This permit relates to land in the City of Kelowna municipally known as

## 610 Academy Way

and legally known as
Parcel A (being a consolidation of Lots 10 \& 11 See CA6510572) Section 3 Township 23 Osoyoos Division Yale District Plan EPP53793
and permits the land to be used for the following development:

## Multiple Dwelling Housing

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

Date of Decision:
Decision By:
Issued Date:
Development Permit Area:

March 18, 2019
Council
March 18, 2019
Comprehensive Development Permit Area

This permit will not be valid if development has not commenced by March 18, 2021.
Existing Zone: RM3 - Low Density Multiple Housing Future Land Use Designation: MRL - Multiple Unit Residential (Low Density)
This is NOT a Building Permit.
In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

## NOTICE

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner: Watermark Developments Ltd., Inc. No. BCo642787
Applicant: Meiklejohn Architects

Ryan Smith
Date
Community Planning Department Manager
Community Planning \& Strategic Investments

## 1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

## 2. CONDITIONS OF APPROVAL

a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule " A ";
b) The exterior design and finish of the building to be constructed on the land be in accordance with Schedule " B ";
c) Landscaping to be provided on the land be in accordance with Schedule " C "; and
d) The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of $125 \%$ of the estimated value of the landscaping, as determined by a Registered Landscape Architect.

This Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

## 3. PERFORMANCE SECURITY

As a condition of the issuance of this Permit, Council is holding the security set out below to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Developer and be paid to the Developer or his or her designate if the security is returned. The condition of the posting of the security is that should the Developer fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the Municipality may use enter into an agreement with the property owner of the day to have the work carried out, and any surplus shall be paid over to the property own of the day. Should the Developer carry out the development permitted by this Permit within the time set out above, the security shall be returned to the Developer or his or her designate. There is filed accordingly:
a) An Irrevocable Letter of Credit in the amount of $\$ 984,695.60$

Before any bond or security required under this Permit is reduced or released, the Developer will provide the City with a statutory declaration certifying that all labour, material, workers' compensation and other taxes and costs have been paid.

## 5. INDEMNIFICATION

Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:
a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

## The PERMIT HOLDER is the CURRENT LAND OWNER. Security shall ONLY be returned to the signatory of the Landscape Agreement or their designates.



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## BUILDING FIRE SAFETY




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This forms part of application
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## ACADEMY RIDG

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## ACADEMY RIDGE

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## CONCEPTUAL

LANDSCAPE PLAN


L1/4


OUTLAND DESIGN LANDSCAPE ARCHITECTURE




## ACADEMY RIDGE

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WATER CONSERVATION/ IRRIGATION PLAN


L2/4

## IRRIGATION LEGEND









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OUTLAND DESIGN

## ACADEMY RIDGE

## Kolomo，BC

WATER CONSERVATION IRRIGATION PLAN


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## ACADEMY RIDGE

Kelowna, BC

## PHASING PLAN

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L4/4

## CITY OF KELOWNA

## MEMORANDUM

Date: July 18, 2018
File No.: DP18-0150
To: $\quad$ Environmental Coordinator (KB)
From: Development Engineering Manager (JK)
Subject: 610 Academy Way , Plan EPP53793
Form and Character

Development Engineering Services comments and requirements pertaining to a Development Permit for the form and character of multiple dwelling housing ( 28 row housing buildings, containing 154 units in total)
a) This Development Permit does not compromise any Municipal infrastructure services.
b) Minimum site distance must be confirmed for a safe entrance location.
c) A second Emergency access must be installed on Vint road.
d) Entry Plaza plantings on Vint rd and Academy way must not block site lines for Vint road.





## Academy Ridge

Development Permit Application DP18-0150
Addendum

January 29, 2019

## 1. ScALE

## Benefits and Opportunities

We recognize that the development is larger than typical townhome developments in Kelowna. While this may be seen as a potential detriment, there are mitigating circumstances as well as significant benefits and opportunities to the larger scale:

- The development is distinctly defined by natural borders: parks to the North and West, roads to the South and East and a major City Park across from Academy Way to the East, lending itself to create a consistent, cohesive neighbourhood.
- The larger size of the land parcel allows the creation of a central park amenity, while still achieving the density requirements.
- The arrangement of homes around the park creates a focus and more intimate feel for the community, rather than just homes and streets.


## Design Considerations for Mitigation

Careful consideration was given trying to mitigate some of the aspects of the larger scale by:

- Creating distinct "destination areas", sort of like "micro neighbourhoods", that will provide a more intimate feel within the community:
o "The Gateway", the entry to the community, designed to present a welcoming sense of arrival;
o "The Square", a designed focal point of the anchor buildings on the corner of Academy Way and Vint Road, complete with trees and street furniture;
o "The Plaza", a street- \& landscaped meeting place with trees and benches at the top of the development. It also will connect the walk/bike path axis through the development connecting the linear park to the West with The Gateway and the East side bus stop.
o "The Park", the center piece of the community with open spaces, trees and outdoor furnishings. This attractive and well-designed green space will provide an amenity for all residents and foster interactions between neighbours. Pathways through the park will facilitate pedestrian/bicycle movement through and across the community.

These areas will be thoroughly designed with attention to the streetscape (different street surfaces and patterns), plantings (variety of trees and shrubs), lighting (catenary lighting, for instance), and street furniture.

- Establishing a "people-friendly" neighbourhood by de-emphasizing the importance of automobiles:
o Living street concept - a shared streetscape designed primarily with the interests of pedestrians and cyclists in mind; and
o Providing mostly indoor parking (aside from visitor parking) to remove cars from the streets.
- Providing a diversity in product, which will appeal to larger segments of purchasers:
o Smaller, more affordable townhomes for first-time buyers, single professionals, couples without children or students; and
o Medium-sized and larger homes for families who might work in the area (UBC, airport district, etc.) or retired snowbirds, who would like to rent out a portion of their home.
- Full integration of the development's internal park and trail system with the bordering road and park infrastructure.


## 2. Form and Character

## Design Rationale

The design rationale for the form and character of the Academy Ridge development are based on the following principles. The objective is to create a distinct neighbourhood feel, identity and recognition.

- Integration of the development within the existing neighbourhood in terms of:
o the natural contours and slopes of the site; and
o architectural context of the neighbouring developments.
- De-emphasizing the buildings in favour of:
o the topography of the site;
o the unique spaces within the community;
o the landscaping of the park amenity; and
o the design of the "people-friendly" streetscape.
- Symmetry in terms of:
o buildings arrangements along the roadway providing a symmetrical streetscape; and
o grouping of buildings around the "destination" areas or "micro neighbourhoods".
- Rhythm in terms of:
o streetscape arrangement and plantings; and
0 street/sidewalk textures, patterns and materials.
- Balance in terms of combining the contemporary arrangement of the homes with:
o organic arrangement of the homes and streets based on the natural slopes of the site; and
o organic forms and shapes of the central park and trail system.


## Design Enhancements

We recognize that due to the scale of the development there is a risk of repetitiveness and lack of visual interest. While the topography of the site and the proposed landscaping plan will substantially mitigate this potential risk, further enhancements are being proposed:

- Addition of selective architectural design elements around the "destination" areas or "micro neighbourhoods" to further accentuate key areas, thereby adding increased visual interest, providing height variations to the buildings, and breaking up long roof lines;
- Use of more organic looking materials, such as natural wood-style siding and board \& batten siding, both likely in form of Hardie- and/or metal siding, to further integrate the development in its natural environment and amongst the neighbouring developments;
- Modified colour scheme, whereby there will be less colour variation within a building, combined with selective use of bold colours to accentuate key areas in order to create some colour "pop"; and
- Two distinct colour schemes to distinguish the 'park homes' along Academy Way and Vint Road from the 'hillside homes' on the upper slopes of the neighbourhood.

These design enhancements are in keeping with the City's Urban Design Guidelines (Chapter 14 of the OCP):
4.1 Building frontages that vary architectural treatment in regular intervals in order to maintain diverse and aesthetically appealing streets.
4.3 Design developments with multiple, separate buildings such that individual buildings are of different but compatible shapes, masses, and exterior finishes.
6.5 Colour should not be used as the predominant feature of a building (for differentiating buildings).

## 3. Tandem Parking

## Design Rationale

The rationale and vision behind tandem garages are based on two main factors:

1. The desire to reduce the importance and visibility of the automobile in favour of a pedestrian-/bicycle-friendly neighbourhood, and
2. Providing an affordable price point for certain segments of the market.

We recognize that it is not the most practical arrangement for two cars, but the alternative to providing the zoning-required two car stalls for a smaller footprint townhome would mean more pavement and open driveway parking.

## Mitigation Measures

We are proposing a number of measures to mitigate the impact and restrictions tandem parking may present:

- Our marketing and sales effort will emphasize the living street concept and the vision to reduce the importance of cars. We will disclose the nature and possible downfalls of tandem parking so that buyers are fully aware of it.
- We will be providing a number of alternatives or incentives to reduce the reliance on automobiles:
o A car share program - we are currently in discussion with Zipcar, who is providing car share services for the Veda micro-suite development. The initial feedback revealed:
o There is a definite interest in providing more car share services in the neighbourhood;
o It would require acritical mass of units to be built - maybe when half of Academy Ridge is built out); and
o Ideally the location of the shared cars would be on Vint Road, so that they are publicly available to other residents, including Academy Ridge, Deerhurst and University Heights.
o Two car charging systems in two visitor parking spaces;
o A common area bicycle wash and maintenance room, fully heated and lit, accessible to all Academy Ridge residents;
o Bicycle racks throughout the neighbourhood; and
o A bicycle storage mount for all garages.
- Spill over parking to Vint Road (there is no on-street parking on Academy Way) should be minimized as the majority of tandem units is quite far from Vint Road.
- The largest block of visitor parking has been placed close to the tandem units.

The following further provides additional rationale for tandem parking spaces:

1. Townhomes with Tandem Garages provide some Unique Attributes

- The townhomes typically:
o have a smaller floor plate, thereby leaving a smaller footprint and requiring less land;
o allow for a larger depth to width ratio, which achieves benefits in certain community layouts;
o allow for a smaller overall building size - generally 1,300-1,400 sqft;
o allow for a greater density while still providing a park amenity.
- They require less resources and are therefore more economical to build.
- Ultimately townhomes with tandem garages can be sold at a lower, more affordable price point.

2. Tandem Garages Provide a Choice and Meet a Need of the Market (Affordability)

- Townhomes with tandem garages provide a choice to the home buyer and add to the diversity of the community.
- Our market research shows that potential buyer segments will include students, first-time buyers, and singles.
- Affordability is one of their key buying criteria.

3. Tandem Garages Provide for a Better Neighbourhood

- An alternative to creating an economical townhome would be a single car garage with a car port or open driveway parking.
- While this would possibly be even more economical to build, it does to fit into our vision of a pedestrian-centric neighbourhood - we want to emphasize the people while reducing the impact and visibility of cars.
- Significant improvement to the urban quality of neighborhood by accessing two required stalls with just one door vs. a 'sea’ of garage doors.

4. Tandem Garages Allow for Better Common Spaces

- The larger depth to width ratio allows to achieve a certain density without requiring more roads and thereby providing for more common park space.
- Academy Ridge's common park space combined with the carefully designed 'destinations' will be very unique and will create a very intimate community feel.

5. There is a Diminishing Need for Garage Space

- There is an overall trend towards reducing the reliance on a personal vehicle.
- Car sharing, ride sharing, more public transit and self-driving cars are some of the latest developments affecting the use of personal vehicles.
- Academy Ridge will have a public transit bus stop right in front of the development on Academy Way.
- Academy Ridge will be about 10 minutes on foot from the future hilltop commercial village, 15 minutes from Academy Hill and 22 minutes from UBC.
- The University Village is designed around pedestrians (boulevards, sidewalks, trails, pedestrian overpass, etc.).
- Other developments in the neighbourhood report a car ownership as low as 0.33-0.40 cars per bedroom, which would mean a 3-bedroom unit would technically only require 1.0-1.2 garage spaces.

6. Tandem Garages are Very Common

- They meet the Zoning Bylaw
- There are numerous examples of developments with tandem garages in Kelowna, West Kelowna, Lake Country, Vancouver, etc.


## 7. Tandem Garages at Academy Ridge

- Only $43 \%$ of the units will have tandem garages
- Based on total Gross Livable Area only $35 \%$ will have tandem garages (because the tandem units are proportionally smaller and more affordable)


## ATTACHMENT $\mathbf{C}$

Planner
nitials KB Kelowna

## 4. TIMING FOR THE INTERNAL PARK/WALKWAY SYSTEM

The construction of the park amenity is planned to be built in stages following each phase. The proposed phased strata plan (Form P), which will need to be approved by the Approving Officer of the City of Kelowna, will show the park boundaries for each phase. These boundaries will follow along the designed trail network of the park.

