on a site, but avoid individual corporate image, color, and signage back-lit signs from dominating the site.                                                                                                  |            |          |          |          |          | ✓        |
| f. Locate, size and design ground-mounted signs to be oriented to pedestrians as opposed to vehicles.                                                                                                                                                                                       |            |          |          |          |          | ✓        |



|                                                                                                                                                                                                |            |          |          |          |          |          |
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| g. Provide shielded, down lighting to provide security and ambient lighting while minimizing light pollution and spill over lighting into adjacent properties.                                 |            |          |          |          |          | ✓        |
| h. Provide weather protection at building entrances close to transit stops, and in areas with pedestrian amenities.                                                                            |            |          |          |          |          | ✓        |
| i. Incorporate substantial, natural building materials such as masonry, stone, and wood into building facades.                                                                                 |            | ✓        |          |          |          |          |
| j. Use an integrated, consistent range of materials and colors and provide variety by, for example, using accent colors.                                                                       |            | ✓        |          |          |          |          |
| <b>6.4 Industrial and Service Commercial</b>                                                                                                                                                   |            |          |          |          |          |          |
| <b>6.4.1 Relationship to the Street</b>                                                                                                                                                        | <b>N/A</b> | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> |
| a. Design primary entries to be clearly visible and accessible from the street.                                                                                                                |            |          |          |          |          | ✓        |
| b. Site the building's primary façade parallel to the street and close to the minimum setback to establish a defined street edge.                                                              |            |          |          |          |          | ✓        |
| c. Include glazing, as a major component of street facing facades.                                                                                                                             |            |          |          |          |          | ✓        |
| d. Maintain and enhance street edge definition by preserving or incorporating street trees.                                                                                                    |            |          |          |          |          | ✓        |
| e. Locate the office, reception, or sales component of the building closer to the street than the plant or warehouse component.                                                                | ✓          |          |          |          |          |          |
| f. Do not locate service doors (e.g., an overhead loading door) facing the street.                                                                                                             |            |          |          |          |          | ✓        |
| <b>6.4.2 Site Planning and Landscaping</b>                                                                                                                                                     | <b>N/A</b> | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> |
| a. Pedestrian pathways should provide clear sight lines and connect the building to outdoor amenity spaces.                                                                                    |            |          |          |          |          | ✓        |
| b. Consider providing landscaped green roofs to manage runoff, add visual appeal, improve energy efficiency, reduce heat island effect, and provide amenity value.                             |            | ✓        |          |          |          |          |
| <b>6.4.3 Site Servicing, Access, and Parking</b>                                                                                                                                               | <b>N/A</b> | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> |
| a. The preferred location for main parking areas is at the rear and/or side of the building.                                                                                                   |            |          |          |          | ✓        |          |
| b. Avoid locating large parking areas between the building and street. A single loaded row of visitor parking and passenger drop-off areas may be located between the building and the street. |            |          |          |          |          | ✓        |
| c. Where parking areas are visible from the street, screen it using strategies such as tree planting, berming, low walls, decorative fencing and/or hedging.                                   |            |          |          |          |          | ✓        |
| d. Break parking areas into smaller blocks defined by landscaping in order to minimize the amount of paved areas.                                                                              |            |          |          |          |          | ✓        |
| e. Locate outdoor storage areas within rear yards and/or interior side yards and screened from street view.                                                                                    | ✓          |          |          |          |          |          |
| <b>6.4.4 Building Articulation, Features and Materials</b>                                                                                                                                     | <b>N/A</b> | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> |
| a. Avoid facing unarticulated facades to the street and use projections, recesses, plantings, awnings, color and texture to reduce the visual size of any unglazed walls.                      |            |          |          |          |          | ✓        |



|                                                                                                                                         |   |  |  |  |  |  |
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| b. Use different exterior materials to distinguish between the plant/warehouse component of a building from the office/sales component. | ✓ |  |  |  |  |  |
|-----------------------------------------------------------------------------------------------------------------------------------------|---|--|--|--|--|--|