

Development Permit & Development Variance Permit DP15-0176 & DVP15-0177



This permit relates to land in the City of Kelowna municipally known as

150 Kneller Rd

and legally known as

Lot 2, Section 27, Township 26, ODYD, Plan EPP63257

and permits the land to be used for the development with variances to the following sections of the Zoning Bylaw 8000:

Section 8.1.11 (a) Size and Ratio

To vary the ratio of vehicular parking stalls sizes from: min 50% Full size, max 40% medium size, & max 10% compact size to 37.8% Full size, 56.8% medium size, & 5.4% compact size.

The development has been approved subject to any attached terms and conditions, and to full compliance with the approved plans bearing the stamp of approval and the above described development permit number.

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

Date of Decision: February 20th 2017

Decision By: CITY COUNCIL

Issued Date: **DATE**

Development Permit Area: Comprehensive Development Permit Area

File Manager: AC

This permit will not be valid if development has not commenced within 2 years of the council approved Date of Decision.

Existing Zone: RM3 – Low Density Multiple Housing

Future Land Use Designation: MRL – Multiple Unit Residential (Low Density)

This is NOT a Building Permit.

In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

NOTICE

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner: Okanagan Metis & Aboriginal Housing Society Inc. No. Soo21636
Address: 240 – 1855 Kirshner Road
City: Kelowna, BC
Phone: n/a

Ryan Smith, Community Planning Department Manager
Community Planning & Strategic Investments

Date

1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

2. CONDITIONS OF APPROVAL

- a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";
- b) The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";
- c) Landscaping to be provided on the land be in accordance with Schedule "C"; and
- d) The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect.

This Development Permit is valid for two (2) years from the Council Date of Decision if applicable, or Community Planning Department Manager approval, with no opportunity to extend.

3. PERFORMANCE SECURITY

As a condition of the issuance of this Permit, Council is holding the security set out below to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Developer and be paid to the Developer or his or her designate if the security is returned. The condition of the posting of the security is that should the Developer fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the Municipality may use enter into an agreement with the property owner of the day to have the work carried out, and any surplus shall be paid over to the property own of the day. Should the Developer carry out the development permitted by this Permit within the time set out above, the security shall be returned to the Developer or his or her designate. There is filed accordingly:

- a) A Certified Cheque in the amount of **\$116,220.10**
- OR**
- b) An Irrevocable Letter of Credit in the amount of **\$116,220.10**

Before any bond or security required under this Permit is reduced or released, the Developer will provide the City with a statutory declaration certifying that all labour, material, workers' compensation and other taxes and costs have been paid.

4. Indemnification

Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:

- a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

**The PERMIT HOLDER is the CURRENT LAND OWNER.
Security shall ONLY be returned to the signatory of the
Landscape Agreement or their designates.**

SCHEDULE A & B

This forms part of application

DP15-0176 / DVP15-0177

Planner
Initials

AC

City of
Kelowna
COMMUNITY PLANNING



ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing and for reproduction to the original site for which it was prepared and publication thereof is expressly limited to such use.

This drawing must not be scaled.
Verify all dimensions and details prior to commencement of work.
Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

SEAL



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

OMAHs - NISSEN CROSSING

78 UNIT MULTI - FAMILY RESIDENTIAL BUILDING ON GERSTMAR AND HIGHWAY 33



ARCHITECTURAL

NEW TOWN ARCHITECTURE & ENGINEERING INC.
1441 St. Paul Street Kelowna, B.C. V1Y 2E5
Roman Yanchukov, Architect ASBC
ph: 250.860.9555, fax: 250.860.9555
roman@newtownservices.ca

A0.00	COVERPAGE AND DWG LIST	A6.00	WALL SECTIONS
A0.01	ZONING & BUILDING CODE REVIEW	A6.01	WALL SECTIONS
A0.02	EXITING AND BUILDING CODE REVIEW PLANS	A6.02	WALL SECTIONS
A0.03	DOOR & WINDOW SCHEDULE	A6.03	WALL SECTIONS
A1.00	BUILDING ASSEMBLIES	A7.00	STAIR 1 DETAILS
A2.00	SITE PLAN	A7.01	STAIR 2 DETAILS
A3.00	PARKADE OVERALL PLAN	A7.02	STAIR SECTIONS
A3.01	LEVEL 1 OVERALL PLAN	A8.00	PLAN DETAILS
A3.02	LEVEL 2 OVERALL PLAN	A8.01	PLAN DETAILS
A3.03	LEVEL 3 OVERALL PLAN	A8.02	SECTION DETAILS
A3.04	ROOF OVERALL PLAN	A8.03	SECTION DETAILS - DECK
A3.05	UNIT PLANS	A8.04	SECTION DETAILS - ELEVATOR
A3.06	UNIT AND COMMON ROOM PLANS	A8.05	SECTION DETAILS
A3.07	ELEVATOR PLANS	A8.06	DETAILS
A3.08	ELEVATOR PLANS	A8.07	DETAILS
A3.09	LAUNDRY ROOMS	A8.08	DETAILS
A3.10	LEVEL 1 - SLAB EDGE PLAN		
A3.11	LEVEL 1 OVERALL - RCP		
A3.12	LEVEL 2 OVERALL - RCP		
A3.13	LEVEL 3 OVERALL - RCP		
A4.00	EXTERIOR ELEVATIONS		
A4.01	PERPENDICULAR ELEVATIONS		
A5.01	BUILDING SECTIONS		
A5.02	BUILDING SECTIONS		

STRUCTURAL

ROY CONSULTING INC. STRUCTURAL ENGINEERS
11-2040 Spryfield Road Kelowna, B.C. V1Y 9B7
Mans Yachukov, P.Eng.
ph: 250.860.9400, fax: 250.860.9400
mns@royconsulting.ca

S1.0	SPECIFICATIONS
S1.1	SPECIFICATIONS
S2.0	FOUNDATION PLAN
S2.1	SUSPENDED SLAB PLAN - BOTTOM REINFORCING
S2.2	SUSPENDED SLAB PLAN - TOP REINFORCING
S3.0	CONCRETE SECTIONS AND DETAILS
S3.1	CONCRETE SECTIONS AND DETAILS
S3.2	CONCRETE SECTIONS AND DETAILS
S4.0	LEVEL 2 FRAMING PLAN
S4.1	LEVEL 3 FRAMING PLAN
S4.2	ROOF FRAMING PLAN
S5.0	WOOD FRAMING SECTIONS AND DETAILS
S5.1	WOOD FRAMING SECTIONS AND DETAILS

MECHANICAL

M1	SYMBOL LEGEND, LOAD SUMMARY TABLES AND INSTALLATION DETAILS
M2	FOUNDATION PLAN - MECHANICAL
M3	PARKADE FLOOR PLAN - MECHANICAL
M4	LEVEL 1 FLOOR PLAN - MECHANICAL
M5	LEVEL 2 FLOOR PLAN - MECHANICAL
M6	LEVEL 3 FLOOR PLAN - MECHANICAL
M7	ROOF PLAN - MECHANICAL
M8	UNIT PLANS - MECHANICAL
M9	LARGE SCALE PLANS - MECHANICAL
M10	SANITARY, DOMESTIC WATER AND CORRIDOR MAKE-UP AIR RISER DIAGRAMS
M11	EQUIPMENT SCHEDULES
M12	MECHANICAL SPECIFICATIONS

ELECTRICAL

PVE ENGINEERING LTD.
16-442 West Fraser Dr. Kelowna, B.C. V1Y 1T5
Stephen Jacob, P.Eng.
ph: 250.815.5750, fax: 250.815.7822
sjacob@pveengineering.com

E1.0	SITE PLAN, SINGLE LINE DIAGRAM, SYMBOL LEGEND
E1.1	PARKING PLAN - WEST
E1.2	PARKING PLAN - EAST
E1.3	LEVEL 1 - WEST
E1.4	LEVELS 2, 3
E1.5	ROOF PLAN
E1.6	SUITE PLANS, TYPICAL SUITE PANEL LUMINAIRE SCHEDULE
E2.0	PANEL SCHEDULES, EQUIPMENT SCHEDULES
E2.1	PANEL SCHEDULES, EQUIPMENT SCHEDULES

CIVIL

S.E. PILLING & ASSOC. LTD.
230-400 Victoria Road Kelowna, B.C. V1Y 4T7
Dale E. Pilling, P.Eng.
ph: 250.762.2716, fax: 250.762.9599
dilling@pilling.ca

2347-C1	COMPOSITE UTILITY PLAN
2347-D1	GRADING PLAN & STORM WATER MANAGEMENT PLAN
2347-D2	S.W.M. & GRADING PLAN
2347-H1	LANDING AND GEOMETRICS
2347-R1	KNELLAR PLAN AND PROFILE
2347-R3	I-BUS MOVEMENTS



TRUE NORTH



project title
**OMAHs - NISSEN
CROSSING**

project address
**1170 HIGHWAY 33,
KELOWNA, BC**

project no. 1580

revision

drawing title

**COVERPAGE
AND DWG LIST**

designed R.Y. mns

drawn J.F./R.B.

checked R.Y.

drawing no.

A0.00

printed Aug 03, 2017

KEYPLAN: 1170 HIGHWAY 33

ZONING SUMMARY		
ADDRESS	1170 HIGHWAY 33W	
LEGAL DESCRIPTION	PARCEL B OF LOT 4, SECTIONS 26 AND 27, TP 26, COYO, PLAN 428	
DEVELOPMENT PERMIT AREA	GENERAL MULTIPLE UNIT OF AREA	
EXISTING ZONING	RUI - LARGE LOT HOUSING	
PROPOSED ZONING	RM0 - LOW DENSITY MULTIPLE HOUSING	
EXISTING LEGAL USE	SINGLE FAMILY	
GRADES	VARIES - SLOPES APPROXIMATELY 1.5m DOWN TOWARD HIGHWAY 33	
NUMBER OF BUILDINGS	1	
REQUIREMENTS FOR ALL DEVELOPMENT PERMIT APPLICATIONS		
	ZONING STANDARD	PROPOSED ZONING
SITE AREA (m ²)	MIN 300m	4,915m
SITE DEPTH (m)	MIN 35m	44m
OFF STREET PARKING	113 STALLS	87 UIC AND 44 AG + 111 STALLS
PRIVATE OPEN SPACE	REFER TO CALCULATIONS	REFER TO CALCULATIONS
HEIGHT OF BUILDINGS / # STOREYS	9m OR 3 STOREYS	3 STOREYS (8.3m)
SITE COVERAGE OF BUILDINGS (%)	40%	35%
SITE COVERAGE INCLUDING BUILDINGS, DRIVEWAYS, AND PARKING	60%	60%
ADDITIONAL REQUIREMENTS FOR COMMERCIAL, INDUSTRIAL, AND MULTIPLE UNIT / INTENSIVE RESIDENTIAL APPLICATION		
	ZONING STANDARD	PROPOSED ZONING
NUMBER OF BICYCLE PARKING SPACES	47 STALLS	47 STALLS
NUMBER OF LOADING SPACES	N/A (RESIDENTIAL)	0
DRIVE AISLE WIDTH	7m	7m
SETBACKS TO PARKING		
SOUTH (FRONT)	N/A	N/A
NORTH (REAR)	1.5m	8.5m
EAST (SIDE)	1.5m	N/A
WEST (SIDE)	1.5m	1.5m
FLOOR AREA NET	REFER TO CALCULATIONS	REFER TO CALCULATIONS
FLOOR AREA RATIO (F.A.R.)	75	75
BUILDINGS, SETBACK		
SOUTH (FRONT)	4.5m (HIGHWAY 33 SETBACK)	4.5m (HIGHWAY 33 SETBACK)
NORTH (SIDE)	4.5m (PLANNING STREET)	8.5m
EAST (SIDE)	1.5m (GROUND ORIENTED SUITES)	1.5m (GROUND ORIENTED SUITES)
WEST (SIDE)	4.5m	20.5m

PARKING CALCULATION			
STALL SIZE		BYLAW 8005 TABLE 6.1	
FULL SIZE CAR STALL		2.5m WIDE x 6.0m DEEP	
MEDIUM SIZE CAR STALL (M1)		2.3m WIDE x 6.0m DEEP	
COMPACT CAR STALL (C2)		2.0m WIDE x 3.4m DEEP	
DISABLE PERSONS CAR STALL		3.7m WIDE x 6.0m DEEP	
DRIVE AISLE (2 WAY, 30 DEGREE STALLS)		7m	
PARKING			
REQUIRED BY BYLAW		PROPOSED	
1.0 STALL PER STUDIO + 0 UNITS		1.0 STALL PER STUDIO + 0 UNITS	
1.25 STALL PER 1 BEDROOM + 0 UNITS		1.25 STALL PER 1 BEDROOM + 0 UNITS	
1.50 STALL PER 2 BEDROOM + 0 UNITS		1.50 STALL PER 2 BEDROOM + 0 UNITS	
TOTAL RESIDENTIAL PARKING		TOTAL RESIDENTIAL PARKING	
OFFICE PARKING (50m) 2.5 PER 100m GFA		OFFICE PARKING (50m) 2.5 PER 100m GFA	
TOTAL PARKING STALLS		TOTAL PARKING STALLS	
PARKING RATIO			
REQUIRED BYLAW		PROPOSED	
LARGE CAR		40%	
MEDIUM CAR		40%	
COMPACT CAR		10%	
HANDICAP PARKING		1% (30%)	
TOTAL PARKING		TOTAL PARKING	
HIC PARKING			
HANDICAP PARKING (1 PER 100 UNITS)		HANDICAP PARKING (1 PER 100 UNITS)	
TOTAL HIC PARKING		TOTAL HIC PARKING	
LOADING ZONE			
BYLAW 8005 TABLE 6.2			
BICYCLE PARKING			
BICYCLE PARKING (CLASS 1) 5 PER DWELLING UNIT		BICYCLE PARKING (CLASS 2) 1 PER DWELLING UNIT	
TOTAL BICYCLE PARKING		TOTAL BICYCLE PARKING	

BUILDING CODE REVIEW			
BUILDING DESCRIPTION: 3 STOREY RESIDENTIAL BUILDING ON A ONE STORY PARKADE			
NOTE: PARKADE IS TO BE CONSIDERED AS A SEPARATE BUILDING IN ACCORDANCE WITH 3.2.1.2			
OCCUPANCY	GROUP C	GROUP F3	
ARTICLE	3.2.2.82	3.2.2.82	
NUMBER OF STOREYS	3 LEVELS ABOVE PARKADE	4 LEVELS INCLUDING PARKADE	
NUMBER OF STREETS FACING	4	4	
MAX. BUILDING AREA	2,400m ²	3,800m ²	
CONSTRUCTION TYPE	COMBUSTIBLE / NON COMBUSTIBLE	NON COMBUSTIBLE	
SPRINKLERED	YES (MPPA 15%)	YES (MPPA 15%)	
ASSEMBLY RATINGS	RESIDENTIAL	PARKADE	
FLOORS	1 HR FIRE RATED ASSEMBLY	2 HR FIRE RATED ASSEMBLY	
WALLS / STRUCTURE	1 HR FIRE RATED ASSEMBLY	2 HR FIRE RATED ASSEMBLY	
ROOF	1 HR FIRE RATED ASSEMBLY	2 HR FIRE RATED ASSEMBLY	

BUILDING AND FLOOR AREAS			
NET AREAS			
UNIT TYPE	UNIT AREA	# UNITS	TOTAL AREA
A ONE BEDROOM	510 sq.ft. (47m ²)	50	25,500 sq.ft. (2,369 m ²)
A2 ONE BEDROOM	572 sq.ft. (53m ²)	2	1,144 sq.ft. (106 m ²)
B TWO BEDROOM	685 sq.ft. (63m ²)	6	4,110 sq.ft. (387 m ²)
C TWO BEDROOM	685 sq.ft. (63m ²)	6	4,110 sq.ft. (387 m ²)
C1 TWO BEDROOM	685 sq.ft. (63m ²)	3	2,055 sq.ft. (193 m ²)
D STUDIO	312 sq.ft. (29m ²)	9	2,808 sq.ft. (261 m ²)
E TWO BEDROOM	867 sq.ft. (80m ²)	3	2,601 sq.ft. (242 m ²)
TOTALS		78	41,883 sq.ft. (3,871 m ²)
LEVEL 1 OFFICE AREA			
LEVEL 1 COMMON AREA			
TOTAL NET AREA FOR F.A.R.			
F.A.R. FOR RM0 = 75			
GFA AREAS			
PARKADE			
LEVEL 1			
LEVEL 2			
LEVEL 3			
TOTAL GFA			
BUILDING EFFICIENCY			

SITE INFO AND AREAS	
SITE AREA	2,299m ² (ORIGINAL LOT) + 1,915m ² (WELLER LANE AREA) + 4,915m ² (52,940 sq.ft.)
LEVEL 1 FOOTPRINT AREA	17,333 sq.ft. (1,610 m ²)
PARKADE LEVEL, DRIVEWAYS, AND PARKING AT GRADE	35,252 sq.ft. (3,275 m ²)
MAX SITE COVERAGE	40% MAX
CURRENT SITE COVERAGE (BUILDING)	20% (17,333 / 82,940 = 20%)
MAX SITE COVERAGE (BUILDING, DRIVEWAYS, AND PARKING)	60% MAX
CURRENT SITE COVERAGE (BUILDING)	(BUILDING + 1,915m ²) + (DRIVEWAYS AND PARKING + 1,957m ²) = 2,872m ²
CURRENT SITE COVERAGE (BUILDING, DRIVEWAYS, AND PARKING)	2,872m ² / 4,915m ² = 58.4%
SITE AREA REQUIRED TO MEET F.A.R. REQUIREMENTS (RM0 = 75)	43,407 sq.ft. / 75 = 578.76 sq.ft. (5,377 m ²)
TOTAL AREA PROVIDED FOR F.A.R.	4,915 sq.ft. + 1,957 sq.ft. = 6,872 sq.ft.

PRIVATE OPEN SPACE	
LEVEL 1	
A ONE BEDROOM	16
A2 ONE BEDROOM	2
B TWO BEDROOM	2
C TWO BEDROOM	2
C1 TWO BEDROOM	1
D STUDIO	3
E TWO BEDROOM	1
TOTAL	
LEVEL 2	
A ONE BEDROOM	17
A2 ONE BEDROOM	1
B TWO BEDROOM	2
C TWO BEDROOM	2
C1 TWO BEDROOM	1
D STUDIO	3
E TWO BEDROOM	1
TOTAL	

SPATIAL SEPARATION	
ARTICLE 3.2.3.1.D	
WINDOW OPENINGS AND WALL CONSTRUCTION ARE UNRESTRICTED AS THE LIMITING DISTANCE EXCEEDS 8m IN ALL CASES, OR THE BUILDING IS FROM A STREET IN ACCORDANCE WITH ARTICLE 3.2.3.1.B	

FIRE PROTECTION	
LOCATION OF HYDRANT TO SHIMSE CONNECTION	45m MAX.
STANDEPPE / HOSE	YES (IN EXIT STAIRS)
SPRINKLERED	YES
FIRE ALARM SYSTEM	YES
EXIT LIGHTS	YES
EMERGENCY LIGHTING	YES

OCCUPANT LOAD	
PARKADE LEVEL	1800 sq.m / 45 sq.m PER PERSON
LEVEL 1 - COMMON ROOM	120 sq.m / 30 sq.m PER PERSON
LEVEL 1 - OFFICES	50 sq.m / 12.5 sq.m PER PERSON
LEVEL 1 - RESIDENTIAL	24 UNITS x 2 PEOPLE / DWELLING UNIT
LEVEL 2 AND 3 - RESIDENTIAL	(27 UNITS x 2 PEOPLE / DWELLING UNIT) x 2
TOTAL OCCUPANT LOAD	

EXIT FACILITIES	
REQUIRED EXITS	2 PER FLOOR MINIMUM
REQUIRED	
PARKADE LEVEL DOORS	6.1m / PERSON x 41
PARKADE LEVEL STAIRS	6.1m / PERSON x 41
LEVEL 1 DOORS	6.1m / PERSON x 97
LEVEL 1 STAIRS	6.1m / PERSON x 97
LEVEL 2 DOORS	6.1m / PERSON x 54
LEVEL 2 STAIRS	6.1m / PERSON x 54

RESIDENTIAL UNITS	
MIN. 1 DOOR - 915mm WIDE PER UNIT	1 DOOR - 915mm WIDE PER UNIT
EXIT THROUGH LOBBY	N/A (ARTICLE 3.4.4.2)
PANIC DOOR HARDWARE	REQUIRED (ARTICLE 3.4.6.16.1)
EXIT EXPOSURE	NO ADDITIONAL PROTECTION NEEDED (ARTICLE 3.2.3.1.B)
MAX TRAVEL DISTANCE	45m (RESIDENTIAL) AND 60m (PARKADE) (ARTICLE 3.4.2.5.1)
EXIT RATINGS REQUIRED	
STAIR SHAFTS	1 HOUR IN RESIDENTIAL / 2 HOURS IN PARKADE (ARTICLE 3.4.4.1)
CORRIDORS	1 HOUR (ARTICLE 3.2.4.1.1)

REQUIRED FIRE SEPARATIONS	
TENANTS / MAJOR OCCUPANCIES	
GROUP C TO F3	2 HOUR (ARTICLE 3.2.1.2)
GROUP C TO C	1 HOUR (ARTICLE 3.2.1.1)
SERVICE ROOMS	1 HOUR (ARTICLE 3.6.2)
JANITOR ROOMS	NON-RATED FIRE SEPARATION (ARTICLE 3.3.1.21.1)

BUILDING FIRE SAFETY	
SMOKE PROTECTION	NOT REQUIRED - SPRINKLERED (ARTICLE 3.2.3.16.4)
FLAME SPREAD RATING	COMPLIES (ARTICLE 3.1.10.2)
METAL SMOKE ASSEMBLIES	N/A (ARTICLE 3.1.14.2)
ROOF COVERING CLASSIFICATION	CLASS "A" (ARTICLE 3.1.15.2)
ATTIC FIRE STOPS	REQUIRED (ARTICLE 3.1.11.1)
MAX. ATTIC AREA	300 sq.m WITH A FLAME SPREAD RATING GREATER THAN 25 (ARTICLE 3.1.11.5.1)
MAX. CRAWL SPACE AREA	N/A (ARTICLE 3.1.11.6.1)
CONCEALED FLOOR AREA	N/A (ARTICLE 3.1.11.6.1)
FIRE ALARM REQUIRED	YES
STANDEPPE REQUIRED	YES
EMERGENCY LIGHTS / EXIT SIGNS REQUIRED	YES

ACCESSIBILITY REQUIREMENTS	
ACCESS TO MAIN ENTRANCE	REQUIRED / PROVIDED
ACCESS TO ALL FLOORS	REQUIRED / PROVIDED
ACCESSIBLE WASHROOMS	REQUIRED / PROVIDED

WASHROOM REQUIREMENTS	
MIN. 1 PER DWELLING UNIT	REQUIRED / PROVIDED
MIN. 1 PER OFFICE AREA	REQUIRED / PROVIDED

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA. This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use. This drawing must not be scaled. Verify all dimensions and details prior to commencement of work. Report all errors and omissions to the Architect.



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-21	ISSUED FOR BP
3	17-02-16	90% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAHAS - NISSEN CROSSING

project address
1170 HIGHWAY 33, KELOWNA, BC

project no.
1560

file no.

drawing title
ZONING & BUILDING CODE REVIEW

designed
R.Y.

drawn
J.F. / R.B.

checked
R.Y.

drawing no.
A0.01

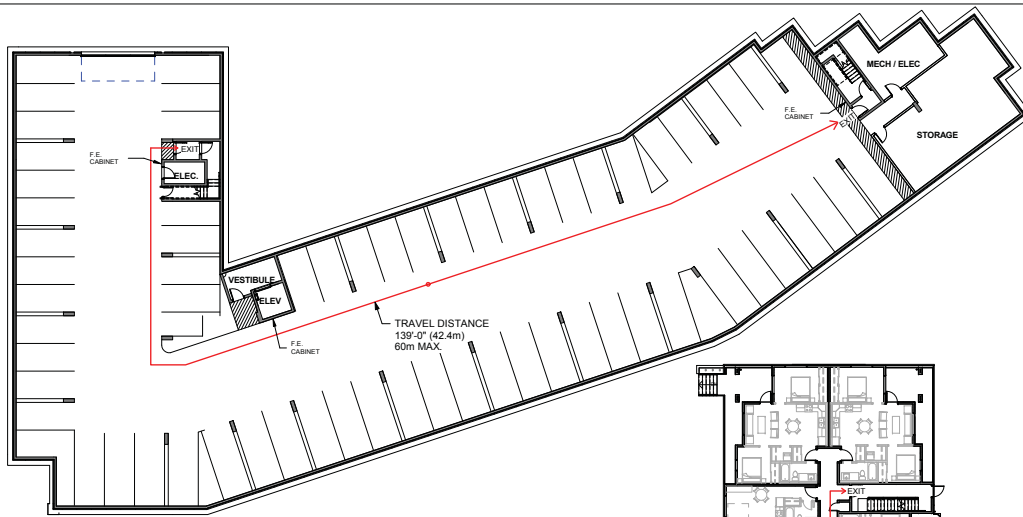
issued
Aug 03, 2017

SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials
AC

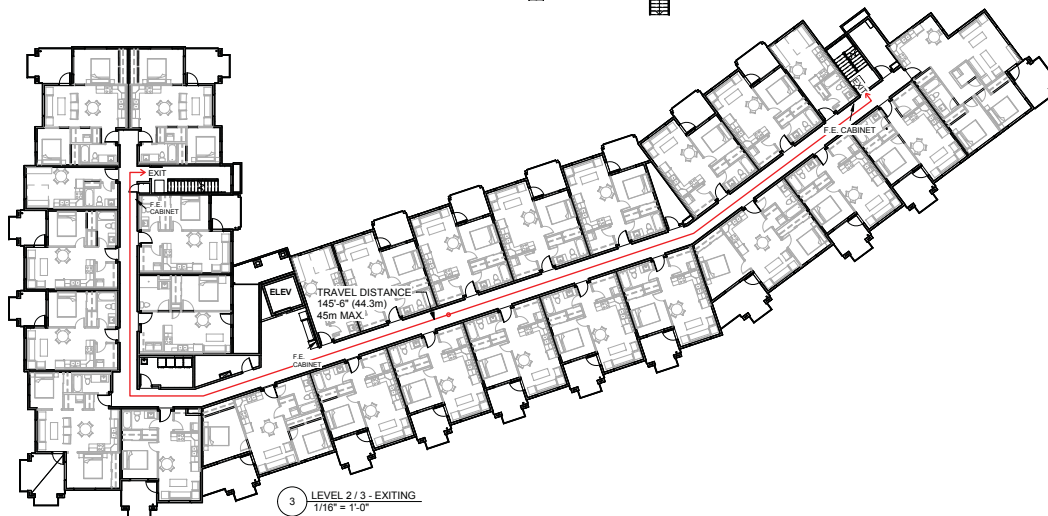




1 PARKADE - EXITING
1/16" = 1'-0"



2 LEVEL 1 - EXITING
1/16" = 1'-0"



3 LEVEL 2/3 - EXITING
1/16" = 1'-0"

SCHEDULE A & B

This forms part of application

DP15-0176 / DVP15-0177

Planner
Initials

AC



ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use.

This drawing must not be scaled. Verify all dimensions and details prior to commencement of work. Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

SEAL



Revisions

1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
**1170 HIGHWAY 33,
KELOWNA, BC**

project no. 1580

file no.

drawing title

**EXITING AND
BUILDING
CODE REVIEW
PLANS**

designed R.Y. scale 1/16" = 1'-0"

drawn J.F./R.B.

checked R.Y.

drawing no.

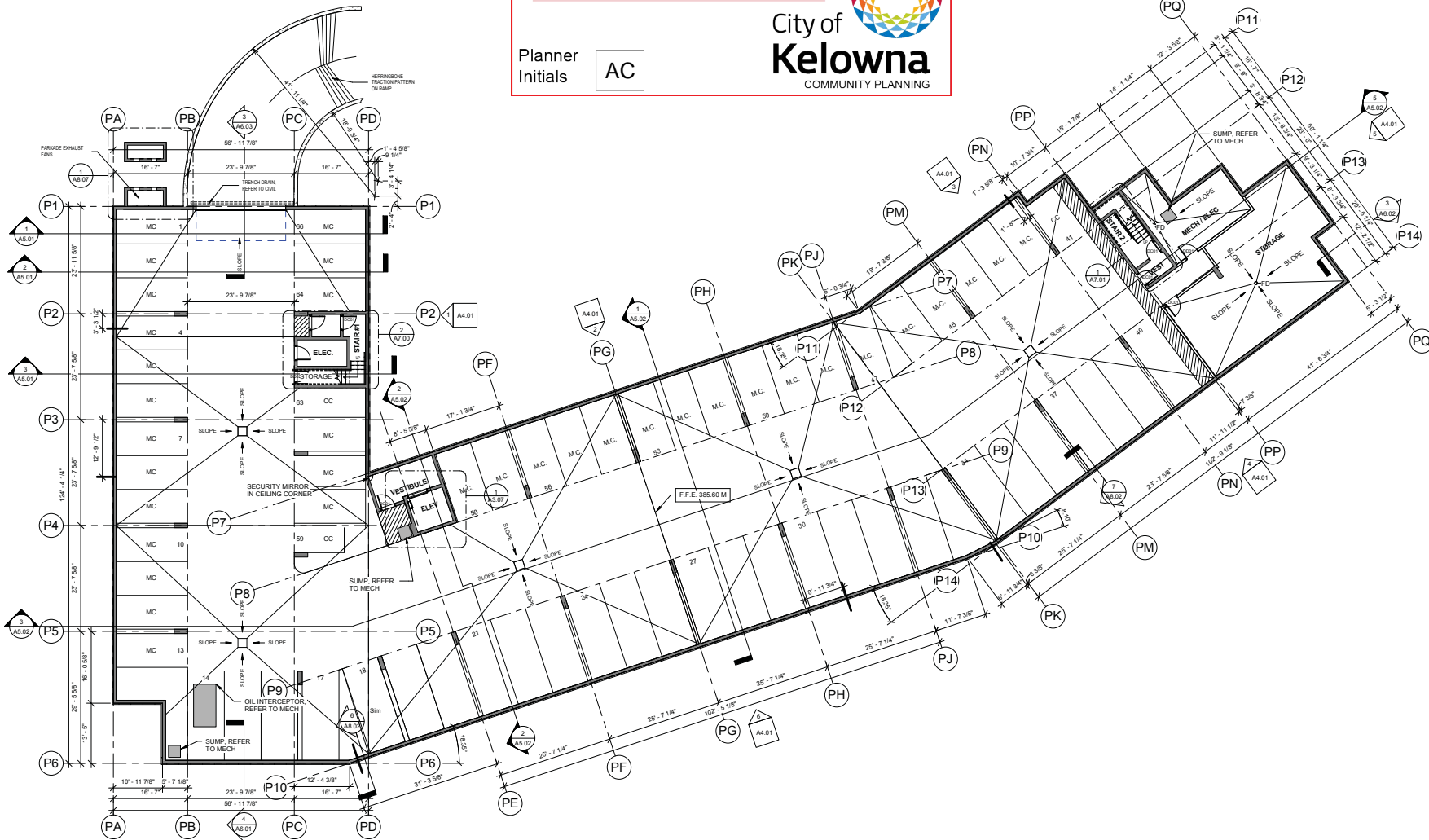
A0.02

issued Aug 03, 2017

SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials AC



ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.
This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use.
This drawing must not be scaled.
Verify all dimensions and datums prior to commencement of work.
Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca



SEAL



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
**1170 HIGHWAY 33,
KELOWNA, BC**

project no. 1980

file no.

drawing title
**PARKADE
OVERALL PLAN**

designed R.Y. scale 3/32" = 1'-0"
draw J.F./R.B.
checked R.Y.
drawing no.

A3.00

printed Aug 03, 2017

1 PARKADE OVERALL PLAN
3/32" = 1'-0"

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use.

This drawing must not be scaled.
Verify all dimensions and details prior to commencement of work.
Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca



SEAL



Revisions

1	17-01-06	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
**1170 HIGHWAY 33,
KELOWNA, BC**
project no. 1580
drawing title
**LEVEL 1
OVERALL PLAN**

designed R.Y. scale 3/32" = 1'-0"
drawn J.F./R.B.
checked R.Y.
drawing no.

A3.01
issued Aug 03, 2017

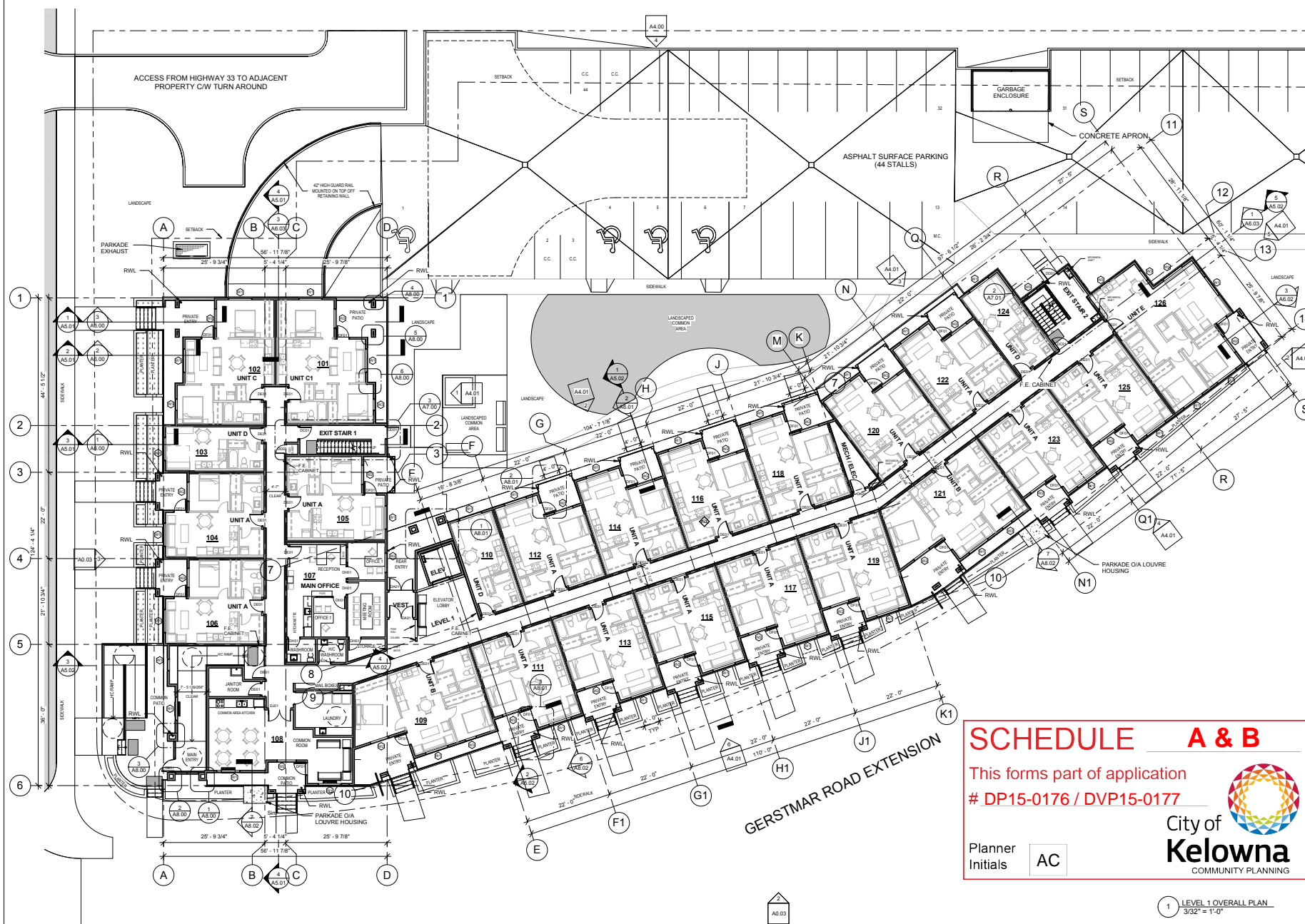
SCHEDULE A & B

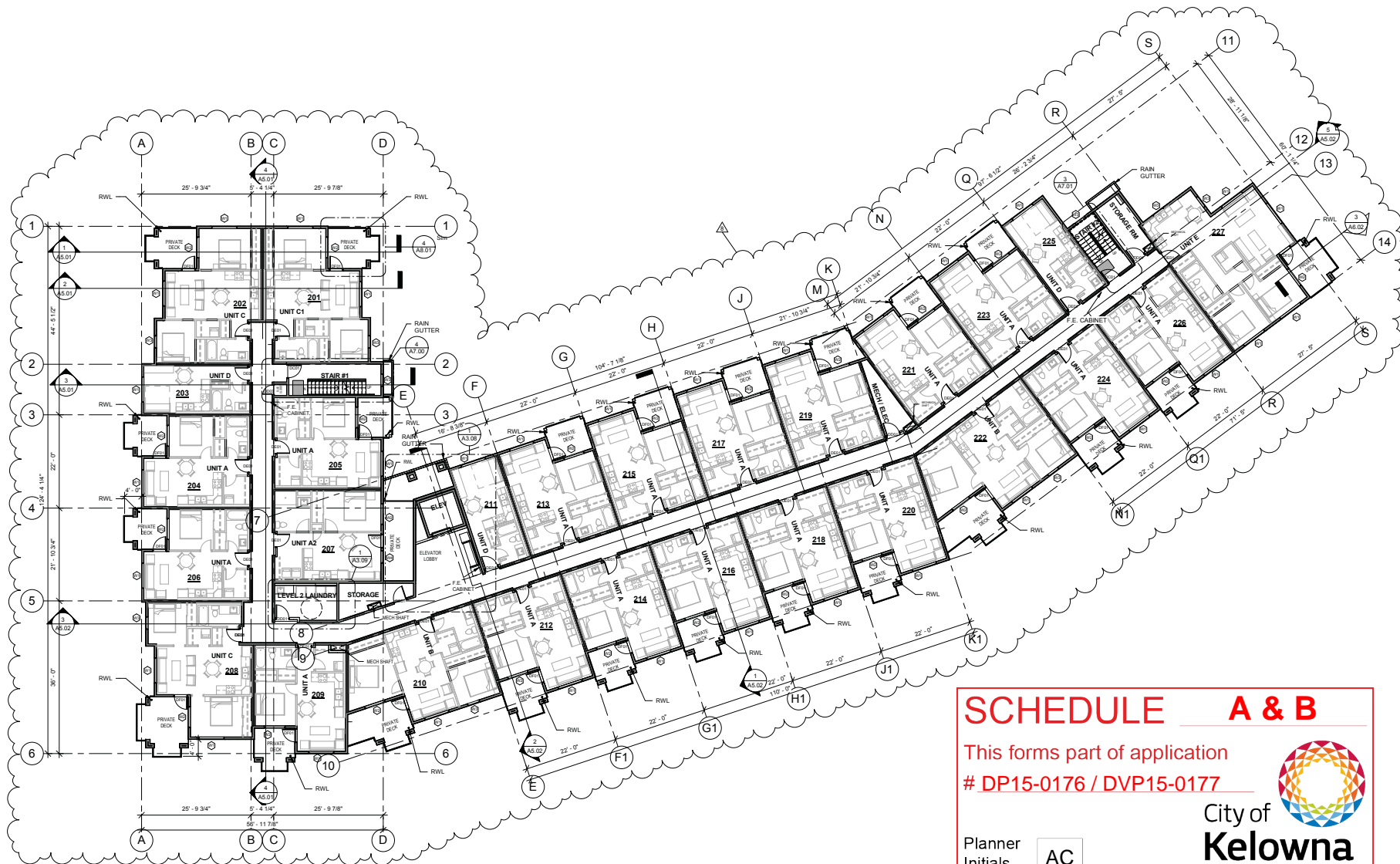
This forms part of application
DP15-0176 / DVP15-0177

Planner Initials **AC**

City of
Kelowna
COMMUNITY PLANNING

1 LEVEL 1 OVERALL PLAN
3/32" = 1'-0"





SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials

AC

City of Kelowna
COMMUNITY PLANNING

1 LEVEL 2 OVERALL PLAN
3/32" = 1'-0"

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.
 This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original site for which it was prepared and publication thereof is expressly limited to such use.
 This drawing must not be scaled.
 Verify all dimensions and datums prior to commencement of work.
 Report all errors and omissions to the Architect.



SEAL



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
1170 HIGHWAY 33, KELLOWNA, BC

project no.
 1580

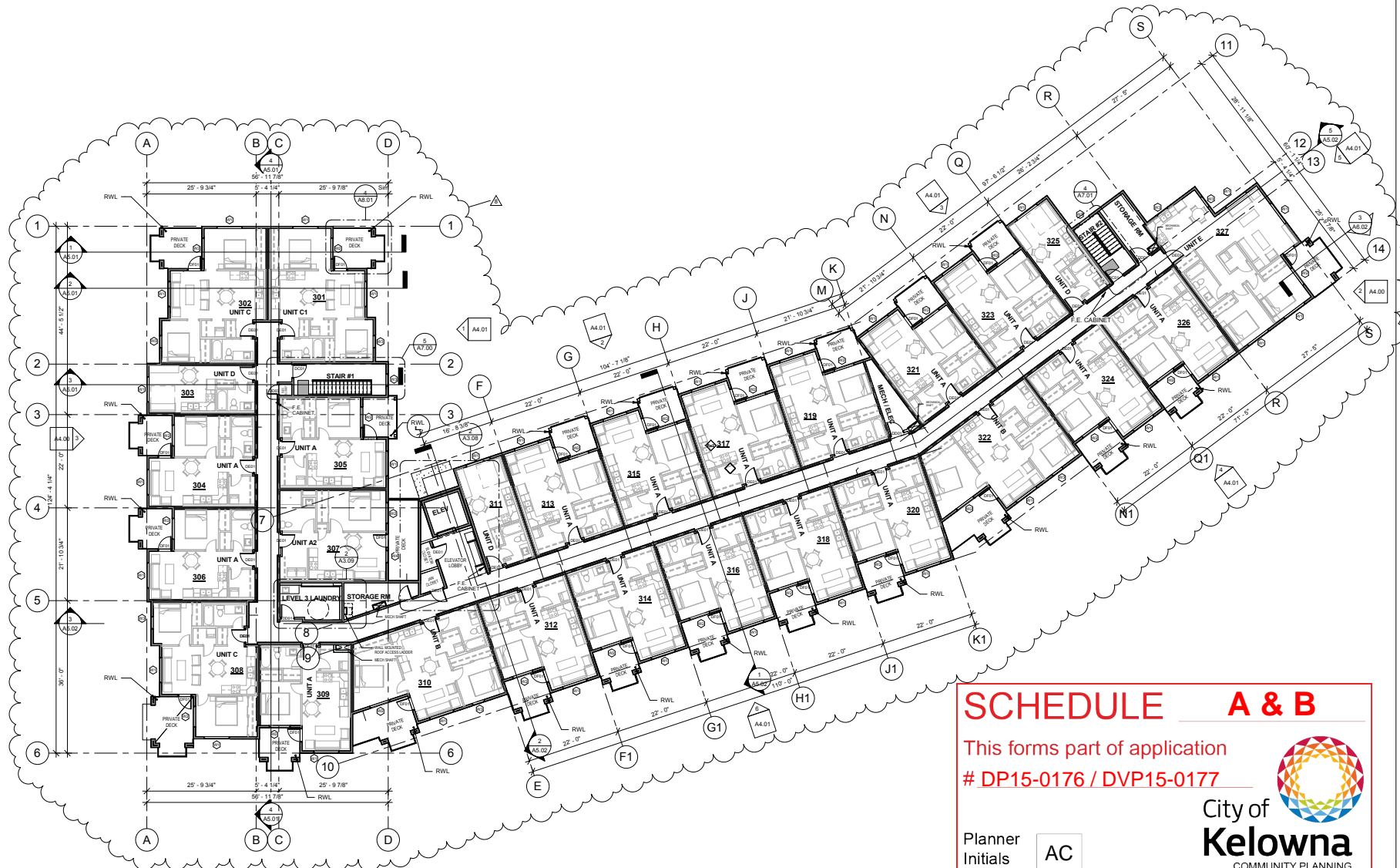
drawing title
LEVEL 2 OVERALL PLAN

designed	R.Y.	scale	3/32" = 1'-0"
drawn			J.F. / R.B.
checked			R.Y.
drawing no.			

A3.02

issued Aug 03, 2017

A4.00



A4.00

SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials

AC

City of Kelowna
COMMUNITY PLANNING

1 LEVEL 3 OVERALL PLAN
3/32" = 1'-0"

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.
This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use.
This drawing must not be scaled.
Verify all dimensions and datums prior to commencement of work.
Report all errors and omissions to the Architect.



SEAL



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
1170 HIGHWAY 33, KELOWNA, BC

project no.
1580

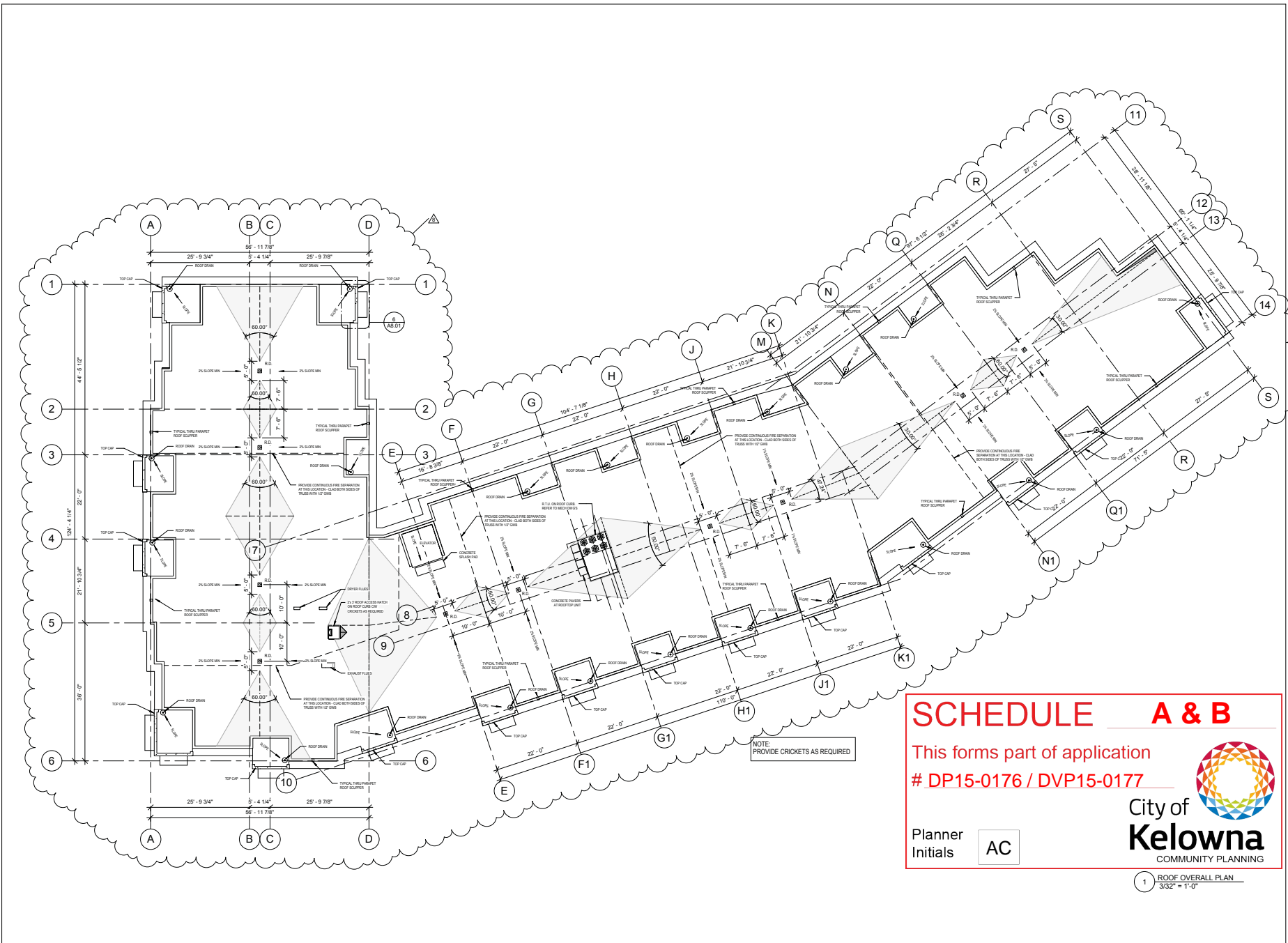
revision
1580

drawing title
LEVEL 3 OVERALL PLAN

designed R.Y. / J.F. / R.B.
drawn J.F. / R.B.
checked R.Y.
drawing no.

A3.03

printed Aug 03, 2017



SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials

AC

City of Kelowna
COMMUNITY PLANNING

1 ROOF OVERALL PLAN
3/32" = 1'-0"

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use.

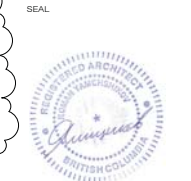
This drawing must not be scaled.

Verify all dimensions and details prior to commencement of work.

Report all errors and omissions to the Architect.

NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

TRUE NORTH



Revisions

1	17-01-06	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
1170 HIGHWAY 33, KELLOWNA, BC

project no.
 1580

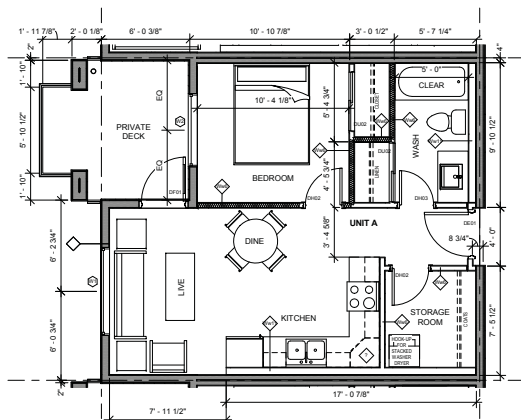
revision
 1580

drawing title
ROOF OVERALL PLAN

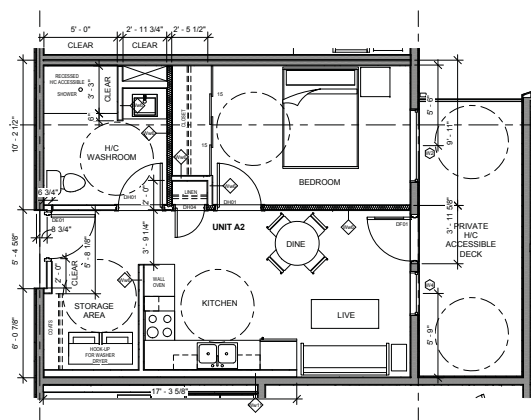
designed: R.Y.
drawn: J.F. / R.B.
checked: R.Y.
drawing no.:

A3.04

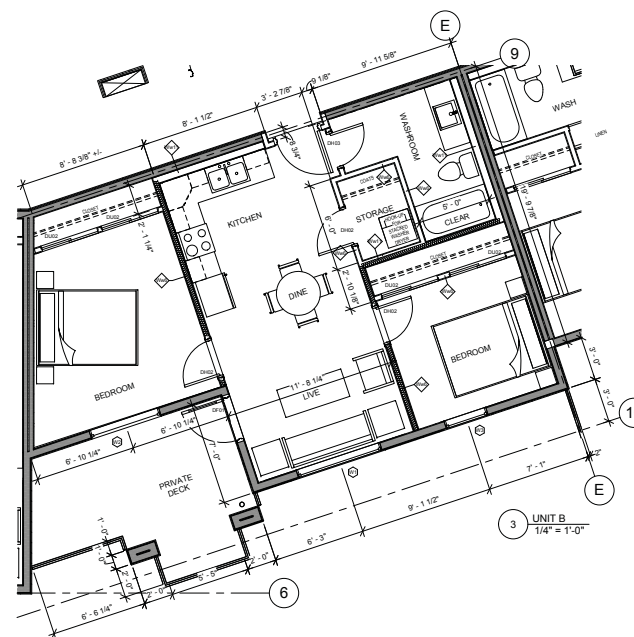
issued: Aug 03, 2017



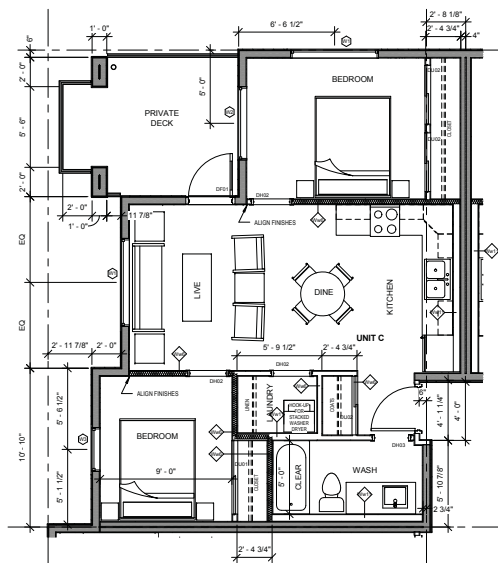
1 UNIT A
1/4" = 1'-0"



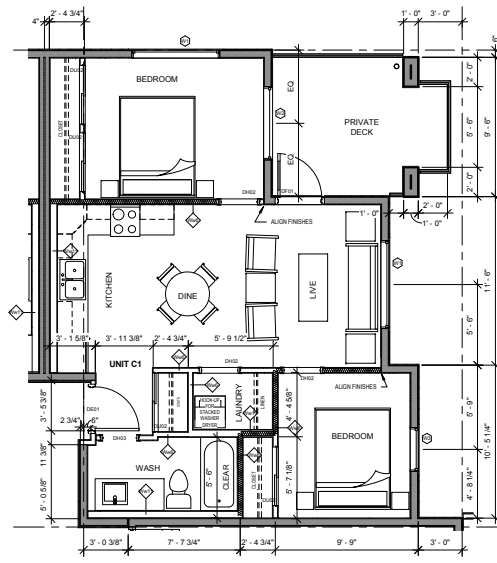
2 UNIT A2
1/4" = 1'-0"



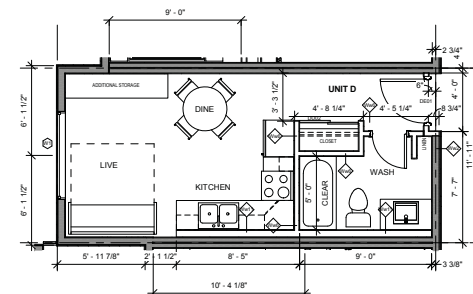
3 UNIT B
1/4" = 1'-0"



4 UNIT C
1/4" = 1'-0"



5 UNIT C1
1/4" = 1'-0"



6 UNIT D
1/4" = 1'-0"

SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials **AC**

City of **Kelowna**
COMMUNITY PLANNING

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing and for reproduction to the original site for which it was prepared and publication thereof is expressly limited to such use.

This drawing must not be scaled.
Verify all dimensions and datums prior to commencement of work.
Report all errors and omissions to the Architect.

NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

SEAL



Revisions		
1	17-01-06	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
1170 HIGHWAY 33, KELLOWNA, BC

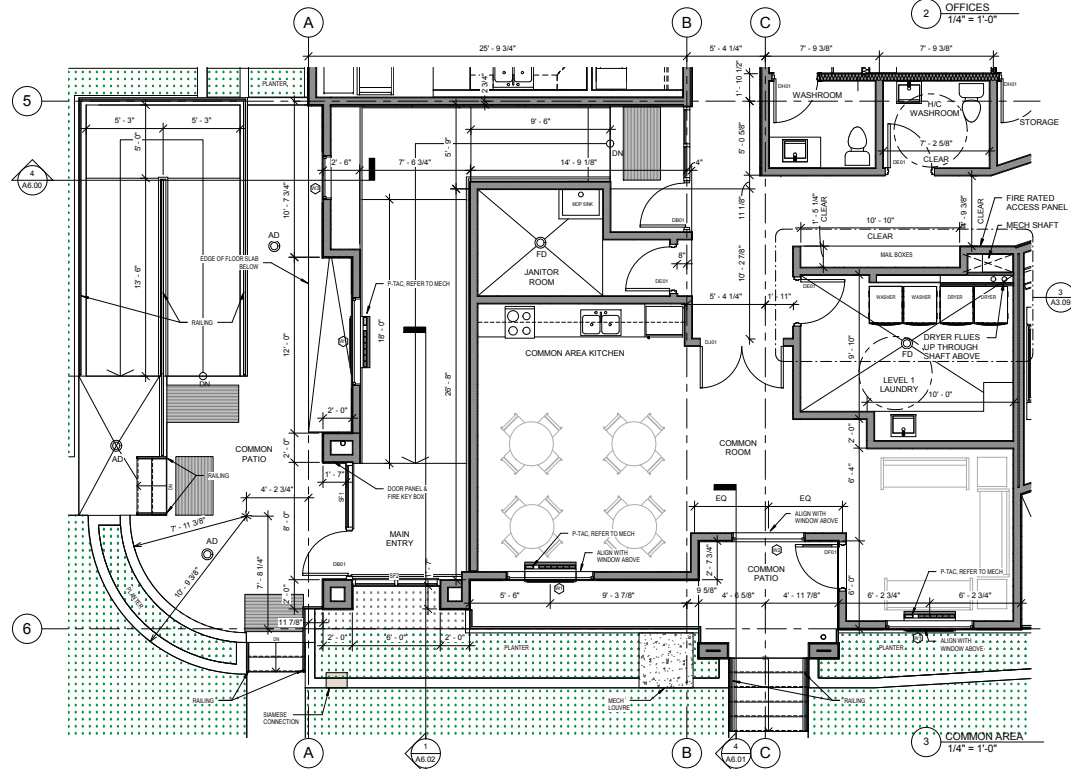
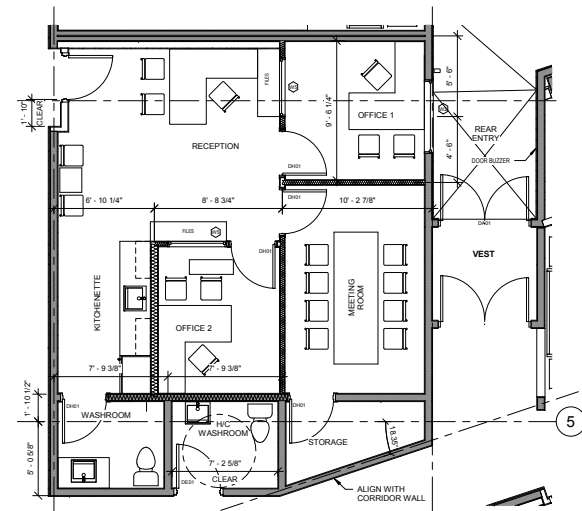
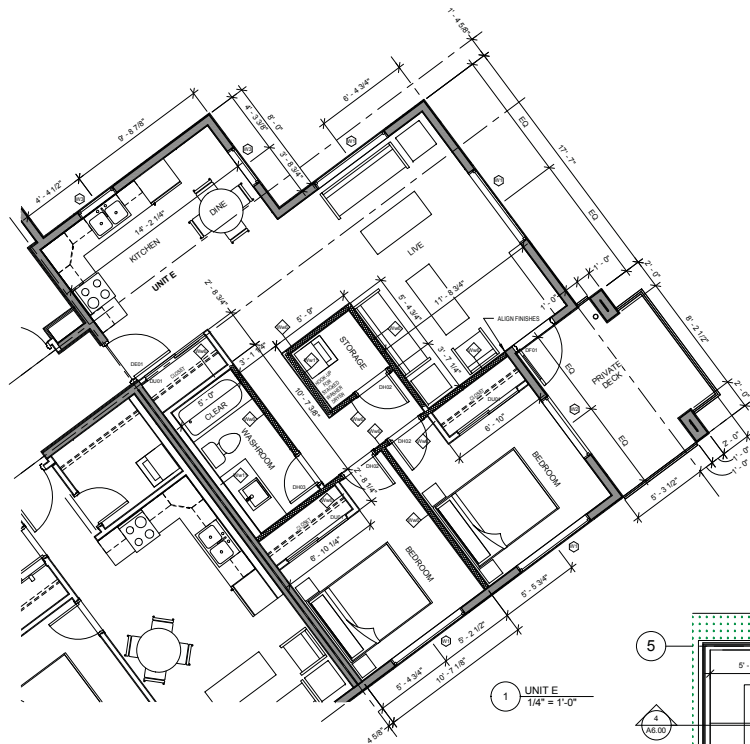
project no. 1580

date

drawing title
UNIT PLANS

designed R.Y. scale 1/4" = 1'-0"
drawn J.F./R.B.
checked R.Y.
drawing no.

A3.05
issued Aug 03, 2017



SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials **AC**



ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use.

This drawing must not be scaled. Verify all dimensions and details prior to commencement of work. Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

SEAL



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
1170 HIGHWAY 33, KELLOWNA, BC

project no. 1580

date

drawing title

UNIT AND COMMON ROOM PLANS

designed R.Y.

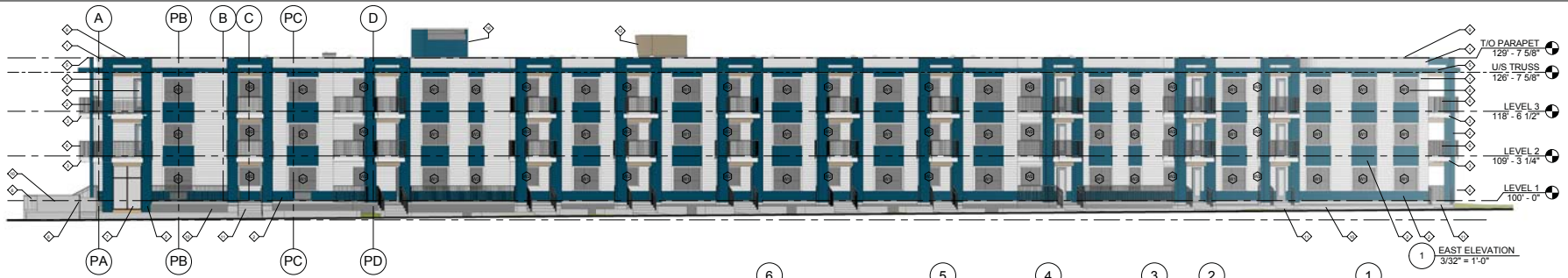
drawn J.F./R.B.

checked R.Y.

drawing no.

A3.06

issued Aug 03, 2017



EXTERIOR MATERIAL LEGEND

1. HORIZONTAL FIBER CEMENT LAPPED SIDING - OFF WHITE
2. FIBER CEMENT PANELS - BLUE / GREY
3. FIBER CEMENT PANELS - OFF WHITE
4. PREFINISHED METAL FLASHING - OFF WHITE
5. PREFINISHED METAL FLASHING - BLUE / GREY
6. PAINTED METAL GUARDRAILS - BLACK
7. ALUMINUM FRAMED WINDOW / DOORS - BLACK
8. PVC FRAMED WINDOWS / DOORS - WHITE
9. PREFINISHED METAL CAP FLASHING - OFF WHITE
10. CONCRETE PLANTERS
11. CONCRETE STAIRS
12. ROOF TOP UNIT
13. CONCRETE RAMP - TO PARKADE
14. PREFINISHED METAL O.H. DOOR - OFF WHITE
15. SIAMSE CONNECTION
16. ELEVATOR SHAFT
17. BEAM
18. RAIN GUTTER & RAIN WATER LEADER

EXTERIOR MATERIAL LEGEND

- HORIZONTAL LAPPED FIBER CEMENT SIDING
COLOUR: OFF WHITE
- FIBER CEMENT PANELS C/W CAULKED BUTT JOINTS
COLOUR: MEDIUM BLUE / GREY
- FIBER CEMENT PANELS C/W CAULKED BUTT JOINTS
COLOUR: MEDIUM BLUE / GREY
- ALUMINUM FRAMED STOREFRONT GLAZING / DOORS
COLOUR: BLACK
- VINYL FRAMED WINDOWS
COLOUR: WHITE
- PREFINISHED METAL FLASHING
COLOUR: MEDIUM BLUE / GREY
- PREFINISHED METAL FLASHING
COLOUR: OFF WHITE
- EXPOSED CONCRETE RETAINING WALLS / PATIOS
COLOUR: GREY



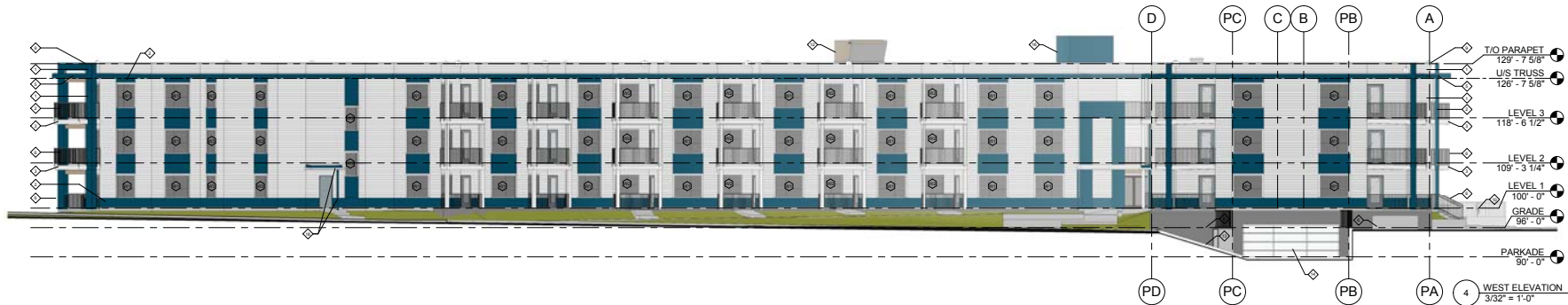
SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177



City of
Kelowna
COMMUNITY PLANNING

Planner
Initials **AC**



ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original site to which it was prepared and publication thereof is expressly limited to such use.

This drawing must not be scaled.
Verify all dimensions and datums prior to commencement of work.
Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

SEAL



Revisions

1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title

OMAH'S - NISSEN CROSSING

project address

**1170 HIGHWAY 33,
KELOWNA, BC**

project no.

1580

files

drawing title

EXTERIOR ELEVATIONS

designed

R.Y.

drawn

J.F. / R.B.

checked

R.Y.

drawing no.

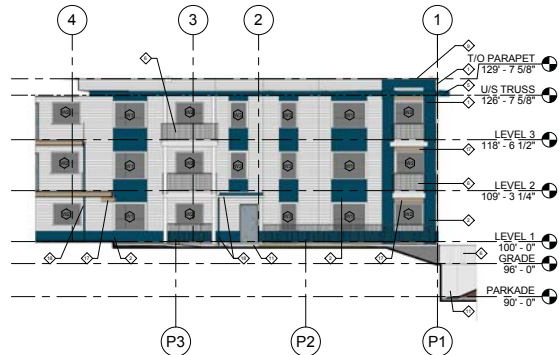
1580

A4.00

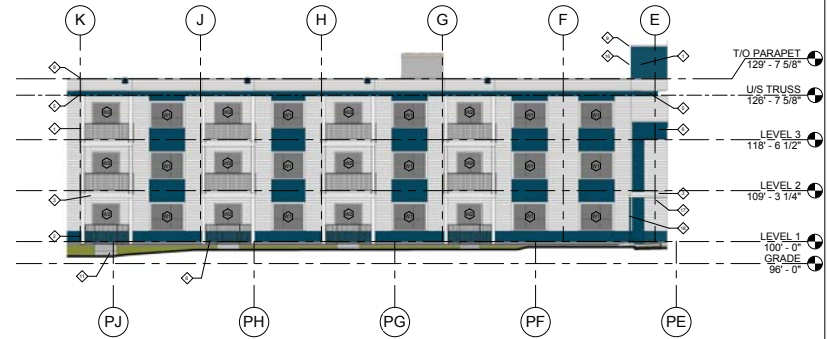
revised Aug 03, 2017

EXTERIOR MATERIAL LEGEND

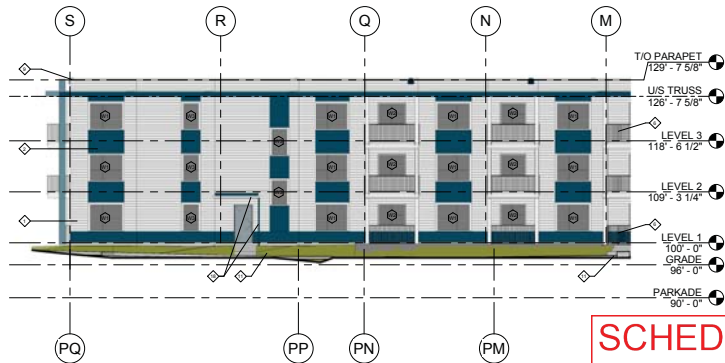
1. HORIZONTAL FIBER CEMENT LAPPED SIDING - OFF WHITE
2. FIBER CEMENT PANELS - BLUE / GREY
3. FIBER CEMENT PANELS - OFF WHITE
4. PREFINISHED METAL FLASHING - OFF WHITE
5. PREFINISHED METAL FLASHING - BLUE / GREY
6. PAINTED METAL GUARDRAILS - BLACK
7. ALUMINUM FRAMED WINDOW / DOORS - BLACK
8. PVC FRAMED WINDOWS / DOORS - WHITE
9. PREFINISHED METAL CAP FLASHING - OFF WHITE
10. CONCRETE PLANTERS
11. CONCRETE STAIRS
12. ROOF TOP UNIT
13. CONCRETE RAMP - TO PARKADE
14. PREFINISHED METAL O.H. DOOR - OFF WHITE
15. SIAMESE CONNECTION
16. ELEVATOR SHAFT
17. BEAM
18. RAIN GUTTER & RAIN WATER LEADER



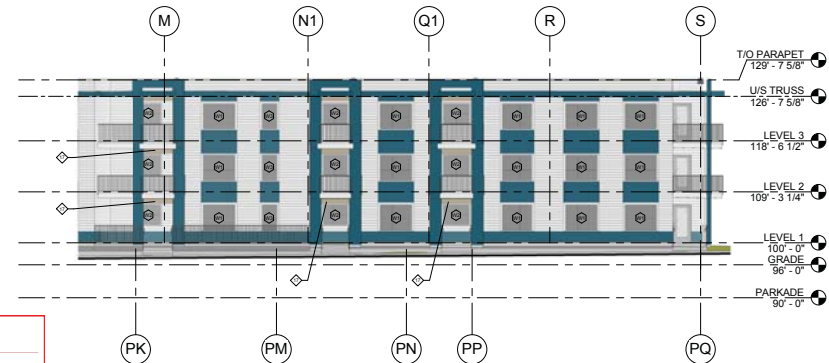
1 NORTH ELEVATION @ REAR ENTRANCE
3/32" = 1'-0"



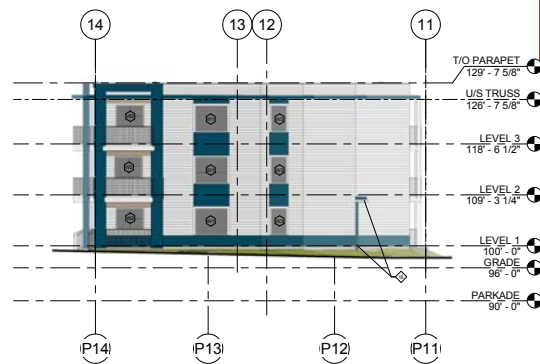
2 WEST ELEVATION @ REAR ENTRANCE
3/32" = 1'-0"



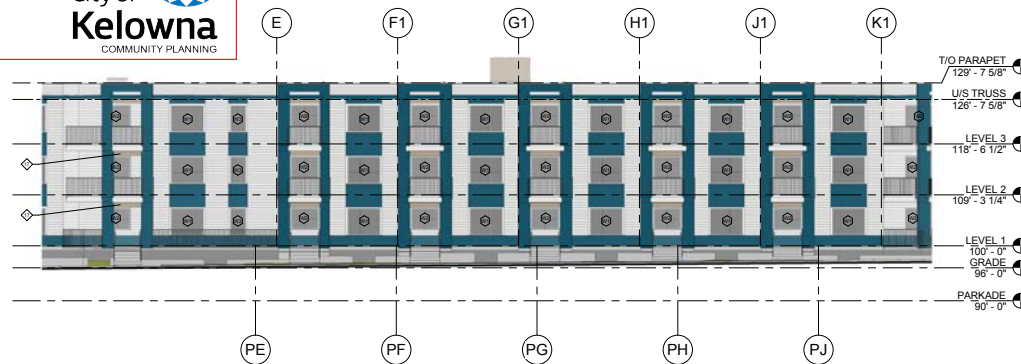
3 WEST ELEVATION @ EXIT STAIR 2
3/32" = 1'-0"



4 EAST ELEVATION @ NORTH END
3/32" = 1'-0"



5 NORTH ELEVATION @ EXIT STAIR 2
3/32" = 1'-0"



6 EAST ELEVATION @ MIDDLE SECTION
3/32" = 1'-0"

SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner
Initials AC



City of
Kelowna
COMMUNITY PLANNING

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.

This drawing is an instrument of service and the property of New Town Services. The use of this drawing and for reproduction to the original site is expressly limited to such use.

This drawing must not be scaled.
Verify all dimensions and details prior to commencement of work.
Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECTURE
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

SEAL



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
**1170 HIGHWAY 33,
KELOWNA, BC**

project no. 1580

files

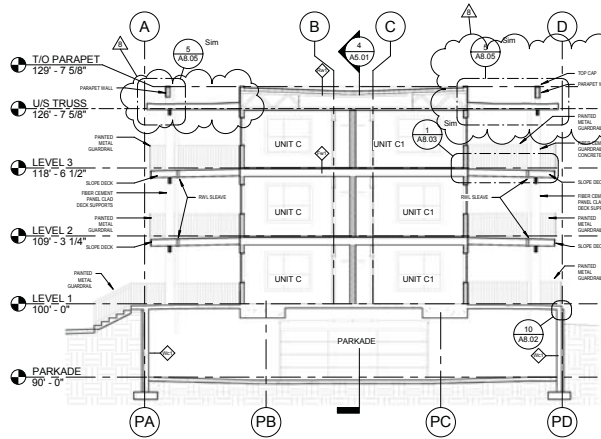
drawing title

PERPENDICULAR ELEVATIONS

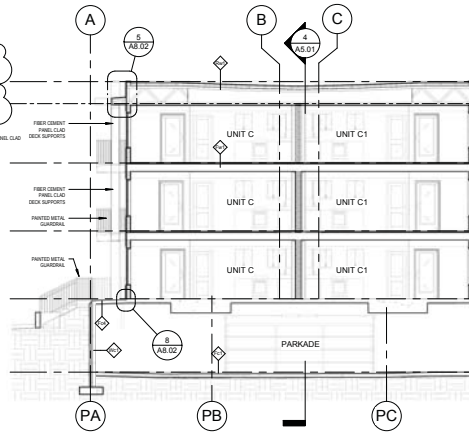
designed	R.Y.	scale	As Indicated
drawn			J.F. / R.B.
checked			R.Y.
drawing no.			

A4.01

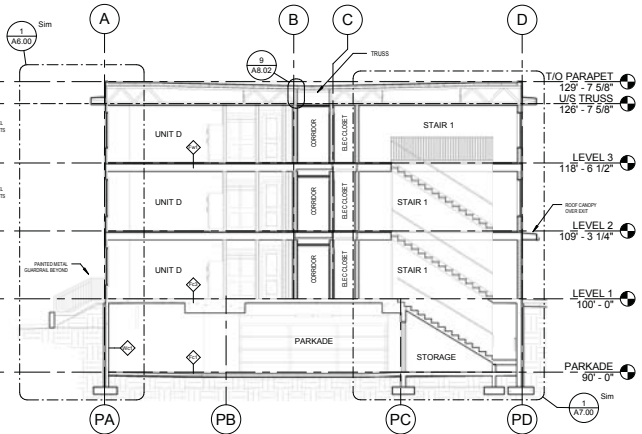
issued Aug 03, 2017



1 Section 1
1/8" = 1'-0"



2 Section 2
1/8" = 1'-0"

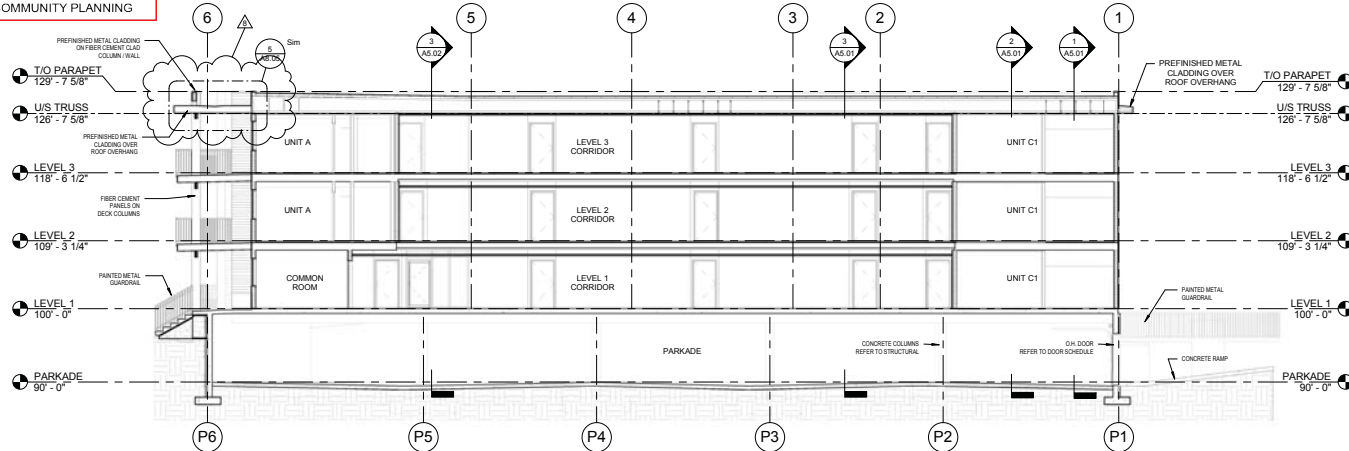


3 Section 3
1/8" = 1'-0"

SCHEDULE A & B

This forms part of application
DP15-0176 / DVP15-0177

Planner
Initials AC



4 Section 4
1/8" = 1'-0"

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA.
This drawing is an instrument of service and the property of New Town Services. The use of this drawing shall be restricted to the original use for which it was prepared and publication thereof is expressly limited to such use.
This drawing must not be scaled.
Verify all dimensions and datums prior to commencement of work.
Report all errors and omissions to the Architect.



NEW TOWN
ARCHITECT
URBAN PLANNING
CIVIL ENGINEERING
www.newtownservices.ca

SEAL



Revisions		
1	17-01-08	70% PROGRESS
2	17-01-24	ISSUED FOR BP
3	17-02-16	95% PROGRESS
4	17-02-20	RE-ISSUED FOR BP
5	17-03-27	ISSUED FOR CONSTRUCTION
6	17-05-29	RE-ISSUED FOR CONSTRUCTION
7	17-06-26	RE-ISSUED FOR DP
8	17-07-11	RE-ISSUED FOR BP

project title
OMAH'S - NISSEN CROSSING

project address
**1170 HIGHWAY 33,
KELOWNA, BC**

project no. 1560

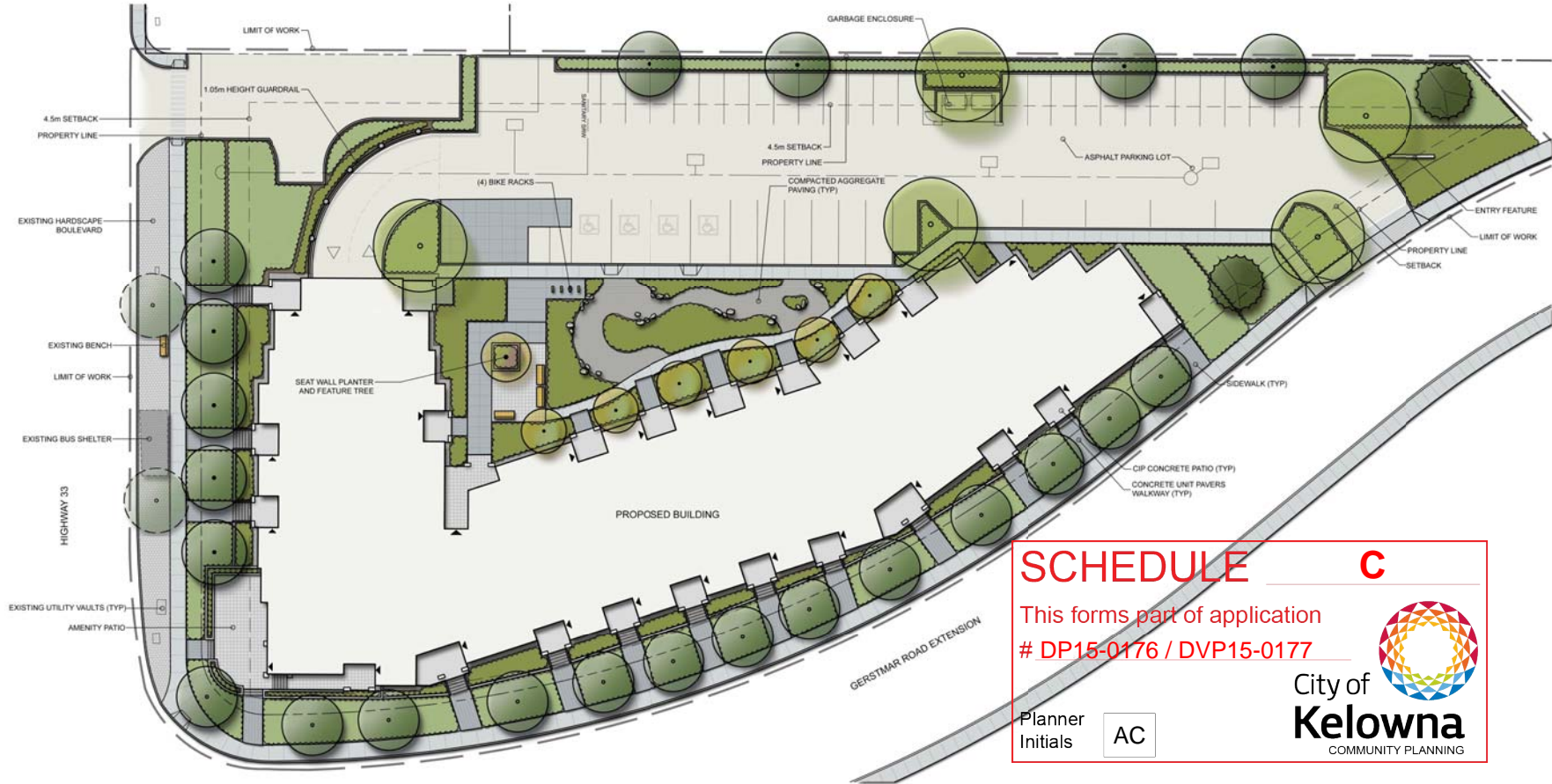
date

drawing title
BUILDING SECTIONS

designed	R.Y.	scale	1/8" = 1'-0"
drawn	J.F./R.B.		
checked	R.Y.		
drawing no.			

A5.01

printed Aug 03, 2017



REVISIONS / ISSUES		
7	MAY 2015	RE-ISSUED FOR DP APPLICATION
6	JULY 2015	RE-ISSUED FOR DP APPLICATION
5	JUNE 2015	RE-ISSUED FOR DP APPLICATION
4	APRIL 2015	RE-ISSUED FOR DP APPLICATION
3	APRIL 2015	ISSUED FOR REVIEW
2	JULY 2014	RE-ISSUED FOR DP APPLICATION
1	JULY 2014	ISSUED FOR REVIEW
NO.	DATE	DESCRIPTION



CONTRACT:
NEW TOWN SERVICES
KELOWNA, B.C.

CLIENT:
OKANAGAN METIS & ABORIGINAL HOUSING SOCIETY
KELOWNA, B.C.

PROJECT:
OMAH'S MULTI-FAMILY RESIDENTIAL BUILDING
KELOWNA, B.C.

SHEET TITLE:
CONCEPT PLAN

DESIGN BY	500
DRAWN BY	500
CHECKED BY	406
PROJECT NO.	15-022
SCALE	1:200

SHEET NO.
L-1

LEGEND:

- RESIDENCE PATIO: CAST IN PLACE CONCRETE
- DECORATIVE CONCRETE PAVING
- RESIDENCE WALKWAY: CONCRETE UNIT PAVERS
- CAST IN PLACE CONCRETE
- COMPACTED AGGREGATE PAVING
- CONCRETE STAIRS AND HANDRAIL
- COBBLE MAINTENANCE EDGE
- BENCH
- SHRUB, GRASSES AND PERENNIAL PLANTING AREA
- LANDSCAPE BOULDERS
- PROPOSED DECIDUOUS TREES
- PROPOSED CONIFEROUS TREES
- EXISTING TREE TO REMAIN
- BUILDING ENTRANCE
- PARKADE ENTRANCE

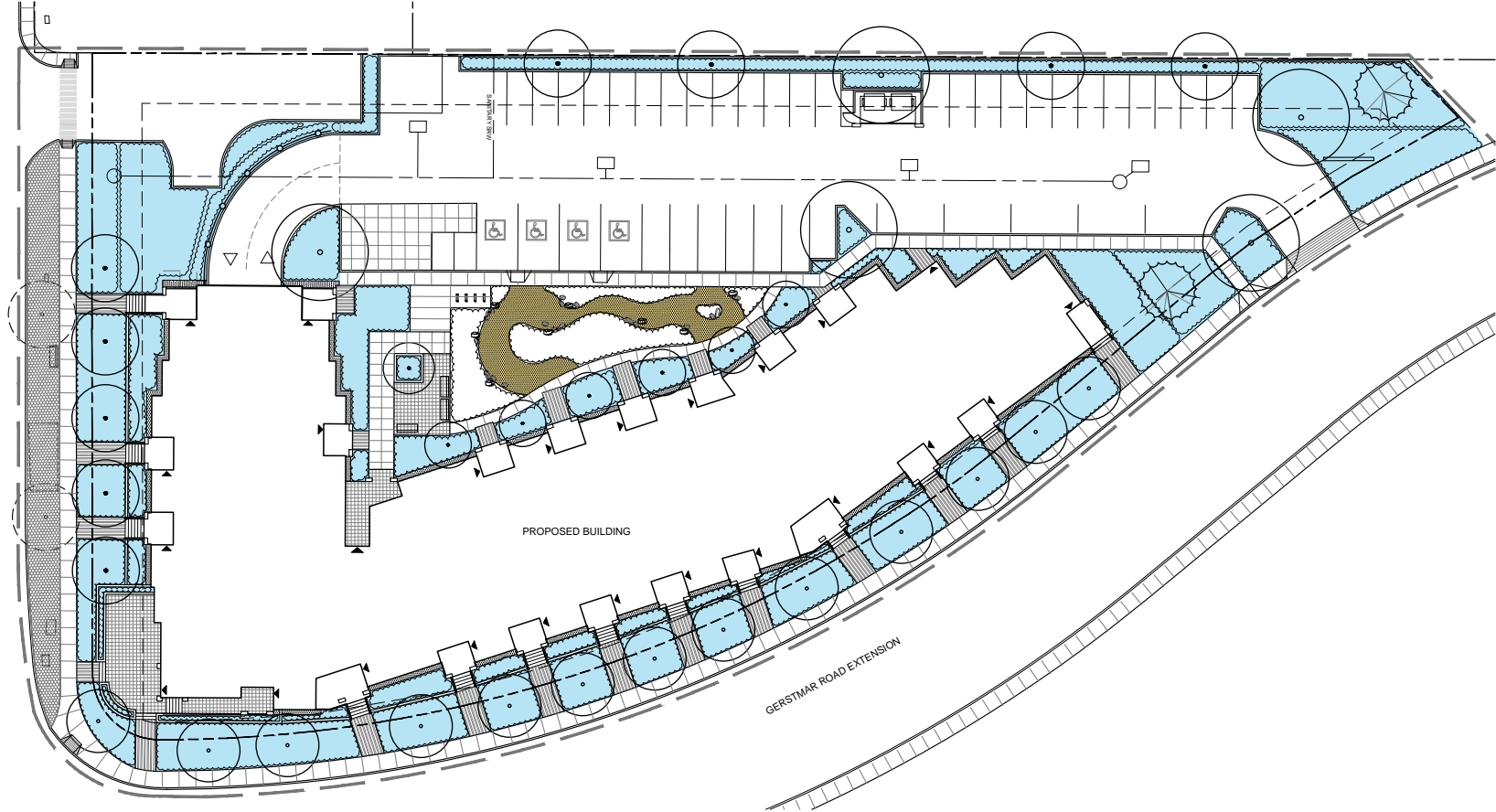
PLANT LIST:

TREES			
Botanical Name	Common Name	Root	
<i>Platanus x acerifolia</i>	London plane	BSB	Planted
<i>Koeleria paniculata</i>	Goldenrain tree	BSB	Planted
<i>Malus 'Spring Snow'</i>	Spring snow crab apple	BSB	Planted
<i>Nyssa sylvatica</i>	Tupelo	BSB	Planted
<i>Quercus alba</i>	White oak	BSB	Planted
<i>Tilia cordata</i>	Greenspire Linden	BSB	Planted
SHRUBS			
Botanical Name	Common Name	Root	
<i>Amelanchier canadensis</i>	Standing Ovation serviceberry	BSB	Planted
<i>Arctostaphylos uva-ursi</i>	Kinnikinnick	BSB	Planted
<i>Artemisia frigida</i>	Pasture sage	BSB	Planted
<i>Arctostaphylos uva-ursi</i>	Black chokeberry	BSB	Planted
<i>Berberis thunbergii</i>	Rose of Sharon	BSB	Planted
<i>Cornus rugosa</i>	Onion caryophyllus	BSB	Planted
<i>Cornus canadensis</i>	Yellow dogwood	BSB	Planted
<i>Cornus sericea</i>	Winter Beauty	BSB	Planted
<i>Erica carnea</i>	Winter heath	BSB	Planted
<i>Eurythmus alatus</i>	Burning bush	BSB	Planted
<i>Hamelis x intermedia</i>	Witch hazel	BSB	Planted
<i>Philadelphus lewisii</i>	Waterford	BSB	Planted
<i>Picea canadensis</i>	White pine	BSB	Planted
<i>Rosa rugosa</i>	Rose	BSB	Planted
<i>Salix purpurea</i>	Willow	BSB	Planted
<i>Sambucus racemosa</i>	Blue elderberry	BSB	Planted
<i>Symphoricarpos albus</i>	Shrubby	BSB	Planted
<i>Taxus x media</i>	Hick's yew	BSB	Planted
PERENNIALS			
Botanical Name	Common Name	Root	
<i>Achillea Millefolium 'Gold Plate'</i>	Gold plate yarrow	BSB	Planted
<i>Achillea millefolium 'Paprika'</i>	Lady's mantle	BSB	Planted
<i>Anemone sylvatica</i>	Wildflower	BSB	Planted
<i>Bergenia ciliolata</i>	Heartleaf bergenia	BSB	Planted
<i>Echinacea purpurea</i>	Purple coneflower	BSB	Planted
<i>Hebe x exoniensis</i>	Lenten rose	BSB	Planted
<i>Heuchera cylindrica</i>	Roundleaf alumroot	BSB	Planted
<i>Hydrangea macrophylla</i>	Hydrangea	BSB	Planted
<i>Lythrum salicaria</i>	Japanese spurge	BSB	Planted
<i>Penstemon digitalis</i>	Dwarf forget-me-not	BSB	Planted
<i>Polygonatum commutatum</i>	Solomon's seal	BSB	Planted
<i>Rudbeckia hirta</i>	Black-eyed susan	BSB	Planted
<i>Sedum spectabile</i>	Autumn Fire	BSB	Planted
GRASSES			
Botanical Name	Common Name	Root	
<i>Calamagrostis canadensis</i>	Korean feathery reed grass	BSB	Planted
<i>Deschampsia cespitosa</i>	Tufted hair grass	BSB	Planted
<i>Festuca ovina</i>	Sheep fescue	BSB	Planted
<i>Koeleria macrantha</i>	Junegrass	BSB	Planted

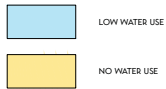
DEVELOPMENT PERMIT NOTES:

- PLANT MATERIALS AND CONSTRUCTION METHODS SHALL CONFORM TO MINIMUM STANDARDS ESTABLISHED IN THE LATEST EDITION OF THE B.C. LANDSCAPE STANDARDS, PUBLISHED BY B.C.L.N.A. AND B.C.S.L.A. AS WELL AS THE CITY OF KELOWNA LANDSCAPE STANDARDS BY BYLAW 790.
- THE LANDSCAPE DESIGN DESIGNATED HEREIN IS CONCEPTUAL BUT REFLECTS THE MINIMUM CITY OF KELOWNA FORM AND CHARACTER REQUIREMENTS.
- PLANT MATERIAL SELECTIONS ARE CONCEPTUAL ONLY. FINAL PLANTING SELECTION MAY VARY DEPENDING UPON AVAILABILITY AT THE TIME OF CONSTRUCTION.
- TREES SHALL BE INSTALLED IN DEFINED SOIL RITS OR PLANTING BED AREAS. ADEQUATE SOIL VOLUME SHALL BE PROVIDED BASED ON THE SPECIFIED TREE SPECIES AND LOCATION.
- ORNAMENTAL SHRUB, GRASS AND PERENNIAL CLUSTERS ARE TO BE PLACED WITHIN DEFINED PLANTING BEDS. ALL PLANTING BEDS SHALL HAVE A MIN. OF 450mm (18") IMPORTED GROWING MEDIUM AND 75mm (3") OF COMPOSTED MULCH OR APPROVED EQUAL.
- DECORATIVE ROCK AREAS SHALL HAVE A MIN. OF 75mm (3") OF DECORATIVE ROUND ROCK. LANDSCAPE FABRIC SHALL BE INSTALLED BELOW ALL DECORATIVE ROCK AREAS. LANDSCAPE FABRIC SHALL BE HEAVY 4540 OR APPROVED EQUAL.
- A HIGH EFFICIENCY IRRIGATION SYSTEM SHALL BE INSTALLED FOR ALL ORNAMENTAL, LANDSCAPE AREAS AND SHALL CONFORM TO THE CITY OF KELOWNA'S IRRIGATION STANDARDS BY BYLAW 7900.

HIGHWAY 33



HYDROZONE LEGEND:



SCHEDULE

C

This forms part of application
DP15-0176 / DVP15-0177

Planner Initials **AC**

City of Kelowna
COMMUNITY PLANNING



COPYRIGHT RESERVED. THIS DRAWING AND DESIGN IS AT ALL TIMES THE PROPERTY OF BENCH SITE DESIGN INC. NO PART OF THIS DRAWING OR DESIGN MAY BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN PERMISSION OF BENCH SITE DESIGN INC.

REVISIONS / ISSUES:		
7	MAY 2015	RE-ISSUED FOR DP APPLICATION
6	JULY 2015	RE-ISSUED FOR DP APPLICATION
5	JUNE 2015	RE-ISSUED FOR DP APPLICATION
4	APRIL 2015	RE-ISSUED FOR DP APPLICATION
3	JULY 2015	ISSUED FOR REVIEW
2	JULY 2015	ISSUED FOR DP APPLICATION
1	JULY 2015	ISSUED FOR REVIEW
NO.	DATE	DESCRIPTION



CONSULTANT:
NEW TOWN SERVICES
KELOWNA, B.C.

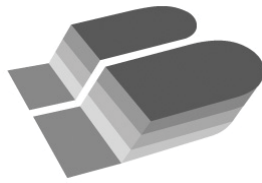
CLIENT:
OKANAGAN METIS & ABORIGINAL HOUSING SOCIETY
KELOWNA, B.C.

PROJECT:
OMAHs MULTI-FAMILY RESIDENTIAL BUILDING
KELOWNA, B.C.

SHEET TITLE:
HYDROZONE PLAN

DESIGN BY	SD
DRAWN BY	SD
CHECKED BY	JS
PROJECT NO.	15-022
SCALE	1:200

SHEET NO.:
L-2



May 26, 2017

City of Kelowna
1435 Water Street
Kelowna, B.C.
V1J 1J4

Attention: Community Planning & Real Estate Division

Project: Okanagan Metis and Aboriginal Housing Society Multi-Family Development - Development Permit Application

Please be advised that a landscape security bond of **\$116,220.10** will be required for the Okanagan Metis and Aboriginal Housing Society development project on Highway 33 in Rutland. This sum is equal to 125% of the estimated installed cost for all soft landscape. As per City of Kelowna requirements, the estimate includes trees, shrubs, grasses, perennials, topsoil, mulch, irrigation, and bicycle racks. Please see the attached Estimate of Probable Costs for Bonding for a detailed breakdown of these costs.

Should you require any additional information, please do not hesitate to contact me.

Sincerely,

Xenia Semeniuk, BCSLA, CSLA, LEED AP
Registered Landscape Architect

cc
Lisa Fraser, New Town Services

SCHEDULE	C		City of Kelowna <small>COMMUNITY PLANNING</small>	Planner Initials
				AC
This forms part of application # DP15-0176 / DVP15-0177				



OMAHS Multi Family Development

Estimate of Probable Costs for Bonding

Prepared on: June 7, 2016

ON SITE

Items Description	Units	Qty.	Price	Item Total
1.0 Plant Material				
1.1 Trees				
1.1.1 6cm Cal.: Deciduous Feature Tree	ea.	5	\$550.00	\$2,750.00
1.1.2 6cm Cal.: Deciduous Street Tree	ea.	18	\$550.00	\$9,900.00
1.1.3 4cm Cal.: Deciduous Columnar Tree	ea.	4	\$400.00	\$1,600.00
1.1.4 4cm Cal.: Deciduous Ornamental Tree	ea.	7	\$400.00	\$2,800.00
1.1.5 1.5m Ht: Coniferous Trees	ea.	2	\$500.00	\$1,000.00
			<i>Sub-Total</i>	<i>\$18,050.00</i>
1.2 Shrubs, Perennials, Ground Covers				
1.2.1 #5 Pot: Shrubs (1.8m O.C.)	ea.	234	\$45.00	\$10,543.63
1.2.2 #2 Pot: Grasses (1.2m O.C.)	ea.	264	\$20.00	\$5,271.35
1.2.3 #1 Pot: Perennials (0.9m O.C.)	ea.	468	\$15.00	\$7,026.60
			<i>Sub-Total</i>	<i>\$22,841.58</i>
			1.0 Total	\$40,891.58
2.0 Topsoil & Mulch				
2.1 Topsoil				
2.1.1 Shrub Bed Topsoil (300mm Depth)	m3	396	\$40.00	\$15,852.00
2.1.2 Tree Pit Topsoil (1000mm Depth)	m3	36	\$40.00	\$1,440.00
			<i>Sub-Total</i>	<i>\$17,292.00</i>
2.2 Mulch				
2.2.1 Shrub Bed Mulch (50mm Depth)	m3	66	\$50.00	\$3,302.50
2.2.2 Cobble Mulch (100mm Depth)	m3	15	\$65.00	\$975.00
			<i>Sub-Total</i>	<i>\$4,277.50</i>
			2.0 Total	\$21,569.50
3.0 Servicing				
3.1 Irrigation				
3.1.1 Sleeving	l.s.	1	\$2,500.00	\$2,500.00
3.1.2 Point of Connection - Electrical & Mechanical	l.s.	1	\$2,000.00	\$2,000.00
3.1.3 Control System	l.s.	1	\$3,000.00	\$3,000.00
3.1.4 Irrigation system (heads, pipes, valves)	m2	1,321	\$15.00	\$19,815.00
			<i>Sub-Total</i>	<i>\$27,315.00</i>
			3.0 Total	\$27,315.00
4.0 Site Construction				
4.1 Furnishings				
4.1.1 Bike Racks	ea.	4	\$800.00	\$3,200.00
			<i>Sub-Total</i>	<i>\$3,200.00</i>
			4.0 Total	\$3,200.00

Total	\$92,976.08
Security Total (125%)	\$116,220.10



City of Kelowna
COMMUNITY PLANNING

AC

Planner Initials

SCHEDULE C

This forms part of application
DP15-0176 / DVP15-0177