

# City of Kelowna

## Regular Council Meeting

### AGENDA



Monday, December 9, 2019  
1:30 pm  
Council Chamber  
City Hall, 1435 Water Street

#### Pages

#### 1. Call to Order

I would like to acknowledge that we are gathered today on the traditional, ancestral, unceded territory of the syilx/Okanagan people.

This meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

#### 2. Confirmation of Minutes

4 - 8

Regular PM Meeting - December 2, 2019

#### 3. Committee Reports

##### 3.1 Civic Awards Nomination Period

9 - 20

To announce the opening of the nomination period for the 45th Annual Civic & Community Awards.

#### 4. Development Application Reports & Related Bylaws

##### 4.1 Hwy 33 E 3215, A19-0010 - William Winter

21 - 25

To consider an application to the Agricultural Land Commission for a Subdivision for the purpose of a Homesite Severance.

#### 5. Bylaws for Adoption (Development Related)

##### 5.1 Rutland Ct 2155-2165, BL11950 (Z19-0106) - CGSB Automotive Group LTD., Inc. No. BC0731187

26 - 26

To adopt Bylaw No. 11950 in order to rezone the subject property from the C2 - Neighbourhood Commercial zone to the C10 - Service Commercial zone.

<b>5.2</b>	<b>Hollywood Rd 150, BL11953 (Z19-0109) - Lexington Enterprises Ltd.</b>	<b>27 - 27</b>
	To adopt Bylaw No. 11953 in order to rezone the subject property from the C4 - Urban Centre Commercial zone to the C4rcs- Urban Centre Commercial (retail cannabis sales) zone.	
<b>5.3</b>	<b>Pandosy St 1636-1652, BL11959 (TA19-0013) - ALM888 Ventures Ltd, Inc. No. BC1089095</b>	<b>28 - 28</b>
	To adopt Bylaw No. 11959 in order to amend the City of Kelowna Zoning Bylaw No. 8000 as outlined in Schedule 'A'.	
<b>5.4</b>	<b>Pandosy St 1636-1652, BL11960 (Z19-0100) - ALM888 Ventures Ltd, Inc. No. BC1089095</b>	<b>29 - 29</b>
	To adopt Bylaw No. 11960 in order to rezone the subject property from the C7- Central Business Commercial zone to the C7rcs- Central Business Commercial (Retail Cannabis Sales) zone.	
<b>6.</b>	<b>Non-Development Reports &amp; Related Bylaws</b>	
<b>6.1</b>	<b>Provisional 2020 Financial Plan</b>	<b>30 - 63</b>
	To provide an overview of the Provisional 2020 Financial Plan.	
<b>6.2</b>	<b>Amendments to Financial Plan Transfer Policy No. 261 and Financial Plan Amendment Policy No. 262</b>	<b>64 - 78</b>
	To amend the Financial Plan Transfer and Financial Plan Amendment Policies with respect to the parameters when Council pre-approval is required during the year and for administrative revisions.	
<b>6.3</b>	<b>OCP 2040 Phase 3 Engagement Results</b>	<b>79 - 124</b>
	To provide Council with a summary of the feedback received as part of the public engagement for Phase 3 of 2040 Official Community Plan Update process and to obtain Council's direction to commence preliminary infrastructure impact analysis of the draft future land use map.	
<b>6.4</b>	<b>Lake Avenue Dog Beach Trial</b>	<b>125 - 141</b>
	To provide Council an opportunity to review the results of the two-year Dog Beach Trial at Lake Avenue and to make a determination about the future of the dog beach in this location.	

<b>6.5</b>	<b>Subdivision, Development and Servicing Bylaw 7900 - Schedule 4 and 5 Update - Stormwater</b>	<b>142 - 170</b>
	To amend the Subdivision, Development and Servicing Bylaw 7900 Design and Construction Standards so it aligns with industry best practice and ensure construction of high quality and long-lasting infrastructure. To update the Engineering Drawing Submission requirements.	
<b>6.6</b>	<b>BL11913 - Amendment No. 20 to Subdivision Development and Servicing Bylaw No. 7900</b>	<b>171 - 201</b>
	To give Bylaw No. 11913 first, second and third reading to amend the Subdivision, Development and Servicing Bylaw No. 7900.	
<b>7.</b>	<b>Bylaws for Adoption (Non-Development Related)</b>	
<b>7.1</b>	<b>West Ave 454-464, BL11955 - Housing Agreement Authorization Bylaw - West Avenue - Mission Group Rentals Ltd., Inc No. BC1151526</b>	<b>202 - 209</b>
	To adopt Bylaw No. 11955 in order to authorize the City of Kelowna to enter into a Housing Agreement with West Avenue - Mission Group Rentals Ltd., Inc No. BC1151526.	
<b>7.2</b>	<b>Lakeshore Rd 4119, BL11956 - Housing Agreement Authorization Bylaw - Whitworth Holdings Ltd., Inc No. BC1059455</b>	<b>210 - 217</b>
	To adopt Bylaw No. 11956 in order to authorize the City of Kelowna to enter into a Housing Agreement with Whitworth Holdings Ltd., Inc No. BC1059455.	
<b>7.3</b>	<b>Sutherland Ave 1165, BL11958 - Housing Agreement Authorization Bylaw - Culos Development (1996) Inc., Inc. No. BC1099204</b>	<b>218 - 225</b>
	To adopt Bylaw No. 11958 in order to authorize the City of Kelowna to enter into a Housing Agreement with Culos Development (1996) Inc., Inc. No. BC1099204.	
<b>8.</b>	<b>Mayor and Councillor Items</b>	
<b>9.</b>	<b>Termination</b>	



## City of Kelowna Regular Council Meeting Minutes

Date: Monday, December 2, 2019  
 Location: Council Chamber  
 City Hall, 1435 Water Street

Members Present Mayor Colin Basran, Councillors, Maxine DeHart, Gail Given, Charlie Hodge, Brad Sieben, Mohini Singh and Luke Stack\*

Members Absent Councillors Ryan Donn and Loyal Wooldridge

Staff Present City Manager, Doug Gilchrist; City Clerk, Stephen Fleming, Divisional Director, Planning & Development Services, Ryan Smith\*; Development Planning Department Manager, Terry Baron\*; Planner, Barbara Crawford\*; Policy & Planning Department Manager, Danielle Noble-Brandt\*; Community Energy Specialist, Chris Ray\*; Long Range Policy Planning Manager, James Moore\*; Planner Specialist, Ross Soward\*; Legislative Coordinator (Confidential), Arlene McClelland

(\* Denotes partial attendance)

### 1. Call to Order

Mayor Basran called the meeting to order at 1:35 p.m.

I would like to acknowledge that we are gathered today on the traditional, ancestral, unceded territory of the syilx/Okanagan people.

Mayor Basran advised that the meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

Councillor Sieben joined the meeting at 1:36 p.m.

### 2. Confirmation of Minutes

Moved By Councillor Hodge/Seconded By Councillor Stack

**R1074/19/12/02** THAT the Minutes of the Regular Meetings of November 25, 2019 be confirmed as circulated.

**Carried**



### 3. Development Application Reports & Related Bylaws

#### 3.1 Hunter Road 2280-2290 - LUC19-0003 - Z19-0111 - Hillahan Holdings Inc. & Go West Investments Inc.

Staff:

- Displayed a PowerPoint Presentation summarizing the application.

Moved By Councillor Given/Seconded By Councillor Stack

**R1075/19/12/02** THAT Application No. LUC19-0003 to discharge LUC77-1085 from Parcel A (KD82312) District Lot 125 Osoyoos Division Yale District, Plan 31272 located at 2280-2290 Hunter Road, Kelowna, BC, be considered by Council;

AND THAT Rezoning Application No. Z19-0111 to amend City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification Parcel A (KD82312) District Lot 125 Osoyoos Division Yale District, Plan 31272 located at 2280-2290 Hunter Road, Kelowna, BC from the A1 – Agriculture 1 zone to the C3 – Community Commercial zone be considered by Council;

AND THAT the Land Use Contract Discharge and Zone Amending Bylaws be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Attachment “A” attached to the Report from the Development Planning Department dated December 2<sup>nd</sup>, 2019;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered subsequent to the approval of the Ministry of Transportation and Infrastructure.

Carried

#### 3.2 Hunter Rd 2280-2290, BL11964 (LUC19-0003) - Hillahan Holdings Inc. & Go West Investments Inc.

Moved By Councillor Singh/Seconded By Councillor Hodge

**R1076/19/12/02** THAT Bylaw No. 11964 be read a first time.

Carried

#### 3.3 Hunter Rd 2280-2290, BL11965 (Z19-0111) - Hillahan Holdings Inc. & Go West Investments Inc.

Moved By Councillor Singh/Seconded By Councillor Hodge

**R1077/19/12/02** THAT Bylaw No. 11965 be read a first time.

Carried

### 4. Non-Development Reports & Related Bylaws

#### 4.1 Community Energy Retrofit Strategy - Options Analysis

Staff:

- Displayed a PowerPoint Presentation outlining the options for an Energy Retrofit Strategy and responded to questions from Council.

Moved By Councillor Hodge/Seconded By Councillor Singh

**R1078/19/12/02** THAT Council receives, for information, the Report from the Community Energy Specialist dated November 15, 2019 with respect to options for a Community Energy Retrofit Strategy;

AND THAT Council directs staff to pursue the recommended approach(es) for the Community Energy Retrofit Strategy outlined in the report.

**Carried**

#### **4.2 Fall 2019 Rental Housing Agreement Bylaws**

Councillor Stack declared a perceived conflict of interest on items 4.2 to 4.6 as his employer applies for rental housing agreements and tax exemptions from time to time and departed the meeting at 2:18 p.m.

Staff:

- Provided an overview of the rental housing agreements and proposed projects.

Moved By Councillor Given/Seconded By Councillor Singh

**R1079/19/12/02** THAT Council, receives, for information, the Report from the Planner Specialist dated December 2, 2019 recommending that the City of Kelowna enter into three (3) Housing Agreements to secure 100 purpose-built rental housing units;

AND THAT Bylaw No. 11955 authorizing a housing agreement between the City of Kelowna and Mission Group Homes Ltd. Which requires the owners to designate 48 dwelling units in a purpose-built rental housing for Lot A, District Lot 14, ODYD, Plan EPP92146, at 454-464 West Avenue, Kelowna, BC.

AND THAT Bylaw No. 11956 authorizing a housing agreement between the City of Kelowna and Whitworth Holdings Ltd. Which requires the owners to designate 16 dwelling units in a purpose-built rental housing for Lot 3, Section 6, Township 26 ODYD, Plan 4912, at 4119 Lakeshore Road, Kelowna, BC.

AND THAT Bylaw No. 11958 authorizing a housing agreement between the City of Kelowna and Culos Development Inc. Which requires the owners to designate 36 dwelling units in a purpose-built rental housing for Lot 2, S19, TWP 26, ODYD, Plan 17148 and Lot "A", D.L. 137, ODYD, Plan 28311, and Lot 7 & Lot 8, D.L. 137, ODYD, Plan 2498, at 1165 Sutherland Avenue, Kelowna, BC.

**Carried**

#### **4.3 West Ave 454-464, BL11955 - Housing Agreement Authorization Bylaw - West Avenue - Mission Group Rentals Ltd., Inc No. BC1151526**

Moved By Councillor Singh/Seconded By Councillor Given

**R1080/19/12/02** THAT Bylaw No. 11955 be read a first, second and third time.

**Carried**

#### **4.4 Lakeshore Rd 4119, BL11956 - Housing Agreement Authorization Bylaw - Whitworth Holdings Ltd., Inc No. BC1059455**

Moved By Councillor DeHart/Seconded By Councillor Sieben

**R1081/19/12/02** THAT Bylaw No. 11956 be read a first, second and third time.



Carried

**4.5 Sutherland Ave 1165, BL11958 - Housing Agreement Authorization Bylaw - Culos Development (1996) Inc., Inc. No. BC1099204**

Moved By Councillor Sieben/Seconded By Councillor DeHart

R1082/19/12/02 THAT Bylaw No. 11958 be read a first, second and third time.

Carried

**4.6 Revitalization Tax Exemption Agreements – Fall 2019**

Staff:

- Provided an overview of the 10 year Revitalization Tax Exemption Agreements for multi-family housing projects.

Moved By Councillor Sieben/Seconded By Councillor DeHart

R1083/19/12/02 THAT Council, receives, for information, the Report from the Planner Specialist dated December 2, 2019 recommending that Council adopt the six Revitalization Tax Exemption Agreements;

AND THAT Council approves the City of Kelowna entering into a Revitalization Tax Exemption Agreement with West Avenue - Mission Group Homes Ltd, for Lot A, District Lot 14, ODYD, Plan EPP92146, at 454-464 West Avenue, Kelowna, BC;

AND THAT Council approves the City of Kelowna entering into a Revitalization Tax Exemption Agreement with Simple Pursuits Ltd., for Lot B, District Lot 128, ODYD Plan KAP83889, at 2080 Benvoulin Court, Kelowna, BC;

AND THAT Council approves the City of Kelowna entering into a Revitalization Tax Exemption Agreement with Whitworth Holdings Ltd., for Lot 3 Section 6 Township 26 ODYD, Plan 4912, at 4119 Lakeshore Road, Kelowna, BC;

AND THAT Council approves the City of Kelowna entering into a Revitalization Tax Exemption Agreement with Drysdale Blvd Kelowna Apartments 2019 LTD, for Lot 2, Section 33 Township 26 ODYD, Plan EPP48909 at 333 Drysdale Boulevard, Kelowna, BC;

AND THAT Council approves the City of Kelowna entering into a Revitalization Tax Exemption Agreement with Ella-Mission Group Homes Ltd, for Lot 1, PlanEPP78300 District Lot 139, ODYD, Plan EPP82631, Volumetric SRW; PID 030-380-499 at 1588 Ellis Street, Kelowna, BC;

AND FURTHER THAT Council approves the City of Kelowna entering into a Revitalization Tax Exemption Agreement with Mission Group Holdings Ltd, for Lot A, District Lot 139, ODYD, Plan EPP81417 at 1471 St. Paul Street, Kelowna, BC.

Carried

Councillor Stack rejoined the meeting at 2:25 p.m.

**5. Bylaws for Adoption (Non-Development Related)**

**5.1 BL11961 - Delegation of Authority to Enter into Agreements Bylaw No. 11961**

Moved By Councillor Stack/Seconded By Councillor DeHart

R1084/19/12/02 THAT Bylaw No. 11961 be adopted.

Carried**6. Mayor and Councillor Items**

Mayor Basran:

- Made comments on the emergency winter shelter initiatives undertaken by the City and stated that a major announcement will be made tomorrow.

Councillor Stack

- Passed on his appreciation for the efforts of Bylaw Enforcement, RCMP and City staff with respect to the winter shelter initiatives.

Councillor Hodge:

- Attended, along with other Council Colleagues, the Grand Opening of JoeAnna's House.

Councillor Given:

- Reminder of the Tiny Tim Breakfast that supports the Salvation Army's Christmas campaign; December 5<sup>th</sup> between 6:30 a.m. and 9:00 a.m. at the Coast Capri Hotel.

**7. Termination**

This meeting was declared terminated at 2:33 p.m.

\_\_\_\_\_  
Mayor Basran

/acm

\_\_\_\_\_  


City Clerk

# Report to Council



**Date:** December 9, 2019  
**To:** Council  
**From:** City Manager  
**Subject:** Civic & Community Awards Nomination Period  
**Department:** Active Living & Culture

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## **Recommendation:**

THAT Council receives, for information, the report from Active Living & Culture, dated December 9<sup>th</sup>, 2019, that announces the opening of the nomination period for the 45<sup>th</sup> Annual Civic & Community Awards, including an outline of award categories for the program.

## **Purpose:**

To announce the opening of the nomination period for the 45<sup>th</sup> Annual Civic & Community Awards.

## **Background:**

The City of Kelowna's Annual Civic & Community Awards recognize the outstanding achievements and contributions made in the city of Kelowna each year. The program includes 13 awards that honour volunteers, artists, athletes, environmentalists and businesses. Up to three finalists are selected in each category, with one recipient being announced during the awards ceremony.

The awards are overseen by a Steering Committee made up of members of the community and a representative from City Council. The Steering Committee provides direction to four sub-committees and two supporting organizations which assist in the operations of the awards program.

A call for steering committee applications closed on October 20<sup>th</sup>, 2019 and the new steering committee was approved by Council for a four-year term on November 4<sup>th</sup>, 2019. Membership of the Steering Committee includes: Adam Schubel, Bob Burge, Dan Rogers, Ellen Boelcke, Karen Graham, Lorraine Ewonus-Ellert, Wayne Moore and Councillor Ryan Donn. The elected Chair of the Steering Committee is Ellen Boelcke and Karen Graham is the Nominating Committee Chair.

## **Discussion:**

The nomination period for the 45<sup>th</sup> Annual Civic & Community Awards commences on Monday December 9<sup>th</sup>, 2019 and will remain open until Friday, February 14<sup>th</sup>, 2020. Criteria for all categories and nomination forms are available on the city website at [kelowna.ca/civicawards](http://kelowna.ca/civicawards).

The nomination forms may be submitted via email or printed and delivered in person to either Parkinson Recreation Centre or City Hall.

Other award categories that are part of the annual event but are not part of this nomination call include the Anita Tozer Memorial Award which is selected by City Council and the Augie Ciancone Memorial Award which recognizes the top male and female high school athletes as selected through the Okanagan Central Schools Athletic Association.

As endorsed by Council on November 4<sup>th</sup>, 2019, the Civic & Community Awards have become more inclusive and streamlined as a result of combining some of the existing categories. The revised award categories include establishing gender neutral awards for the citizen volunteer categories and simplifying categories where confusion surrounded the nomination process. The updated categories, criteria and recipients from the 44<sup>th</sup> annual Civic & Community Awards (latest award winners) are included below:

Category	Criteria	2018 Recipient
<b>Teen Honour in the Arts and Honour in the Arts</b>	Awarded to an adult and youth who have made outstanding contributions to Kelowna through cultural and/or artistic efforts.	<b>Teen Honour: Annette Bakala</b>  <b>Honour: Ryan Grenier</b>
<b>Young Citizen of the Year *</b>	Awarded to a young male or female in recognition of their overall outstanding voluntary contributions to Kelowna.	<b>Young Male Volunteer: Matthew Richardson</b>  <b>Young Female Volunteer: Keneisha Charles</b>
<b>Citizen of the Year *</b>	Awarded to a man or woman in recognition of their overall outstanding voluntary contributions to the city of Kelowna.	<b>Fred Macklin Memorial Man of the Year: Giuseppe (Joe) Iafrancesco (1947-2018)</b>  <b>Sarah Donalda Treadgold Memorial Woman of the Year: Angie Lohr</b>
<b>Bob Giordano Memorial Award</b>  <b>Coach or Sport Administrator of the Year</b>	Awarded to an individual who has contributed significantly to Kelowna through voluntary service to amateur sport, such as coaching or administrative support.	<b>Devin Rubadeau</b>

<b>Bryan Couling Memorial Athletic Team of the Year</b>	Awarded to the Kelowna based team (amateur or professional) who brought the greatest amount of recognition to Kelowna.	<b>Kelowna Ringette U16A Elite</b>
<b>Male and Female Athlete of the Year</b>	Awarded to the athlete (amateur or professional) who brought the greatest amount of recognition to Kelowna.	<b>Female Athlete: Kelsey Serwa</b>  <b>Male Athlete: Fynnian McCarthy</b>
<b>Champion for the Environment *</b>	Awarded to an individual or business whose actions and achievements have shown outstanding environmental leadership or innovative contributions having a direct benefit on the city of Kelowna.	<b>Gwen Steele</b>
<b>Corporate Community of the Year *</b>	Awarded to the Kelowna businesses that has provided outstanding support for employee volunteerism in addition to financial contributions and initiatives having a direct benefit on the city of Kelowna.	<b>Small Business: Secure-Rite Mobile Storage Inc.</b>  <b>Medium/Large Business: Raymond James - Corporate Branch</b>
<b>The Central Okanagan Foundation Volunteer Organization of the Year</b>	Awarded to a Kelowna volunteer organization that has provided outstanding community services with direct benefits to the city of Kelowna.	<b>Helping Out People Exploited (HOPE) Outreach</b>
<b>Augie Ciancone Memorial Award</b>  (not part of this call for nominations)	Most outstanding male and female high school athlete of the year, in the area of the Central Okanagan.	<b>Female Athlete: Lonica McKinney</b>  <b>Male Athlete: Brandon Frechette</b>
<b>Anita Tozer Memorial</b>  (not part of this call for nominations)	Awarded by Council to an individual or group in recognition of an extraordinary and positive contribution to the quality of life in Kelowna.	<b>The Journey Home Taskforce</b>
<b>* Denotes newly revised award category</b>		

#### **Conclusion:**

The 45<sup>th</sup> Annual Awards night recognizing individuals and businesses who made extraordinary contributions in 2019 will be held on Wednesday April 29<sup>th</sup>, 2020, at the Rotary Centre for the Arts. The

private Mayor's Reception event will take place prior to the awards night and provides an opportunity for Mayor and Council to honour each finalist with a special plaque from the city.

Each year the award recipients are further recognized at Jim Stuart Park, with their names on an individual name plate.

**Internal Circulation:**

J. Gabriel, Divisional Director, Active Living & Culture  
M. Siggers, Community & Neighbourhood Services Manager  
K. O'Rourke, Community Communications Manager  
L. Ruether, Communications Advisor

**Considerations applicable to this report:**

Existing Policy: Council Policy 382

**Considerations not applicable to this report:**

*Legal/Statutory Authority:*

*Legal/Statutory Procedural Requirements:*

*Financial/Budgetary Considerations:*

*External Agency/Public Comments:*

*Communications Comments:*

Submitted by:

M. Moran, Recreation Technician

**Approved for inclusion:**

JG

cc:

*J. Gabriel, Divisional Director, Active Living & Culture*

*C. Weaden, Divisional Director, Corporate Strategic Services*





# The City of Kelowna's Civic & Community Awards

- ▶ Recognize the outstanding achievements and contributions made in our community each year
- ▶ Program includes 13 awards that honour volunteers, artists, athletes, environmentalists, businesses and organizations



# Civic & Community Awards

- ▶ Civic & Community Awards program is overseen by a Steering Committee
- ▶ New committee membership includes:
  - ▶ Adam Schubel
  - ▶ Bob Burge
  - ▶ Dan Rogers
  - ▶ Ellen Boelcke
  - ▶ Karen Graham
  - ▶ Lorraine Ewonus-Ellert
  - ▶ Wayne Moore
  - ▶ Councillor Ryan Donn



# Nomination Period

- ▶ Nomination period:
  - ▶ December 9<sup>th</sup> – February 14<sup>th</sup>, 2020
- ▶ All nomination forms and award category information is available online at [kelowna.ca/civicawards](http://kelowna.ca/civicawards)
- ▶ Two easy ways to nominate:
  - ▶ Via email
  - ▶ Drop-off at the Parkinson Recreation Centre or City Hall

# Completing the Nomination Form

- ▶ Selections are based on achievements and contributions in 2019
- ▶ Award selections are based on the information provided in the nomination package
- ▶ Finalists will be announced in mid March



# Award Categories

Award Categories	Number of Awards	Award Categories	Number of Awards
Young Citizen of the Year	1	Fred Macklin and Sarah Donaldson-Treadgold Memorial Citizen of the Year	1
Champion for the Environment	1	Corporate Community of the Year	1
Bob Giordano Memorial Coach/Admin of the Year	1	Bryan Couling Memorial Team of the Year	1
Male and Female Athlete of the Year	2	Augie Ciancone Male and Female High School Athletes (not part of this nomination call)	2
Teen Honour in the Arts	1	Honour in the Arts	1
Central Okanagan Foundation Volunteer Organization of the Year	1	Anita Tozer Memorial (not part of this nomination call)	1
Blue denotes newly revised award category			

# 45<sup>th</sup> Annual Civic & Community Awards Night

*Wednesday April 29<sup>th</sup>, 2020  
at the Rotary Centre for the Arts*





*Questions?*  
*[Kelowna.ca/civicawards](http://Kelowna.ca/civicawards)*



# REPORT TO COUNCIL



**Date:** December 9, 2019

**To:** Council

**From:** City Manager

**Department:** Development Planning Department

**Application:** A19-0010

**Owner:** William Winter

**Address:** 3215 HWY 33 E

**Applicant:** William Winter

**Subject:** ALR Homesite Severance Application

**Existing OCP Designation:** REP – Resource Protection Area

**Existing Zone:** A1 – Agricultural 1

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## **1.0 Recommendation**

THAT Agricultural Land Reserve Appeal No. A19-0010 for Lot 9, Sections 8, 9 and 17, Township 27, ODYD, Plan 32677, located at 3215 Hwy 33 E, Kelowna for a Homesite Severance Subdivision of agricultural land in the Agricultural Land Reserve pursuant to Section 21(2) of the Agricultural Land Commission Act, be supported by Council.

AND THAT Council direct Staff to forward the subject application to the Agricultural Land Commission for consideration.

## **2.0 Purpose**

To consider an application to the Agricultural Land Commission for a Subdivision for the purpose of a Homesite Severance.

## **3.0 Development Planning**

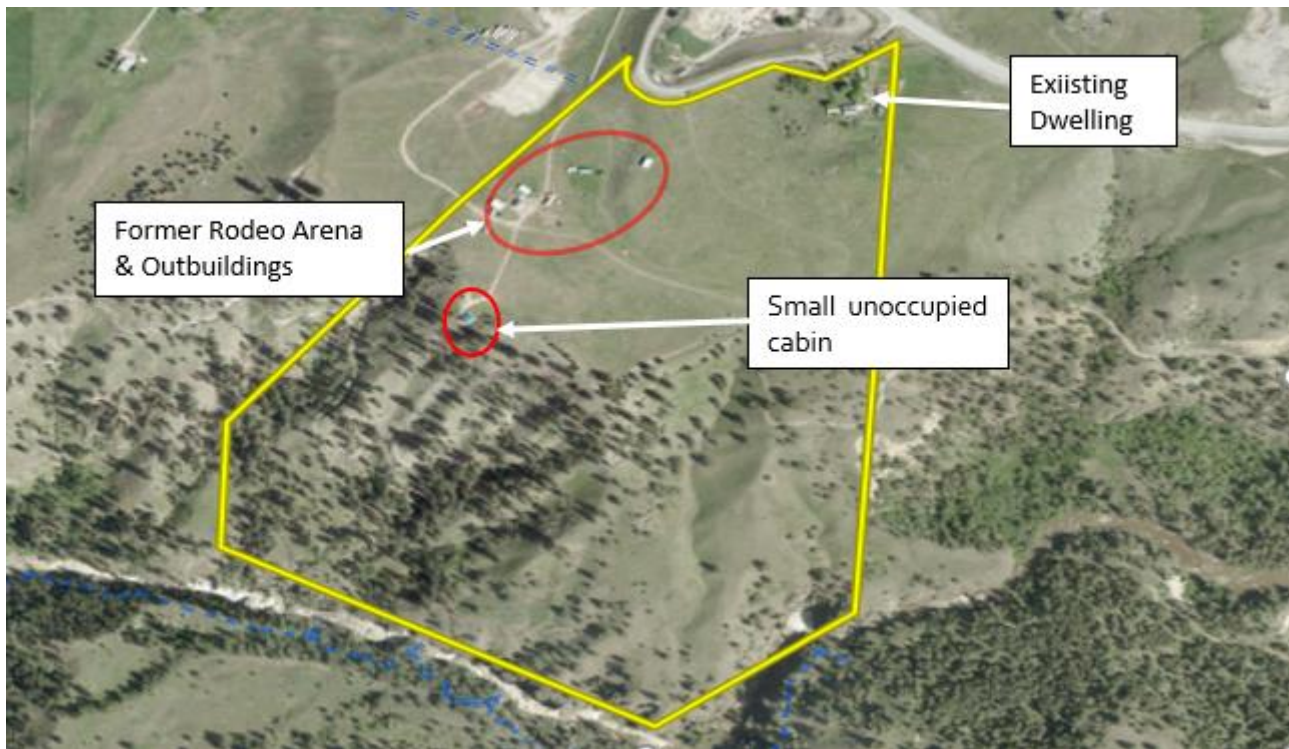
Development Planning supports this homesite severance application as it meets the requirements of the Agricultural Land Commission (ALC) regarding homesite severance applications. The City of Kelowna's Official Community Plan policy also allows for Homesite severance applications. ALC regulations enable farmers who were owner-occupants of a parcel in the ALR since December 21, 1972 to apply to dispose of the parcel but retain a homesite on the land to retire or sell the property while retaining the homesite for themselves. The homesite severance parcel has been minimized in size to help ensure the remainder is a viable agricultural parcel.

#### 4.0 Proposal

##### 4.1 Background

Approximately 10 ha (24.7 acres) of the property is currently utilized for pasture with an additional 0.7 ha (1.73 acres) used for foraging and paddock. Approximately 24.3 ha (60 acres) of land located at the southern portion of the property is unfarmed due to a significant slope and is not within the Agricultural Land Reserve (ALR).

Existing buildings located on the subject property include; a primary single detached dwelling, former Black Mountain Pro Rodeo (1960s to 2008) arena & outbuildings, a secondary cabin dwelling, tractor and feed shed and a barn for horses located in the northeast corner and the western boundary of the property.



##### 4.2 Project Description

The proposal is to subdivide the subject property, which is a total of 36.7 ha (90.83 acres) in size, to create a new homesite near the north and east property lines abutting Hwy 33 E. The applicant is seeking a 0.5 ha (1.48 acres) subdivision for the homesite. If the proposed subdivision was to be permitted, the remaining parcel would be 36 ha (89.35 acres) in size. The homesite severance area proposed by the applicant as shown below is located in the north east corner of the property. Homesite severances are encouraged to be located along front and side property lines to reduce the impact to farming.



#### 4.3 Site Context

5.0 The subject property is located in the Belgo – Black Mountain Sector of the City. There are a number of adjacent rural residential properties located to the west and north of the subject property.

#### 6.0 Current Development Policies

##### Kelowna Official Community Plan (OCP)

Objective 5.33: Minimize the impact of urban encroachment and land use conflicts on agricultural land.

Policy 5.33.7 Subdivision. Maximize potential for the use of farmland by not allowing the subdivision of agricultural land into smaller parcels (with the exception of Homesite Severances approved by the ALC) except where significant positive benefits to agriculture can be demonstrated.

#### 7.0 Technical Comments

##### Development Engineering Department

Development Engineering has no comments at this point in time with regard to this application, however, the Land Capability Assessment Report will be assessed at the time of development application submission when the Agricultural Land Commission agrees to the proposed activity on the subject property.

## **Agricultural Advisory Committee**

The above noted application was reviewed by the Agricultural Advisory Committee at the meeting held on October 10, 2019 and the following recommendation was passed:

- *The AAC recommends that Council support the ALR Application No. A19-0010 with the following anecdotal comment: The Committee would like to encourage the logical minimal size that follows the natural contours of the land be pursued for the homesite severance and that staff be encouraged to work with the applicant to achieve this.*

The following anecdotal comments were passed:

- *The Agricultural Advisory Committee would like to encourage the logical minimal size that follows the natural contours of the land be pursued for the homesite severance and that staff be encouraged to work with the applicant to achieve this.*

### **8.o Application Chronology**

Date of Application Received: June 6, 2019  
Date of Agricultural Advisory Committee: October 10, 2019

**Reviewed Prepared by:** Alex Kondor, Acting Development Planning & Development Manager

**Approved for Inclusion:** Terry Barton, Development Planning Department Manager

### **Attachments**

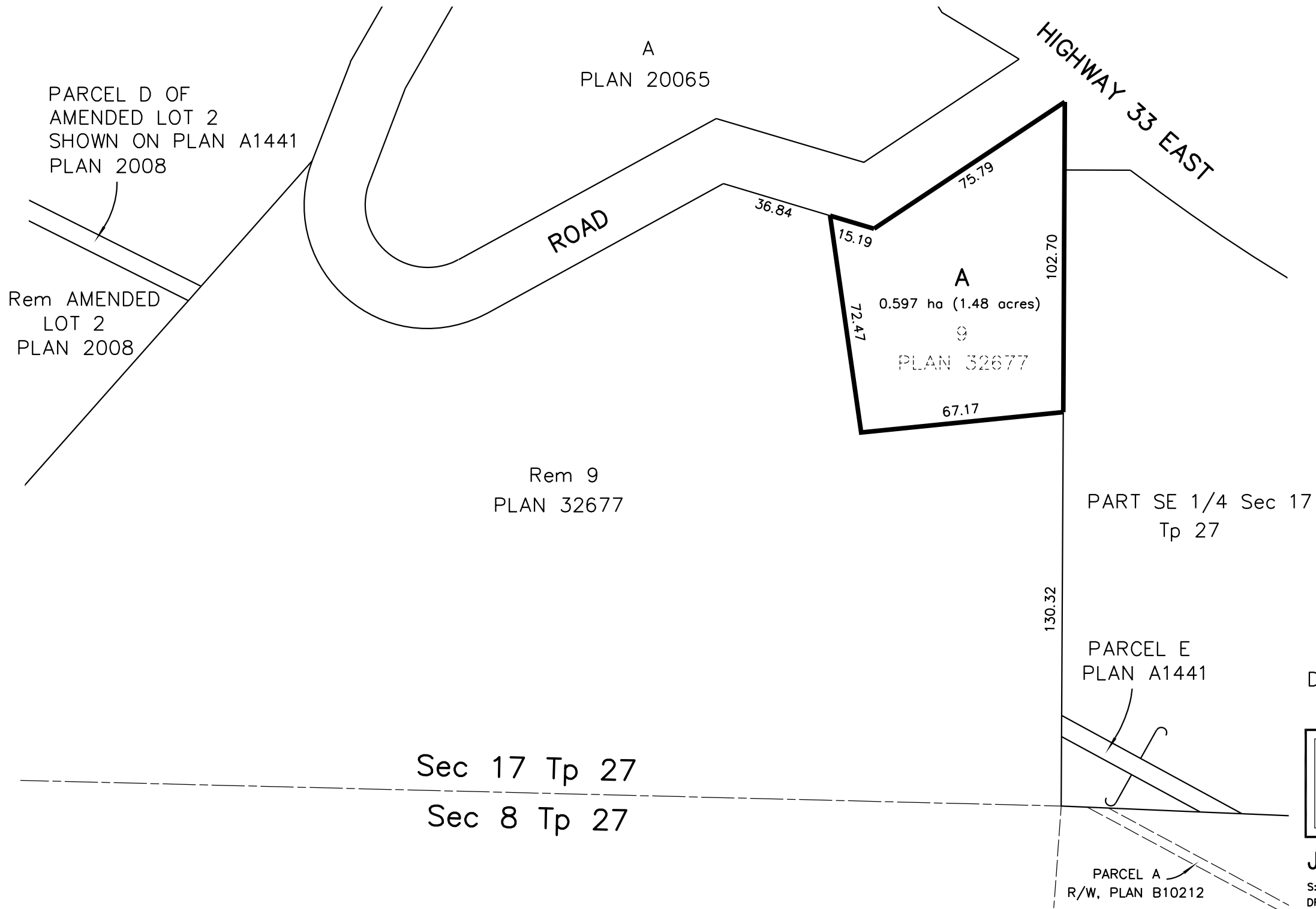
Attachment "A" Plan of Proposed Subdivision

PLAN OF PROPOSED SUBDIVISION OF PART OF LOT 9  
SECTIONS 8, 9 AND 17 ODYD PLAN 32677

BCGS 82E.084



THE INTENDED PLOT SIZE OF THIS PLAN IS 432mm IN WIDTH BY 280mm IN HEIGHT (B SIZE) WHEN PLOTTED AT A SCALE OF 1:1500



ATTACHMENT A

This forms part of application  
# A19-0010

Planner  
Initials SS



DATE: NOVEMBER 8, 2019

Ferguson Land Surveying & Geomatics Ltd.  
B.C. AND CANADA LAND SURVEYORS  
404-1630 PANDOSY STREET, KELOWNA, B.C.  
TELEPHONE (250) 763-3115 FAX (250) 763-0321

JOB No 22021-PROPOSED-SUB

S:\Shored (Aug 10, 2006)\Job\_Files\220XX\22021\22021-PROPOSED-SUB.DWG  
DRFTD BY: CF FB/PG: SEE FILE

**CITY OF KELOWNA**  
**BYLAW NO. 11950**  
**Z19-0106 - 2155-2165 Rutland Court**

---

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 1 Section 35 Township 26 Osoyoos Division Yale District Plan 9018, located on Rutland Court, Kelowna, BC from the C2 – Neighbourhood Commercial zone to the C10 – Service Commercial zone..
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 28<sup>th</sup> day of October, 2019.

Considered at a Public Hearing on the 3<sup>rd</sup> day of December, 2019.

Read a second and third time by the Municipal Council this 3<sup>rd</sup> day of December, 2019.

Approved under the Transportation Act this 4<sup>th</sup> day of December, 2019.

Audrie Henry  
\_\_\_\_\_  
(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

**CITY OF KELOWNA**  
**BYLAW NO. 11953**  
**Z19-0109 – 150 Hollywood Rd S**

---

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of all land shown on Strata Plan KAS2403, located on Hollywood Rd S, Kelowna, BC from the C4-Urban Centre Commercial zone to the C4rcs - Urban Centre Commercial (Retail Cannabis Sales) zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 18<sup>th</sup> day of November, 2019.

Considered at a Public Hearing on the 3<sup>rd</sup> day of December, 2019.

Read a second and third time by the Municipal Council this 3<sup>rd</sup> day of December, 2019.

Approved under the Transportation Act this 4<sup>th</sup> day of December, 2019.

Audrie Henry  
\_\_\_\_\_  
(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

# CITY OF KELOWNA

## BYLAW NO. 11959

### TA19-0013 – Commercial Zones Cannabis Amendments

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT **Section 9 – Specific Use Regulations, 9.16 RETAIL CANNABIS SALES ESTABLISHMENTS** be amended by adding the following:

**"9.16.8 Site Specific Regulations**

Regulations apply for Retail Cannabis Sales Establishments on a specific basis as follows:

	<b><i>Legal Description</i></b>	<b><i>Civic Address</i></b>	<b><i>Regulation</i></b>
1.	Lot B, District lot 139, ODYD, Plan 5934	1636-1652 Pandosy St.	To allow for a retail cannabis sales establishment within 500M of the approved retail cannabis sales establishment located at 547-559 Bernard Avenue.

"

2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 4<sup>th</sup> day of November, 2019.

Considered at a Public Hearing on the 3<sup>rd</sup> day of December, 2019.

Read a second and third time by the Municipal Council this 3<sup>rd</sup> day of December, 2019.

Approved under the Transportation Act this 4<sup>th</sup> day of December, 2019.

Audrie Henry  
(Approving Officer-Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk



# CITY OF KELOWNA

## BYLAW NO. 11960

### Z19-0100 – 1636 - 1652 Pandosy Street

---

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot B, District Lot 139, Osoyoos Division Yale District Plan 5934, located on Pandosy St, Kelowna, BC from C7- Central Business Commercial zone to C7rcs- Central Business Commercial (Retail Cannabis Sales) zone;
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 4<sup>th</sup> day of November, 2019.

Read a second and third time by the Municipal Council this 3<sup>rd</sup> day of December, 2019.

Approved under the Transportation Act this 4<sup>th</sup> day of December, 2019.

Audrie Henry

---

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

---

Mayor

# Report to Council



**Date:** December 9, 2019  
**To:** Council  
**From:** City Manager  
**Subject:** Provisional 2020 Financial Plan  
**Department:** Financial Services Division

---

## **Recommendation:**

THAT COUNCIL receives, for information, the presentation from the Divisional Director Financial Services and the Infrastructure Engineering Manager dated Dec.9, 2019 with respect to the Provisional 2020 Financial Plan.

## **Purpose:**

To provide an overview of the Provisional 2020 Financial Plan.

## **Background:**

The attached presentation provides a summary of the key financial impacts for the Provisional 2020 Financial Plan prior to Budget Deliberation Day on Thursday, Dec. 12, 2019.

Submitted by: Genelle Davidson, CPA, CMA Divisional Director Financial Services

cc: Infrastructure Engineering Manager, Financial Planning Manager

## **Attachment:**

1. 2020 Financial Plan Volume 1 Powerpoint.

# 2020 Financial Plan overview

Dec. 9, 2019  
Council Chambers

#kelownabudget

[kelowna.ca/budget](http://kelowna.ca/budget)







# Agenda

- ▶ Provisional budget process
- ▶ Taxation impact
- ▶ General fund
- ▶ Capital program
- ▶ Other municipal funds
- ▶ Reserve and debt
- ▶ Assessment and taxation



# 2020 Provisional Budget timeline

Budget letter & guidelines	May 13
Council outlook	June. 19
Division work plans complete	Aug. 16
Deadline for budget to Finance	Sept. 6
Performance measures, accomplishments	Sept. 23
2020 drivers & activities	Sept. 23
City Manager's review	Oct. 8-10
Council update	Nov. 4
Council overview financial plan	Dec. 9
Council review budget	Dec. 12



# Analysis of tax demand

	2019	2020
Operating	\$139.9	147.6
General revenues	(11.2)	(11.2)
Taxation capital	<u>13.8</u>	<u>14.6</u>
	\$142.5	151.0
New construction revenue	(3.6)	(2.9)
Municipal	1.83%	2.08%
Infrastructure levy	2.27%	1.82%

# 2020 Base budget changes

Annualized (including Infrastructure Levy)	\$3.54M
One-time	<u>(956k)</u>
Budget impacts from 2018 + 2019	2.6M
2020 Divisional adjustments	<u>1.1M</u>
Total 2020 net impacts	\$3.7M





# 2020 Operating budget

**Total \$8.2M priority one operating requests**

**\$4.0M taxation**

General Revenue

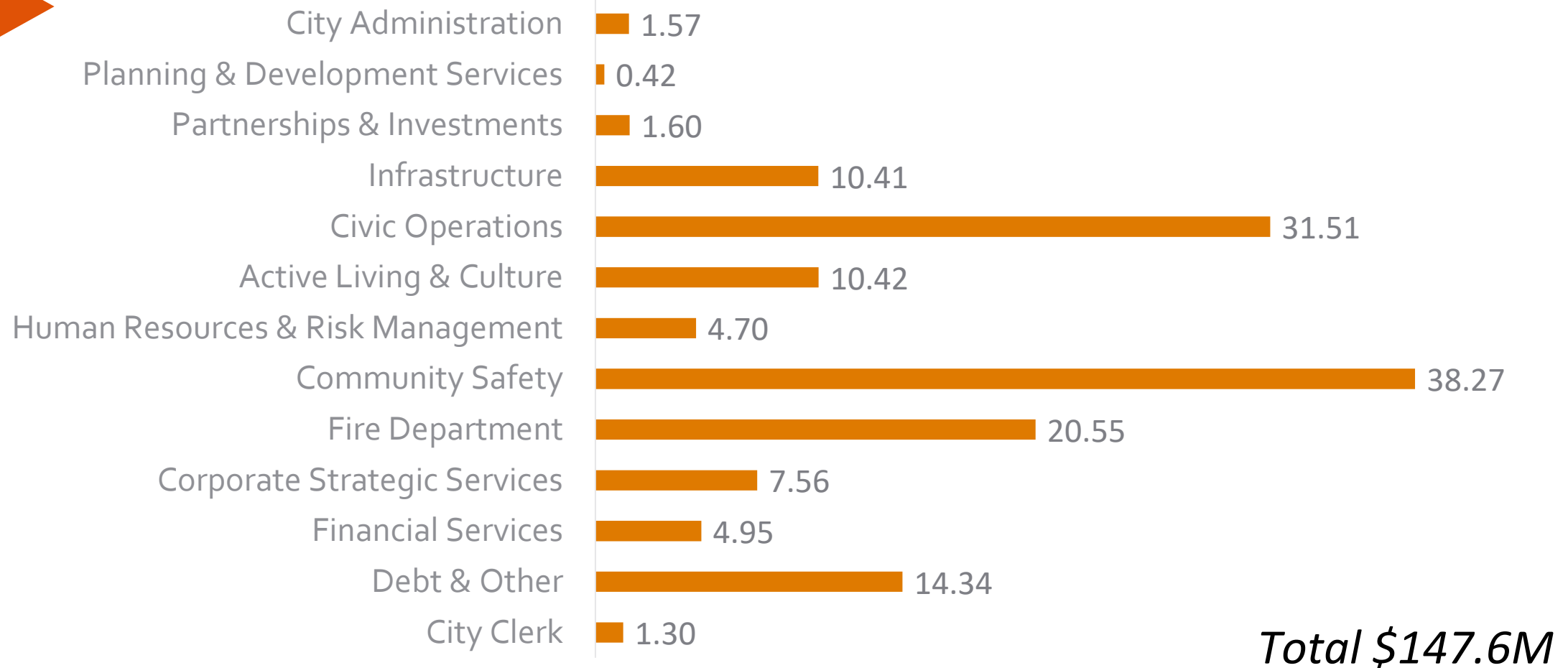
Expenditure reductions

Operating requests





# Operating budget (general fund)





# 2020 Capital (general fund)

Total Capital	\$149.8M
Taxation:	
Pay-as-you-go	14.6M
Infrastructure levy	2.6M
Operating budget impacts	187k

# 2020 Budget summary

Base budget changes	\$3.7M
2020 operating budget	<u>4.0M</u>
Operating change	7.7M
2020 pay-as-you go capital	0.8M
New construction revenue	<u>(2.9M)</u>
Net tax impact	\$5.6M

Municipal	2.08%
Infrastructure levy	1.82%



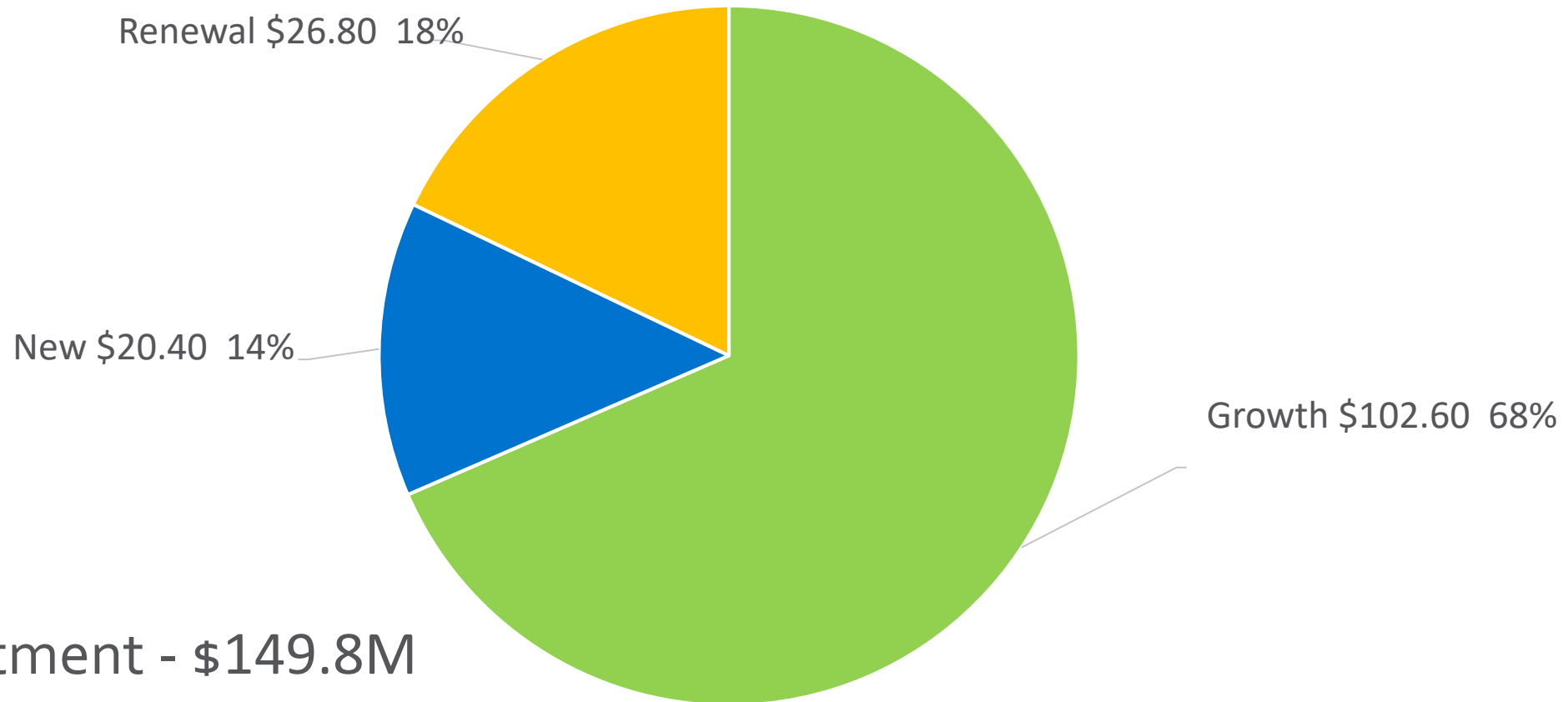
# 2019 Capital Summary

# Capital Budget Comparison

Measures	2017	2018	2019	2020
Total Projects (P1)	103	123	116	119
Capital Budget	\$64.0M	\$59.5M	\$105.8M	\$149.8M
Total Taxation	\$12.2M	\$12.2M	13.6M	\$14.6M

Big Projects	Parks, Street Lights, Asset Renewal	Parks, Roads Resurfacing, Information Services, Asset Renewal	Parks, Roads Resurfacing, South Perimeter Rd, Asset Renewal	Airport AIF Program, Parks, Roads Resurfacing, Asset Renewal
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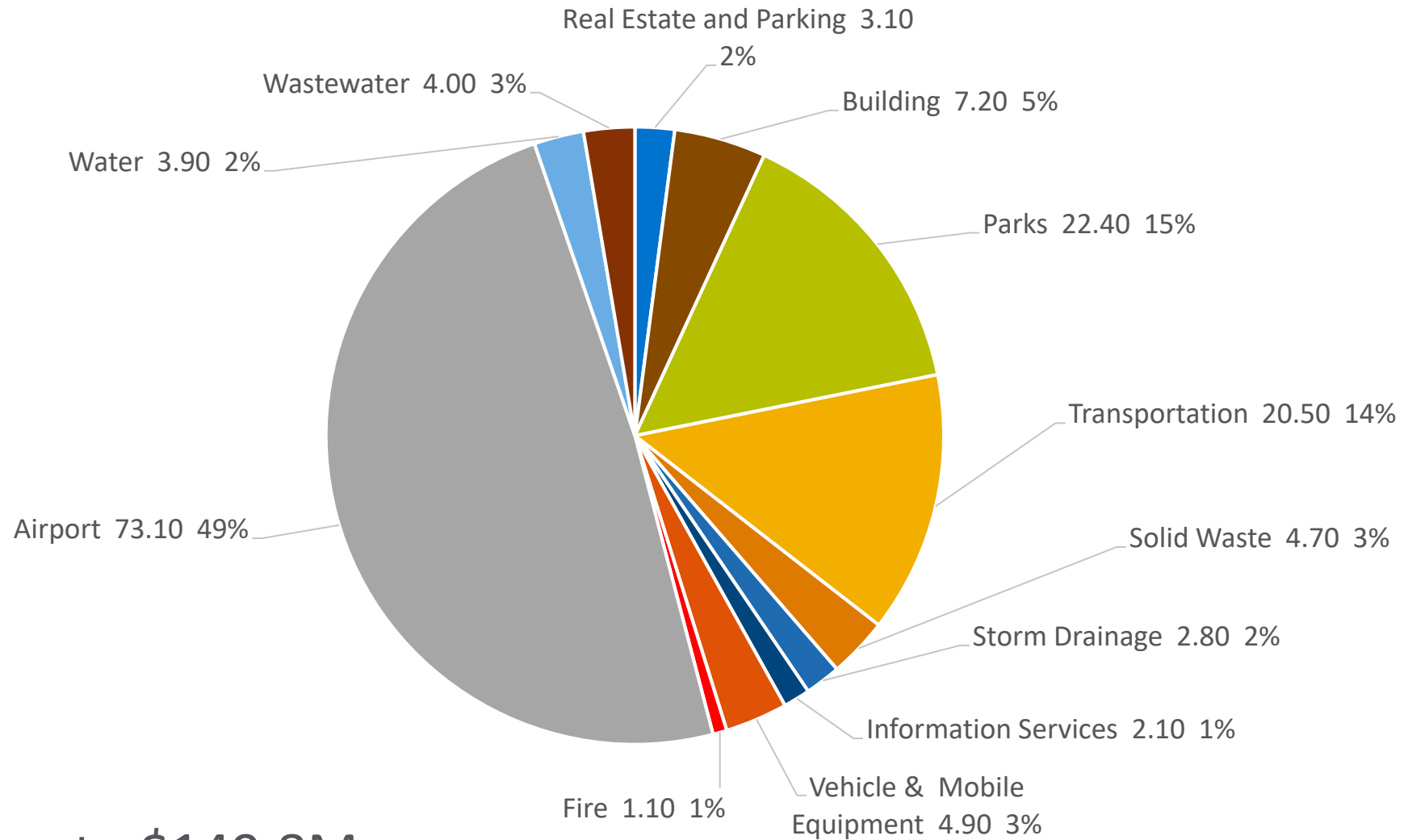
# Infrastructure value by category (\$ millions)



Total investment - \$149.8M

- New Capital - infrastructure required to support enhanced service levels,
- Growth Capital - infrastructure required to accommodate growth,
- Renewal Capital - infrastructure that replaces or renews existing assets.

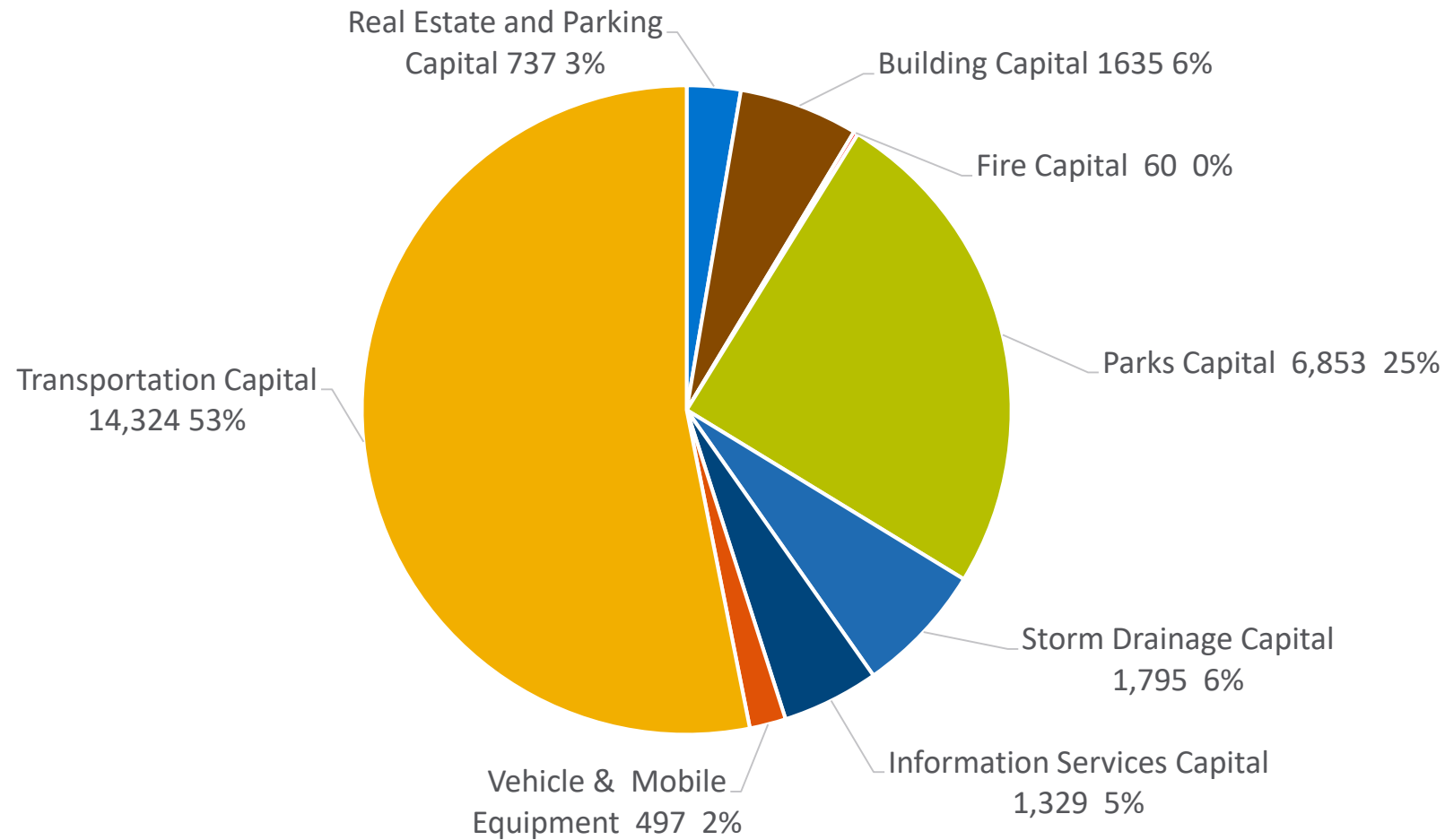
# 2020 Infrastructure investment (\$ millions)



Total investment - \$149.8M



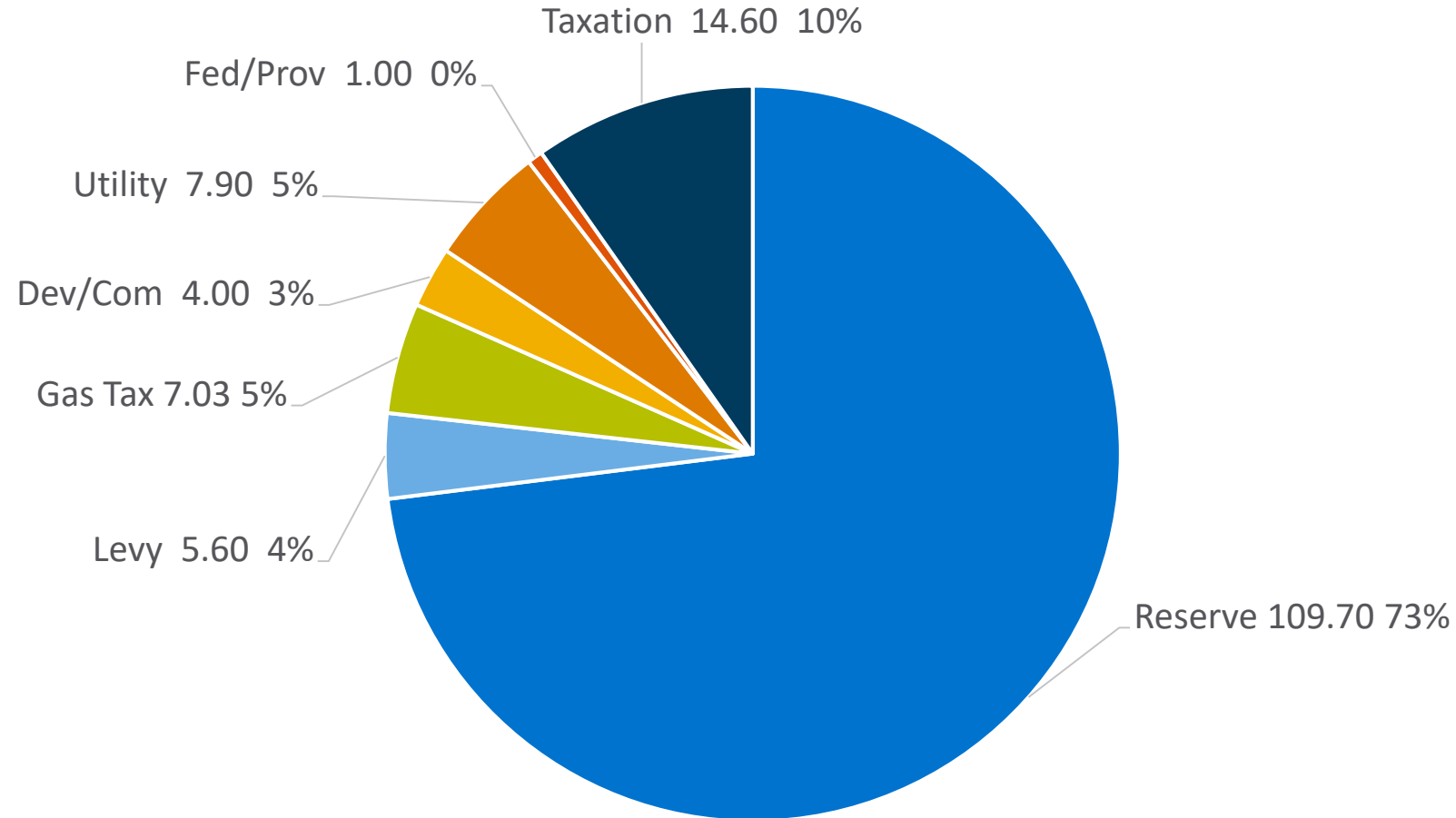
# 2020 Taxation, gas tax and levy (\$ thousands)



Taxation - \$14.6M  
 Gas tax - \$7.0M  
 Inf. Levy - \$5.6M  
**Total - \$27.2M**



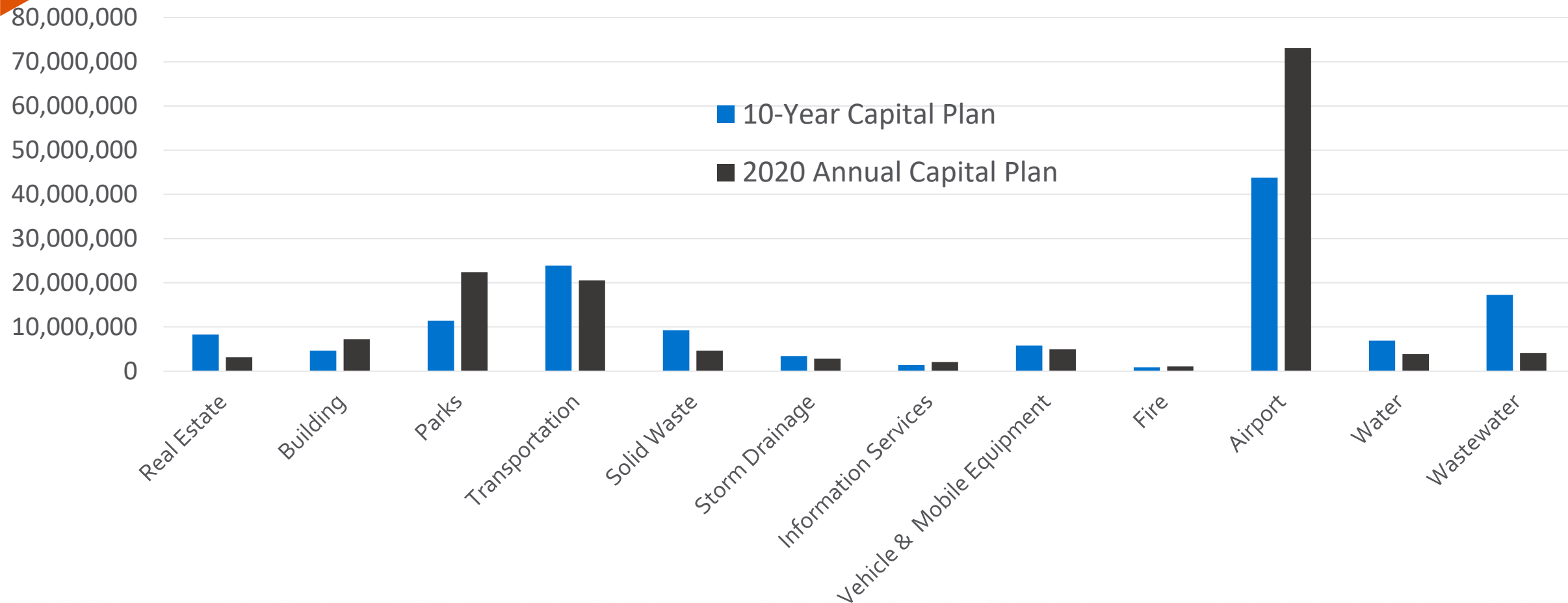
# 2020 Capital funding sources (\$ millions)



Total investment - \$149.8M

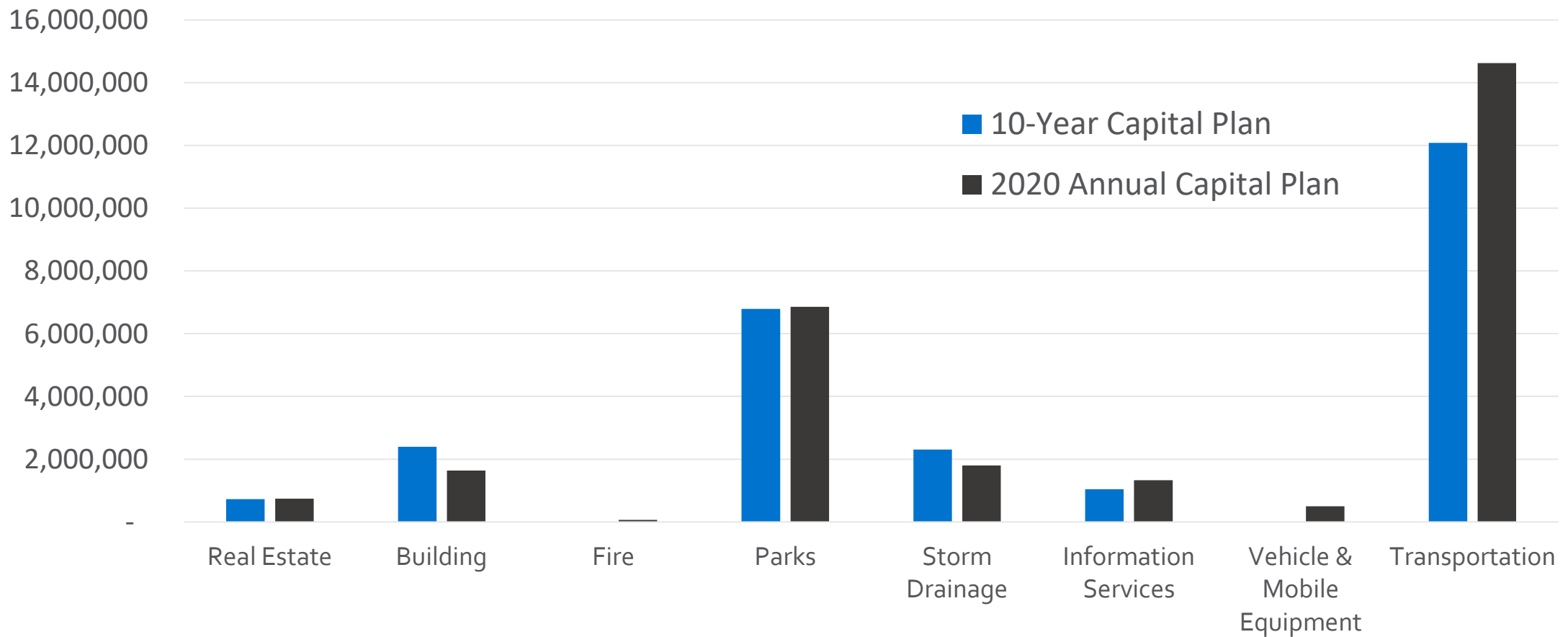
# Infrastructure Investment \$149.8M

## 10-Year Capital Plan vs. 2020 Annual Capital Plan

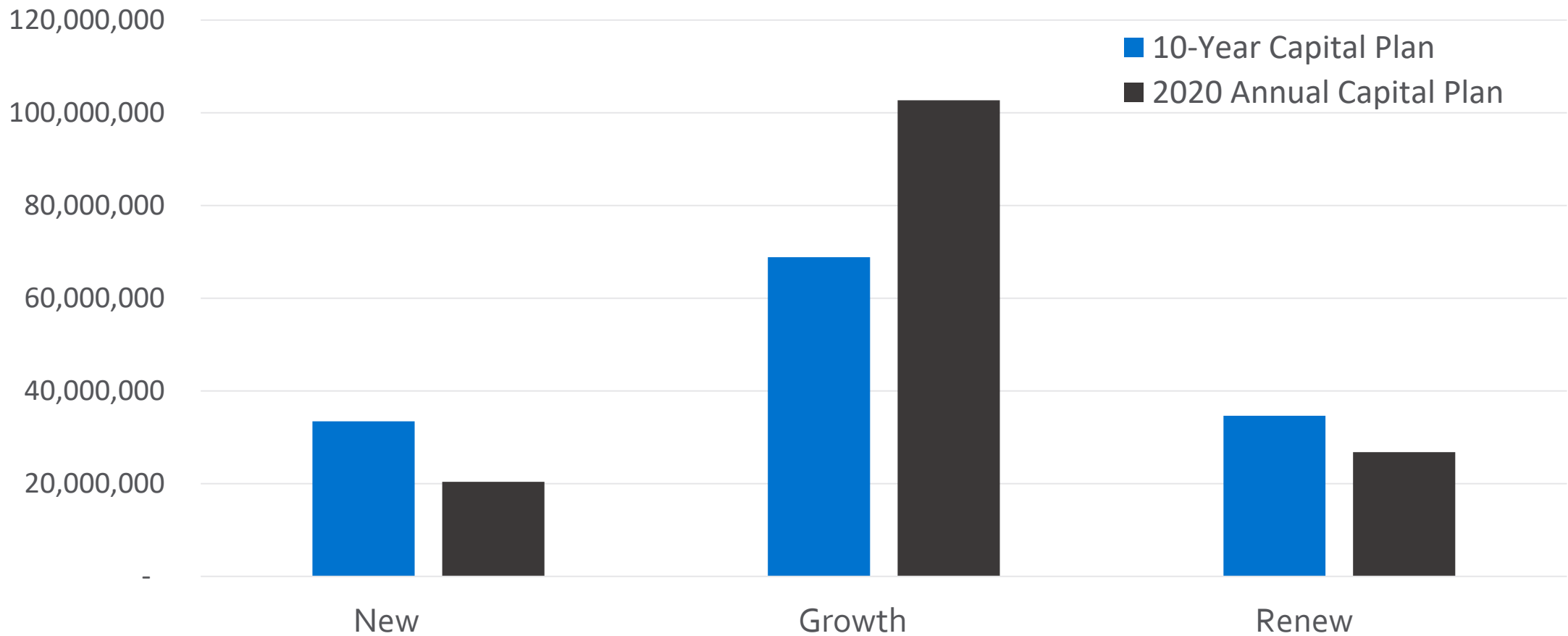


# 2020 Taxation, gas tax and levy

## 10-Year Capital Plan vs. 2020 Annual Capital Plan



# New/Growth/Renewal 10-Year Capital Plan vs. 2020 Annual Capital Plan



# Infrastructure Levy

Projected revenue from infrastructure levy (\$ millions)

Year	2019	2020	2021	2022	2023	2024	2025	2026	2027	2028	Total
Total	\$3.0	\$5.6	\$5.6	\$5.6	\$5.6	\$5.6	\$5.6	\$5.6	\$5.6	\$5.6	\$53.8

## 2020 (total \$5.6M)

- Parks Development - \$3M
- Transportation Renewal - \$1.6M
- Mill Creek Flood Protection- \$1M + 40% matching grant

# Other municipal funds


- ▶ Wastewater utility
- ▶ Water utility
- ▶ Kelowna International Airport



# Wastewater utility

Revenue	\$22.7M
Operating cost	15.2M
Capital program	3.7M
Planned surplus	3.8M





# Water utility

Revenue	\$19.4M
Operating cost	14.8M
Capital program	3.4M
Planned surplus	1.4M

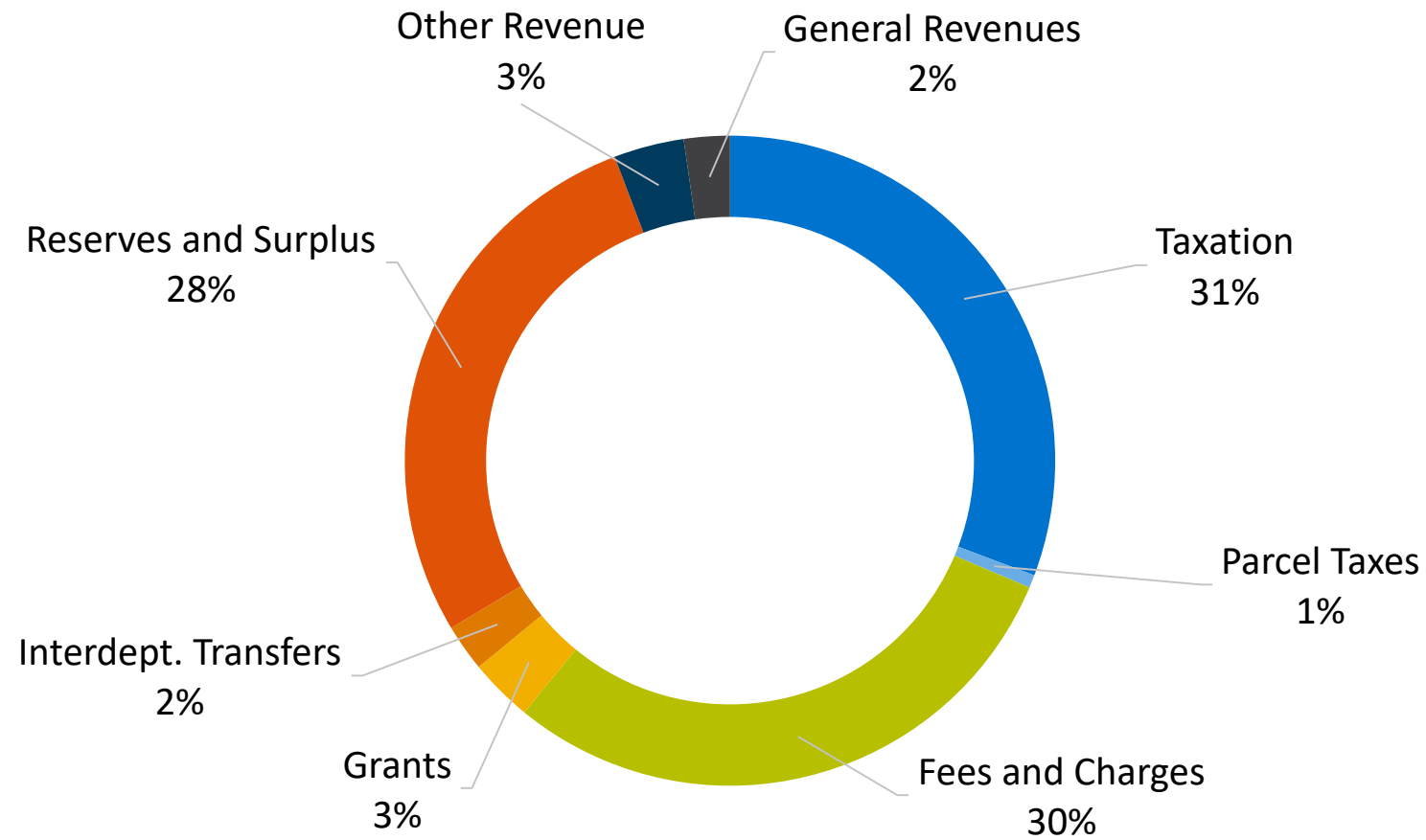




# Kelowna International Airport

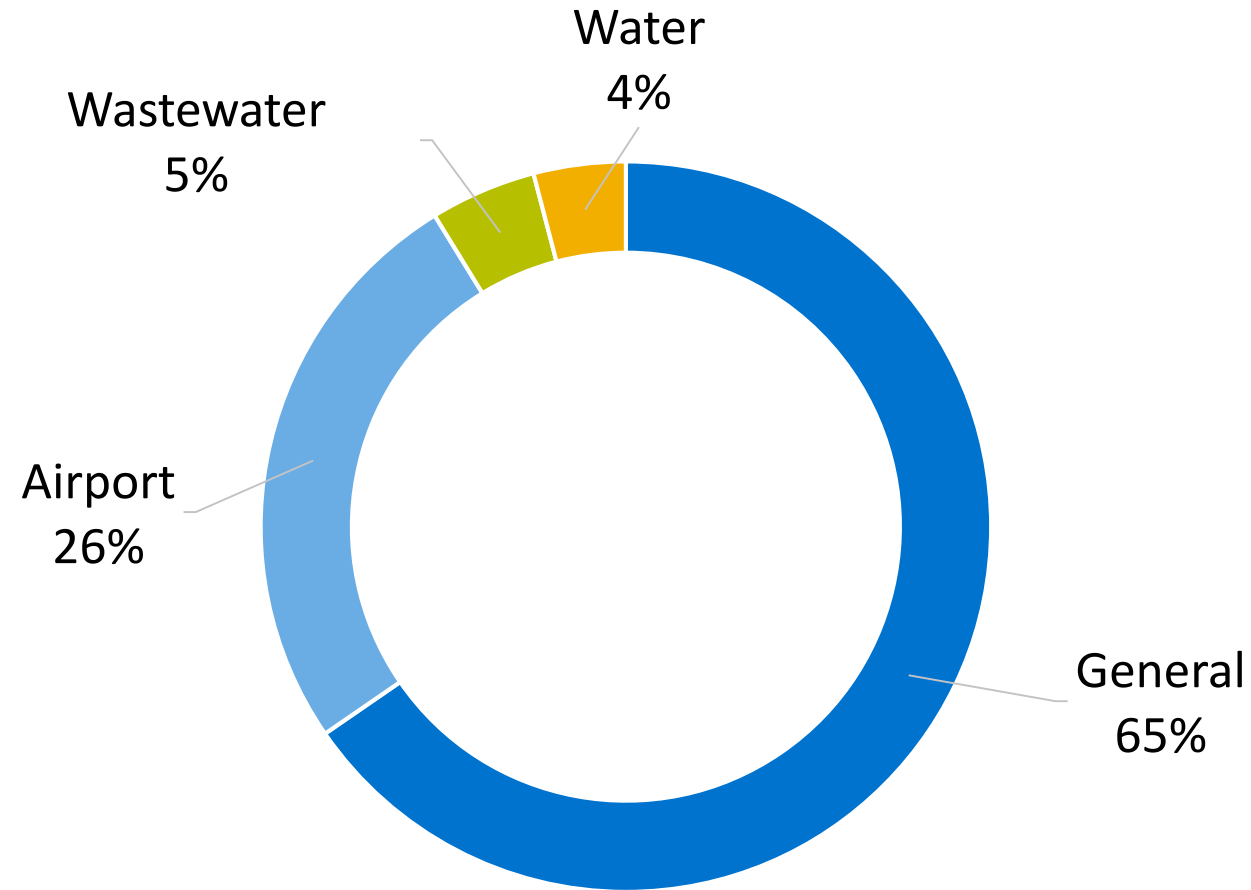
Revenue	\$46.9M
Operating cost	21.5M
Reserve contribution	24.4M
Planned surplus	932k
Capital program	73.1M

# Total revenue (all funds)



*Total \$491.1M*

# Total expenditures (all funds)



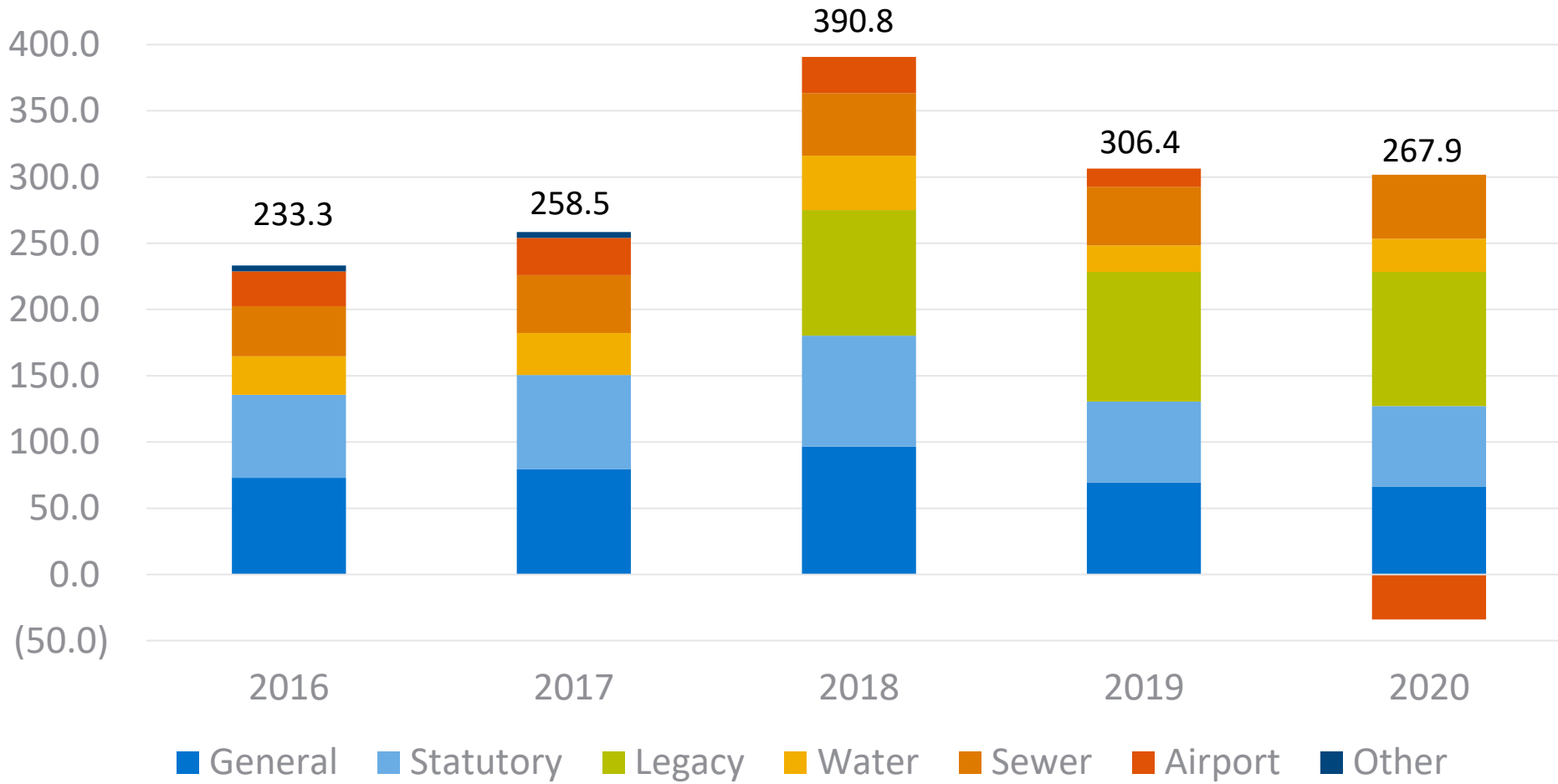
*Total \$491.1M*



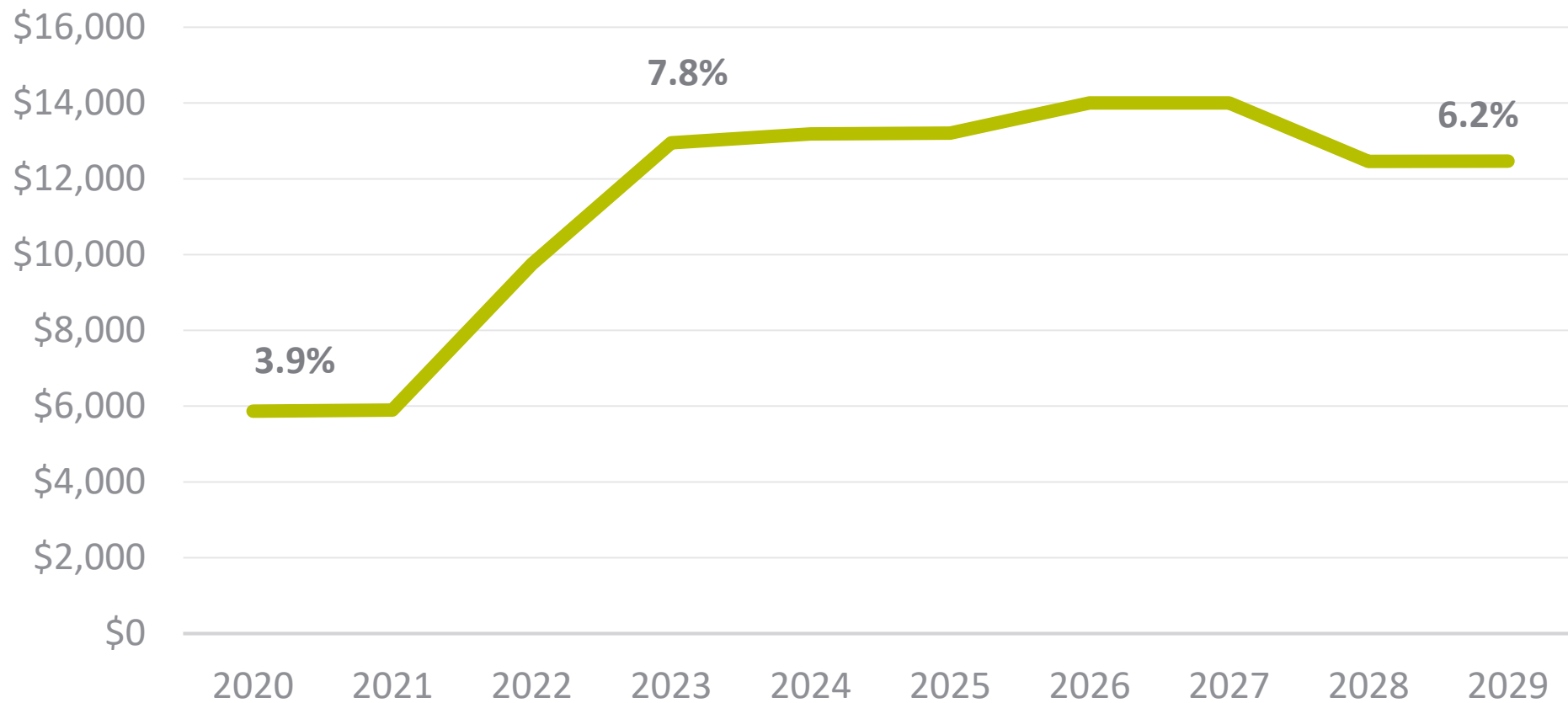
# Reserve and debt



# Reserve balances (in millions)



# General fund debt (in millions)



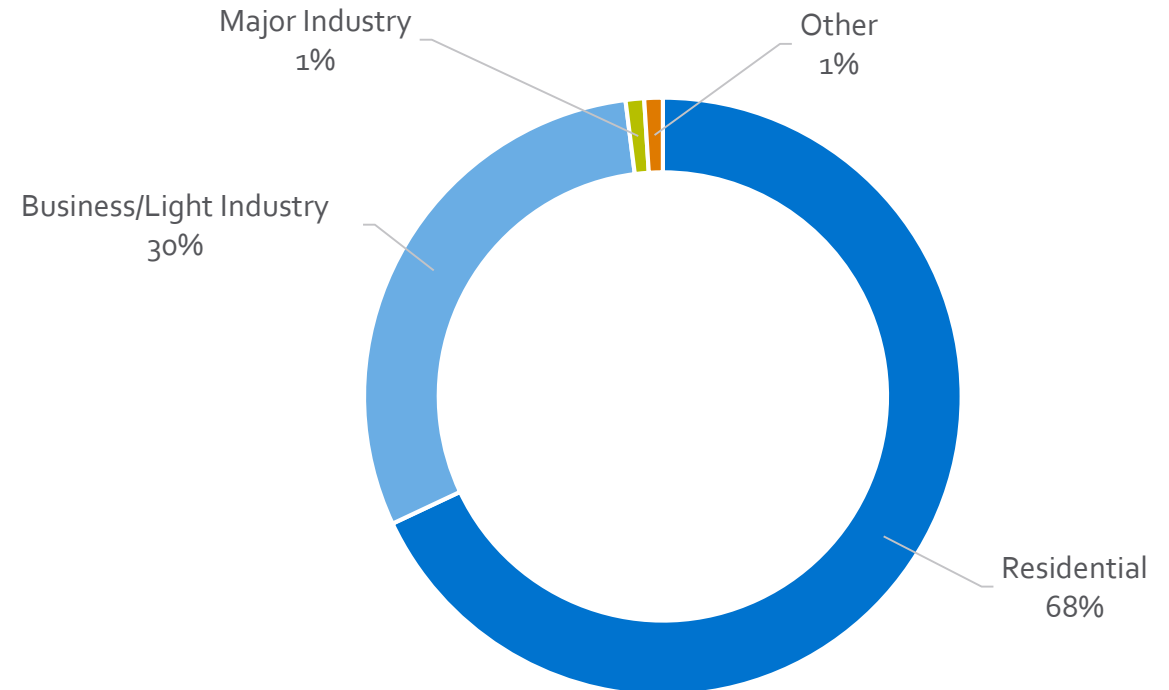
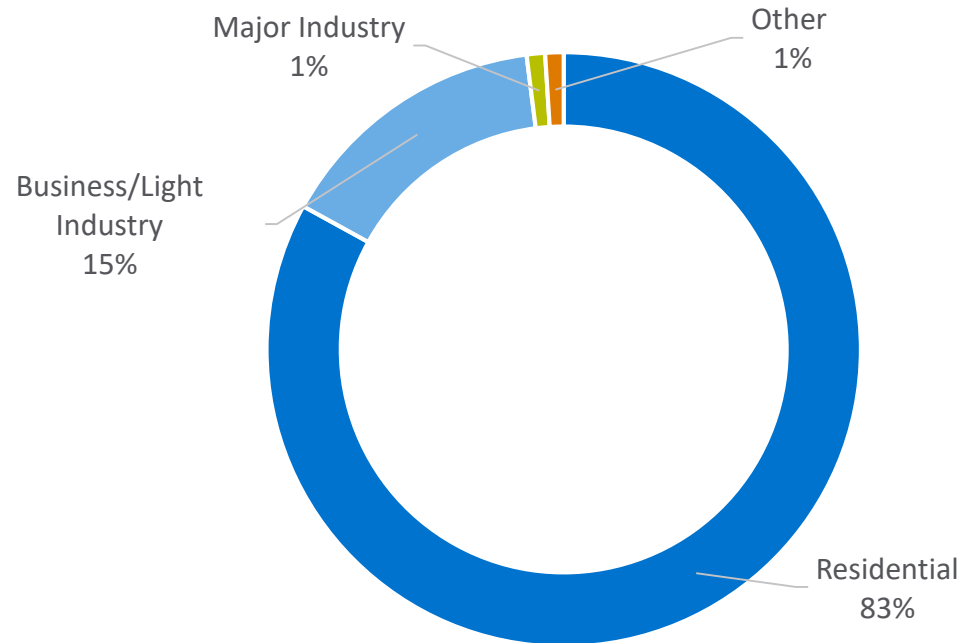


# Assessment and taxation

# Assessment and taxation revenue

Assessment \$39.7B

Revenue \$151.0M





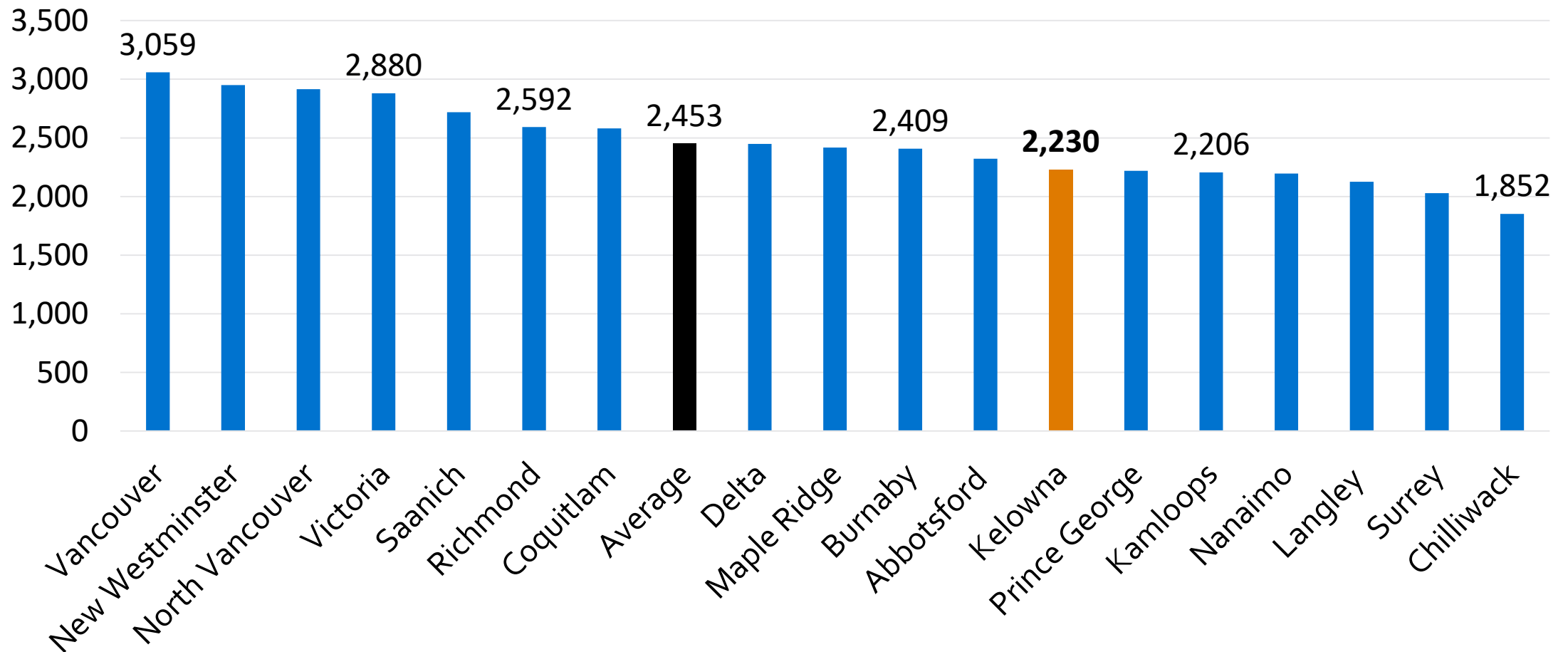


# Taxation impact

- ▶ Average Single Family Home assessed at \$670,900
- ▶ Municipal Tax estimated at \$2,153 for 2020
- ▶ Increase approximately \$7 month
  - ▶ Municipal \$43 year
  - ▶ Infrastructure levy \$38 year

# 2019 Property tax information

(Population > 75k)







City of  
**Kelowna**

## Questions?

For more information, visit  
[kelowna.ca/budget](http://kelowna.ca/budget)

**2020 Financial Plan deliberations**

**December 12, 2019 @ 9a.m.**

**Council Chambers**

# Report to Council



**Date:** December 9, 2019

**To:** Council

**From:** City Manager

**Subject:** Amendments to Financial Plan Transfer Policy No. 261 and Financial Plan Amendment Policy No. 262

**Department:** Financial Services

---

## **Recommendation:**

THAT Council Policy No. 261, being Financial Plan Transfer Policy, be amended as outlined in the Report from the Financial Planning Manager, dated December 9, 2019, Amendment to Financial Plan Transfer Policy No. 261;

AND THAT Council Policy No. 262, being Financial Plan Amendment Policy, be amended as outlined in the Report from the Financial Planning Manager, dated December 9, 2019, Amendment to Financial Plan Amendment Policy No. 262.

## **Purpose:**

To amend the Financial Plan Transfer and Financial Plan Amendment Policies with respect to the parameters when Council pre-approval is required during the year and for administrative revisions.

As required by Section 165 of the Community Charter, the City of Kelowna prepares a Financial Plan that is adopted annually, by bylaw, before the annual property tax bylaw is adopted. The planning period for the Financial Plan is 5 years, that period being the year in which the plan is specified to come into force and the following 4 years.

Financial Plan transfers are defined as the movement of budgeted funds within the annual Financial Plan after it has been enacted by Council. Transfers do not result in an increase to the City's annual adopted Financial Plan. Financial Plan amendments are defined as a net change (increase or decrease) to the annual Financial Plan after it has been enacted by Council. Amendments may increase the City's total budget only where funding is by a source other than taxation (i.e. provincial grant, private contribution, etc.).

While it is expected that the City operate within the Council approved annual Financial Plan, changes to the Financial Plan are frequently required after it has been approved by Council. The reasons vary and

can include approved grant funding, emergent events, new Council-directed initiatives and spending required as a result of new legislation or regulations.

Financial Plan Transfer Policy No. 261 and Financial Plan Amendment Policy No. 262 provide the parameters for when Council pre-approval during the year is required prior to completing a Financial Plan Transfer or Financial Plan Amendment, respectively. All transfers and amendments are presented annually, in a Report to Council by Financial Services, in order to amend the Five-Year Financial Plan bylaw. Financial Plan Transfers and Financial Plan Amendments which do not require Council pre-approval are subject to the approval processes set out in the Corporate Financial Policy FIN-031 Financial Plan Transfer Policy and Corporate Financial Policy FIN-032 Financial Plan Amendment Policy. These Corporate policies provide for a high level of internal control, requiring multiple layers of approvals.

The last revision to the Council policies had been conducted in April 2010. Since that time, the magnitude of the City's Financial Plan has increased significantly. As a result, the approval levels as set out in the Council policies were no longer appropriate. Furthermore, the policies required updating to remove references to funds no longer in existence (e.g. Electrical and Natural Gas funds), renamed Departments and other small administrative changes.

Under the revised Financial Plan Amendment Policy, prior Council approval is required for amendments over \$200,000 (as compared to \$50,000 under the previous provisions).

The revised Financial Plan Transfer Policy clarifies the situations whereby prior Council approval is required. In all cases, Financial Plan Transfers must respect the integrity of the approved Financial Plan, unless prior Council approval is received. For further clarity, prior Council approval is required if a transfer involves the cancellation of an approved program or project to meet the needs of an anticipated over-expenditure in another program or project. Furthermore, transfers cannot be used to fund new programs or projects without prior Council consent.

**Conclusion:**

Financial Plan Transfer Policy No. 261 and Financial Plan Amendment Policy No. 262 set out the parameters for Council pre-approval during the year of changes to the Financial Plan, in order to ensure appropriate oversight and control as well as compliance with the Community Charter.

The amended policies, as set out in Appendix 1 - Financial Plan Transfer Policy No. 261 and Appendix 2 - Financial Plan Amendment Policy No. 262, provide an appropriate level of oversight and control while reducing the number of low value approvals required by Council. Council will continue to be presented with an annual report of all transfers and amendments in order to amend the Five-Year Financial Plan bylaw.

**Internal Circulation:**

Divisional Director, Infrastructure  
Senior Airport & Corporate Services Manager  
Infrastructure Administration Manager  
Budget Supervisor  
Financial Services Supervisor



Financial Analyst, Infrastructure

**Considerations applicable to this report:**

The Financial Plan Transfer Policy No. 261 and Financial Plan Amendment Policy No. 262 comply with Section 165 of the Community Charter and are in accordance with the guidelines as set out in the City of Kelowna Principles and Strategies for Financial Strength and Stability Report, August 2015.

***Legal/Statutory Authority:***

Community Charter [SBC 2003] Chapter 26, Part 6 – Financial Management, sections 165 and 173

***Existing Policy:***

Financial Plan Transfer Policy No. 261

Financial Plan Amendment Policy No. 262

**Considerations not applicable to this report:**

Community

Communications Comments

External Agency/Public Comments

Financial/Budgetary Considerations:

Legal/Statutory Procedural Requirements:

**Submitted by:** G. King, Financial Planning Manager, Financial Services

**Approved for inclusion:** Genelle Davidson, CPA, CMA, Divisional Director, Financial Services

**Appendices**

Appendix 1 – Financial Plan Transfer Policy No. 261

Appendix 2 – Financial Plan Amendment Policy No. 262



City of Kelowna  
1435 Water Street  
Kelowna, BC V1Y 1J4  
250 469-8500  
kelowna.ca

POLICY 261

## **Council Policy**

### **Financial Plan Transfer Policy**

APPROVED December 9, 2019

RESOLUTION: xxx 19/12/09

REPLACING: R1039/08/11/24; R529/05/05/30; R392/03/04/28; R1000/00/12/18; R727/1998/09/14; Policy No. 17; R-1971/06/07; R375/10/04/26

DATE OF LAST REVIEW: December 2019

#### **Purpose**

As required by Section 165 of the Community Charter, the City of Kelowna prepares a Financial Plan that is adopted annually, by bylaw, before the annual property tax bylaw is adopted. The planning period for the Financial Plan is 5 years, that period being the year in which the plan is specified to come into force and the following 4 years.

This policy sets out the parameters for Financial Plan transfers. A Financial Plan transfer is defined as the movement of budgeted funds within the Financial Plan after it has been enacted by Council. Transfers do not result in an increase to the City's annual adopted Financial Plan.

#### **Policy Scope**

This policy applies to all Financial Plan transfers.

#### **Policy Statement**

It is expected that Divisions operate within their Council-approved budgets. Transfers of funds within the approved Financial Plan may be required in order to meet the City's internal control objectives, to provide a means for a predictable operating result and to ensure the early detection and management of over-expenditures. Transfers must respect the integrity of the approved Financial Plan, unless prior Council approval is received.

Council consent is required if a transfer involves the cancellation of an approved program or project to meet the needs of an anticipated over-expenditure in another program or project. Transfers also cannot be used to fund new programs or projects without prior Council consent.

Transfers between capital cost centres require a separate report to the Divisional Director, Financial Services, who will determine if Council approval is required considering the impact of the transfer on the approved Financial Plan.

All transfers, including those that do not require Council approval throughout the year, must be presented in an annual Report to Council by Financial Services and receive Council to amend the Five-Year Financial Plan bylaw.

#### **Related References**

City of Kelowna Principles and Strategies for Financial Strength and Stability, August 2015  
City of Kelowna Financial Plan Transfer Policy FIN-031  
City of Kelowna Financial Plan Amendment Council Policy No. 262  
City of Kelowna Financial Plan Amendment Policy FIN-032  
City of Kelowna Purchasing and Invoice Approval Policy FIN-001

#### **Legislative Authority**

Community Charter [SBC 2003] Chapter 26, Part 6 – Financial Management, sections 165 and 173

#### **Procedure for Implementation**

Administered by the Financial Planning branch of the Financial Services Division.



**City of Kelowna**  
1435 Water Street  
Kelowna, BC V1Y 1J4  
250 469-8500  
kelowna.ca

POLICY 262

## **Council Policy**

### **Financial Plan Amendment Policy**

APPROVED December 9, 2019

RESOLUTION: XXX 19/12/09  
REPLACING: R1039/08/11/24; R727/1998/09/14; R1000/00/12/18; R392/03/04/28;  
R375/10/04/26  
DATE OF LAST REVIEW: December 2019

#### **Purpose**

As required by Section 165 of the Community Charter, the City of Kelowna prepares a Financial Plan that is adopted annually, by bylaw, before the annual property tax bylaw is adopted. The planning period for the Financial Plan is 5 years, that period being the year in which the plan is specified to come into force and the following 4 years.

This policy sets out the parameters for Financial Plan amendments. A Financial Plan amendment is defined as a net change (increase or decrease) to the annual Financial Plan after it has been enacted by Council.

#### **Policy Scope**

This policy applies to all Financial Plan amendments.

#### **Policy Statement**

It is expected that City of Kelowna Divisions operate within their Council-approved budgets. Changes to the budget are frequently required after the City's Financial Plan has been approved by Council. The reasons for amendments vary and include confirmed grant funding, emergent events, new Council-directed initiatives, and spending required as a result of new legislation or regulations. Amendments are also required when unforeseen expenditures are deemed necessary but were not included in the approved Financial Plan.

Amendments may increase the City's total budget only where funding is by a source other than taxation (i.e.: provincial grant, private contribution, etc.).

Council approval, through a Report to Council ("RTC"), is required for amendments greater than \$200,000, prior to Financial Plan amendments being made. All amendments, including those that do not require Council approval throughout the year, must be presented in an annual RTC by Financial Services to amend the Five-Year Financial Plan bylaw.

For amendments over \$200,000, which require a RTC, prior review and approval of the RTC is required by the Divisional Director(s) of the impacted Division(s), the Divisional Director Financial Services and the City Manager.

#### **Related References**

City of Kelowna Principles and Strategies for Financial Strength and Stability, August 2015  
City of Kelowna Financial Plan Transfer Council Policy No. 261  
City of Kelowna Financial Plan Amendment Policy FIN-032  
City of Kelowna Financial Plan Transfer Policy FIN-031  
City of Kelowna Purchasing and Invoice Approval Policy FIN-001

#### **Legislative Authority**

Community Charter [SBC 2003] Chapter 26, Part 6 – Financial Management, sections 165 and 173

#### **Procedure for Implementation**

Administered by the Financial Planning branch of the Financial Services Division.





# Financial Plan Policy Updates - Policy 261 & 262

December 2019







# Agenda

- ▶ Why the change?
- ▶ Financial Plan Transfer Policy
- ▶ Financial Plan Amendment Policy
- ▶ Corporate oversight
- ▶ Annual Report to Council



# Why the changes?

- ▶ No longer reflects current realities
- ▶ Approval thresholds no longer appropriate
- ▶ Opportunity to reduce low value activities

# Financial Plan Transfer Policy

*A Financial Plan transfer is defined as the movement of budgeted funds within the Financial Plan after it has been enacted by Council.*

*Transfers do not result in an increase to the City's annual adopted Financial Plan.*

# Transfer policy highlights

Council approval is required for:

- ▶ Program or project cancellation
- ▶ New program or project

# Financial Plan Amendment Policy

*A Financial Plan amendment is defined as a net change (increase or decrease) to the annual Financial Plan after it has been enacted by Council.*

*Amendments may increase the City's total budget only where funding is by a source other than taxation.*

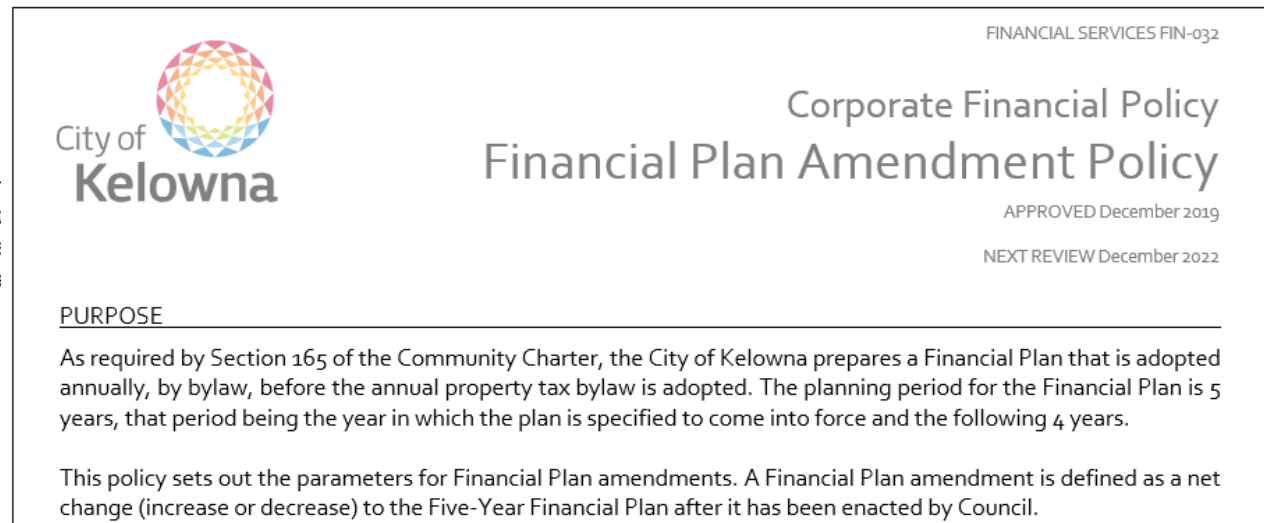
# Amendment policy highlights

Council approval is required for:

- ▶ Increases greater than \$200k



# Strong financial management



# Annual Report to Council

## Report to Council



**Date:** April 15, 2019  
**File:** 0220-20  
**To:** City Manager  
**From:** Budget Supervisor  
**Subject:** Amendment No. 1 to the Five Year Financial Plan 2018-2022

Report Prepared by: Kam Dosanjh, Accountant

### Recommendation:

THAT Council receives, for information, the Report from the Budget Supervisor dated April 15, 2019 with respect to amendments to the Five Year Financial Plan 2018-2022 Bylaw;

AND THAT Bylaw No. 11795 being Amendment No. 1 to the Five Year Financial Plan 2018-2022 Bylaw No. 11583 be advanced for reading consideration.

### Purpose:

To amend the Five Year Financial Plan 2018-2022 as required by the Community Charter so that it includes the authorized transfers and amendments that occurred throughout the year.

### Background:

The City of Kelowna, in order to comply with section 165(1) of the Community Charter (Financial Management), amends the financial plan to provide for expenditures required after adoption of the Five Year Financial Plan Bylaw. These expenditures, in all cases, do not impact taxation demand but rather result in the shift of funding from one source to another and/or shifts in expenditures within, or from, one municipal purpose area to another.

The amended financial plan is impacted on a departmental basis by contingency fund transfers and other transfers as permitted under the Budget Amendment Policy #262 and Budget Transfer Policy #261. The attached Schedule A includes the authorized transfers and amendments that occurred throughout the year.



## *Questions?*

For more information, visit [kelowna.ca](http://kelowna.ca).



# Report to Council



**Date:** December 9, 2019  
**To:** Council  
**From:** City Manager  
**Subject:** OCP 2040 Phase 3 Engagement Results  
**Department:** Policy and Planning

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## **Recommendation:**

THAT Council receives the report from the Policy and Planning Department, dated December 9, 2019, for information;

AND FURTHER THAT Council directs staff to report back with preliminary financial impacts of servicing the endorsed growth scenario.

## **Purpose:**

To provide Council with a summary of the feedback received as part of the public engagement for Phase 3 of 2040 Official Community Plan Update process and to obtain Council's direction to commence preliminary infrastructure impact analysis of the draft future land use map.

## **Background:**

In 2018, the City of Kelowna began the process to update its Official Community Plan (OCP) and Transportation Master Plan (TMP). Since then, both projects have undertaken separate and joint public and stakeholder engagement initiatives. Public engagement and communication conducted in 2019 has sought to keep residents informed of directions being taken and decisions being made for these plans, as well as to consult with them on key elements in order to inform policy development.

At its meeting of August 12, 2019, Council received a report that provided a draft Future Land Use Map for its consideration for use as part of fall 2019 public engagement. This report serves to provide Council with a summary of the feedback received from this engagement and how this feedback will be used to inform the 2040 OCP update process moving forward.

Engagement for the OCP and TMP in 2019 has focused on consulting with the public and stakeholders on key elements of the plans to inform policy development, project selection and land use planning. As such, the objectives of this most recent round of engagement sought to:

- Inform and consult with the public and stakeholders on directions being taken in the draft Future Land Use Map;
- Inform and consult with the public and stakeholders on objectives of the OCP Infill Strategy and collect a list of infill housing preferences;
- Inform and consult with the public on directions being taken in the Transportation Master Plan and receive a list of transportation challenges and ideas that will inform the Transportation Master Plan update;
- Inform and consult with the public on parks development being proposed in the Official Community Plan update and receive feedback on parks preferences that will inform the future direction of park acquisitions across our City;
- To inform the public on the purpose of, and directions being taken in, the 20-Year Servicing Plan, to increase public understanding of the plan; and
- To inform and consult the public on directions being taken in the Zoning Bylaw update.

<p><b>4</b></p>  <p>public neighbourhood expos</p>	<p><b>6</b></p> <p>stakeholder workshops</p>
<p><b>~740</b> in-person &amp; online public participants</p> 	
<p><b>~720</b></p> <p>free-form public comments received</p>	<p><b>142</b></p> <p>survey responses</p>

Complete existing neighbourhoods

Protecting ALR

Walkability

General

Nothing

KGH growth

Density

UBC growth

Urban centres

Limiting rural development

Protecting heritage areas

Permanent growth boundary

Increasing building heights

Limiting sprawl

Mixed-use development

Okanagan College expansion

This engagement approach took the form of four “Neighbourhood Expos” and online feedback, which provided residents with the opportunity to attend one of four possible in-person sessions in different neighbourhoods of the city (Kelowna Community Theatre, Capital News Centre, Orchard Park Mall, and Rutland Boys & Girls Club) throughout late September to learn about proposed directions of the plans and provide input and feedback. An online component was available for people who were unable to attend the Expos in person.

The Neighbourhood Expos ran between September 16 to October 4, 2019. Participation results from this process are summarized in Figure 1.

A summary of the feedback from participants is provided below. A full report on the engagement results is included with this report as Attachment 1.

A word cloud of planning ideas for the Tolko site. The words are arranged in a circular pattern, with some words being larger and more prominent than others. The words include:

- Reduce traffic congestion
- Protect natural areas
- Complete existing suburban neighbourhoods
- Increase neighbourhood commercial development
- Increase density in core area
- Improve diverse transportation options
- Limit sprawl
- Nothing
- Improve affordability
- Change Tolko site
- Only low building heights next to lake
- Protect tree canopy
- Increase parks
- Increase building heights
- Support single-family development



### *Land Use Directions*

Overall, participants in the Expo expressed support for the directions outlined in the draft Land Use Plan. There was support signaled for increasing density in the Urban Centres and the Core Area, slowing suburban development and protecting agricultural lands. There was also positive feedback provided on supporting continued growth at UBCO and Okanagan College and continuing to protect heritage areas (see Figure 2).

When asked what they would change about the draft Land Use Plan, some participants noted that they would go farther in increasing density in the Urban Centres and the Core Area while pulling back even more on suburban development. Others spoke to needing assurances that more transportation options and parks are made available to serve these denser neighbourhoods, while the need to accommodate more commercial uses in suburban Village Centres was also raised (see Figure 3).

### *Transportation Challenges and Solutions*

Participants had the opportunity to learn more about the findings of the Transportation Master Plan - Existing and Future Conditions report and share ideas on projects they would like included in the plan. As outlined in the Report to Council dated November 18, 2019 entitled "Transportation Master Plan: Options Development", this input was used to craft the various projects, policies and programs that are being considered for inclusion in the draft TMP.

### *Infill Housing*

The growth strategy relies on a robust infill housing strategy in accommodating new residential units in a variety of housing forms. How that infill development takes place in existing neighbourhoods will be critical to ensuring that these evolving neighbourhoods continue to be attractive and desired places to live. Participants expressed a strong preference for higher density forms of infill that provided more affordability options and supported more local shops and services within walking distance. However, they were split regarding allocation of space on infill development sites for private green space and on-site parking.

### *Parks*

As more parks will be needed to accommodate growth in the Urban Centres and Core Area, participants were asked to indicate preferences for a variety of park types. Participants felt strongly more parks were needed, and that having smaller parks closer to where they lived would be preferred over larger ones farther away. They also felt strongly that parks that offered more amenities were preferable to those that allocated more of their space to parking. This feedback will be used for identifying future park sites for the 2040 OCP update but will also lay the foundation for the pending Parks Master Plan development.

### *20 Year Servicing Plan*

Panel content informed the public on the purpose of, and directions being taken in, the 20 Year Servicing Plan update. The goal was to increase public education and awareness of the Plan and its relationship to the 2040 OCP implementation. Ultimately, choices made in the growth strategy have a strong interdependency with how the City will fund and prioritize the corresponding servicing/infrastructure components to deliver on that growth vision.

### **Stakeholder Engagement**

In addition to public engagement, Staff hosted a series of workshops with community stakeholders to get further feedback.

- Infill strategy stakeholder workshops (June – September, 2019)
- Form and Character Development Permit guidelines workshop (October 3, 2019)
- OCP and TMP Stakeholder Group workshop (October 17, 2019)

Summaries of these workshops are found in Attachment 1.

Staff have also engaged directly with other key stakeholders, including, but not limited to, BC Transit, School District #23 and Tourism Kelowna.

### **Indigenous Engagement**

In keeping with the Truth and Reconciliation Calls to Action, the City is striving to ensure that the development of the 2040 OCP is inclusive and representative of the diverse voices of indigenous communities in the area. The community's vision for Kelowna, which is encapsulated in *Imagine Kelowna*, includes the following goal: *Engage with the Okanagan's traditional past and heritage as foundations for building a fair and equitable community.*

In September 2019, the City engaged the services of a local indigenous consultant to assist in the development and delivery of a process to engage with indigenous communities and to ensure policies are crafted with an equitable lens, in keeping with the Truth and Reconciliation Calls to Action. To date, the City has been in contact with Westbank First Nation, Okanagan Indian Band, the Kelowna Friendship Centre and the Kelowna Metis Association. Consultation is expected to continue throughout the course of the plan development and implementation, and a report to Council will be provided when completed.

### **Refinements to Draft Land Use Plan and Policy**

With the Neighbourhood Expo process complete, staff are taking the following steps to incorporate the feedback into the three planning processes:

- Continue refinement of minor changes<sup>1</sup> to the draft Future Land Use Plan based on feedback regarding specific properties. No fundamental changes to the map or the Growth Strategy are proposed;
- Development of project options for the TMP, as outlined in the staff report dated November 18, 2019 entitled "Transportation Master Plan: Options Development";
- Incorporation of feedback into the development of OCP policies that will guide infill development, recognizing the preferences for housing affordability and support for local services, and the split on options on space allocation for parking and private greenspace; and
- Recognition of a preference for smaller, more local parks as part of a comprehensive process for the identification for new park spaces in the OCP process;
- Preliminary, macro-analysis of the order of magnitude financial costing of the servicing aspect to support the growth strategy. This is the first step in assessing financial commitments to deliver on the growth strategy over a 20-year time period.

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<sup>1</sup> Note: For example, technical changes include amending the FLU of Shadow Ridge Golf Course from Rural Land Use to Public/Service Utilities, and the property south of Munson Pond to Public/Service Utilities to acknowledge the future Waste Water Facility Treatment Plant.

### Next Steps

The 2040 Official Community Plan (OCP) Update process is in Phase 3, of which the major deliverable is a draft OCP document anticipated to be complete Fall 2020.

To complete Phase 3, staff will be undertaking the following:

- *Continued land use map refinement:* As indicated above, staff will continue to make refinements to the draft Future Land Map, which are expected to be minor. A report will be presented to Council in early 2020 that provides an updated map that reflects these changes.
- *New park locations:* With the draft Future Land Use Map taking shape and input received from the public, staff is working towards identification of specific sites for future parks that are needed to accommodate anticipated growth. Staff will report to Council on proposed new sites in early 2020, allowing for their inclusion in the refined Future Land Use Map.
- *OCP Policy Development and Development Permit Area Guidelines:* Staff are in the process of reviewing and updating each of the Development Permit Guideline chapters in the OCP. Some of the Chapters require thorough updates to align with the direction of the 2040 OCP and/or to be updated to align with new regulations and policy (e.g. Energy Step Code).
- *Continuing Engagement:* As part of the OCP policy development, staff will continue to consult with key stakeholders for ideas and feedback throughout Spring 2019. Once these policies are complete and a draft OCP developed, staff will present the draft OCP to Council for its consideration for additional public engagement in Fall 2020.
- *Preliminary Infrastructure Impact Analysis:* Using the draft Future Land Use Map, staff would undertake a macro analysis to assess the impacts of required utility infrastructure, transportation, and parks projects. The integration of a servicing plan and financing strategy (developed as part of a comprehensive OCP update), is necessary to ensure that the Plan is affordable in a form that Council and the community is being asked to support and adopt as a blueprint for future development. There is a general recognition that the cost of new infrastructure to accommodate growth will evoke tradeoffs about how quickly we can implement the Imagine Kelowna and how much future taxation will be able to cost-share the financial burden.

Once a draft OCP is endorsed by Council for future consultation, the project will move into Phase 4, which includes public engagement on the draft OCP document and execute the final refinements to the Plan. The Transportation Master Plan (TMP) will be completed in advance of the 2040 OCP endorsement, however once Council has endorsed the package of options for inclusion in the TMP, an implementation strategy will be developed to be included in the 20 Year Servicing Plan and Financial Strategy.

### Conclusion:

With the completion of this public engagement process, the 2040 OCP update process is moving into a new and directional stage. Where much of the discussion and deliverables to date has focused on high level visioning for the Plan and identification of values and key concepts, the discussion can now shift to

more detailed policies and actions, and ultimately a draft OCP document that Council will be able to consider in 2020. Alongside this next element of core work is the initial financial analysis that correlates the major servicing needs required to service the growth projections as outlined in the growth strategy. Ensuring that land uses and the corresponding servicing strategy are aligned and financially feasible is critical to delivering on the Plan's vision – this will ensure the City is positioned to deliver on the operational, recreational, cultural and safety demands of a growing and thriving city.

**Attachments:**

- 1) Engagement Report: Official Community Plan, Transportation Master Plan, & 20-Year Servicing Plan Updates

**Internal Circulation:**

Divisional Director, Planning & Development Services  
Divisional Director, Partnerships & Investment  
Department Manager, Real Estate  
Department Manager, Development Planning  
Divisional Director, Financial Services  
Divisional Director, Infrastructure  
Divisional Director, Corporate Strategic Services  
Divisional Director, Active Living and Culture  
Department Manager, Integrated Transportation  
Strategic Transportation Planning Manager  
Infrastructure Engineering Manager  
Parks and Buildings Manager  
Communications Advisor

***Legal/Statutory Authority:***

*Local Government Act, Section 471*

***Legal/Statutory Procedural Requirements:***

*Local Government Act, Sections 472-478*

***Existing Policy:***

Imagine Kelowna  
2030 Official Community Plan  
20 Year Servicing Plan  
Council Policy No. 372: Engage Policy

Submitted by: Robert Miles, OCP Project Planner

**Approved for inclusion:**



Danielle Noble-Brandt, Dept. Manager of Policy & Planning



# Engagement report: Official Community Plan, Transportation Master Plan & 20-Year Servicing Plan updates

Fall 2019

**Purpose of engagement:** To inform and consult with citizens and stakeholders on specific directions being taken with the Official Community Plan and Transportation Master Plan, as well as to inform citizens of key directions being taken with the 20-Year Servicing Plan update and Zoning Bylaw update.

**Engagement timeline:** September to October 2019





## Background

In 2018, the City of Kelowna began the process to update its Official Community Plan (OCP) and Transportation Master Plan (TMP). Since then, both projects have undertaken separate and joint public and stakeholder engagement initiatives.

One early step taken was to develop a 20-year growth strategy would set the foundation for these guiding documents. Council endorsed a growth strategy in winter 2019, which identifies generally where future residential growth would be targeted between 2020 and 2040. The growth strategy has guided the draft Future Land Use Plan and other policies for the Official Community Plan, Transportation Master Plan, and 20-Year Servicing Plan.

Public engagement and communication conducted in 2019 has sought to keep residents informed of directions being taken and decisions being made for these plans, as well as to consult with them on key elements in order to inform policy development. This report focuses on feedback received through 2019 public engagement activities and includes brief summaries of stakeholder engagement activities and results.



## Timeline



## Strategy

This phase of engagement was one of several public and stakeholder engagement activities undertaken as part of updates to the Official Community Plan (OCP) and Transportation Master Plan (TMP). As these two plans are being developed concurrently alongside the 20-Year Servicing Plan, the project team chose to combine these three plans in the public engagement process. Engagement focused on consulting with the public and stakeholders on key elements of the OCP and TMP to inform current policy development, as well as informing participants of directions being taken with the 20-Year Servicing Plan update.

## Engagement objectives

- To inform and consult with the public and stakeholders on directions being taken in the draft Future Land Use Map and receive a list of comments that will inform development of the final Future Land Use Map and Official Community Plan.
- To inform and consult with the public and stakeholders on objectives of the OCP Infill Strategy and collect a list of infill housing preferences that will inform development of the OCP Infill Strategy.
- To inform and consult with the public on directions being taken in the TMP and receive a list of transportation options that will be incorporated into options being considered for the TMP.
- To inform and consult with the public on parks development being proposed in the OCP update and receive feedback on parks preferences that will inform refinement of the OCP parks policies.
- To inform the public on the purpose of, and directions being taken in, the 20-Year Servicing Plan, to increase public understanding of the plan.

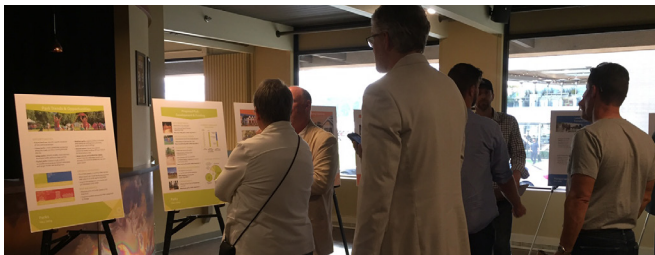
## Engagement techniques

### Public neighbourhood expos

September 19, 21, 25 & 28

Four public in-person opportunities were hosted throughout Kelowna – in the Rutland, downtown, south Kelowna, and midtown areas – featuring displays and an activity passport to collect feedback on a range of topics related to the Official Community Plan (OCP) and Transportation Master Plan (TMP) updates and provide information about the 20-Year Servicing Plan and Zoning Bylaw updates.

The Strong Neighbourhoods team attended three of four events to provide residents with short-term ideas about how to enhance their neighbourhoods.



### Infill strategy stakeholder workshops

June 5, June 27, July 24, September 11

The purpose of these four workshops was to: share with community partners the objectives of the OCP Infill Strategy; explore how to encourage infill housing as a means of increasing housing choice, reducing urban sprawl, and supporting diverse neighbourhoods; and discuss different housing types that are suitable for infill housing in core area neighbourhoods.

### Development Permit Guidelines stakeholder workshop - October 3

The purpose of this workshop was to gather a diverse group of representatives from a variety of design disciplines alongside the development industry to explore a refresh to the City's Form and Character Development Permit Guidelines.

### OCP stakeholder workshop - October 17

The purpose of this workshop was to gather a diverse group of representatives from a variety of stakeholder groups, such as Interior Health, School District #23, the Urban Development Institute and the Kelowna Chamber of Commerce, to explore the draft future land use map as well as the direction for OCP objectives and policies.

### Online

The material from the public neighbourhood expo was converted to a digital format on the City's online engagement platform, [getinvolved.kelowna.ca](https://getinvolved.kelowna.ca). The online participation opportunity was made available for two weeks from September 16 to October 4.

## Engagement results

Please note that results from open surveys such as this are a collection of opinions and perceptions of interested or potentially affected residents and do not represent a statistically significant, random sample of all Kelowna citizens. This report contains results from an open public survey; therefore, due to the opt-in and open method, results are qualitative in nature and cannot be said to represent views of all Kelowna citizens.

Some comments received from the public are very specific and related to specific areas of the city. These comments will be considered individually by staff.

The chosen in-person and online public engagement techniques and topics supported the need to inform and consult on a variety of topics related to current policy development being undertaken as part of updates to the Official Community Plan, Transportation Master Plan, 20-Year Servicing Plan, and Zoning Bylaw. Given the extensive scope of information to share, and feedback to collect, engagement had a strong educational component.



"I LOVE that the City is moving towards establishing Urban Centres and filling in our existing neighbourhoods. We definitely do not need to support anymore new residential development outside of existing neighbourhoods."

"Love the high density in downtown and Landmark urban centres. The mixed use component is very important. I like the continuous of support single family development in growing communities."

"I like the densification of the urban centres, protection of ALR lands, and limiting development in unserved areas."

– Survey comments

## What we heard - public neighbourhood expos

### Land use directions

To help refine the land use directions being proposed in the updated Official Community Plan, staff presented the draft land use directions and land use map and asked the public what they like about the proposed land use directions and what they would change or want to see more of.

#### *Proposed directions that respondents' like*

The most common comments received about what respondents like about the directions include: increasing density, urban centre developments, limiting suburban development in rural areas, protecting ALR, limiting sprawl, and the general direction of the land use map.

Other positive comments about proposed directions referred to mixed-use development, supporting UBC growth, Okanagan College expansion, growth in the hospital area, and protecting heritage areas.

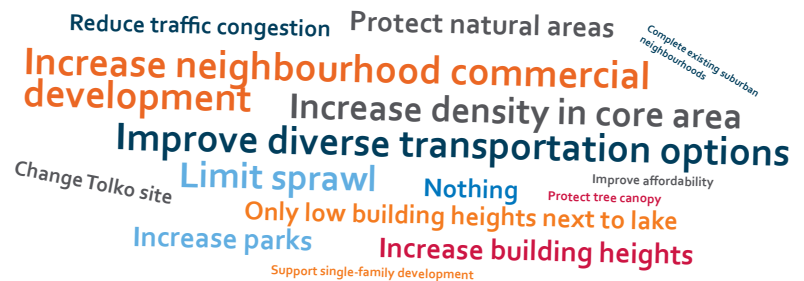
#### Complete existing neighbourhoods



#### *Changes suggested by respondents*

The most common comments received about what respondents want to see more of in the land use directions include: improving diverse transportation options, limiting sprawl, increasing density, reducing growth-related traffic congestion, and increasing commercial development in neighbourhoods (including suburban neighbourhoods).

Other comments related to a desire to increase parks, protect natural areas (including by not developing on hillsides), protect the tree canopy, and allow only low building heights near the lake.



"Stop suburban sprawl. The infrastructural costs are crippling over the long term, it increases congestion & driving, increases homes at risk with wildland fire interface."

"Development could still happen in the suburbs that makes them more sustainable. (small amounts of office, retail and shopping in residential neighbourhoods would be great)."

"Within reason, I believe the areas of the city that allow commercial development need to be expanded... residential neighbourhood (s) would benefit from some mixed residential/commercial buildings."

"Generally in agreement. Still need single family housing... Kids need yards to play in."

– Survey comments

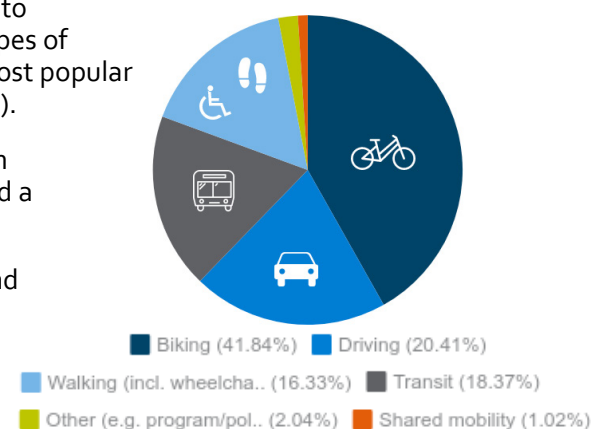
### Transportation challenges and solutions

Participants were invited to place a pin on a map (either in-person or online) to share ideas for walking, biking, transit, driving, shared mobility and other types of improvements. Approximately 135 people provided 242 option ideas. The most popular topics were biking (41 per cent), driving (20 per cent) and transit (18 per cent).

Common challenges identified by respondents include a lack of bike lanes on Gordon Drive north of Springfield Road; congestion in the Midtown area; and a lack of sidewalks in Rutland, particularly around schools.

Common solutions proposed by respondents include installation of a safe and convenient bike connection between the Okanagan Rail Trail and Mission Creek Greenway; increased transit service frequency, particularly to major destinations such as Kelowna General Hospital and UBC Okanagan; extension of Burtch Road or Spall Road toward the south; extension of Clement Avenue to Highway 33; and creation of better alternatives to driving.

#### *Transportation interests*

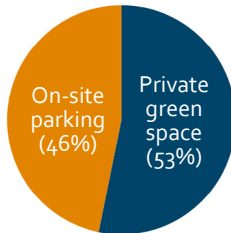


## Infill housing preferences

As infill housing can provide diverse housing choices in areas that are primarily single-family and generate density to support transit and local amenities, the public was asked to identify their priorities for these areas in a series of trade-off questions.

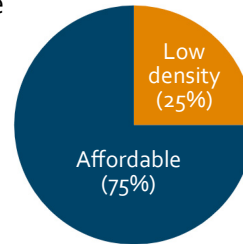
### Private green space and on-site parking

Results for this question are somewhat even, with the majority of respondents (53 per cent) indicating that they would prefer more private green space and less on-site parking on infill property.



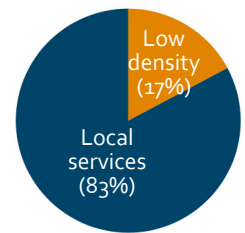
### Density and affordability

When considering that, as a general rule, the greater number of units that can be accommodated on an individual lot, the more affordable those units can be, most respondents (75 per cent) indicated a preference for affordability compared with low density.



### Density and local services

When asked to consider how adding more housing diversity through, for example, townhouses and houseplexes, to support new neighbourhood services in Kelowna's core area, most respondents (83 per cent) indicated a preference for more local services rather than low density.



*"More zoning for infill housing and rowhousing would be appreciated. Re-evaluating use of residential infill such that it is accessible for first time home buyers and shifting workforce will aid Kelowna's growth."*

- Survey comment

*"Kelowna is very spread out and therefore I like that the infill will join the pockets of communities and maybe create some cohesiveness to the community."*

- Survey comment

## Parks preferences

To inform refinement of Official Community Plan policies related to parks acquisition and development, staff asked the public several questions to assess general use of the types of parks in Kelowna and what residents' priorities might be for developing parks.

### Frequency of park use

The parks most frequently visited by respondents (between 1-2 times/month and 4+ time/week) include local parks/playgrounds, linear parks natural areas, and beach parks.

### Parks amenities, size and parking

When asked about their preferences, most respondents (73 per cent) would rather have more smaller, local parks that are within walking distance but with fewer amenities. For programmed sports, respondents were split almost exactly evenly between preferring either one large facility with more amenities or more recreation fields with fewer amenities. Most respondents (75 per cent) indicated they would prefer a park with more amenities and less parking, rather than a park with more parking and fewer amenities.

### Parks budget

Most respondents (59 per cent) indicated that if they could direct the parks acquisition budget, they would rather purchase land for a future park rather than build new amenities into an existing park site.

## General themes

Regarding land use directions, most positive comments from respondents related to the directions being proposed for increased density, signaled growth in urban centres, limiting suburban development in rural areas, and protecting ALR. Top comments regarding suggested changes to the land use directions included ensuring transportation options meet growth demands and limiting sprawl.

There is a strong interest among public respondents to ensure traffic congestion does not worsen as Kelowna's population grows, and respondents proposed several solutions such as connecting roads and improving alternate transportation options. Respondents want to be sure that infrastructure will meet demands of growth.



## What we heard – stakeholder workshops

### Infill strategy workshops

Over the course of four interactive infill workshops, stakeholders voiced strong support for a range of housing types in the City's core area neighbourhoods. The workshop explored how everything from small apartments to duplexes could be supported to achieve the City's growth strategy through infill housing. Further, participants emphasized the importance of delivering improved streetscapes with continuous sidewalks, street trees and green space as densification occurs. Other key directions from the workshops related to support for exploring innovative approaches to parking to promote affordability and encourage more sustainable transportation. Overall, the workshops reinforced that as density occurs it should be accompanied by amenities in the form of nearby parks and attractive streetscapes.

### Urban design stakeholder workshop

Participants provided insights into how development permit guidelines for high-rise, mid-rise and ground-level development can achieve guiding principles of the Official Community Plan and set up Kelowna for success. Discussions focused on issues, priorities, common challenges and constraints in existing guidelines, as well as how the guidelines can be more robust, incorporate energy step code components, and create a local sense of place.

### Official Community Plan stakeholder group

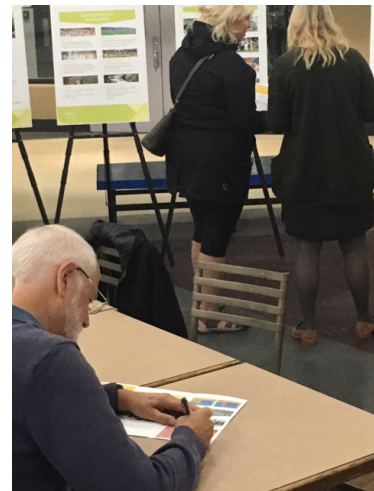
As part of the workshop, participants discussed what types of neighbourhoods, transportation options and amenities should be considered to ensure young people can live, work and play in Kelowna. Major themes discussed included a greater variety of affordable housing with good walkability; more transportation options; more parks, recreational and cultural facilities and programs; greater food security and community health; safety, equity and inclusivity; supporting emerging sharing economy; and economic growth.

Stakeholders also offered specific insights when discussing how the growth strategy pillars, which would act as the foundations for development of the OCP, align with the outcomes described in the previous paragraph.

Participants also provided input on the draft future land use map and specific topics such as growth management, infill and redevelopment, suburban development, industrial lands, education and employment, environment, parks, agriculture and food security, and transportation.

## Outputs & outcomes

- List of transportation challenges and ideas
- List of comments about likes and suggested changes for land use directions
- List of parks use frequency and preferences
- List of infill housing preferences
- Most participants understand the information
- Most participants can provide an informed opinion about the project/s
- 740 "informed" participants who either attended an in-person expo or completed the online survey, not including stakeholder workshops



*"I am glad to have had a chance to offer input. My overall impression is that the people working on this plan know what they are doing. Their guiding principles and values have been well communicated and I support them."*

*– Survey comment*



## Public outreach

Channel	Reach
News release, Sept. 6 (media and email)	3,150 recipients, 38% open rate, 118 unique clicks to project page
News release, Sept. 23 (media and email)	2,158 recipients, 34% open rate, 2 unique clicks to project page
PSA, Sept. 17 (media and email)	3,174 recipients, 37% open rate, 57 unique clicks to project page
Email to Kelowna 2040 subscribers	1,827 recipients, 37% open rate, 50 unique clicks to project page
Email to Get Involved subscribers	421 recipients
CityViews e-bulletin	1,715 recipients, 36% open rate, 3 unique to project page
Newspaper ads (4)	Per paper distribution = ~11,000
Facebook Event pages (4)	5,602 reach, 120 responses
Project video	5,620 views
Paid and organic Facebook posts (6)	9,958 reach, 4% engagement rate
Twitter posts (6)	5,758 reach, 1.3% engagement rate
Instagram posts and stories (~6)	6,584 reach, 6.5% engagement rate
Electronic message boards near locations (4)	Rutland, Hwy 97, Gordon Drive, Kelowna Community Theatre
Posters (various locations)	N/A
Landing page highlights on kelowna.ca and getinvolved.kelowna.ca	1,600 page visits
Direct email to business associations, resident associations, UBC Okanagan, Okanagan College, Accelerate Okanagan, Enactus clubs, and student unions	N/A

## Engagement feedback

Public engagement met objectives to inform and consult with interested members of the public on the various project topics. Nearly all (91 per cent) in-person respondents indicated that they understood the presentation information, while more than half (58 per cent) of all respondents indicated that the material provided enough information for them to provide an informed opinion about the project.

Despite most respondents indicating that they understood the information, a review of feedback received both in-person and online indicates that the online experience was more challenging to complete than the in-person experience. Conversion rates were low for the number of people who attended an expo or visited the online project page then completed the survey; however, attendance rates are considered normal in comparison with past City public engagement projects.

## Next steps

Next steps include further Land Use Plan refinements and detailed servicing impact and costing analysis; further policy development, including development permit guidelines; and development of the draft 2040 Official Community Plan, Transportation Master Plan, and 20-Year Servicing Plan.

Public and stakeholder feedback will help guide and refine policy development related to the Official Community Plan and Transportation Master Plan.



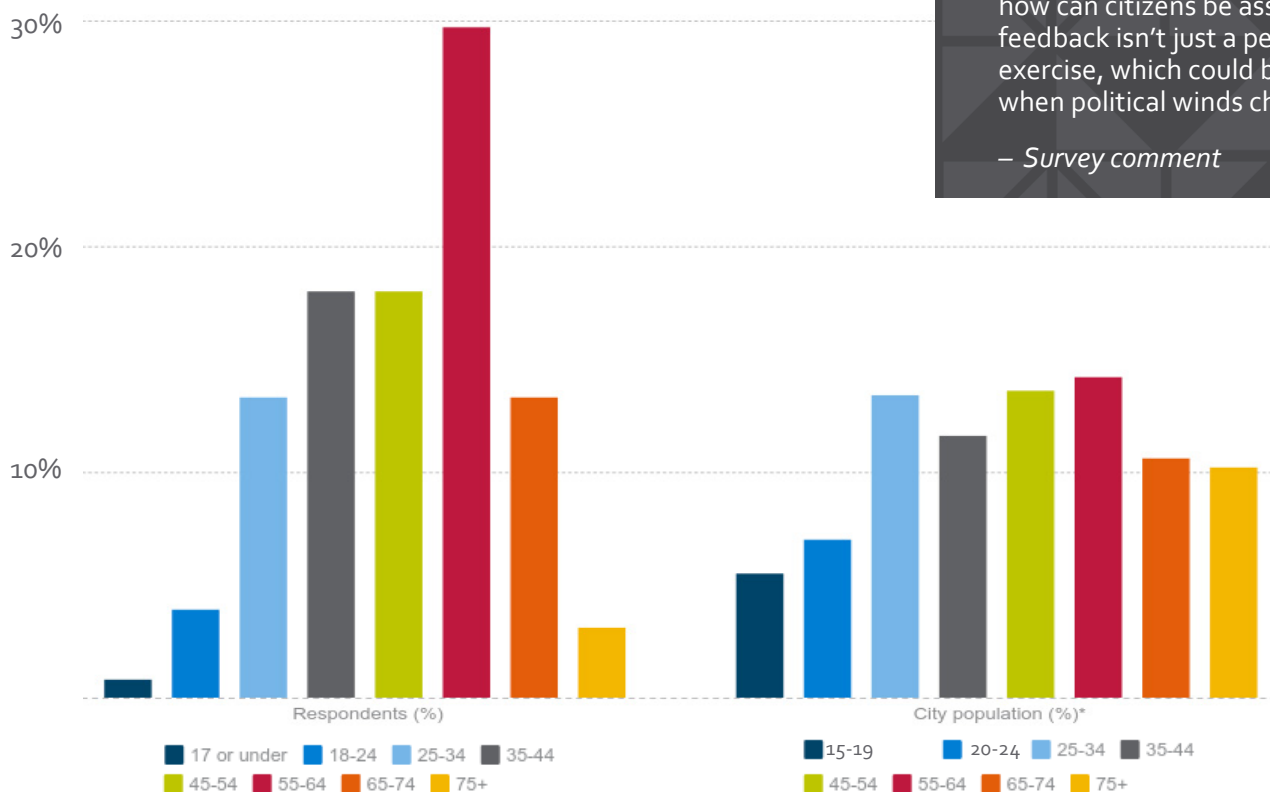
## About the participants

In comparison with the average age demographics within Kelowna, based on 2016 Census data, there was an under-representation of the 17 or under, 18 to 34 and 75+ age groups, as well as an over-representation of the 55 to 64 age group.

Most respondents (46 per cent) indicated that there are two people living in their household, followed by four or more (27 per cent), three (15 per cent) and one (12 per cent).

Most respondents indicated they live in either the V1Y and V1W postal code areas.

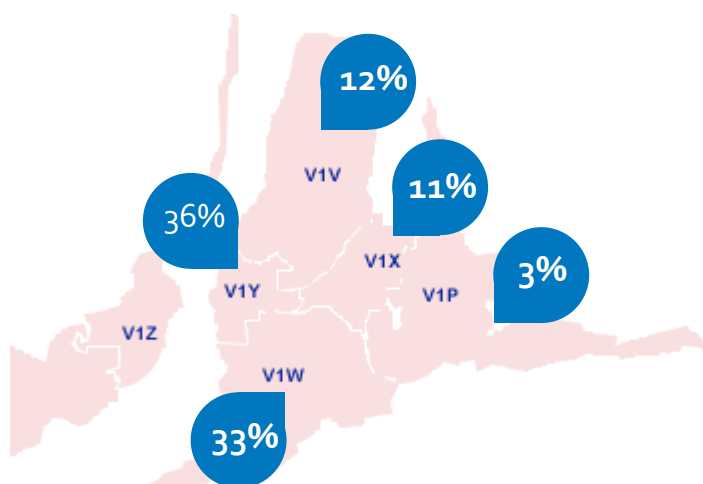
### Age



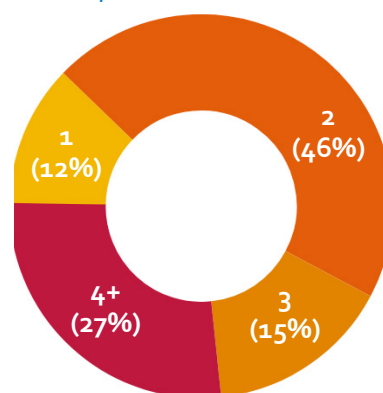
"If you do adopt feedback officially, how can citizens be assured this feedback isn't just a perfunctory exercise, which could be overturned when political winds change?"

– Survey comment

### Respondents' postal code



### How many people live in respondents' households



"I am glad to have had a chance to offer input. My overall impression is that the people working on this plan know what they are doing. Their guiding principles and values have been well communicated and I support them."

– Survey comment

# Appendices

## Appendix A: Public survey comments

### Transportation challenges and ideas

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
2410 Glenmore Road North, Kelowna, British Columbia V1V 2B6, Canada	Biking	There are no bike lanes, a limited shoulder, on Glenmore Drive N, making it an unsafe bike ride. This could be a popular bike route, otherwise.	Install painted bike lanes.
"5549 Airport Way, Kelowna, British Columbia V1V 2S8, Canada	Transit	No direct transit route from downtown to the airport.	Add new route to kelowna transit.
"804 Leon Avenue, Kelowna, British Columbia V1Y 6J7, Canada	Walking (incl. wheelchair/ accessibility)	Vehicles often speed down this residential side street, making it unsafe for people walking or biking.	Lawrence Ave in this same block has been traffic calmed. The same could be done on Leon.
"839b Sutherland Avenue, Kelowna, British Columbia V1Y 5X4, Canada	Transit	Our child goes to St Joseph's, needs to be dropped off between 7-8:30 to before school care, or school. He has not learned to bike, I do not own a vehicle. There is no bus that is convenient. We would have to walk down town to Queensway which is far from our house for young children.	Run smaller sized buses, in more convenient loops.
"1354 Rutland Road North, Kelowna, British Columbia V1X 5E3, Canada	Transit	I used to volunteer teaching English to a family of Syrian refugees who lived out here on weekends. I had to stop because Transit was so inconvenient. Also was an issue when I lived in this area and worked evening shifts. People who work late in restaurants can't afford to live downtown, but can't afford to drive, then the bus stops running. Weekend Transit, as well as after work Transit (after 6) is spaced hours in between routes and I do not drive. The stops aren't well updated on the APPs either. Often stops are actually closed due to construction, and I've ended up almost at the airport. Sometimes you have to wait a few hours in your commute in the library or a coffee shop, just waiting. Hard for people who are busy.	More short buses that run more often, during times working aged people take the bus. Buses don't run late enough, or often enough.
"1100 Lawrence Avenue, Kelowna, British Columbia V1Y 3G9, Canada	Transit	The transit in Kelowna overall is absolutely terrible for a city of this size. Most routes reduce their level of service to once every hour after 6:00pm. That is very early considering that we are trying to be a vibrant city and encourage residents to partake in activities after their work day ends. This is extremely difficult to do when the transit service is so infrequent. The level of service for transit routes is poor.	Move the Transit Yards to a location that can accommodate a higher number of buses. That needs to happen ASAP if we want to see real change in this city and stop being so car-oriented. If the level of service cannot increase, then people will continue to drive their cars and we will be stuck in this transportation rut for many more years to come.
"1748 Large Avenue, Kelowna, British Columbia V1P 1L6, Canada	Transit	Transportation times are terrible. I have just started to go to UBCO as a student, and there are few buses to link up to the bus at Rutland Exchange. I need to come home at lunchtime every day, as I do not have classes in the afternoon. There is a 3 hour block with no buses to Black Mountain. On Sundays, there's a bus at 8:30 and the next is close to noon.	Pretty simple - better bus times, more buses. If we are to feel that Black Mountain is part of Kelowna and vehicular transit is to be discouraged, using transit has to be a possibility.
"1131 Springfield Road, Kelowna, British Columbia V1Y 8T4, Canada	Biking	Biking along Springfield is dangerous, bike lanes too narrow	I would love to see separate bike lanes along Springfield, maybe shared with pedestrians?
"2850 Burtch Road, Kelowna, British Columbia V1W 3P3, Canada	Biking	This is a great cycling link between each section of Burtch Rd, but the rough surface is hard on a road bike.	Pave the path between each section of Burch Rd.
"2850 Burtch Road, Kelowna, British Columbia V1W 3P3, Canada	Biking	This is a great cycling link between each section of Burtch Rd, but the rough surface is hard on a road bike.	Pave the path between each section of Burch Rd.
"3687 Benvoulin Road, Kelowna, British Columbia V1W 4R7, Canada	Biking	The greenway has a lot of potential to be a commuter cycling corridor between the Mission, Midtown, and Rutland, but the rough surface is hard on most bikes.	Create a paved multi-use path on one side of the creek.
"3687 Benvoulin Road, Kelowna, British Columbia V1W 4R7, Canada	Biking	The greenway has a lot of potential to be a commuter cycling corridor between the Mission, Midtown, and Rutland, but the rough surface is hard on most bikes.	Create a paved multi-use path on one side of the creek.
"177 Rutland Road North, Kelowna, British Columbia V1X 6A6, Canada	Biking	Riding southbound on Rutland Rd here, the painted cycle path abruptly ends. Most cyclists end up riding on the sidewalk for both directions. Meanwhile the second northbound traffic lane in this particular section doesn't really seem needed.	Make Northbound traffic single-lane until Mugford to make room for continuous bike lanes.

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"177 Rutland Road North, Kelowna, British Columbia V1X 6A6, Canada	Biking	Riding southbound on Rutland Rd here, the painted cycle path abruptly ends. Most cyclists end up riding on the sidewalk for both directions. Meanwhile the second northbound traffic lane in this particular section doesn't really seem needed.	Make Northbound traffic single-lane until Mugford to make room for continuous bike lanes.
"2605 Ethel Street, Kelowna, British Columbia V1Y 5C1, Canada	Biking	Ethel Street bike path is a fantastic resource, but it's unsafe for children and teens to bike all the way to KSS/KLO Middle, because the bike path ends.	Extend the bike path all the way to Raymer. I don't know how to improve the safety of the access to OC, KSS, and KLO Middle. But someone can figure it out, I'm sure!
"2303 Abbott Street, Kelowna, British Columbia V1Y 5J5, Canada	Biking	Biking from downtown (or even further afield) to the Pandosy Village area would be lovely — and useful — for residents and tourists. The Pandosy area is marred by awful traffic congestion and it would be good to get more people out of their cars and onto their bikes.	A dedicated bike path is the safest and most pleasant way to get from downtown to the Pandosy Village area.
"Queensway Bus Loop, Kelowna, British Columbia V1Y 1P3, Canada	Transit	There need to be more frequent buses in the evening. People should be able to easily use buses to get to/from hockey games, OSO concerts etc. But the buses are so infrequent, this is impractical. We need fewer cars on the roads!	More buses in the evening. Make a bus an easy, practical choice after an evening out downtown.
"4355 Gordon Drive, Kelowna, British Columbia V1W 1B9, Canada	Walking (incl. wheelchair/ accessibility)		
"540 Knowles Road, Kelowna, British Columbia V1W 1H4, Canada	Transit	Frequency of transit . Difficult for UBCO students living at home in Mission area to get to school and back without frequent bus service and direct routes	Direct bus service to College and University for local UBCO/ OK College students
"364 Christleton Avenue, Kelowna, British Columbia V1Y 5H8, Canada	Walking (incl. wheelchair/ accessibility)	Horrible corner for vehicle traffic not separated from walking or bicycle traffic. Only a matter of time until a child is killed at this intersection.	Extend the Abbott Street Linear park from Rose ave to Birch immediately.
"343 Christleton Avenue, Kelowna, British Columbia V1Y 9G7, Canada	Biking	Inter-vehicular traffic and bicycle/pedestrian traffic lack separation. The curbs and sidewalks are nonexistent. Vehicles make U-Turns in the middle of the road to find parking at the hospital.	Need to extend the Abbott Street Linear Park from Rose to at least Birch as soon as possible.
"2250 Abbott Street, Kelowna, British Columbia V1Y 1E1, Canada	Other (e.g. program/policy, education, etc.)	Late evening and middle of the night vehicle traffic and after-bar flush gather and party in the park.	Gate the park like all of the other pocket parks along abbott st.
"1097 Paret Crescent, Kelowna, British Columbia V1W 4P2, Canada	Transit	transportation options to grade schools is inadequate. The SD has limited the availability of school buses to grade schools in the district to those over 4 km The nature of the hill sides and the location of the schools leave driving their children to schools (sometimes multiple schools depending on age) and this only adds to congestion. The city bus does not have enough route to meet the needs of the neighborhoods to use this as an option to attend school.	"traffic congestion will only increase as the growth increased unless the city adds transportation - even in the less dense areas, including mandating school bus options (even if there is a user fee)  I am sure this issue is not limited to the Mission. Transit and bussing needs to be increased immediately. the city should be planning for future growth by adding some type of space for rapid transit later (plan corridors and land acquisition now as in time it will only become more costly."
"3802 Gordon Drive, Kelowna, British Columbia V1W 3Y3, Canada	Biking	"Speed of traffic Unprotected bike lane Could be a great bike corridor for commuters but cyclists don't feel safe along here."	Separated / protected bike land. The sidewalk is wide enough to accommodate a shared bike/walking lane. only a sidewalk and an unprotected bike lane. This is a busy and dangerous road with speed issues
"1301 Glenmore Road North, Kelowna, British Columbia V1V 2H1, Canada	Biking	There is currently no separate bike path from Snowsell St N to John Hindle DR.	Adding a bike path on one side of the road would encourage people to ride to the university for work or for education. Something like what is currently on John Hindle DR would be perfect.
"771 Raymer Avenue, Kelowna, British Columbia V1Y 2R6, Canada	Biking	There is currently no separate bike path on Raymer Ave from Richter St to Gordon DR.	Adding a bike path similar to what was just installed on Southerland would get more families out and make it safer for kids to ride to school. The number of people riding to work in the Pandosy area would also increase.
"1175 Gordon Drive, Kelowna, British Columbia V1Y 7E3, Canada	Biking	Currently it is slightly unsafe and confusing when crossing from the rails for trails towards downtown to go on the freshly paved rail line.	Improvement of Rails with Trails Phase 1 connection to new rail trail heading towards downtown. Adding better signage and cleaning up the road on either side would make it safer currently. In the future, a bridge crossing like the one across the highway by Parkinson Rec Center could be built where the rail line used to cross Clement.
"396 Woodpark Court, Kelowna, British Columbia V1V 2L2, Canada	Biking	Chain and fence impossible to maneuver on bike. Currently there is a chain across most of the passage with a small walk through fence on the side. It is impossible for a bike to ride through. I have seen many moms with strollers have to lift up and over the chain because they do not fit through.	Adding a wider part for bikes and strollers to pass through would help people who want to access Knox out of Magic.
"Okanagan Rail Trail, Kelowna, British Columbia V1V 2K6, Canada	Biking	Removing fence and negotiating a passage for the rail trail. The highway to Lake Country is not super safe for riding and another option would be appreciated.	Discuss with stakeholders who need to be influenced that this is a good idea that will help Kelowna in the long run. In the future, this could be paved to improve access for everyone.
"4719 Lakeshore Road, Kelowna, British Columbia V1W 4H6, Canada	Biking	Bike path from Collett RD to the corner of Lakeshore and Barnaby RD. There is currently a small shoulder which is usually filled with gravel and glass.	Adding a bike path similar to what is on Southerland on the right hand side of the road would aid safety of cyclists.

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"1050 Raymer Avenue, Kelowna, British Columbia V1Y 8H8, Canada	Walking (incl. wheelchair/ accessibility)	Currently there is a smoke pit from the local high school which often blocks the public access path.	Remove smoke pit from CoK property(its a park) and/or install signs and enforce use of area as a walk through not a smoking area
"880 Leathead Road, Kelowna, British Columbia V1X 6S5, Canada	Biking	Lack of bike path connecting the new Rail Trail to Rutland area.	Start adding bike lanes/ separated bike paths to make it safer for those from Rutland to ride downtown or the other way around too.
"886 Raymer Avenue, Kelowna, British Columbia V1Y 4Z9, Canada	Driving	This intersection can become very busy and confusing.	Adding a roundabout here would reduce traffic congestion and make this intersection safer.
"2301 Ethel Street, Kelowna, British Columbia V1Y 9W8, Canada	Driving	Busy intersection with confusion.	Either convert this intersection to a two way stop or a roundabout.
"1101 British Columbia Route 97, Kelowna, British Columbia V1Y 1H2, Canada	Walking (incl. wheelchair/ accessibility)	This applies to many major intersections. I'm just picking this one as it's local to me. Walking across this intersection is hazardous. In the summer because cars make poorly informed turns on a yellow light and pedestrians are often also hazards in themselves. Walking is hazardous or even IMPOSSIBLE in the winter due to snow removal priorities. Pedestrian traffic (not to speak of strollers, walkers, and wheelchairs) are degraded to 3rd-rate citizens. Many folks (those afraid to break a hip, elementary school students, wheel chair users, stroller pushers) feel FORCED to not use the pedestrian walkways.	"Re-align priorities regarding snow removal. Sure, the roads there are ""priority roads"" but the sidewalks are also ""priority sidewalks"". I ESPECIALLY am offended by intersections (even plowed intersections) being made unusable for days on-end because of the stree snow removal! Aggressively enforce yellow light best-practice. Perhaps build more over and underpasses (I don't know if the one by Parkinson Rec is successful or not) so that there is essentially better through traffic, ie cars don't need to wait for pedestrians and pedestrians don't need to hope."
"1120 Bernard Avenue, Kelowna, British Columbia V1Y 8L7, Canada	Transit	A bus may or may not show up on time. Because of that an unreasonable amount of extra time needs to be planned into a trip. Sometimes a bus arrives (or blasts through) early - but then again it's maybe just extremely late. Either way, having to count on the bus being usable is extremely time consuming - time which folks with other options aren't willing to invest.	Figure out how to ensure that all buses are on time. More than 5 minutes late is very much too late. More than 1 minute too early is very much too early. Synchronize the busses so that transfers are realistically possible. Create an atmosphere where this is the expected service standard. Look at other jurisdictions (probably outside of North America) where the the bus service is usable for folks on a schedule. Feel free to even decrease bus service to meet this fundamental goal. Probably standardize times.
"1075 Bernard Avenue, Kelowna, British Columbia V1Y 8L7, Canada	Biking	This is an example of something good. I appreciate the new lines where bikes have a dedicated space and cars are forced into a narrower lane. I even appreciate it as a car driver as the lane is simplified.	
"1415 Westside Road, West Kelowna, British Columbia V1Z 3M5, Canada	Biking	No space for single or a family of cyclists	Other forms of transport other than cars seems to be forgotten. If it wasn't so difficult and slow to ride I would not drive to work. In areas with not enough density to support public transport why not add a bike lane to start getting cars off the road. Separated bike lanes to support west Kelowna bike commuters
"700 Ellis Street, Kelowna, British Columbia V1Y 7R5, Canada	Transit	Heavy industry in an emerging tourist area	Turn the space into a new transit hub connecting the north and west of Kelowna to the urban core with a new green bridge with bike lanes, bus and or rail lines. The space has significant value to the future growth of Kelowna and would be best suited to other greener, smarter uses to best cater to future generations, recreation, entertainment, living needs.
"1810 Gordon Drive, Kelowna, British Columbia V1Y 6A8, Canada	Biking	Dangerous to travel via bicycle.	Bike lane on Gordon from Springfield to Clement. Add bike lane on Gordon from Springfield to Clement. Currently great cyclist access to this area but dangerous along this stretch of road.
"Blenz Coffee, 297 Bernard Avenue, Kelowna, British Columbia V1Y 6L3, Canada	Biking	bike racks are often full and/or unsuitable for proper U-Locks	Add bike parking spaces downtown.
"4091 Lakeshore Road, Kelowna, British Columbia V1W 1A4, Canada	Biking	No bike path from Bluebird beach to Hobson Rd.	Adding a bike path similar to what is already along Lakeshore would get more people out and riding.
"2955-2957 Conlin Court, Kelowna, British Columbia V1Y 2R8, Canada	Shared mobility	Skinny bridge crossing creek and rough path.	Adding to the existing bridge or creating one that is wider would let people with strollers pass. The path along the fence at the treatment plant is currently rough and not very well taken care of, so either paving it or just fixing it up would help.
"3031 Abbott Street, Kelowna, British Columbia V1Y 1G8, Canada	Other (e.g. program/policy, education, etc.)	Speed of cars on this section of Abbott.	Introduce some traffic calming measures, and ensure the bike lane is clear of speed bumps.
"905 Ellis Street, Kelowna, British Columbia V1Y 1Z1, Canada	Biking	Rough and messy shoulder.	Add bike lane lines and cleanup shoulder from Bay ave north to the base of Knox. A multi use bike path like what is on Abbott would be great for in the future.
"Smith Avenue, Kelowna, British Columbia V1Y 1J1, Canada	Walking (incl. wheelchair/ accessibility)	Art walk incomplete.	Finish artwalk to connect to Doyle Ave cutting through old RCMP property.



Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"British Columbia Route 97, Kelowna, British Columbia V1Y 5W2, Canada	Biking	There is fence that makes it very hard to get onto bridge.	Remove fence/railing to make access easier. There is no way for a car to get to here so it has now become redundant.
"1600 Abbott Street, Kelowna, British Columbia V1Y 5W2, Canada	Walking (incl. wheelchair/ accessibility)	Skinny, steep bridge is a challenge to cross in both directions.	Redoing bridge to be wider and easier for bikes, walkers and strollers to cross would help!
"1039 K.L.O. Road, Kelowna, British Columbia V1Y 4X8, Canada	Other (e.g. program/policy, education, etc.)	Currently there is no real "deal" for students to get a bus pass.	Partner with BC Transit to price better and increase use of transit system.
"454 West Avenue, Kelowna, British Columbia V1Y 1M5, Canada	Biking	No route from Abbott to Raymer Elementary that is a separate bike path.	Change alley to a one way alley and add a two way bike path on the south side of the lane. This would be a simple fix by painting lines on the ground and/or concrete barricades.
"1503 Macleay Crt, Kelowna, British Columbia V1Y 3N2, Canada	Biking	No separate bike lane up Clifton Rd to the pedestrian cut through to Sonora Dr/up Clifton Rd.	Do a bike path similar to what has been installed on the south end of Clifton Rd all the way up
"1891 Aitkins Crt, Kelowna, British Columbia V1Y 1W5, Canada	Other (e.g. program/policy, education, etc.)	Lack of signage/ knowledge about bike route and walking route through Glenmore.	Add signage to improve use of pathways and back roads to keep cyclists and walkers safe and off of busy roads.
"Okanagan Rail Trail, Kelowna, British Columbia V1Y 9N5, Canada	Transit	Lack of different future transportation.	Planning to install a raised LRT system that connects UBCO, the airport, Rutland, downtown, South Pandosy and the mission(CNC?) along the rail trail would connect the city better and more efficiently.
"Central Okanagan Bypass, Kelowna, British Columbia V1Y 8H2, Canada	Transit	Bus center and City yards	Connect Clement Ave through where the city yards are past to Dilworth Rd. Move the City Yards and bus center to by UBCO.
"1721 British Columbia Route 97, Kelowna, British Columbia V1Y 6G3, Canada	Transit	Lack of integration between urban development and transit corridor	Encourage dense urban development along rapid transit stations on the Harvey Corridor to encourage transit use. Encouraging density along transit lines will strengthen the argument for LRT.
"1130 Brookside Avenue, Kelowna, British Columbia V1Y 5T4, Canada	Biking	Problem - loss of bike lane on Gordon Drive . The bike route along Gordon Drive has a major break in it between Springfield and Bernard. It is a very dangerous place to bike.	I recommend at minimum creating a bike lane, ideally creating a shared use path (as on Ethel).
"Scotiabank, 101-1835 Dilworth Drive, Kelowna, British Columbia V1Y 6H4, Canada	Biking	Selected this intersection but most intersections with the highway have the same issues. Safely crossing the highway on a bicycle. Most roads that have a bike lane cease to have a bike lane as they cross the intersection. The pedestrian island often juts out into the highway.	Continue the bike lanes through the intersection.
"315 Glenmore Road, Kelowna, British Columbia V1Y 1V6, Canada	Biking	This comment is about Glenmore Road in general. The road has multiple changes from bike lanes to shared lanes to no bike lanes making it a dangerous commute.	When upgrading roads, be consistent with the bike route along the entire length of the road. This is especially true for long roads that traverse the city. This would make it much clearer for both drivers of motor vehicles and riders of bicycles.
"841 Rose Avenue, Kelowna, British Columbia V1Y 5K3, Canada	Biking	Bike lane from Ethel to Pandosy along Rose is currently not functioning well/safely. Because of the hospital staff parking lot at Burnett, pedestrians walk on the bike path (as it is on the hospital side of the road) and make it difficult and sometimes impossible for safe cycling. This is very dangerous in the winter for both cyclists and pedestrians.	Recommend prioritizing a multi-use path from Ethel to Pandosy and maintaining it in the winter the same as the paths on Ethel.
"1390 Glenmore Drive, Kelowna, British Columbia V1Y 6S3, Canada	Biking	To increase the number of people who commute by bike in the winter, a multi-use path that is ploughed in the winter along Glenmore would be great. In the winter, the bike lanes are never ploughed along Glenmore and the sidewalks are rarely cleared as well. This makes cycling challenging and dangerous along Glenmore until a rider can access the multi-use path along Clement.	Initially, ploughing the sidewalks along Glenmore southbound would help. Longterm, creating a multi-use path like the one along Clement and ploughing it daily as needed.
"2206 Longhill Road, Kelowna, British Columbia V1V 2G2, Canada	Walking (incl. wheelchair/ accessibility)	Sidewalks on longhill. Kids want to walk to school and from dilworth to Glenmore amenities. Too busy.	Sidewalks
"1875 Mckinley Road, Kelowna, British Columbia V1V 2P8, Canada	Driving	from this point, connect a new road to John Hindle Drive, supporting new development in Wilden and access from the North. this number of additional homes needs a major fire exit route, Excludin Clifton Rd	"If you can build Upper Canyon Road, you can build this access; rom this point, connect a new road to John Hindle Drive, supporting new development in Wilden and access from the North "
"1480 Guisachan Road, Kelowna, British Columbia V1Y 7X7, Canada	Driving	This roundabout is a dangerous intersection. People enter it driving too fast; they don't look right or yield right of way; and cars enter the circle tail-gating creating a non-stop stream of traffic (from Guisachan Rd & Byrns Rd) never giving space for Burtch Rd traffic to enter until all traffic on Guisachan/Byrns has cleared. I know of two neighbours who have had accidents in this circle. I dread driving through this circle for fear of an accident due to inconsiderate fast drivers.	"1. Enlarge the traffic circle to give more space inside the circle to allow more space between entrance/exits. 2. Put speed bumps at the circle entry ways to force cars to slow down (they don't pay any attention to the signs)."

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"2175 Benvoulin Road, Kelowna, British Columbia V1W 2C7, Canada	Biking	High traffic retail area.	City should make a safe bikepath connection from Mission Creek park area to Rail Trail pathway, for those traveling to UBCO or beyond from South Kelowna. Create at least one safe two-way bike corridor, perhaps removing one lane of car traffic, which could both dissuade some drivers and provide a safe alternative for cyclists. Incorporate a walking path into the protected route to try to see if multiuse safe path can be made with only narrowing of traffic lanes.
"4105 Gordon Drive, Kelowna, British Columbia V1W 2Z6, Canada	Transit	Currently people don't take transit much because it requires 3 buses to get from this large residential area to UBCO. You need to invest in a convenient option for transit, and preannounce it, so people will consider using this modality rather than driving, or even moving.	Put a direct express bus route from Recreation Centre area, which is serviced by buses from the whole Lower and Upper Mission area, to UBCO and Airport. Run direct express buses from a central Mission area to UBCO especially weekdays from 7 am to 9am and 3 pm to 7 pm. Less frequent service between high commute times could allow some to transit to airport rather than using personal cars or taxis.
"2265 Pandosy Street, Kelowna, British Columbia V1Y 1T2, Canada	Transit	One of the city's biggest employers but with inconvenient transit from many large residential areas. People drive here and park because they have to take multiple buses from residential areas to get to work on time, and this is time-consuming, inconvenient, and unreliable.	Survey hospital staff to find out what areas people commute from and how many would take the bus if they had only to take a single bus. Find out what peak times people would need to travel, which is not necessarily identical to business employers, due to shift work. Run more buses from the most populous neighbourhoods, direct to the hospital area, at high travel demand times. Incentivize transit use by providing transit pass sales at KGH, with a discount for hospital employees, and by increasing the prices for parking, other than for EVs, to offset those increased costs. This option will also appeal to those visiting patients, potentially, and that would be a bonus, rather than the main users.
"207 Bernard Avenue, Kelowna, British Columbia V1Y 1A8, Canada	Other (e.g. program/policy, education, etc.)	Approach area businesses and hotels to raise funds for a shared electric vehicle shuttle to bring tourists to main area attractions and shopping Too many vehicles on the road, at parks, parking on shared streets, causing congestion, emissions, road wear, air pollution, accidents, and depriving cyclists and walkers of safe room on streets.	As above. Provide business co-funded clean transit and give out tourism transit passes for 1 week, including on public transit and this perhaps seasonal service. Target location for shuttle service could vary by season, depending whether it's beach season, rainy season, winter ski/snowshoe/skate season, wine tourism festival, etc. The small costs to offer free public transit to visitors for one week may reduce a lot of congestion and limit the need to continue expanding parking lots by all our parks and beaches.
"775 British Columbia Route 97, Kelowna, British Columbia V1Y 2M5, Canada	Transit	transit on 97 that takes over two lanes, goes through directly, has priority over all cars, and is therefore quicker, and reduces noise and pollution of traffic on 97. how to make Kelowna a transit city. Make 97/Harvey the up and down, and then have feeders (small busses, etc.) that deliver people. Those who live on Richter, Pandosy, Gordon, in Glenmore, on Burtch, and anywhere else, who want to take public transit, need to have frequent, reliable service. Presently, all those streets are little versions of 97, and even the large busses (1) on Pandosy are a joke! Too infrequent.	Make 97/Harvey the up down road, dedicated lanes for busses, tram, so they are quicker than cars, and make feeders from all other streets (also with dedicated lanes for mini-buses, etc). make transit more frequent, better, if the mini feeder is full, but another is coming in five minutes. people will accept that. A big bus every 30 minutes, or less, is worthless. I take the bus whenever I can, but never rely on the 1, the 8, or the 5 gordon bus
"4265 Lakeshore Road, Kelowna, British Columbia V1W 4S9, Canada	Biking	separate bike lane all along lakeshore, especially between bluebird and dehart	add bike path
"4265 Lakeshore Road, Kelowna, British Columbia V1W 4S9, Canada	Walking (incl. wheelchair/ accessibility)	sidewalk all the way from the roundabout on bluebird and dehart there is some sidewalks but they cut off. there is not a sidewalk the walk and its dangerous on such a busy road	adding a connecting sidewalk the entire way
"4066 Lakeshore Road, Kelowna, British Columbia V1W 1V6, Canada	Walking (incl. wheelchair/ accessibility)		sidewalks need to connect here
"1992 Dilworth Drive, Kelowna, British Columbia V1X 5X7, Canada	Transit	Most shopping centre and grocery store are near orchard park mall. but There is no transit going through Benvoulin Rd. I live in upper mission. To go to orchard park mall or any major shopping centre, I need at least 3 buses. 1) go to mission rec. 2) go to downtown 3) go to orchard park mall. It takes 90 minutes to go there.	There should be bus routes going through Benvoulin Road. Then I do not need to go to downtown to go to Rutland.
"Frost Road, Kelowna, British Columbia V1W 4M4, Canada	Transit	There is no buses between kettle valley and south ridge/ the ponds. There is limited school buses from south ridge to chute lake elementary school and from kettle valley to canyon falls middle school. Lots of students should walk 4-5 km regardless weather condition because there is no city buses.	City buses should be available between kettle valley and the ponds, especially before school start and after school end.
"855 Lexington Drive, Kelowna, British Columbia V1W 3G4, Canada	Transit	I live in upper mission. To go to UBCO, it takes 2 hours because I need to go to downtown to change buses. 1) going to mission rec. 2) going to downtown Queensway exchange 3)UBCO	There should be express buses from Mission Rec exchange to UBCO for students who live in mission area.

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"417 Rio Drive South, Kelowna, British Columbia V1V 2L2, Canada	Transit	Having the bus system come up to magic estates	
"870 Ethel Street, Kelowna, British Columbia V1Y 2S8, Canada	Biking	To get cyclists/pedestrians from the Ethel St pedestrian/ cycling corridor to the Knox Mtn area	Complete a short paved path from the end of Ethel to Trench to connect the Ethel St corridor to the Knox Mtn park area.
"249 Black Mountain Drive, Kelowna, British Columbia V1P 1S1, Canada	Biking	Create a suitable route for cyclists/pedestrians to connect from Blk Mtn to Swainson	Paved path (no cars) for cyclists/pedestrians from Blk Mtn Dr to Swainson for recreational cyclists/pedestrians.
"1952 Union Road, Kelowna, British Columbia V1V 2E8, Canada	Transit	The number 6 bus consistently is too full and leaves behind students trying to go to UBCO at this stop and the two stops after it.	Increase the number of buses going through at peak times from every half hour to every fifteen minutes. Or bring back the double decker buses
"979 Raymer Rd, Kelowna, British Columbia V1W 3B4, Canada	Biking	A lot of kids from Crawford will be going to Canyon Middle School, but there doesn't seem to be a good active way to get there.	Any kind of public pedestrian crossing, either just to cross the river, or a pedestrian suspension bridge, would be really cool.
"979 Raymer Rd, Kelowna, British Columbia V1W 3B4, Canada	Biking	A lot of kids from Crawford will be going to Canyon Middle School, but there doesn't seem to be a good active way to get there.	Any kind of public pedestrian crossing, either just to cross the river, or a pedestrian suspension bridge, would be really cool.
"1771 Cooper Road, Kelowna, British Columbia V1Y 9X4, Canada	Driving	Allow the left turn arrow at Cooper road to be enabled during rush hour.. traffic is already high volume at all hours of the day and during rush hour having the left turn arrow signal at Cooper road disabled creates a jam in the left westbound lane because left turn traffic lane is so backed up trying to turn south onto cooper road that it bask up onto the left through lane. Creates greater accident risks for the left turners as well as the left lane through traffic that hits an unexpected stop during a green and reduces overall traffic movement because the left lane becomes impeded for flow.	Keep the left turn arrow enabled at all hours of the day.
"1755 Burtch Road, Kelowna, British Columbia V1Y 9K8, Canada	Driving	Rush hour traffic backlogs for traffic crossing Burtch northbound across the highway. Northbound traffic on Burtch cut off from being able to cross the highway during green light because left-turning northbound vehicles having to yield to oncoming traffic back up onto the single through lane.	Either add a northbound left turn arrow on the traffic light at the Burtch highway crossing, or make the Burtch northbound left turn lane longer, so it doesn't impede the single lane of through traffic trying to cross the highway northbound.
"Clement Avenue, Kelowna, British Columbia V1Y 8T6, Canada	Driving	Clement needs to be extended to Dilworth then to Leathead then McCurdy/Hwy 97. Enterprise is a mess. Enterprise is congested. Springfield is getting worse.	Extend Clement in phases. Needs to start immediately. Secure property now! Grade separate at Glenmore and Dilworth. The idea is to get people from the downtown to the airport quickly and also other destinations avoiding the Highway 97.
"Central Okanagan Bypass, Kelowna, British Columbia V1V 1H7, Canada	Driving	money. Need to secure property now for Bypass route.	Spend less money on downtown 'streetscapes'.
"1825 Richter Street, Kelowna, British Columbia V1Y 2M9, Canada	Driving	Need to implement the Richter-Pandosy one way couplet between downtown and the Mission (Richter/Lakeshore). buy in from the public.	Tell the public how much time they will save instead of sitting in traffic. Promote the safety benefits of a one way street. Go to the residents first, NOT the businesses.
"1120 Bernard Avenue, Kelowna, British Columbia V1Y 8L7, Canada	Biking	Staying alive on my bike while on Gordon	Need a safe cycling route on Gordon. Need more north-south safe cycling routes like Ethel Street. The side walk on Gordon from Hwy 97 south to Springfield is wide enough for a off-road cycle path.
"1285 Ethel Street, Kelowna, British Columbia V1Y 2W7, Canada	Biking	continue the off road bike path from Cawston to Clement. Also put in a push-button to allow crossing at Ethel and Clement. Dangerous to cross Clement on Ethel. no light and +++ vehicle traffic.	Put in a push button light.
"1149 Sutherland Avenue, Kelowna, British Columbia V1Y 3H9, Canada	Biking	Getting into the Capri Shopping Centre from Sutherland Ave is from the west is dangerous. Too close to the entrance of Capri Shopping Centre from Gordon.	Something safer...the transportation engineers will know a safe cycling path.
"1749 Abbott Street, Kelowna, British Columbia V1Y 1B4, Canada	Biking	This is the worst intersection in Kelowna for cyclists. The wait is too long and when going north the merging into the centre lane is tricky. The bike lane is non existent on Abbott between Hwy 97 and Leon.	bike lane
"1687 Water Street, Kelowna, British Columbia V1Y 6C2, Canada	Biking	going north across Hwy 97 and merging into the lane going straight is tricky. on Water St. - Hwy 97 north to Leon.... merging into the lane that goes downtown to Bernard is dangerour.	

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"330 Boyce Crescent, Kelowna, British Columbia V1Y 1K2, Canada	Biking	Water St. near the Hwy 97 on south side is dirty, bumpy and often glass. Seeing the homeless at the old McDonalds is also depressing. avoiding getting a flat from the glass on the road.	clean up this area
"539 Sutherland Avenue, Kelowna, British Columbia V1Y 5X3, Canada	Biking		I love this new bike lane (albeit 2 directions)...thanks for making Sutherland Ave between Ethel & Pandosy so much safer !! to get it finished....supposed to be paving today. Yeah
"1859 Ethel Street, Kelowna, British Columbia V1Y 2Z4, Canada	Biking		love the Ethel Street off road bike path....so smooth too (compared to north side of HWY 97 on Ethel)
"2025 Springfield Road, Kelowna, British Columbia V1Y 5V7, Canada	Biking	difficult when cycling east to make a left hand turn as +++ traffic despite the turning lanes as so many cars going very fast.	
"1755 Dilworth Drive, Kelowna, British Columbia V1Y 8R1, Canada	Driving	Constant traffic & short distance between Highway 97, Enterprise, & Leckie roads. Traffic along Dilworth is constantly a hazzard to Highway 97 as there is not enough room for the vehicles travelling north bound. They back up onto the highway as people are not willing to wait for another light change. People cross from the far right to the left hand turn lane onto Enterprise.	This section of Dilworth road from Highway 97 to Leckie should be 2 lanes north bound. This will allow more vehicles to pass through the Dilworth & Enterprise intersection quickly, leaving fewer vehicles waiting in the highway intersection for the Enterprise traffic light to turn green.
"Okanagan Rail Trail, Kelowna, British Columbia V1Y 9N5, Canada	Driving	The rail trail crossing of Dilworth often results in traffic backing up & near misses of accidents. While the use of the rail trail should be encouraged, this crossing is constantly activated, thereby impeding traffic, increasing driver frustration, & is an accident waiting to happen.	The rail trail crossing should be changed to avoid crossing Dilworth Road. Either a tunnel under the road or an overpass over the road. This will allow better traffic flow, increase rail trail user safety, & decrease the risk of a deadly accident.
"3441-3443 Benvoulin Road, Kelowna, British Columbia V1W 4M5, Canada	Transit	This pin is intended to be in the vicinity of St. Charles Garnier Church. There's no remotely-convenient bus from Glenmore Road to this vicinity, nor is there a way to safely walk along this stretch of Benvoulin.	Expand bus service and transition to electric buses. Maybe more buses, but smaller and electric, would help.
"2255 Springfield Road, Kelowna, British Columbia V1X 7N7, Canada	Driving	Turning south off springfield onto dilworth is dangerous. the back up for the turn lane is often past durnin. traffic back up and safety issues	maybe a double turn lane would assist this issue.
"2649 Benvoulin Road, Kelowna, British Columbia V1W 2E2, Canada	Biking	biking and walking along benvoulin is extremely dangerous. fast moving traffic and no protection for humans on small "bike" portion of road. i won't allow my family to use this route unless in a car and worry about the students, elderly, disabled and sports enthusiasts using this route as it is treacherous and dangerous.	need dedicated, protected (barrier) and wide bike/human lane like along lakeshore. this would help connect south/mission to existing SAFE bike routes.
"4551 Stewart Road West, Kelowna, British Columbia V1W 4N5, Canada	Biking	we live on this road and have witnessed for years the high speed, reckless driving along stewart road (speeds in excess of 80-100km regular). we've witnessed many crashes into neighbouring properties including ours and close calls with humans/animals. the hill is blind with minimal shoulders. with the new improvements this has worsened. this is a key bike route for locals and tourists and heavily used agricultural area. in addition to countless bikers/joggers as well as equestrian riders, this is an agricultural area with many farm vehicles, large animals, large animal transportation vehicles coming in/out of driveways. it's also a main route to a family residential area with many kids on bikes and school buses, etc. when quail's opens there will be more traffic of vehicles and bikers. we've tried to warn the city for years that tragedy is inevitable unless speed is reduced."	large speed bumps like on byrns or narrowed borders like gordon south OR other methods need to be put in place as soon as possible.
"3765 Casorso Road, Kelowna, British Columbia V1W 4M7, Canada	Biking	casorso is an unsafe road for bikers/walkers. i often see bikers, joggers, dog walkers, children, disabled travelling on the very narrow shoulders of this busy road.	a dedicated, protected and safe lane is needed to ensure the safety and encourage more bike commuters
"3631 Gordon Drive, Kelowna, British Columbia V1W 5B4, Canada	Biking	safety along gordon for anyone not in a vehicle is a big issue. many people in our neighbourhood won't think of biking to work as it's too dangerous to ride a bike along most routes from east, south and southeast kelowna to midtown or downtown.	to protect those bikers and disabled people using this transportation route, a safe, protected dedicated lane is required like on lakeshore or ellis
"855 Dehart Road, Kelowna, British Columbia V1W 4Z9, Canada	Biking	commuters, recreational riders, and kids from east kel/crawford aren't able to bike to h20/schools due to unsafe roads	need safe and protected dedicated lane for bikers/walkers

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"2855 Burtch Road, Kelowna, British Columbia V1W 2G7, Canada	Driving	burtch around the dolphins/midtown is a nightmare for traffic and non vehicle transport. gordon and benvoulin are backed up .	direct route on burtch from guisachan to klo would help a lot with dolphins/mid town traffic heading south - good opportunity for dedicated/safe bike lane to KLO
"2150 Spall Road, Kelowna, British Columbia V1W 2X7, Canada	Driving	spall is a key street and to get south have to detour to gordon or benvoulin causing traffic/congestion in both and problems at intersections of springfield and gordon/cooper	extend spall south to byrns. also include safe and dedicated bike lane which will help riders link with rail trail to UBCO
"2737 Shayler Road, Kelowna, British Columbia V1V 2P8, Canada	Driving	Entering Shayler Rd from Shayler Ct, visibility is limited due to the curve in the road. Visibility is further decreased by the hill, and when coupled with excessive southbound travel speeds, the City have a dangerous intersection.	Install speed humps on Shayler Rd at several locations north of Shayler Ct. and north of Shayler Place.
"2979 Pandosy Street, Kelowna, British Columbia V1Y 1W2, Canada	Biking	Bicycle theft and vandalism is a huge problem in Kelowna. Need to reduce emphasis on car parking, re-allocating space and resources to bicycle parking.	An array of city managed 'bicycle parking lots' that have secure racks and security cameras, maybe occasional supervision. Could be located in existing car parking lots.
"1730 Ethel Street, Kelowna, British Columbia V1Y 9S1, Canada	Biking	Crossing very busy highway can be difficult and unsafe, especially at night or during winter.	An overhead walkway, similar to that crossing the highway by Parkinson Rec Center.
"2368 Abbott Street, Kelowna, British Columbia V1Y 5J5, Canada	Walking (incl. wheelchair/ accessibility)	Incomplete waterfront path. Waterfront path from Maude Roxby Wetland ends halfway to Strathcona Park.	Complete the pedestrian path using public land below the high water mark.
"2934-2936 Pandosy Street, Kelowna, British Columbia V1Y 4Y5, Canada	Walking (incl. wheelchair/ accessibility)	Abbott Park makes a great pedestrian connection between Pandosy and Abbott, but it is not obvious from Pandosy.	Eventually, extend the park through the lot to the immediate east, along Pandosy. In the meantime, add signage on Pandosy directing people to Abbott Park.
"1910 Windsor Road, Kelowna, British Columbia V1Y 4R1, Canada	Driving	Congestion on Hwy 97	promote multi user ride share and transit. Too many single user vehicles on the road at peak times.
"1227 Jack Smith Road, Kelowna, British Columbia V1W 4P1, Canada	Driving	Not enough exit routes from upper mission/kettle valley	Need to complete loop to Crawford. put in the infrastructure before we start building
"1431 Gordon Drive, Kelowna, British Columbia V1Y 6V6, Canada	Biking	Very unsafe biking area.no way to safely access these retail establishments no bike paths in this area	widen Gordon to add bike lanes
"3726 Lakeshore Road, Kelowna, British Columbia V1W 3L4, Canada	Biking	Narrow sidewalks, busy traffic - hazard for cyclists	add wider bike lanes
"870 Leathead Road, Kelowna, British Columbia V1X 6S5, Canada	Biking	I agree with need to provide access from Mission Pathway to ORT. In general, there is no safe/easy way to go from the ORT to the Mission Pathway. The city needs to add another safe way for cyclists to cross Harvey	Add another crossover on Harvey North of Spall so cyclists can access the Mission Creek. Add one between UBCO (underpass) and the one at Parkinson.
"3036 Springfield Road, Kelowna, British Columbia V1X 3P2, Canada	Biking	biking down springfield, especially the hill, dangerous with speeding cars. They often come over into bike lane while going down hill. bike lane paint faded. Bike lane often full of gravel due to cars speeding down the turn.	repaint bike lines, paint green where cars tend to come over. Frequent sweeping of bike lanes, especially where lots of traffic.
"590 Dodd Road, Kelowna, British Columbia V1X 2X2, Canada	Walking (incl. wheelchair/ accessibility)	well walked street to the schools and Y, no sidewalk or street lights at night.	Add sidewalk and streetlights.
"275 Rutland Road North, Kelowna, British Columbia V1P 1G7, Canada	Shared mobility	Modo car share only available downtown and midtown. Would love multiple cars in Rutland area.	Bring in more Modo cars or other car sharing services.
"1252 Glenmore Road North, Kelowna, British Columbia V1V 2H1, Canada	Biking	No segregated bike path for approx. 1km along North Glenmore Road between Snowsell and John Hindle. Deters vulnerable users. Particularly problematic given the speed limit is 60 kmh.	Connect the bike path. I know it is in the plan in like 10 years or so, but this needs to happen sooner to support the connectivity of Glenmore with UBCO. The buses along this route are way above capacity during main commuting hours.



Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"2005 Pandosy Street, Kelowna, British Columbia V1Y 5R1, Canada	Biking	The road is narrow and doesn't feel safe for biking. Lots of overhanging trees, low visibility, lots of traffic, uncontrolled crossings	Trim back trees on corners, put in a bike lane, pedestrian activated lights
"4260 Glenmore Road North, Kelowna, British Columbia V1V 2B4, Canada	Biking		better bike lanes from lake country border to downtown Kelowna.
"3691 Gordon Drive, Kelowna, British Columbia V1W 4T5, Canada	Biking	Biking is dangerous. Cars are driving 70-80km/h and should not be beside bikes. So much space for pedestrians on the sidewalk and trees. But very few pedestrians at any time.	Widen side walk and have bike lane beside sidewalk instead of road....like East side between H2O and Casorso
"1756 Byrns Road, Kelowna, British Columbia V1Y 7X7, Canada	Biking	General for all road improvements. Bikes beside cars... bad idea, from some who has spent a year recovering from a collision that was not my fault. ICBC would probably appreciate this too. And I am also a driver.	Take bike lanes off road, cars would like this too. Put them beside sidewalks other than areas where there are lots of pedestrians, like downtown. Sidewalk on East side of Gordon, between H2O and Casorso is a great example.
"Glenmore Road, Kelowna, British Columbia V1V 2E8, Canada	Walking (incl. wheelchair/ accessibility)	Risk to Children. Vehicles turning at same time as signal for children to cross. I have seen so many near misses near North Glenmore Elementary	Sequencing of lights to avoid vehicles and cars getting the go signal at the same time.
"British Columbia Route 97, Kelowna, British Columbia V1Y 5W2, Canada	Driving		
"759 Crowley Avenue, Kelowna, British Columbia V1Y 2K5, Canada	Biking	Internal comment: city-wide comment	"Reduce Co2 = car trips (local amenities in centres, bike routes, urban rail) More quality of life on streets"
"672 Okanagan Boulevard, Kelowna, British Columbia V1Y 2C3, Canada	Biking	General North End biking/walking	Connect through industrial to the city core. Street life - pubs & retail & sport. Connect Knox Park to the city core. Extend Ethel, Water, St. Paul and Gordon corridors. Plant street trees.
"519 Osprey Avenue, Kelowna, British Columbia V1Y 5A3, Canada	Biking	Inadequate provision of safe cycling lanes on Pandosy. Design is built around automobiles.	Create bicycle lanes.
"751 Saucier Avenue, Kelowna, British Columbia V1Y 2M6, Canada	Walking (incl. wheelchair/ accessibility)	Streets are designed for auto (remaining text illegible)	Walking paths that cut through development
"1319 K.L.O. Road, Kelowna, British Columbia V1W 3N7, Canada	Biking	With more housing going in along KLO and more cars using the road, it is frightening to ride along it. Sidewalks have lots of pedestrians who don't want to share with cyclists.	Protected bike lanes (two-way) on each side of the road. Stop using the bike lanes for snow storage (very dangerous in winter if you are biking). Connect greenway to bike lane on KLO.
"1799 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	It's hard to travel down Gordon, because the bike lanes are unprotected and very narrow. I have to dismount and walk between Springfield and Sutherland on Gordon.	Expand bike lanes, create protected bike lanes, link these bike lanes in some easy way to the rail trail.
"1923 Burtch Road, Kelowna, British Columbia V1Y 8G1, Canada	Biking	It's hard to travel (bike) down Burtch, because the bike lanes are unprotected and very narrow.	Expand bike lanes, create protected bike lanes, link these bike lanes with the rail trail. There isn't an easy way to get to the rail trail by bike along Burtch.
"656 Francis Avenue, Kelowna, British Columbia V1Y 5G4, Canada	Walking (incl. wheelchair/ accessibility)	(install) sidewalk on east side of Richter Street and improve sidewalk on west side between Sutherland and Raymer.	Spend the DCCs from the Ru7 builds on Richter and put a hold on the Ethel street corridor.
"Okanagan Rail Trail, Kelowna, British Columbia V1Y 8H2, Canada	Biking	Bikes need to stop for traffic on Hardy Street. It should be that Hardy Street traffic stops for the rail trail bike and walking trail.	Change the location of existing signage. Minimal cost.
"532 Bernard Avenue, Kelowna, British Columbia V1Y 6N8, Canada	Walking (incl. wheelchair/ accessibility)	Bicycle(s) on sidewalk downtown Bernard/Pandosy/Richter.	Lower speed limit to 40km on Bernard. Cyclists are afraid of the speed of the cars. No one uses the bike lanes that were just put in from Richter to Glenmore.
"818 Lawson Avenue, Kelowna, British Columbia V1Y 6S8, Canada	Transit	The bus routes need to be unified. People need to walk to routes.	

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"992 Sutherland Avenue, Kelowna, British Columbia V1Y 5X6, Canada	Biking		Complete the 2 way bike path from Ethel to Gordon on Sutherland.
"Simpson Walk, Kelowna, British Columbia V1Y 9P3, Canada	Walking (incl. wheelchair/ accessibility)	Scooter and bike traffic. Too many dogs.	Restrict them 100% or to time when there are fewer walkers and runners. Give people priority. Enforcement of bylaws.
"5000 Frost Road, Kelowna, British Columbia V1W 4M3, Canada	Driving	Frost Road does not connect to Chute Lake Road.	Frost Road needs to connect to Chute Lake Road. Finish Frost Road.
"1291 Ethel Street, Kelowna, British Columbia V1Y 8S6, Canada	Biking		Connect bike path on ethel to clement.
"944 Cawston Avenue, Kelowna, British Columbia V1Y 2X1, Canada	Biking	Cawston Ave bike lanes intersecting with laneways. Bikers nearly being hit by cars entering Cawston from lane.	Block entrances to Cawston from side laneways.
"680 Doyle Avenue, Kelowna, British Columbia V1Y 9N2, Canada	Walking (incl. wheelchair/ accessibility)	"Narrow, low quality sidewalks (with exception of Bernard Ave). Missing sections. Gravel shoulders, no sidewalks."	Wide sidewalks should be a requirement of all redevelopment. I don't support redevelopment if it doesn't deliver a nice sidewalk. I don't think on-street parking needs to be on all streets.
"Glenmore Road, Kelowna, British Columbia V1V 2H4, Canada	Transit	Not frequent enough bus service. No reliable. Need better quality buses.	Buses every 15 minutes in peak times. Smaller but higher quality buses. Better quality bus stops. Increase density along bus stop routes to pay for service.
"2303 Abbott Street, Kelowna, British Columbia V1Y 5J5, Canada	Biking	Rose and Abbott to Christleton and Abbot. No sidewalk. Bike lanes often have parked cars. Bikes going north cross road diagonally to get to Abbot bike path. Christleton/Abbott intersection in blind for vehicles going north and turning left.	Continue bike/sidewalk path to at least Christleton/Abbott intersection.
"1656 Dilworth Drive, Kelowna, British Columbia V1Y 8B8, Canada	Biking	"Access from rail trail to Greenway. 1. Leckie - no bike lane from Dilworth to south of Baron. 2. McCurdy - no bike lane from Hwy 87 to block south of Hollywood."	Create bike lanes on Leckie and McCurdy. Dilworth is way too busy and too dangerous at the Hwy 97 intersection.
"2470 Glenmore Road North, Kelowna, British Columbia V1V 2B6, Canada	Biking	Safety.	Improve or create safe bike lanes on Glenmore out to McKinley.
"5535 Lakeshore Road, Kelowna, British Columbia V1W 4J4, Canada	Biking		Complete bike lane on Lakeshore to Okanagan Mountain Park.
"4824 Lakeshore Road, Kelowna, British Columbia V1W 4H6, Canada	Transit		Need public transit on Lakeshore heading south from Chute Lake Road. Perhaps minibus or van.
"1374 McInnes Avenue, Kelowna, British Columbia V1Y 3Z7, Canada	Biking		I would like to see more bike lanes that provide some sort of barrier or separation from traffic. Look at countries like Holland, Denmark that promote safe cycling.
"1374 McInnes Avenue, Kelowna, British Columbia V1Y 3Z7, Canada	Walking (incl. wheelchair/ accessibility)	Not enough crosswalks with and without flashing lights, etc.	New York City implemented changes for pedestrians and cycling safety in several neighbourhoods.
"575 Doyle Avenue, Kelowna, British Columbia V1Y 9N2, Canada	Walking (incl. wheelchair/ accessibility)	Rushing vehicle traffic fail to yield right of way to pedestrians.	More speed checks.
"2315 Gordon Drive, Kelowna, British Columbia V1Y 7X3, Canada	Transit	Not necessarily here, but other areas need better bus service.	Better bus system! Reliable, more of it. Mindset needs to change that public transport is good.
"575 British Columbia Route 97, Kelowna, British Columbia V1Y 8B4, Canada	Driving	Too many cars in this city.	Encourage more cycling, transit and walking. Change the mindset/culture of having to drive everywhere. Not easy.
"1953 Pandosy Street, Kelowna, British Columbia V1Y 1R7, Canada	Biking	The new bike lane actually dramatically hinders vehicular traffic flow. I don't know to what end.	The wide curbing is unnecessary and I think if possible the sidewalk space could be encroached. Specifically I think we lost too many left hand turn lanes.
"575 British Columbia Route 97, Kelowna, British Columbia V1Y 8B4, Canada	Walking (incl. wheelchair/ accessibility)	With the roads going up mountains it is difficult to have walking paths for some without walking beside roads with fumes and danger.	Wider buffer between road and path, but understand cost would limit paths.
"1631 Ethel Street, Kelowna, British Columbia V1Y 9V4, Canada	Biking		Love the work and results on Ethel Street.
"2241 Springfield Road, Kelowna, British Columbia V1W 2C7, Canada	Biking	Springfield biking to the farmers market on the south side is a disaster.	
"1211 Ethel Street, Kelowna, British Columbia V1Y 2W5, Canada	Walking (incl. wheelchair/ accessibility)		Ethel and Clement requires a crosswalk or traffic light to access breweries, BC Tree Fruits, etc.
"630 Ellis Street, Kelowna, British Columbia V1Y 1Y6, Canada	Driving	Reducing traffic congestion in the core.	Circle road (bypass) include Kelowna and Westbank. Alternative routes, perhaps a bridge over the lake near airport (north end).

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"1875 Dilworth Drive, Kelowna, British Columbia V1Y 9N4, Canada	Biking	Traveling by bicycle north-south on Ethel is excellent. Dilworth from ORT (rail trail) to Springfield (greenway) is an accident waiting to happen.	
"1981 Gordon Drive, Kelowna, British Columbia V1Y 3C2, Canada	Biking	No bike lane on Gordon (between) Springfield and Sutherland.	Add a bike lane.
"1981 Gordon Drive, Kelowna, British Columbia V1Y 3J2, Canada	Walking (incl. wheelchair/ accessibility)	No pedestrian crosswalk.	Add crosswalk on Gordon at Brookside Ave.
"1981 Gordon Drive, Kelowna, British Columbia V1Y 3C2, Canada	Walking (incl. wheelchair/ accessibility)	No pedestrian crosswalk to Mill Creek.	Add crosswalk (on Gordon at Mill Creek)
"867 Gerstmar Road, Kelowna, British Columbia V1X 1B7, Canada	Biking		Bike lanes on Gerstmar Rd leading to Gerstmar Park.
"2702 Springfield Road, Kelowna, British Columbia V1X 4B7, Canada	Transit		Extend rapid transit line to include all of Gerstmar. Consider bus route which goes to university, along Gerstmar.
"867 Gerstmar Road, Kelowna, British Columbia V1X 1B7, Canada	Driving	Heavy traffic and constant flow of traffic. People using Gerstmar as shortest route from Rutland to Central Kelowna, Mission, etc. People don't use Hollywood or Rutland Rd south of Hwy 33 to get to Springfield Rd.	Widen Gerstmar Rd
"715 Rutland Road North, Kelowna, British Columbia V1X 3E2, Canada	Walking (incl. wheelchair/ accessibility)	North side of RMS (Rutland Middle School) has no sidewalk. Youth navigate around cars to walk northbound, as there is no sidewalk.	Build a sidewalk.
"847 Mayfair Road, Kelowna, British Columbia V1X 5S1, Canada	Driving	Mayfair Road not aligned.	Complete road
"865 Franklyn Road, Kelowna, British Columbia V1X 3Y1, Canada	Walking (incl. wheelchair/ accessibility)	All roads between Leathead and McCurdy lack sidewalks. Extremely dangerous.	Build sidewalks.
"2670 Glenmore Road North, Kelowna, British Columbia V1V 2B6, Canada	Biking	No bike lanes, narrow shoulder, busy traffic.	Build a bike path along the road to make commuting safer. Extend rail trail north of airport so you don't have to take Glenmore Rd as alternate to Hwy 97.
"4920 Chute Lake Road, Kelowna, British Columbia V1W 4M3, Canada	Walking (incl. wheelchair/ accessibility)	No sidewalks makes walking around, for example to a winery, unsafe.	Build a sidewalk by Chute Lake.
"4920 Chute Lake Road, Kelowna, British Columbia V1W 4M3, Canada	Walking (incl. wheelchair/ accessibility)	No sidewalk to walk to winery, which is only one near a large community.	Add sidewalk on west side of Chute Lake Road.
"Old Vernon Road, Kelowna, British Columbia V1V 2K6, Canada	Biking	Rail trail ends at airport and forces cyclists onto highway to continue north.	Complete and pave rail trail to far north. Do fundraising request to community to fund.
"British Columbia Route 97, Kelowna, British Columbia V1Y 6B7, Canada	Driving	Getting from A to B at certain hours. HOV lane is in the wrong place and does not help move traffic.	Move the HOV lane to the left side where traffic can pull off onto a turning lane to turn. Don't allow motor homes or large trucks in HOV lane, despite having 2 people in vehicle.
"705 Rutland Road North, Kelowna, British Columbia V1X 7W8, Canada	Transit	High volumes at school rush hours - buses leaving kids, riders behind.	More capacity! Double decker, improved frequency.
"919 Guisachan Road, Kelowna, British Columbia V1Y 5K5, Canada	Biking		"Old" Guisachan road linking to Guisachan heading east needs a bike path to connect.
"1481 Springfield Road, Kelowna, British Columbia V1Y 5V3, Canada	Biking		Burtch at Springfield bike lane needs green paint.
"John Hindle Drive, Kelowna, British Columbia V1V 2Z4, Canada	Biking	Glenmore turning right onto John Hindle is a tight hard right. (bike)	need an easy smooth right turn path onto the rail trail.
"3132 Benvoulin Road, Kelowna, British Columbia V1W 2E5, Canada	Driving		Merge traffic south of KLO.
"4263 Gordon Drive, Kelowna, British Columbia V1W 2L3, Canada	Transit	Long wait times for bus on Gordon (going from Paret Place to KLO with Mission Rec Exchange).	More buses with smaller capacity during peak commute times (8-9:30am and 2-5:30pm)
"4263 Gordon Drive, Kelowna, British Columbia V1W 2L3, Canada	Biking	Lack of divided/safe bike path along Gordon (e.g. from Paret Place to KLO).	More divided safe (bike) paths
"4091 Lakeshore Road, Kelowna, British Columbia V1W 1A4, Canada	Walking (incl. wheelchair/ accessibility)	Lakeshore north of Lequime unsafe for walking	Need sidewalk for kids to get through safely.

Location	Type	Challenge	Ideas for solutions
1931 Gordon Drive, Kelowna, British Columbia V1Y 3C4, Canada	Biking	There are no bike lanes between the Cawston bike path and Springfield, limiting bike connectivity.	Install painted bike lanes
"1479 Oakridge Road, Kelowna, British Columbia V1W 3A8, Canada	Transit	Transportation for Students going to nearby public schools. The SD is not capable of providing the bus required for safety transport our kids to nearby public schools, which results in large congest in school zones during school opening and closing times. The cut-off walking distances in the SD policy is ridiculous, in particular for areas kids walking to elementary school without sidewalks or safe walking routes or walking in areas with steep terrain. The SD and the City are not coordinated in the bussing efforts, resulting in poor service for both methods.	Have one busing system transporting students to local schools. Right now, there is no coordination between the two.
"1489 K.L.O. Road, Kelowna, British Columbia V1W 3N9, Canada	Transit	South Pandosy - Orchard Park (or all the way to UBCO) Bus Route. No real transit options east of K.L.O. Road, especially if trying to get to Midtown, Glenmore, or Rutland	Either turn the #4 into a full-time Frequent Transit route or create a separate Frequent Transit route connecting the South Pandosy and Orchard Park transit exchanges via K.L.O. and Benvoulin.
"770-772 Rutland Road North, Kelowna, British Columbia V1X 3E2, Canada	Driving	The southbound turning lane for RMS parking begins at the point where the Legacy II driveway meets Rutland Rd. This driveway is also used by most of the Legacy I residents so there is a lot of traffic daily. When trying to enter the driveway while traveling northbound, drivers must encroach on the RMS turning lane or block northbound traffic as they wait for an opportunity to turn.	Shorten the RMS southbound turning lane, even by 2 car lengths and create a northbound turning lane.
"2388 Baron Road, Kelowna, British Columbia V1Y 9N4, Canada	Driving	Traffic backs up on Baron Rd from Dilworth almost to Underhill due to the fact that the majority of the vehicles approaching Dilworth have to be in the left lane. The right lane is for right turns only.	Make the left lane for left turns only and move the through traffic into the right lane.
"Hollywood Road North, Kelowna, British Columbia, Canada	Driving	Parking extremely limited, creating dangerous parking/traffic patterns	More parking and public transit to UBCO as it continues to grow... maybe LRT one day? :)
"5507 Airport Way, Kelowna, British Columbia, Canada	Transit	direct bus service to downtown core	changes to bus service/ add buses such as express service during peak arrival times of flights
"5300 South Ridge Drive, Kelowna, British Columbia V1W 5C5, Canada	Transit	More transportation via buses to such areas of Kelowna with direct travel to KGH, Pandosy, H2O, the Orchard Plaza Mall & "please" to the airport/UBC! Limit bus travel to above areas of KGH, Pandosy, H2O, the Orchard Plaza Mall & especially to the airport/UBC. Complicated routes and transfers needed.	additional direct routes for city bus transit to KGH, Pandosy, H2O, the Orchard Plaza Mall & to the airport/UBC. We have students renting in the upper mission going to UBC and Okanagan College with no proper services to get to schools. Taxi services are away to expensive for transportation to college/university & airport with a one way ride to the University/airport of \$90.00. Additional bus services are needed for cost and convenience. The provincial gov't is encouraging rental housing for the outskirts areas to fill the rental void but this is not conducive if transportation is not provided in sufficient ways.
"2447 Hwy 97th N, Kelowna, British Columbia V1X 6A1, Canada	Driving	97 is the only main road and it is full of traffic lights, and extremely poorly timed, at that - no exits or merging. It needs to change or this very small city will become gridlocked within 10 years.	We need to undergo a major rehaul. Small changes and adding bike lanes and better public transportation will do absolutely nothing and if we try that, we are wasting precious time to address the real issue. Traffic lights need to be timed impeccably for traffic flow, which they are currently not, and exits and merging need to be added (think overpasses, etc) in order to keep traffic moving in all directions.
"Transit Way, Kelowna, British Columbia, Canada	Transit	Buses are overcrowded and will generally have to leave people waiting for at least 30 minutes to get on a bus.	Add more frequent bus routes from UBCO / create more diverse routes to popular areas
"3135 Gordon Drive, Kelowna, British Columbia V1W 3M4, Canada	Driving	Huge traffic congestion turning left from K.L.O. to Gordon in the morning between 7:45 and 8:00 Turn signal allows only 4 cars through and traffic coming towards the college is 2 lanes and packed. Sometimes takes 2 lights to make it through.	Longer turn signal.
"3057 Burtch Road, Kelowna, British Columbia V1W 3P3, Canada	Driving	Too many cars on K.L.O. between GORDON and Ritcher.	Make Butch rd. Go through to K.L.O.
"1155 K.L.O. Road, Kelowna, British Columbia V1Y 8L4, Canada	Driving	Huge traffic congestion if someone tries to turn into the strip mall travelling west even though there are 2 solid yellow lines and no left turn lane.	Block off entrance or put in a left turn lane or Rework parking lot
"1358 Ladner Road, Kelowna, British Columbia V1W 3C1, Canada	Driving	Again too many vehicles on GORDON. Allow residents to access other main arteries . Not just 1 option.	Make Ladner or Bothe join GORDON to Benvoulin.

## Land use directions - public comments

Question: Is there anything you like about the land use directions?	Question: Is there anything you would you change about the land use directions?
Development around existing infrastructure	Limit development in Thompson Flats. Services, infrastructure is maxed out, and that is good amenity space for local residences
Well thought out plan	No
Housing development alongside transit routes. Overall densification and development of self-contained community centres.	Public transit up Clifton Rd and over Wilden to connect with North Glenmore
Developing Capri area is a good idea	I don't think urban industrial growth should stay around the mill if it closes down. That area would be better suited for park/beach/recreation with coffee shops/restaurants if the mill is demolished.
I like the focus on urban centres	I would like to see more development in the area between Casorso, Benvoulin, Springfield, and Gordon, especially along K.L.O. east of the college and along Benvoulin north of K.L.O.
Support of the university, college and ohh with expansion in those areas. As well i like the idea of high density housing in areas near the orchard park mall etc.	Do not develop land in the Thomson Flats. There is Okanagan mountain park just beside it as well as hiking trails and rock climbing areas near. The rock climbing area called Lonely crags is already being encroached on by the Kettle Valley développement and it has impacted the quality and atmosphere in that area
"Like that large format retail is kept mainly in Midtown, appropriately away from main highway and lakeshore (busy areas). Like limited development in rural areas - I believe the rural areas in city boundary are a major appeal to residents and tourist. Like areas labelled ""protect ALR"" Support OK College expansion. "	Change South Pandosy designation from Urban Centre to Multimodel Urban. Need to prevent high rise developments (over 8 stories) in South Pandosy as they will ruin the view from land and lake as well as change the Pandosy Village feel. Current and future infrastructure cannot support high rise development in terms of parking, bus service. Anything over 8 stories should not be built within 6 blocks of lake or major throughfare. Thus a change in designation is necessary.
I like the KGH development, UBCO growth and UBCO housing growth, all are practical and very necessary!	No, I think it actually looks very realistic and do-able.
I'm pleased to see that sprawl is intended to be managed in the land use directions presented.	Glenmore Road is already so heavily used, it does seem like a lot of development still to come, so hopefully there are transportation considerations to support that.
I like the long-range planning and support urban development in clusters to reduce traffic.	Yes, concern re increased development of healthcare services in midst of Heritage Area. Planning MUST start now for eventual move of advanced care to University (UBCO) as there will be continued conflicts for living in harmony and traffic patterns.
Increased residential density and focus on multi-use development. Limited rural development. Supporting UBC growth.	
In general, I support the direction signaled by the maps.	Within reason, I believe the areas of the city that allow commercial development need to be expanded. For example, every current residential neighbourhood would benefit from some mixed residential/commercial buildings that make goods and services more accessible closer to home so people don't have to get in their cars to buy milk or get a haircut. Also, in the central city, the area to the north designated for "urban industrial growth" should also allow commercial development, as commercial real estate is much more in demand than industrial land.
Limiting sprawl/growth in rural, under developed areas.	"Do not complete or further develop areas like Wilden or Kettle Valley until adequate public and school busing transportation is available. If you cannot complete master transportation plan goals, halt growth in new areas and concentrate on existing.  Do not further allow pro-car culture developments in urban areas such as gas stations that do not have EV charging stations in their plans."
	Reduce, rather than allow to continue the development of land in the interface areas around the exterior of the city.
I see a fair number of areas where Kelowna plans distributed village centres in a number of neighbourhood areas. I hope these will be well thought out, attractive, and enabled for neighbourhood enjoyment by significant incorporation of walking, cycling, and public transit access, favoured over vehicular access.	Hard to get a sense of scale and plan but I hope these village centres will provide enough services, close to neighbourhoods, that this cuts down on the need for driving everywhere. It needs to become more cost-prohibitive to drive and park everywhere. These village centres need to be made attractive and safe, especially for families, including young and old, and for non-vehicular commuters.
OK with most of it	"Support Urban Industrial Growth" in the North Downtown area? This area is changing rapidly in spite of lack of direction from City planners away from industrial... perhaps planners should get ahead of the trend and plan for the eventual conversion of this I4 zone to higher uses. Already the North side of Clement is turning commercial (Richter to Ethel), as is Richter St North to Bay Ave. Dinosaur industries like Tolko and Packing Houses are dying out. Land is getting too valuable in this area and making it necessary for conversion to higher uses to keep businesses profitable in this area.
Lots of centralized high-rise development.	"I wouldn't limit density around KGH. Fill the gaps between the marked multi-modal corridors."
Makes sense to drive greater density and limit sprawl.	
I think that the "Future Land Use Maps" displayed here have no surprises which I am sure most of the community is happy with. Finishing the existing housing projects is adding convenient services to those areas is very favourable for most. I like that the downtown core will place and emphasis on high density building with a mix of residential and commercial space.	No I don't think so - this plan seems to generally follow the existing plan with expansions to the surrounding areas and completion of existing projects. Kelowna is very spread out and therefore I like that the infill will join the pockets of communities up and maybe create some cohesiveness to the community.
Everything	"Use the OCP as the tool to up zone all properties to the designated highest and best use as defined by the OCP. This changes the developers risk level from can we get an approval to what does the project look like that is outright approvable. RU7 style but on all sites so there is no immediate change in certain property values. By exclusion some areas benefited more by this change than others."



Question: Is there anything you like about the land use directions?	Question: Is there anything you would you change about the land use directions?
limiting development outside the urban core protecting ALR lands from speculation encouraging growth in urban centres	too much suburban growth in completing existing neighbourhoods at city edge. it will simply decrease sustainability, increase cost of services, increase traffic, and take away potential growth from urban core
I like the general sense of the plan. It makes sense to me. It's hard to say this in the words I mean, but it "looks right". It fits the contours of Kelowna. I'd like more information about "Black Mountain Village" because I live there, and I don't see the services that I think are necessary to say it's a village rather than a collection of houses.	Nope
its really nice to see three different more established "neighbourhoods" ie downtown, the mission, city centre to offer different experiences	I would like to see more dense living between Benouvoulin and Springfield on that farm land.
most of it	there is already too many homes and congestion in upper mission. thompson flats development doesn't make sense as will exacerbate these problems.
RE: Hall road area... Very important to keep the character of the area, while allowing some infill. There is a certain amount (limited) of wasted land that could be better used to increase housing. No one that lives here want "development". The city on the other hand makes it impossible to build unless you're a big developer. True conundrum.	
Yes, limit dev. in Hall Road and Gallaghers Canyon area and Limit dev. in unserviced and rural areas	
	ALR exclusion allowed in areas that do not support agriculture. Specifically, Monford Road (end of).
	Not enough trees through re-development. Loss of tree canopy needs to be addressed.
	Too much land dedicated to parking. Reduce parking standards and increase fees.
Lots! Emphasis on density and green space in the core and multi-use pathways.	Add swales for minor stormwater absorption.
Somewhat. More specific on block by block identified. More uniform uses on spreaded all over city area.	Yes, engage the property owner in entire pblock and areas, not just each as now! Have guidelines equalized for entire zone, on street, or block.
Everything except commercial build up adjacent to Highway 97.	Eliminate all commercial and residential access to Highway 97 and Glenmore Road and restrict direct access to all other arterial routes. Purpose is to enhance and speed up traffic getting through the city.
It appears somewhat reasonable.	Not sure, at this time.
I like the idea of protecting ALR. I like the emphasis on walkable, livable urban centres. I like the idea of clustering densifying development around commercial, village centres. I like the limiting of growth in unserviced areas. I like the idea of limiting growth in rural areas. I like the idea of protecting heritage buildings and areas.	I understand that completing some neighbourhoods is necessary due to contractual or legal obligation, but I think far-flung, car-dependent, hillside suburbs are part of the problem, not the solution, so I do not support exploring residential development in Thomson Flats, for example.
I like the protection of the heritage area and use of infill housing in the Central area. However it is critical development of infill housing be accompanied by improved streetscape with treed boulevards; sidewalks; safe bike paths and pocket parks to retain and improve livability for people of all ages.	Important in transition to higher density in central neighbourhood around KGH to work with existing residents to ensure there will be benefits for the neighbourhood and not just costs. Traffic and parking are major issues caused by KGH and need to be resolved.
I LOVE that the City is moving towards establishing Urban Centres and filling in our existing neighbourhoods. We definitely do not need to supporting anymore new residential development outside of existing neighbourhoods and it's great that the City recognizes this also.	I think that we need to add more multi-modal corridors onto the maps. As UBCO continues to grow, we need to encourage those who work and study on campus to take alternate forms of transportation, and this will be incentivized through creating more multi-modal corridors on major routes such as Glenmore to John Hindle. Since more development is anticipated in the South Pandosy neighbourhood and infill in the neighbourhoods surrounding it, I think that we could also extend the multi-modal corridor on Gordon all the way down to Lexington. The 5 and 1 transit routes already go to the H2O so this would make sense.
	a need for more parks space needs to be automatic in the areas. As the growth pushes away from the downtown, the lakeshore access is more limited, a trail, boardwalk to connect the areas would be beneficial. One only has to look at the use of the current board walk to see the positive results
It's a bit too vague, to be sure. But I like increasing density in urban centres and along urban corridors (as opposed to more sprawl). I also like mixed use residential/commercial.	"I'm worried about the McKinley Beach and Wilden communities adding to sprawl with huge houses.  There's no mention of extending the bike path system. This should be high priority."
"Residential Infill focus Airport Expansion"	"Exploration of residential development in Thomson Flats. There are no services in Upper Mission. Services and roads need to be in place before any further discussion on additional development. The City of Kelowna keeps saying it wants to encourage people to walk or bike, but with no services in Upper Mission this is next to impossible given the lack of services in the area. Thomson Flats should continue to have a Future Use designation in the OCP.  More focus needs to be made on ensuring the development of the South Gordon Village Centre."
I like the mix of high and low density in the designated areas	No
"Not developing any further in rural areas! Creating higher density in already developed areas! Let's use what we have and not have to create more infrastructure further out which leads to more traffic and car use"	
I like that the Capri-Landmark area is getting some attention. Capri Mall has been a sore-spot for decades.	I wonder about Tomson Flats. How far and high is Kelowna supposed to grow? The views are great and thus probably beneficial to the the tax base but still? How reasonable is it to build even further out like that?
Walkability of the urban core	Support ubco with good transport to higher density residential and commercial mixed use.

Question: Is there anything you like about the land use directions?	Question: Is there anything you would you change about the land use directions?
"Densification of the core and urban centres is good. More zoning for infill housing and rowhousing would be appreciated. Re-evaluating use of residential infill such that it is accessible for first time home buyers and shifting workforce will aid Kelowna's growth. Increased high-density residential around Capri-Landmark and South Pandosy are great ideas, but will require better alternative transportation (i.e. bike lanes)."	Connect Clifton Rd. to McKinley Beach. Provide better arterial transportation for residential areas, as well as alternative transportation arteries to make areas like Black Mountain and McKinley beach more viable with less single use vehicle traffic.
Yes, increased densification in urban cores.	High density from downtown all the way to high density College area. The area around the hospital should also be densified. Additional areas would better support this desirable area and the healthcare workers.
Looks consistent with current development.	Provide connections between neighbourhoods and schools for kids to walk or bike to schools. I.e. safe commuting options for kids.
No ... none.	"Absolutely ... Wilden, West to McKinley Beach, and East to Hwy 97 needs a new major road connecting to John Hindle (immediately)" to support the 1000+ homes; and therefore 2000+ vehicles. City of Kelowna also will need this new road, for improved Transit system to link to the UBCO Exchange."
yes the Urban Center in the first Central picture	No
realization that there must be density in key areas	The plan shows limited to in-existant realization of the centrality of highway 97/Harvey. This is because the city does NOT understand two key points. Harvey, by virtue of it being the Highway through Kelowna, will always be the main hub. What can the city do to maximize the value of this, and diminish its terrible effect on the city? Make it the transit hub, allowing zoning for up to five stories residential along the highway, make one lane on each side for a tram or LTR, and invest as little as necessary to create feeders to this system along Gordon, Pandosy, etc. Frequency of service, parking for transit users. Building up near the highway. The residential infill is a classic example of lost opportunity.
No	No
South - Complete the ponds neighbourhood	complete kettle valley neighbourhood - This should be done after traffic problem has solved. There are too much traffic during rush hour in Lake shore road.
Love the high density in downtown and Landmark urban Center. The mixed use component is very important. I like the continuous of support single family development in growing communities like Tower Ranch and Kitchener Mountain.	I would like to see low rise only in the midtown urban Center. I think landmark and downtown are our two downtowns we need to grow and support. I think allowing high density in midtown make an awkward Hwy 97 corridor with kilometres of towers. Keeping midtown urban Center low-density I think would be important.
	I see Shadow Ridge Golf Club (ALR land) is identified with an intent to 'Protect Industrial Lands'. Kelowna Springs Golf Club, south of Shadow Ridge is NOT in the ALR and should be earmarked for future term industrial use (perhaps many years down the road). There is a City road easement which will take +/- 33' of the property at the N end of the property, and provide secondary future access. The golf course is slowly being pinched by other commercial and industrial property, and the City will clearly have a need for additional future options for industrial use.
I like the resources in the upper mission.	I wish glenmore would have more community resources.
	Too much densification in the downtown core. Would like to see the density more spread out into the clement/Gordon quadrant or further north towards Knox mountain instead of reserving the north waterfront for industrial growth. Industrial use of the waterfront lands in the heart of the city should be relocated to other areas of the city.
Keeping the town centres (to avoid spread of residential housing to the suburbs) growing.	Stop spread to the suburbs....the traffic coming into town is terrible for the environment. Check out Gordon and Lakeshore during work hours.
I like the planned gentrification of the Capri-Landmark area.	Increase the parkland to include the area on the northside of Brookside east of Gordon. Continue with a walking path along the creek and expand it to continue right down to the lake.
I like the densification of the urban centres, protection of ALR lands, and limiting development in unserved areas.	
	I back the ALR, but I think the city has to make more north/south through roads so that traffic can be more dispersed. Currently Benvoulin/Gordon & Lakeshore take virtually all the traffic.
I like the areas designated as village centers, rather than just the urban center of downtown Kelowna	No
Growth overall is nice as someone looking to possibly move there in the future.	There are gaps in the Urban Centre which doesn't really make sense. It feels like the cutoff would be very abrupt.
UBCO growth, Okanagan College Expansion, KGH Growth	"North end 40 acre Tolko site needs to be included in future Urban. Continued desire to have Large format retail along #97 only after highway access improved or driving behaviour must change. Gridlock will occur as the city supports 400 plus units at lakeshore/lanfranco, 900 plus units at Shasta/Hiwatha, 450 units at Cook Road Aqua and 1,500 additional units at the thompson flats. the greenfield infilling coupled with the south pandosy/lower mission densification will require improved road infrastructure."
"densification, fill in what is already developed keep alr, more greenspace"	"less focus on outlying neighbourhoods that will contribute to sprawl and more driving less digging up of mountain space for houses, want to look up and see trees not houses "
I like concentrating retail etc in select areas	roads need to keep up with this plan - keeping cyclists in mind Some roads (Gordon) will get too busy and be hazardous for cyclists as well
Increased density downtown	Less expansion into green spaces
Walking trail through City is great. Bike riding trail to Vernon is great.	

Question: Is there anything you like about the land use directions?	Question: Is there anything you would you change about the land use directions?
MR4	Limit MR1 & MR2 as these consume too much land relative to density. Instead plan for MR4 to continue to grow out from exiting town centres. Should require more open space/parks, particularly when increased density is covering more land rather than going up.
	I am concerned about traffic congestion on Benvoulin Court. We have three big buildings 55+ and I understand that three similar buildings are being built on our site (two apts each) and a 5 story building across the street. We have no controlled intersection to cross our street. Please help.
I like the development of a Glenmore "Centre" as access to nearby services in Glenmore is near impossible without a vehicle.	Completing the expansion of Wilden will add even more problems relating to Urban sprawl. Wilden is already lacking the necessary transport infrastructure to sustain it. Focus on adding proper public transport to the already existing residents first.
I think they checked all the boxes on the direction of their plan in getting ready for the volume.	More in depth look at the industrial plan.
	I think there needs to be a much larger modern shopping mall with more up ?? stores. I also think it would be an excellent idea to have a theme park like Disney because it would create lots of jobs and be somewhere for all ages especially teenagers.
Like the urban centre - residential zoning around "town" centres.	More land for bike pathways!
Focus on enviro issues; focus on density.	All C1 + C2 zones should be C3. Limit street development & invest more in bus & bike/walking. Use height in meters instead of stories - more exact.
Growth Boundary, Urban Centres	Suburban car use; provide local sores, services, pubs.
Increased mid-level densification.	"There needs to be more parks & green space. Lack of direction of goals around bicycle transportation."
I like the densification, focusing on the urban centres.	More active transportation routes around town, with protected cycling lanes. Less suburban development that clogs routes through downtown. Remove parking minimums. Make more room for people and less for cars.
I like the trend densify the core areas of the city.	I would like to see that in RU6 zoning to allow carriage house plus secondary suites in the main building allowing 3 units.
Focusing development into the urban center and away from ALR/Rural/Hillside development is a bonus.	"Head back in time and stop/limit hillside development. Increase density around transportation routes.
	Assure zoning changes neighbouring ALR are compatible with agricultural uses. Consult with AAC.
Good presentation	More commercial on Clement between Water & Gordon
"Generally in agreement. Still need single family housing. Thomson Flats area need to go ahead.  Kids need yards to play in."	Don't be so against single family development in outlying areas.
"Neighbourhood development. Less suburban - more mixed use, diversity."	
I like the flexibility potentially offered by the new future land use designations.	Create a density metric based on lot area rather than lot width.
"More inclusive land use designations. Never understood all the different designations in the existing OCP. Fewer, but more clear designations should serve the vision well."	"Agree that growth should be concentrated in the urban areas of the city. Development could still happen in the suburbs that makes them more sustainable. (small amounts of office, retail and shopping in residential neighbourhoods would be great)."
Yes - restricting suburbia; growing urban areas	
I like the idea of stopping urban sprawl.	
I like the ideas presented.	
Cohesive	
I think in the future having all the skylines (tops of hills and mountains) natural, with no buildings, parking lots, or other unnatural intrusions will help maintain the "natural" "fresh air" atmosphere of the region and city.	
"Poster - well done & explains the land use future.  Suggest - Add a timeline to the map i.e. 2019 _____ 2020 _____ 2030 _____ 2040 - indicate what & when within the map."	"Industrial in Beaver Lake. Increase park land. Congestion issue solved by increased cycle paths. Add the heritage buildings & the history."
You are going in the right direction with densification.	
	Understand more people moving here, but worried about lack of green space/tree cover. Want to know that as areas developed there is an effort made to protect & enhance green space (for climate & its effects eg: flooding & for wildlife).
"- Densification is key, and well envisioned. - Preserving agri space is good. "	"- Need some commercial, office, retail space to serve expanding south end of town.
"I'm very happy to see there will continue to be a mix of urban and rural within the city. That is one thing that makes Kelowna special. Have the plan to extend/improve the walk along the lake @ the info center."	Beach access.
"Good plan for densification in areas that are already key for growth & close to busy areas. Creating mini-urban areas (purple areas) is also positive. I also appreciate conservation of agricultural and park land."	The south neighbourhood's remain quite suburban and could benefit from a village centre (in addition to the Ponds Village Centre).

Question: Is there anything you like about the land use directions?	Question: Is there anything you would you change about the land use directions?
""- Expansion of higher density areas. - Completely reworking boundaries. "	
Protect ALR we need our farms.	"Glenmore needs an urban centre. May need more density in Mission Creek area. Need more highrise in urban centers (and pet friendly)."
More building height around Capri and Midtown.	Parking issues make it difficult to go downtown.
No	Stop development until a functional transportation plan is in place.
Affordable rents are a real problem. Should be part of palling to zone certain blocks for future low cost housing & renting projects.	Zone certain portion of suites in each condo complex to be low rental.
I think higher density along main traffic areas is a great idea. I like the way the City is starting to look with densification replacing old looking houses.	
	Open the parcel of land on Springfield, south side at Ambrosi Ave.
""- Protect agriculture & resource land. - Make industrial lands feasibly for private investors. "	"- Most of water edge, buffer for indigenous species planted for wildlife and habitual of land & water species livelihood.
	South Gordon Village Centre would require that the South Perimeter Road be completed.
"I support increased density. Please only allow buildings over 12 stories East side of Lakeshore and Pandosy."	Save the Westside (i.e: lakeside) for lower buildings so all can enjoy the beauty of the lake forever.
Greater density of the core.	Stop suburban sprawl. The infrastructural costs are crippling over the long term, it increases congestion & driving, increases homes at risk with wildland fire interface.

## Appendix B: OCP stakeholder workshop summary

Date of workshop: October 17, 2019

### Background

The purpose of this Workshop was to gather a diverse group of representatives across a variety of disciplines and explore the draft land use map as well as the direction for OCP's policies. Participants spoke about their vision for what Kelowna could look like in the future, provided feedback on a series of "pillars" that form the foundational directions for the planning process and discussed the draft land use map and its implications.

Below is a summary of the feedback provided by the participants for each exercise.

### Exercise #1: fast forward

#### *Major themes*

Discussion topic: Thinking into the future, what types of neighbourhoods, transportation options and amenities should we consider in order to ensure young people can live, work and play in Kelowna?

Stakeholder comments:

- Neighbourhoods that offer a greater variety of more affordable housing options within walking distance to jobs, schools and services.
- A transportation system that offers more choices, like transit, cycling and walking as well as shared mobility.
- Increased investments in parks and recreational and cultural facilities and programs.
- Our natural and agricultural assets are protected and enhanced, providing greater food security and community health.
- Kelowna is a safe, equitable and inclusive community.
- Kelowna is adapting to new technologies as part of the emerging sharing economy.
- The city's economy is continuing to grow and attract talent with excellent educational institutions.

### Exercise #2: pillars discussion

*Discussion topic: develop Urban Centres as the primary magnets for residents, jobs, shopping and culture.*

Stakeholder comments:

- We need to be careful that the development of the Urban Centres does not push people out who will rely on the services and the transportation options that the Urban Centres offer. There needs to be a balance between intensification and affordability.
- How do we balance the amenities needed for these neighbourhoods with the fees and charges that increase housing costs?

*Discussion topic: Promote more housing diversity in the Core Area.*

Stakeholder comments:

- There is already pressure for redevelopment in our heritage neighbourhoods and properties. We need policies to manage the impacts of infill and redevelopment in these areas. Are there programs and incentives to make it more viable to preserve heritage properties?
- Consider being clearer on where redevelopment is being encouraged.
- The loss of mature trees in these neighbourhoods is a problem. We need to find ways to retain them as infill and redevelopment takes place.
- This level of flexibility for the Core Area should be considered in Suburban neighbourhoods. They would also benefit from more housing variety, services, and transportation options.
- Suites are a viable approach to densification – the City should be more supportive of these.
- Often, a vision is set for new development in existing neighbourhoods, but when a project is proposed that is inconsistent with this vision, it still gets support. The City needs to be sure that when policies are created that they are followed by staff and Council.

*Discussion topic: deliver more housing with supports near jobs, services, and amenities.*

Stakeholder comments:

- Solutions are needed to address the issues of homelessness and addictions in Kelowna.
- The City has an important role to play, however it cannot address this complex issue alone. Senior levels of governments need to be more involved in working towards a solution.

*Discussion topic: prioritize transit, active transportation and shared mobility where it works.*

Stakeholder comments:

- Kelowna needs a better transit options, and densities to make those options more viable. However, the transit service needs to come sooner so that future development can build around it.
- Goods movement, which mostly happens by truck, cannot be forgotten, especially with the growing share of online shopping.
- There are other ways of managing congestion that involve changing our behavior. For example, staggering hours of operation for businesses would help ease congestion at peak times.
- Housing unaffordability may push people out into more distant communities. This will require more transportation options to connect people across the region.



- Social connections can be increased through active transportation opportunities such as sidewalks and multi-modal pathways.

*Discussion topic: strengthen the city's role as a regional hub for employment, research and innovation*

Stakeholder comments:

- The quality of life that Kelowna offers is important to attracting jobs to the area.
- Traffic congestion and transportation challenges could make continued economic growth difficult. We need to recognize that they support each other.
- Fostering growth at our major employment, research and innovation hubs, like Kelowna General Hospital for example, is critical to this pillar.
- Attracting young people to the city can be a barrier to business, especially with high housing prices.

*Discussion topic: stop planning new suburban neighbourhoods*

Stakeholder comments:

- Large homes in sprawling neighbourhoods don't meet our current housing needs but use caution in slowing new suburban development. Supply and demand must be balanced to moderate prices.
- As single family suburban growth slows in Kelowna, it will likely occur more in neighbouring communities, which will create its own set of challenges.
- We can't push too hard against what the market wants, and it will market will push new housing towards to employment areas.
- It makes financial sense for the City to develop where existing infrastructure is located.

*Discussion topic: target growth along transit corridors that connect our Urban Centres*

Stakeholder comments:

- Greater housing diversity is needed in these areas so that more people are able to connect to safe and enjoyable commuting options.
- These neighbourhoods need to be walkable and designed to redefine what "Quality of Life" means.
- Develop cultural infrastructure and services to support vibrant Urban Centre and neighbourhoods across the city.
- A Central Okanagan Performing Arts Centre will act as a regional destination, and therefore needs regionally scaled transportation access.

### Exercise #3: Draft Future Land Use Map

*Discussion topic: growth management*

Stakeholder comments:

- Generally, Kelowna is transitioning from growing out to growing up, which is a positive change.

- There will be pressure to grow in the north Kelowna along Highway 97 and Glenmore Road corridors, especially as Lake Country signals growth just across from the city boundary. In the longer term, the City should explore more development in these areas.
- Growth in the southern neighbourhoods, like the Mission for example, should be slowed to better manage growing traffic congestion in that area.

*Discussion topic: infill and redevelopment*

Stakeholder comments:

- The urban corridors along frequent transit routes makes sense. The City should explore extending these corridors to more areas. In particular, Hollywood Road in Rutland was identified as having potential to be an urban corridor that connects to UBCO.
- There are concerns that policies and guidelines for infill and redevelopment will not be followed. Participants identified areas where the OCP signaled a certain height or density, but a much larger scale project was supported.
- The cost of infrastructure in the Urban Centres was discussed, with the Capri-Landmark Urban Centre Plan's transportation, infrastructure and upgrades being used as an example.
- The development of the Midtown Urban Centre and the Highway 97 corridor will be challenging, especially with the proposed new Costco site. Hubs of activity will be needed to be very strategically located.
- Uses to support the Cultural District are not explicitly identified in the draft map. It appears that they will be integrated into mixed use buildings, which dilutes these assets and doesn't protect them in the long-term.

*Discussion topic: suburban development*

Stakeholder comments:

- Development in suburban neighbourhoods should be more flexible, allowing for a greater range of housing types and commercial uses integrated into village or neighbourhood centres. This includes allowing for more infill development in Suburban neighbourhoods adjacent to the Core Area, such as south Rutland and the Lower Mission.
- More transportation options are needed in suburban neighbourhoods, including between suburban neighbourhoods.

*Discussion topic: industrial lands*

Stakeholder comments:

- The industrial lands north of Downtown are under pressure to redevelop, and this is expected to increase. A strategy is needed to protect these lands and retain the industrial uses to keep these jobs close

to growing residential neighbourhoods. However, some key sites should be considered for mixed use development and to maintain alignment options for a second bridge crossing.

- Locating industrial lands along the Highway 97 corridor has its advantages, but it also causes congestion and access challenges. When new industrial lands are considered, they should have easy access to Highway 97 without being located directly on the highway.
- More mixed use opportunities should be explored in industrial areas.

#### *Discussion topic: education and employment*

Stakeholder comments:

- The shift to a more urban form of development, new approaches are needed to acquire school sites, particularly in or near Urban Centres. Sites may have to be smaller and more intensive, be co-located or integrated with other forms of development and respond to emerging trends in how education is provided.
- The Gateway district, which includes UBCO and the Airport should evolve into a larger live-work hub, with more opportunities for residential development nearby.

#### *Discussion topic: environment*

Stakeholder comments:

- To promote more ecological protection, the City should explore incentives to development, such as density transfers and density bonusing.
- Foreshore protection along Okanagan Lake is critical. As development takes place along the lake, opportunities for protection as public property and restoration should be taken advantage of.
- Adding Okanagan Lake to the Future Land Use map would help with explicit policies.
- Greater habitat connectivity needs to be explored to connect natural areas and wildlife corridors.

#### *Discussion topic: parks*

What we heard:

- A pathway connection between Mission Creek and Rail Trail is needed. A through Midtown, near Leckie Road, would be the shortest route for such a connection.
- Consider developing pathway network that connects major parks and natural areas, such as Knox Mountain to Wilden and on to McKinley, for example.
- More smaller neighbourhood parks are needed.

#### *Discussion topic: agriculture and food security*

Stakeholder comments:

- While agricultural lands are signaled for preservation,

the City needs to “walk the talk” more. For example, many properties are being removed from the ALR under the pretense that they are needed for airport expansion, but other uses are now being signaled.

- More clarity is needed for the Rural – Agricultural and Resource (R-AGR) designation. Will industrial packing and processing facilities to be considered on these lands?

#### *Discussion topic: transportation*

Stakeholder comments:

- Glenmore Road is becoming a much more congested route with development in the north and in Lake Country. Improvements are needed, including better cycling connections to John Hindle to UBCO.
- A shift in transportation modes is needed but will likely involve more options than we are anticipating. More investments in sidewalks are needed, as are streets that are better designed for multiple users, but there are other “out-of-the-box” ideas that should be explored, such as a lake ferry service and introducing for park n’ rides, for example.
- Congestion pricing may have to be explored to raise revenue and manage demand.
- Major road projects that bypass major choke points need to be explored further, such as a second bridge crossing, various ring road alignments and overpasses.
- Kelowna needs to be ready for autonomous vehicles, including preparing for obsolete infrastructure and assets, like parkades, for example.
- Lower speed limits would increase safety, improve social connections and promote greater use of alternative transportation.

#### *Discussion topic: other*

Stakeholder comments:

- The City should explore development of a “dark skies” bylaw, which would regulate lighting at night.
- The City is going to be challenged to keep up with expected levels of service with this new growth.
- One way to achieve the OCP’s goals would be to fast-track development applications to promote sustainability options as part of development regulations.
- Glenmore fire department – a full-time fire department in Glenmore will be required as we continue to grow and expand into this area. This may include relocating the existing facility to a more central location.

### **Next steps**

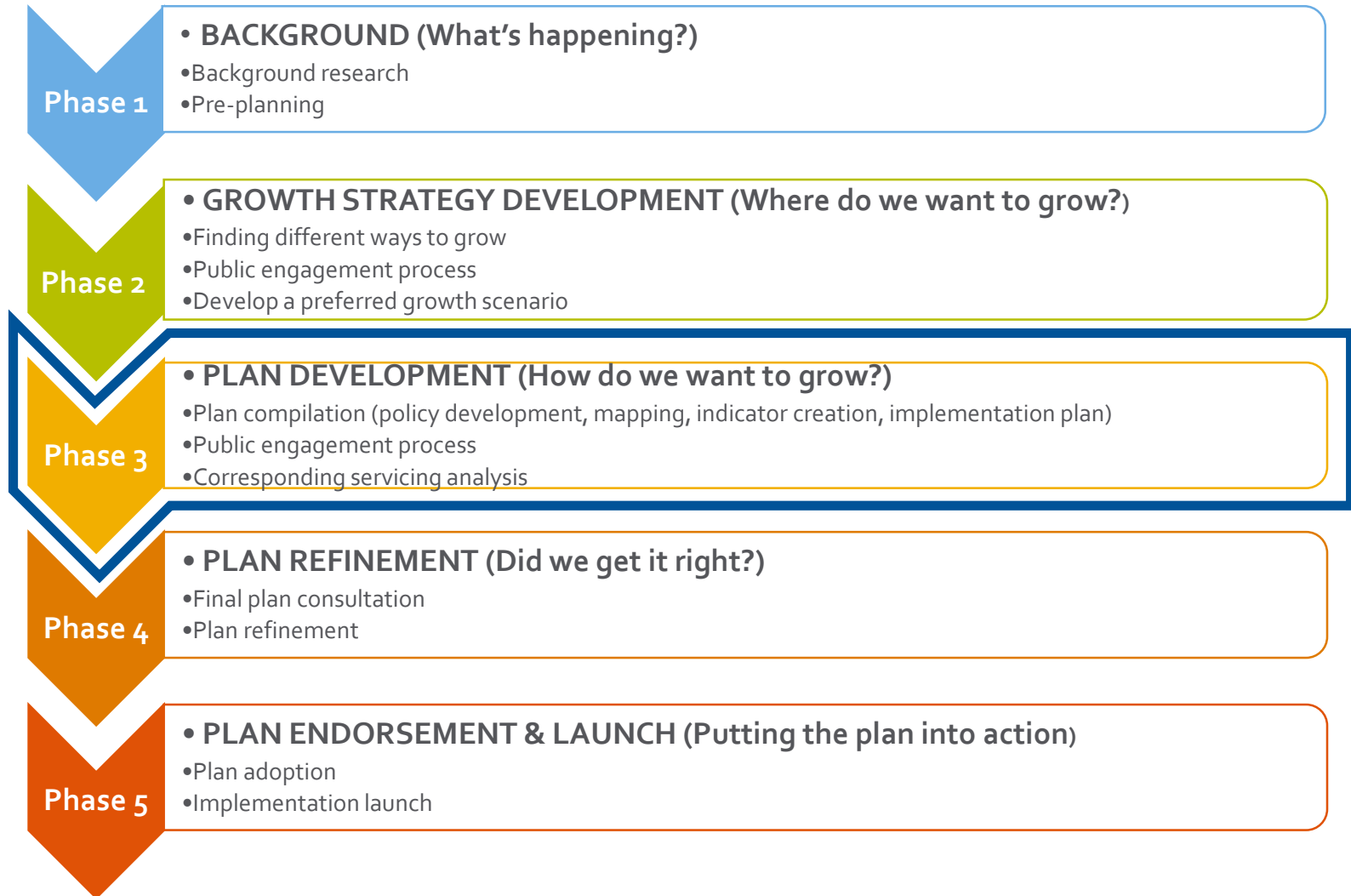
Feedback will be used to refine the draft land use map, strengthen the language in the pillars and to help develop new policies for the OCP.



# Official Community Plan 2040



# Project Timeline



# OCP 2040 Engagement Process

Imagine Kelowna

Phase 2 Engagement

- Pick Your Path
- Stakeholder Workshops

Phase 3 Engagement

- Neighbourhood Expos
- Stakeholder Workshops

Phase 4 Engagement (to follow  
draft OCP)



# Growth Strategy

Urban Centres grow as the hubs for shopping, employment, and residential living

Transit oriented urban corridors between Urban Centres and other key destinations

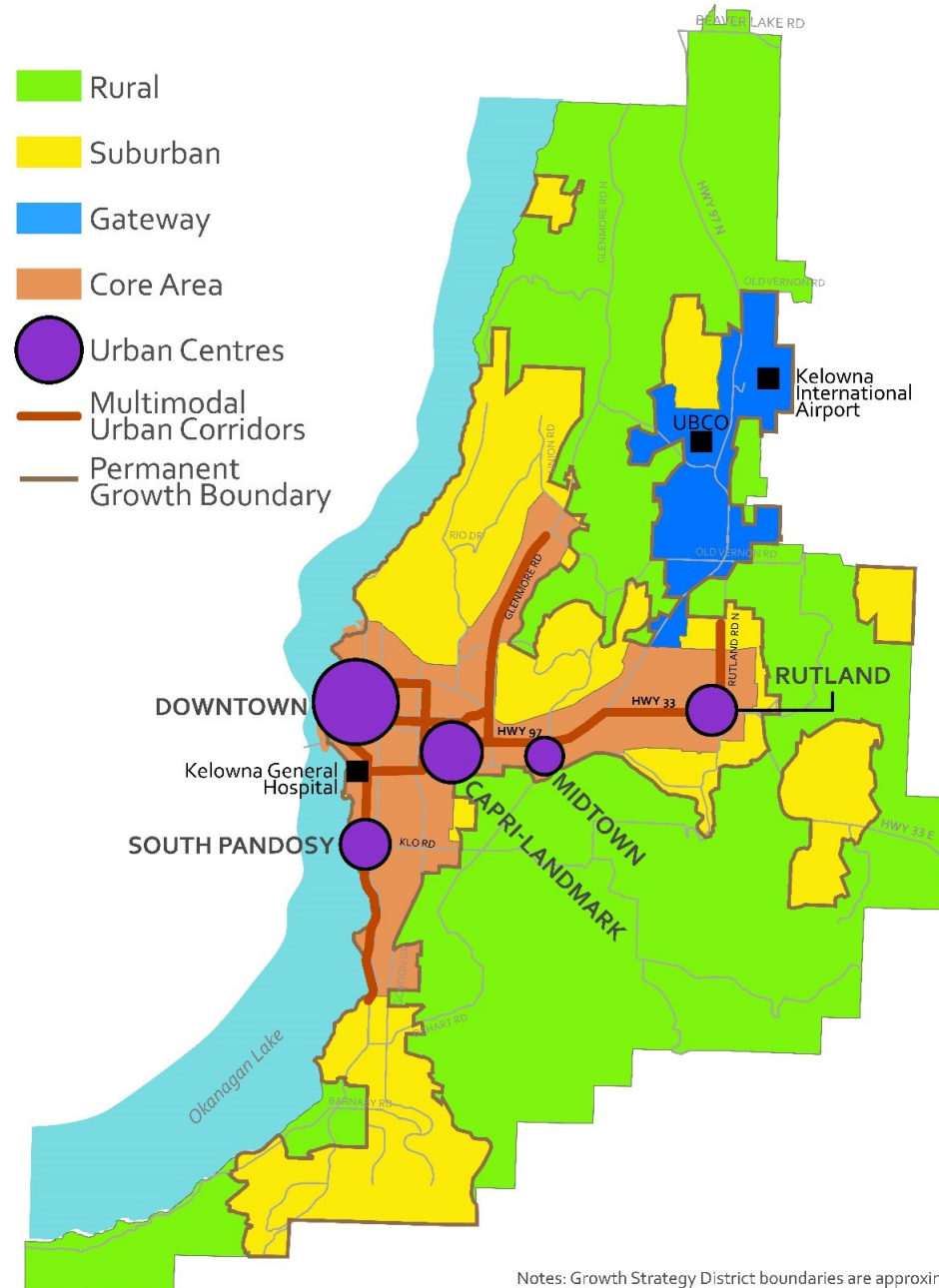
More housing variety signaled in the Core Area

Approved suburban neighbourhoods continue to grow

Protect agricultural and rural lands with the Permanent Growth Boundary

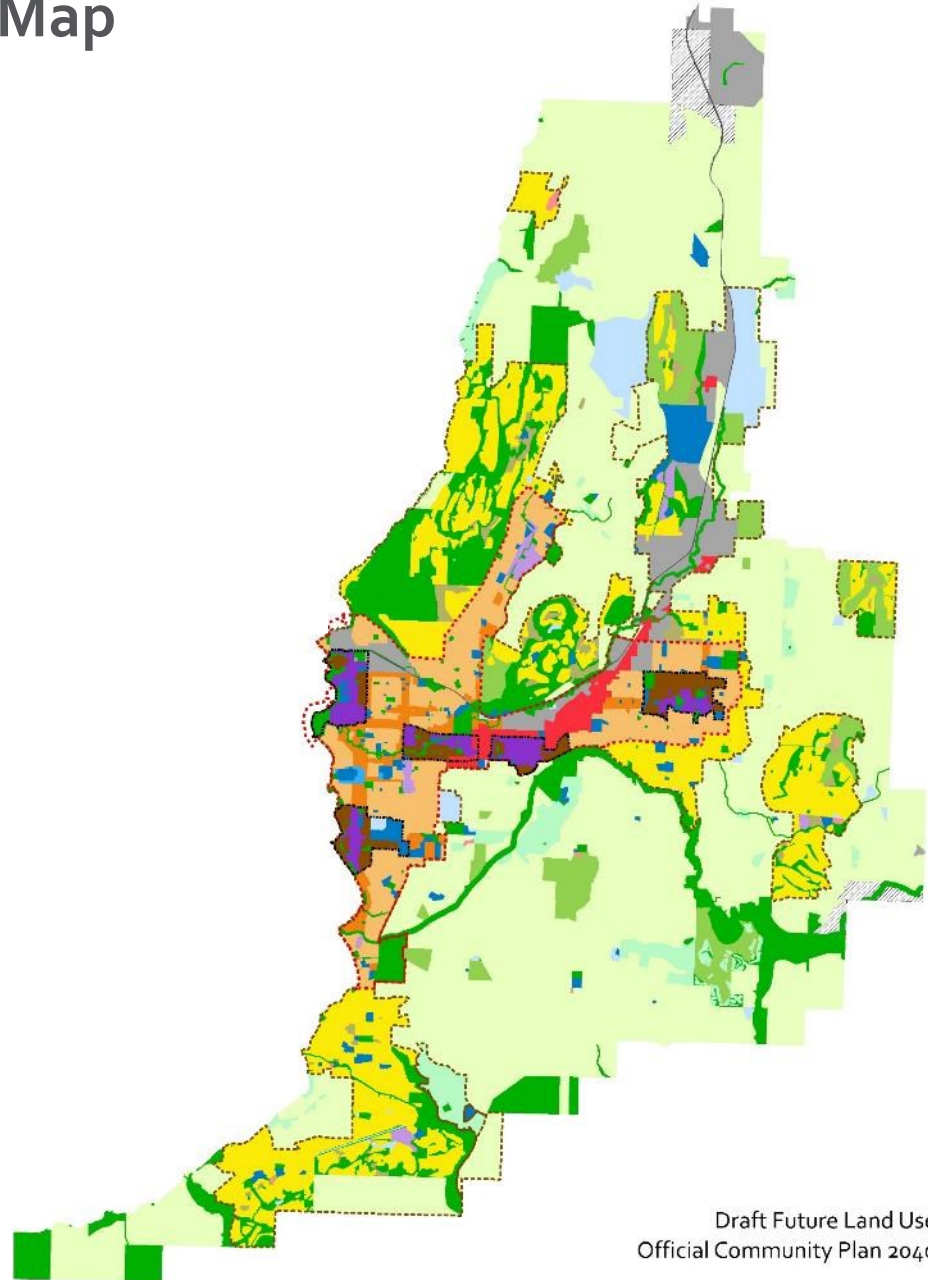
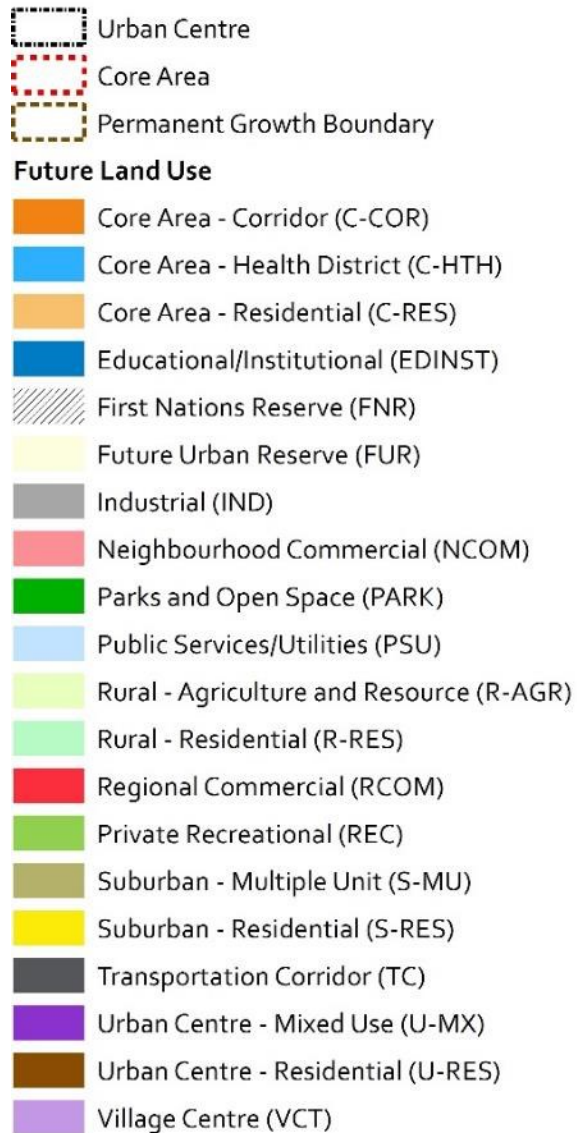
Protect industrial lands and grow other key employment hubs


Prioritize parks and public space



Notes: Growth Strategy District boundaries are approximate.

# Draft Future Land Use Map





# Phase 3 Engagement Objectives

- ▶ Feedback on the draft Future Land Use Map
- ▶ Directions for policy development
- ▶ Directions for parks acquisition priorities
- ▶ Ideas for the Transportation Master Plan
- ▶ Inform about the 20 Year Servicing Plan





# Phase 3 Engagement

- ▶ 4 Neighbourhood Expos:
  - ▶ Downtown
  - ▶ Midtown
  - ▶ South Pandosy/Mission
  - ▶ Rutland
- ▶ Online Expo:
  - ▶ September 16 – October 4, 2019
- ▶ Stakeholder Workshops
  - ▶ Infill strategy
  - ▶ Urban design guidelines
  - ▶ OCP/TMP Stakeholder Group
- ▶ Indigenous Engagement
  - ▶ Underway

# Engagement Results



**4**

public  
neighbourhood expos



**6**

stakeholder  
workshops

**~740**

in-person &  
online public  
participants



**~720**

free-form public  
comments received

**142**

survey  
responses



# What have we heard?



Support for density in Urban Centres and Core Area.



Protection of agricultural lands.



More frequent transit desired.



More active transportation options desired.

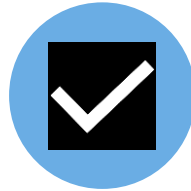


Support for more infill housing that supports affordability and shops and services.



More park spaces are needed closer to where people live.

# Next Steps



CONTINUED LAND USE  
MAP REFINEMENT



NEW PARK LOCATIONS



OCP POLICIES AND  
DEVELOPMENT PERMIT  
AREA GUIDELINES



CONTINUING  
ENGAGEMENT



DETAILED  
INFRASTRUCTURE  
IMPACT ANALYSIS



*Questions?*



# Our Kelowna 2040



# Report to Council



**Date:** December 9, 2019  
**To:** Council  
**From:** City Manager  
**Subject:** Lake Avenue Dog Beach Trial Results  
**Department:** Parks and Buildings Planning

---

## **Recommendation:**

THAT Council receives for information, the report from the Parks & Buildings Planning Manager dated December 2, 2019 with respect to the Lake Avenue Dog Beach Trial Results;

AND FURTHER THAT Council directs staff to use the budget currently allocated for Lake Avenue Dog Beach for a permanent off-leash dog beach at Lake Avenue.

## **Purpose:**

To provide Council an opportunity to review the results of the two-year Dog Beach Trial at Lake Avenue and to make a determination about the future of the dog beach in this location.

## **Background:**

From August 2017 to September 2019, we invited residents to share their thoughts and experiences regarding the Lake Avenue Off-Leash Dog Beach Access. During the original engagement session at Lake Avenue in spring 2016, City staff received many strongly felt comments from nearby residents opposed to a dog beach in this location. However, it also received broad support from the wider community both online and at the city-wide open house. To allow for a better understanding of public opinion, a two-year trial was conducted. This also allowed both nearby residents and visitors to the beach to experience the space as an off-leash site.

In September of 2016, Council provided direction to staff to undertake a two-year trial for the use of Lake Avenue Beach Access as an off-leash dog beach. The trial considered water quality, community feedback, service requests, and considerations from Community Policing in the neighbourhood. This report summarizes these results and presents a plan with improvements, within the existing budget allocation for this project, reflecting the requests from the community. Suggestions from the community included better fencing, improved signage, and the addition of a doggie bag dispenser. A plan for these improvements is attached.



**Discussion:**

**Community Engagement.** The results from community engagement were strongly supportive of the off-leash dog beach, or supportive with conditions. Two of the 67 contributors were opposed. For those supportive with conditions, suggestions of additional fencing, signage and doggie bag dispensers were most prominent.

**Water Quality.** Water quality was tested over the summers of 2018 and 2019. The results for Vimy Avenue and Lake Avenue geomean were well within the guidelines for recreational water quality. Based on the past three years of data, continuous dog use did not seem to have notable impacts on Lake Avenue Beach Access on E. coli counts.

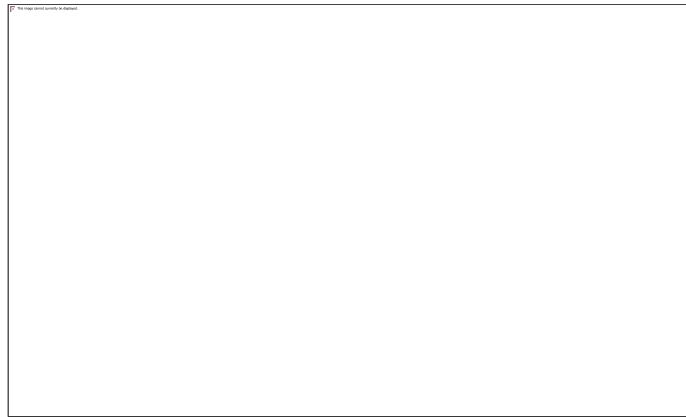


Figure 1. Water Quality Results

**Service Requests and RDCO Dog Control.** There were nine service requests specific to Lake Avenue during the two-year trial. One expressed concern over water quality and several over the need for additional fencing. Of the service requests, 4 were supportive of the dog beach with conditions. Five service requests were opposed, of which four were from the same resident. RDCO Dog Control had 2 complaints in 2017, one in 2018, and 2 in 2019.

**Community Policing.** Staff received feedback the Downtown Enforcement Unit. The Corporal who was familiar with the area spoke very favourably of the dog park. She said that since the dog park was introduced, she found that the park was not a problem, and felt there was fewer calls for service. The current Corporal for the Unit supported these comments noting there has been an increase in positive activity, for example, people with dogs appropriately using the park.

**Conclusion:**

The results of the trial indicate that the majority of contributors are in favour of making Lake Avenue Dog Beach permanent. Additionally, the results for water quality, as well as Dog Control and Community

Policing, are positive for the continued use of the beach for dogs. Many of the suggestions through the community engagement platform [getinvolved.kelowna.ca](http://getinvolved.kelowna.ca), as well as the Service Request system, cited the need for additional fencing, as well as doggie bag dispensers to be installed at Lake Avenue. Existing budget is available to improve fencing, creating greater separation between the dog beach area and the path of cyclists and pedestrians. Staff notes that it is not permitted to fence below the high-water mark, in order to allow free passage along the beach. Signage will be installed reminding dog owners to respect neighbours and keep their dogs within the designated area.

**Internal Circulation:**

Community Communications  
Infrastructure Operations  
Parks Operations  
Water Quality

**Considerations applicable to this report:**

***Existing Policy:***

Council Priorities 2019-2022 identified measures to transform this vision into action. Specifically, relevant to this report:

- Vibrant neighbourhoods, by creating animated parks and public spaces
- Vibrant neighbourhoods, through developing accessible and multi-purpose amenities.

**Financial/Budgetary Considerations:**

Existing budget as noted.

**External Agency/Public Comments:**

As noted.

**Considerations not applicable to this report:**

Legal/Statutory Authority:  
Legal/Statutory Procedural Requirements:  
Communications Comments:

Submitted by: M. Steppuhn, Parks Planner

Approved for inclusion: D. Edstrom, Divisional Director, Partnerships and Investments

cc: Ian Wilson, Infrastructure Operations Department Manager  
K. O'Rourke, Community Communications Manager

Attachment: Appendix A – Lake Avenue Dog Beach Summary of Engagement Results

# Appendix A



## Lake Avenue Dog Beach Summary of Engagement Results

From August 2017 to September 2019, we invited residents to share their thoughts and experiences regarding the Lake Avenue Off-Leash Dog Beach Access. During the original engagement session at Lake Avenue in spring 2016, City staff received many strongly felt comments from nearby residents opposed to a dog beach in this location. However, it also received broad support from the wider community both online and at the city-wide open house. To allow for a better understanding of public opinion, a two-year trial was conducted. This also allowed both nearby residents and visitors to the beach to experience the space as an off-leash site.

### Engagement Summary

The City's engagement website, [getinvolved.kelowna.ca](http://getinvolved.kelowna.ca), was utilized as the primary tool to capture feedback during the two-year trial. Residents could provide open ended comments to "tell us what they think" about Lake Avenue as an off-leash dog beach or ask questions.

The feedback opportunity was advertised:

- *On site at Lake Avenue Beach Access.*  
This was the most important consideration as we wanted to hear from those who visit/frequent the park and those who live nearby. A sign (pictured) was posted at the entrance of the park for the duration of the trial.
- *kelowna.ca/website.* Information was hosted on a number of webpages including the [Lake Avenue Beach Access page](#), [dog park public engagement webpage](#) and was intermittently featured on the [kelowna.ca](http://kelowna.ca) homepage.
- *Get Involved site and social media promotions.* Featured on the Get Involved site for duration of the two-year trial including on the featured projects section along with general social media posts about opportunities to provide feedback on City projects and initiatives including this trial.



### Engagement Results

From August 2017 to September 2019:

- 1245 were aware of the project (viewed the get involved Lake Avenue Dog Beach Trial page)
- 283 were involved (viewing or reviewing the page at a more detailed level)
- 92 engaged (providing feedback or asking questions)

*Get Involved – Tell Us What You Think (67 contributors)*

SUPPORT	44
SUPPORT w CONDITIONS	21
OPPOSED	2

*Get Involved – Ask a Question (24 questions, 15 of which expressed support or opposition)*

SUPPORT	8
SUPPORT w CONDITIONS	6
OPPOSED	1

*Service Request System (note that the SR system is not a traditional avenue to voice support)*

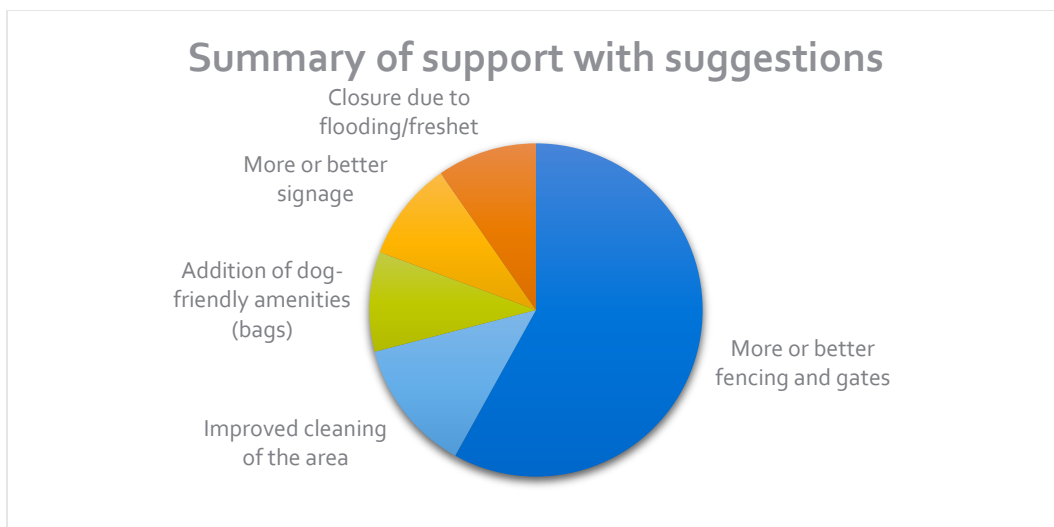
SUPPORT	0
SUPPORT w CONDITIONS	4
OPPOSED	5*

*\*four of the five service requests were from the same resident*

**Support with Conditions:**

Of the 31 total responses that supported the off-leash dog beach with suggestions, these included:

- Numerous requests for improved fencing and gating (18)
- Requests for better signage (3)
- Removal of glass/increased safety sweeps (4)
- Complaints of closure during flooding/freshet (3)
- Addition of dog-friendly amenities such as bag dispensers (3)



## Examples of Feedback Received:

- I took my dogs to this beach on August 12th. What a fantastic location for this! I always wanted to take my dogs downtown in the summer but until now there is really nowhere to let them off leash safely/legally. This beach provides enough room for them to actually run around and enjoy themselves. One small feedback would be to extend the fence further down towards the water or put up some other form of natural barrier.
- We love this new dog park! It is walking distance to our house and in great proximity for those hot days we walk downtown. We have not had any issues with other dogs like we have found at fenced in dog parks and this is our new favourite park!
- We already have enough beaches with dogs everywhere and dog dodo on the ground. I and my family want to swim and sit on beaches and not deal with wet dogs running around and angry dog owners who don't want to control their dogs because they say I am on their beach!!!!
- We own property one house away from Lake Ave beach We were strongly opposed to having a dog park there. We were pleasantly surprised of the success of this dog park and have made a 100% change in our opinion. It is working better than we expected.
- We have been to the new dog beach at Lake Ave several times and it has been wonderful. We live in the neighbourhood so we are very happy we now have somewhere to walk to with our little dogs. We have been driving 11kms to the Cedar Creek dog beach for years. We've met lots of nice people....locals and tourists...and the beach has been spotless. Hope everyone keeps up the good work so we can continue to enjoy the beach with our dogs!
- I think it's a great location for a dog beach. Hopefully more people start using it for their dogs and less for other activities. Just need a bit more signage so people know it's a dog beach.
- This is a fantastic idea. My dog loves to swim and the only place I can take her is all the way up Lakeshore. Great thinking.
- One of the best ideas the City has had! Kelowna was called the most pet friendliest city in BC, but pet owners felt there was a lack of beaches to take their dogs to (only one off leash) before Lake. It's so great to be able to take the dogs for a swim on a hot day! A couple of things we did notice is that it wasn't fully gated like the Richter dog park (that I believe we will be losing in the near future?) I would be better to have a gate at both entrances because of the people going over the walking bridge.
- I hate that the city took what was a mostly adult only beach and turned part of it into a dog park. I want to go to a beach that isn't over crowded and doesn't have screaming children running around. The city has ruined a beautiful beach because it wants to curb drug activity. I think you went about it all wrong. There aren't enough beaches to accommodate people and the city turns one into a dog park.





# Lake Avenue Dog Beach

## Trial Results

9 December 2019

# Lake Avenue Dog Beach Trial

- Two-year trial
- August 2017 to September 2019



# Process





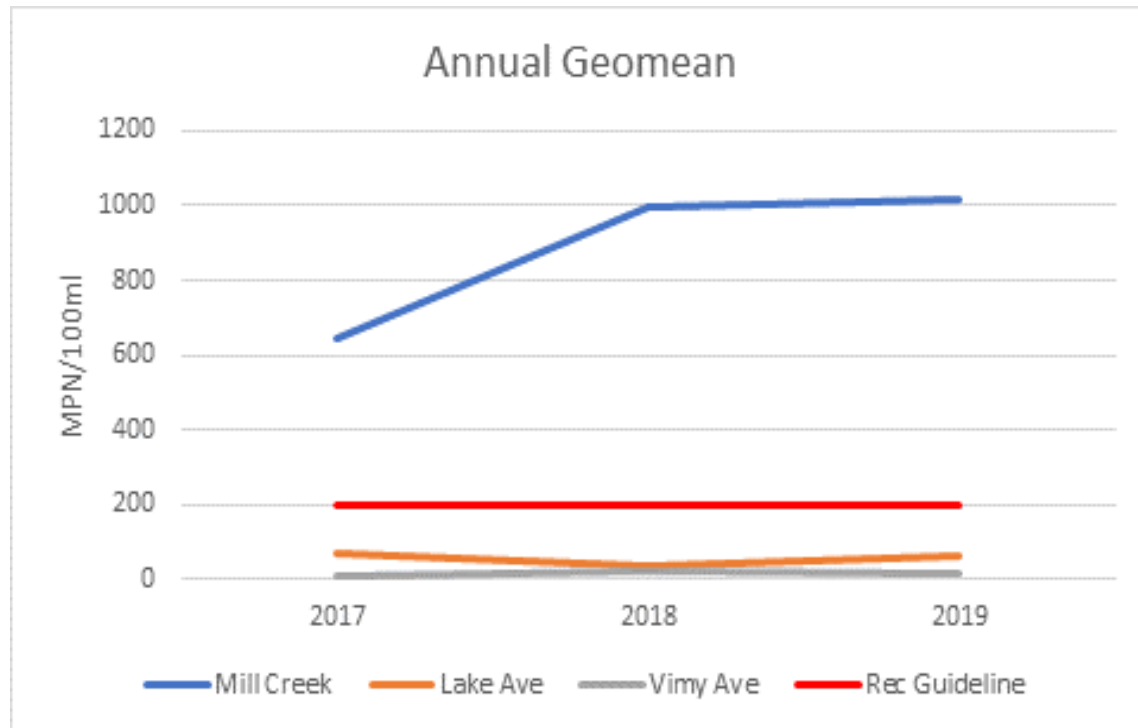


- Water Quality
- Community Engagement
- Service Requests
- Community Policing

Trial

# Water Quality

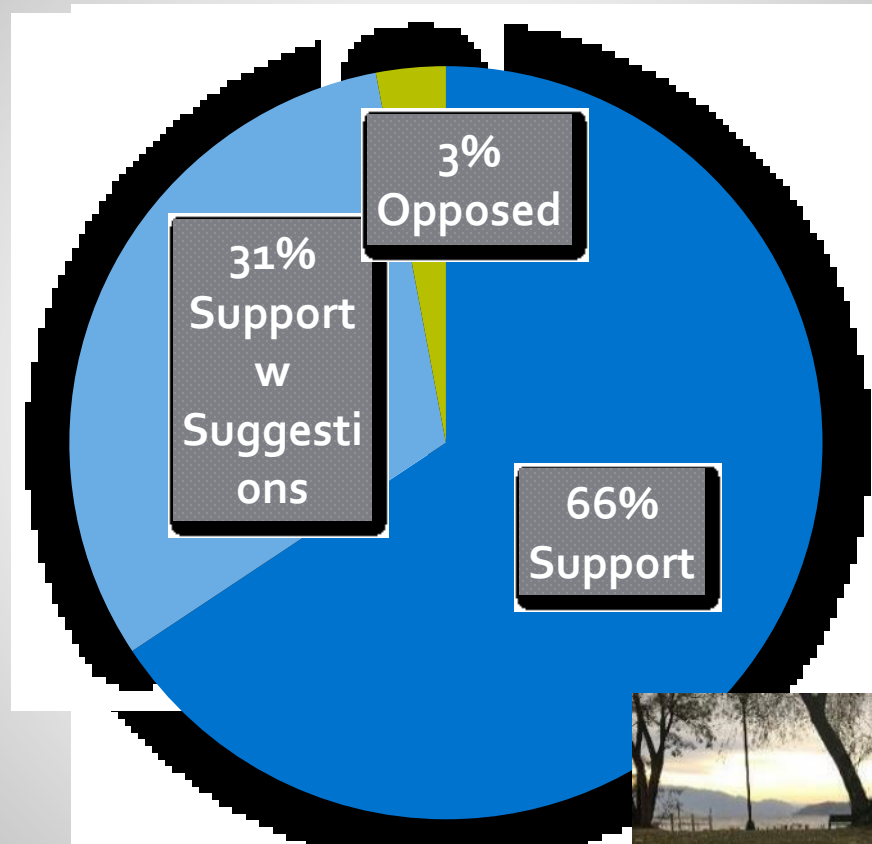
- Tested from summers 2017 to 2019
- Geomean for both Lake Avenue and Vimy Avenue were below the E.coli counts for Recreational Water Quality Guidelines
- Water Quality input from Mill Creek would greatly overshadow any influence from dog activity





# Get Involved

*Tell Us What You Think (67 contributors)*



## Engagement

"We own property one house away from Lake Ave beach We were strongly opposed to having a dog park there. We were pleasantly surprised of the success of this dog park and have made a 100% change in our opinion. It is working better than we expected."



# Service Requests and Dog Control

- Service Requests
  - 4 supportive with conditions
  - 5 opposed (4 from same resident)
- RDCO Dog Control
  - 2017 – 2 complaints
  - 2018 – 1 complaints
  - 2019 – 2 complaints



# Community Policing

- Favourable
- Fewer calls for service since trial began
- Increase in positive activity





# Summary

- Majority in favour
- Water Quality favourable
- Dog control
- Community Policing
- Supportive
  - Better fencing
  - Improved signage

# Recommendation

- ▶ THAT Council receives for information, the report from the Parks & Buildings Planning Manager dated December 2, 2019 with respect to the Lake Avenue Dog Beach Trial Results;
- ▶ AND THAT Council directs staff to use the budget currently allocated for Lake Avenue Dog Beach for a permanent off-leash dog beach at Lake Avenue.





## *Questions?*

For more information, visit [kelowna.ca](http://kelowna.ca).

# Report to Council



**Date:** December 9, 2019  
**To:** Council  
**From:** City Manager  
**Subject:** Subdivision, Development and Servicing Bylaw 7900 - Schedule 4 and 5 Update – Stormwater Management  
**Department:** Infrastructure

---

## **Recommendation:**

THAT Council receives, for information, the report from the Infrastructure Engineering Manager dated December 9, 2019, with respect to amending the Subdivision, Development and Servicing Bylaw 7900;

AND THAT Bylaw No. 11913, being Amendment No. 20 to Subdivision, Development and Servicing Bylaw No. 7900 be forwarded for reading consideration.

AND FURTHER THAT Council approve the revised Council Policy 265 - Engineering Drawing Submission Requirements shown as Attachment 1.

## **Purpose:**

To amend the Subdivision, Development and Servicing Bylaw 7900 Design and Construction Standards so it aligns with industry best practice and ensure construction of high quality and long-lasting infrastructure. To update the Engineering Drawing Submission requirements.

## **Background:**

Schedules 4 and 5 of Bylaw 7900 Subdivision, Development & Servicing Bylaw set the minimum standards and specifications for works and services in connection with developing and servicing lands within the City boundaries. Schedule 4 outlines the design standards and Schedule 5 outlines the construction standards for municipal infrastructure consisting of water distribution, sanitary sewer, drainage, roads, sidewalks, traffic signals, street lighting and landscaping.

The City periodically updates these standards and provides regular updates to assure that the infrastructure installed is high quality, long lasting and supports service delivery. These standards are also reviewed against industry best practice and practices in other communities to ensure the City is obtaining and delivering best value. Staff's goal is to maintain Bylaw 7900 as a living document.

To this end, a working committee was formed with representation from various internal groups, including Infrastructure Engineering, Infrastructure Delivery, Development Engineering, Community Planning and Civic Operations. The working group meets regularly to review sections of the Bylaw, engage with internal and external stakeholders (such as the Urban Development Institute) and recommend changes to bring forward to Council. The changes identified below form part of the major components as part of this revision.

## **Rationale for Stormwater Management Updates**

Since the last major update to the Drainage standards, the City has experienced unusual events, including high groundwater levels, landslides, lake flooding, pond overtopping, creek flooding and shore erosion.

This Stormwater Management standard update addresses some of the increased risks associated with managing uncertain rainfall patterns, snow depths, high groundwater and lake levels impacted from a changing climate. The City is committed to work with design professionals and provide discretion where necessary to minimize impacts to new and existing minor systems (sewers, manholes, gutters). Particular attention has been focused on drainage impacts from Hillside Areas, where additional flows can be expected into the storm system to prevent downslope issues.

Where existing Area Servicing Plans or Subdivision plans are in progress, new elements will be designed to the latest standards. There will be an expectation in that constructed minor works will remain in place. The City will continue to rely on design professionals to advise where downstream drainage system under-capacity situations may occur. The City Engineer can then evaluate whether additional works are required.

## **Summary of changes to Schedule 4 of Bylaw 7900 – Design Standards include:**

### **Chapter 0 - General**

1. Minor grammatical changes.
2. Change to “Chapter 3 – Stormwater Management” in title.

### **Chapter 1 - Water**

#### **Section 1.5 – Fire Flows**

This change to on-site Fire Flow requirements was required to address building fire flow requirements at the building permit approvals stage. Off-site infrastructure sizing will remain based on area zoning. On-site fire flow requirements at the Building Permit stage are now consistent with Section 3.2.5.7 of the BC Building Code. This modification is significant for those buildings with automated sprinkler systems constructed to NFPA 13 criteria. The calculated value must be confirmed to the City by an experienced professional. The Owner or Developer must report to the City that the calculated fire flow does not exceed the minimum requirements for that zoned area.

### **Chapter 3 – Stormwater Management**

1. The old Chapter 4 – Drainage in previous version has been replaced and revised.

2. The new Chapter 3 is now called Stormwater Management. The section is formatted for consistency with the Master Municipal Construction Documents (MMCD – Platinum version) Design Guidelines.
3. Climate change is now incorporated into the standard. Stormwater quantities will include a rainfall intensity increase of 15% in the calculations. This will confirm the capacity of critical structures and assure that minimum standard sizing is adequate for such items as sewers, mainlines, structures or culverts.
4. Stormwater quality is connected to the health of Okanagan Lake, our main City water supply. Some of the important features include:
  - a. No permanent pumping of groundwater will be permitted into the City storm sewer system. Where permanent pumping cannot be avoided, water licensing and separate private works will require approval from both the City and the Province.
  - b. Water quality will be measured against Provincial Recreational Water Quality Guidelines.
5. Hillside area infrastructure requirements have been updated and focus on safely conveying stormwater. This includes:
  - a. Eliminating infiltration to ground (in most instances).
  - b. All roof leaders and site drainage must connect to the City storm system.
  - c. A new map has been added to identify areas of allowable infiltration to ground.
6. All stormwater design must clearly demonstrate a path that does not impact downstream properties.
7. Erosion and Sediment Control Guidelines have been enhanced to assure that responsibilities on construction sites is clearly defined.
8. Stormwater infrastructure details have been updated. This includes, but not limited to:
  - a. Minor Systems: manholes, catch basins, service connections, mainline sizing, roof leaders, curved sewers, modelling requirements, etc.
  - b. Detention Controls: parking lot storage, underground storage, detention ponds, outlet Controls, etc.

**Summary of changes to Schedule 5 of Bylaw 7900 – Construction Standards include:**

- SS-S50 - Catch Basin Lead Sizing – Revised chart adjustments for combination of leads.
- SS-S56 - IDF Curves – City of Kelowna at Kelowna International Airport (YLW) - Revised intensity duration curves from Environment Canada (generated from 2014 data).
- SS-S58 - Groundwater Recharge Suitability Map (new)

The working group continues to work closely to align other service areas like roads, traffic signals, landscaping, agricultural irrigation and wastewater with MMCD where practical. Updates on this work will be brought forward to Council at a later date.

**Attachment 1 - Update to Council Policy 265 - Engineering Drawing Submission Requirements**

The purpose of this policy update is to provide revised and new minimum standards and requirements that the City will accept in the submission of work(s) and services.

**Internal Circulation:**

City Clerk  
 Community Engagement Manager  
 Development Engineering Manager  
 Infrastructure Delivery Department Manager

Infrastructure Operations Department Manager  
Legislative Coordinator  
Project Manager - Water Integration Project  
Utility Planning Manager

**Legal/Statutory Authority:**  
Community Charter Section 8.

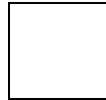
**Considerations not applicable to this report:**

Legal/Statutory Procedural Requirements  
Existing Policy  
Financial/Budgetary Considerations  
Personnel Implication  
Communications Comments  
Alternate Recommendation

Submitted by:

Joel Shaw, P.Eng., Infrastructure Engineering Manager

**Approved for inclusion:**



A. Newcombe, Division Director, Infrastructure

Attachment 1 - Council Policy 265 - Engineering Drawing Submission Requirements  
Attachment 2 – Bylaw 7900 – Schedule 4 and 5 Update Presentation

cc: City Clerk



# Bylaw 7900 – Subdivision, Development and Servicing Bylaw 2019 Updates

Joel Shaw, P. Eng. & Rod MacLean, P. Eng. | December 9, 2019

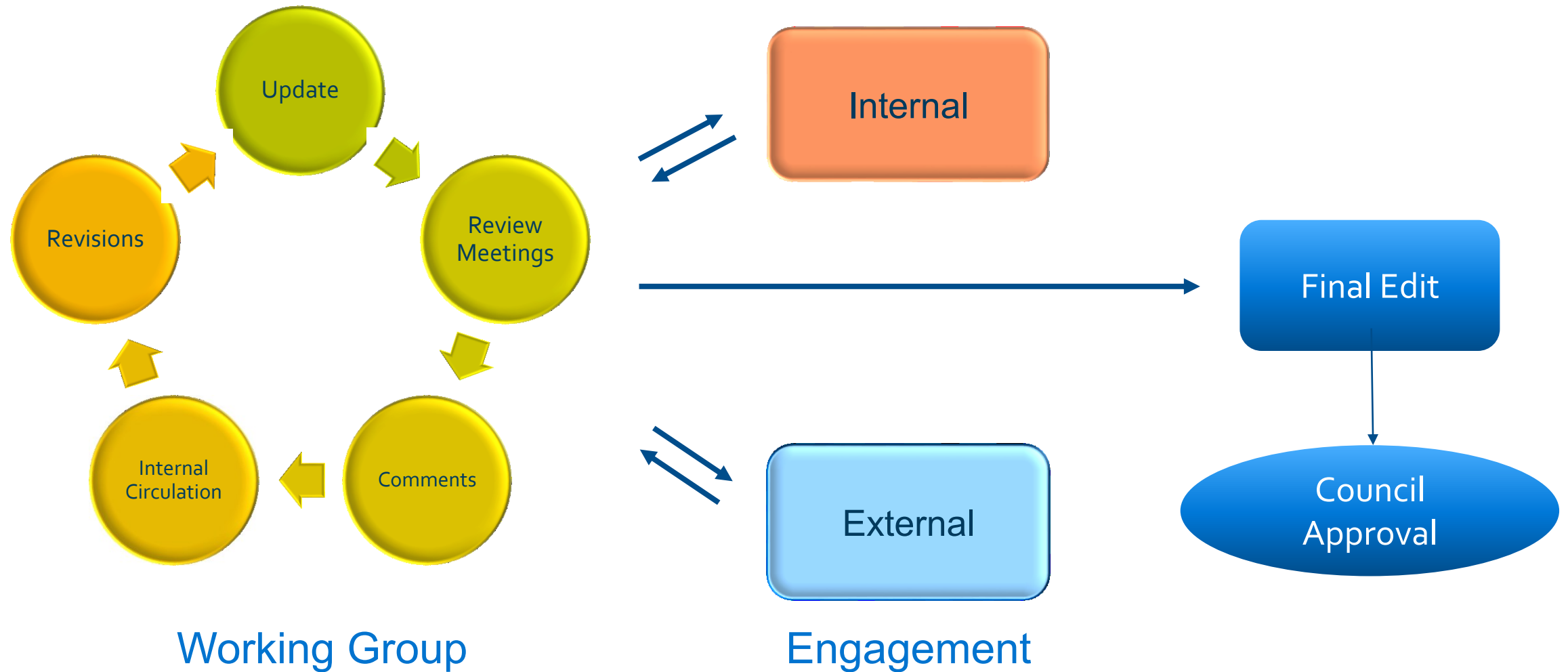
# Subdivision, Development and Servicing (Bylaw 7900)

- ▶ Sets out subdivision application procedures
- ▶ Provides Design and Construction Standards for works and services
  - ▶ Schedule 4 – Design Standards
  - ▶ Schedule 5 – Construction Standards
- ▶ Approved Products List (Policy 266)

# Objectives

- ▶ Update Design and Construction Standards in Bylaw 7900
  - ▶ Format to Municipal Infrastructure Design Guidelines - (MMCD, 2014)
- ▶ Ensure quality and long-lasting infrastructure
- ▶ “Living Document”
- ▶ Complements:
  - ▶ MMCD Specifications and Standard Detailed Drawings.
  - ▶ City - Schedule 5 – Supplementary Specifications and Supplementary Standard Detailed Drawings.
  - ▶ City - Policy 265 (Engineering Drawing Submission Requirements).

# Bylaw 7900 Review Workflow



# Design and Construction Standards

- ▶ Water
- ▶ Sanitary Sewer
- ▶ Drainage
- ▶ Transportation
- ▶ Electrical, Street Lighting & Signals
- ▶ Landscaping & Irrigation
- ▶ Hillside Development Standards



# Design and Construction Standards

- ▶ Water
- ▶ Sanitary Sewer
- ▶ ~~Drainage~~ Stormwater Management
- ▶ Transportation
- ▶ Electrical, Street Lighting & Signals
- ▶ Landscaping & Irrigation
- ▶ Hillside Development Standards

# Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ Improve Water Quality
- ▶ Address Climate Change
- ▶ Hillside Issues



# Focus in 2019 - Stormwater

## ► Protect Okanagan Lake

- Route minor systems to natural watercourses
- Eliminate permanent groundwater pumping into the City storm sewer.
  - Reduce capacity issues near outlets to lake
  - Eliminate reliance on deeper water table
  - Improve groundwater quality into Okanagan Lake.

► Improve water quality

► Address Climate Change

► Hillside and Hazardous Slope Issues



# Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ **Improve water quality**
  - ▶ Emphasis on water quality to Okanagan Lake.
  - ▶ Must meet Provincial (MOE) Recreational Water Quality Guidelines.
  - ▶ Control manholes added
  - ▶ Expanded Erosion & Sediment Control process
- ▶ Address Climate Change
- ▶ Hillside and Hazardous Slope Issues





# Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ Improve water quality
- ▶ **Address Climate Change**
  - ▶ Consultant to consider climate change in calculations
  - ▶ Flow conditions/debris flow
  - ▶ Minimum 15% capacity increase
  - ▶ Response required.
- ▶ Hillside and Hazardous Slope Issues





# Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ Improve water quality
- ▶ Address climate change
- ▶ **Hillside Issues**
  - ▶ Focus on safe conveyance of stormwater to natural watercourse
  - ▶ Roof/site drainage direct to Storm
  - ▶ No infiltration to ground (except foundation)



# Focus in 2019

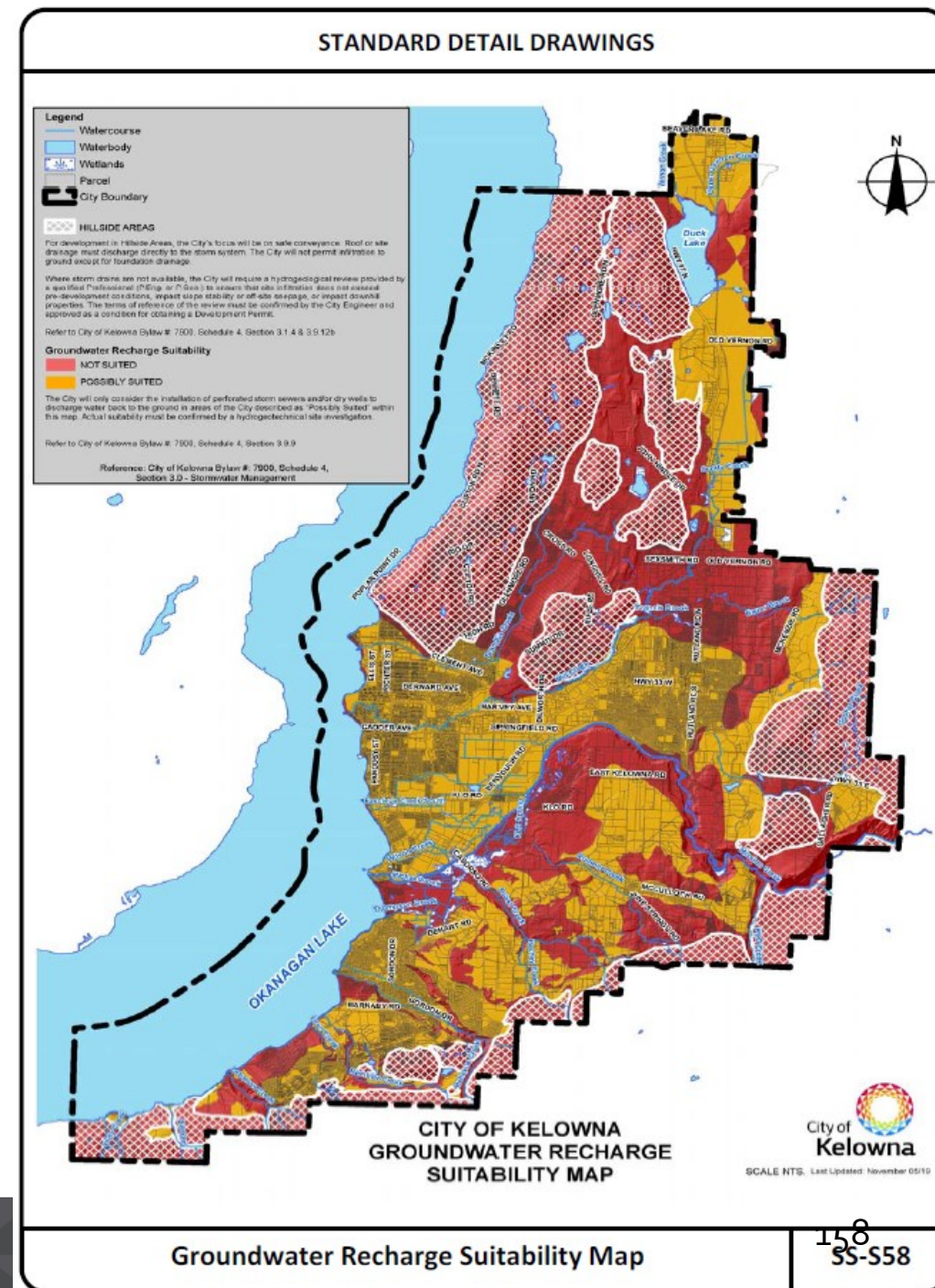
- ▶ Fire Flows (Section 1.5)
  - ▶ Development Permit Stage
    - ▶ No change – Use table.
  
- ▶ Building Permit Stage
  - ▶ Section 3.2.5.7 of the BC Building Code.
  - ▶ Automated sprinkler systems to NFPA 13
  - ▶ Confirmation required.
  - ▶ Calculated fire flow must not exceed the minimum requirements for that zoning



# Drawing Updates

## New

- Map identifying where Infiltration to Ground is suitable.

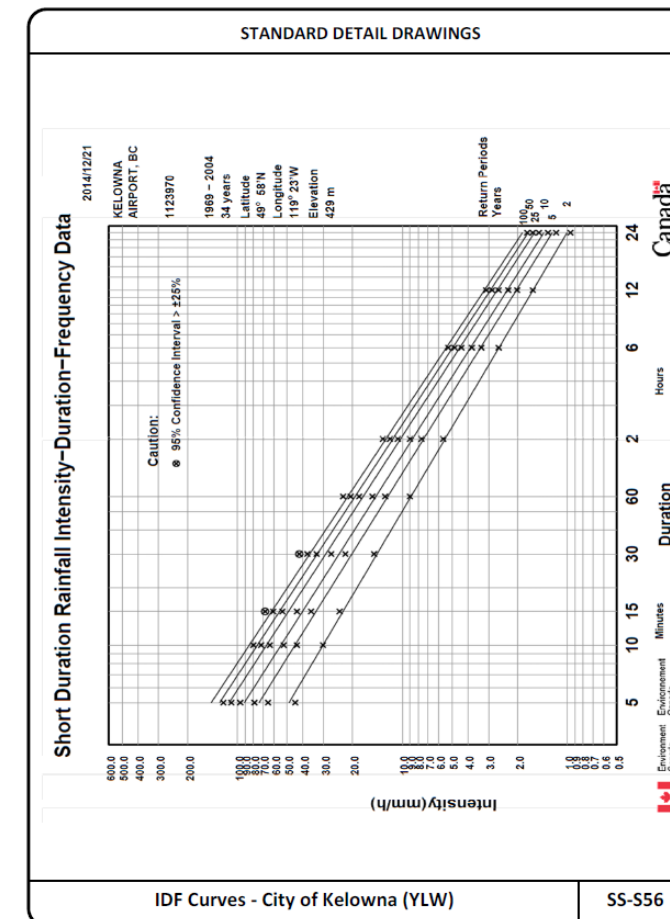
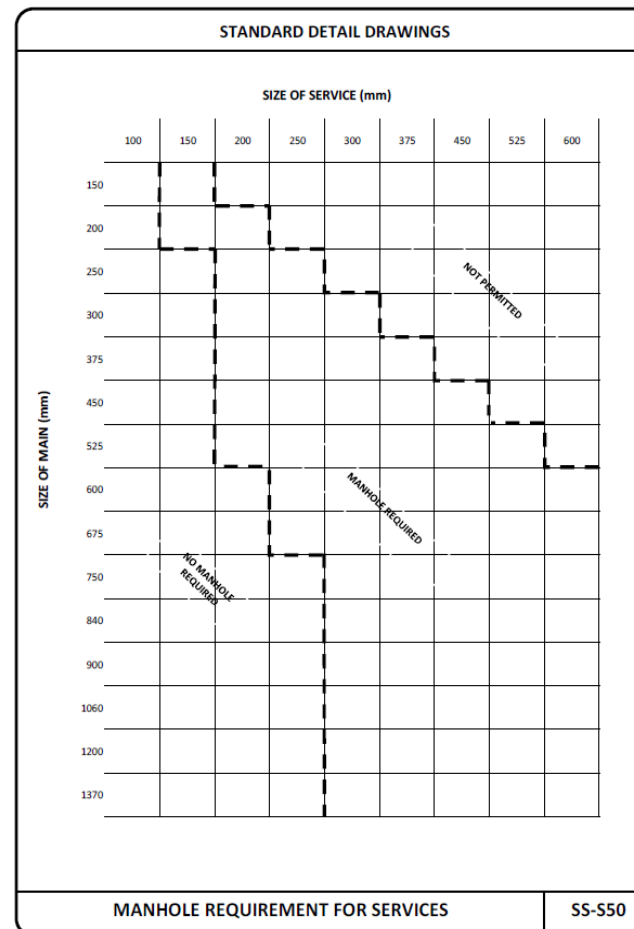




# Drawing Updates

## Updated

- Service/Manholes Sizing
- IDF Curve



# Policy Update



City of Kelowna  
1435 Water Street  
Kelowna, BC V1Y 1J4  
250 469-8500  
[kelowna.ca](http://kelowna.ca)

POLICY 265

## Council Policy

### Engineering Drawing Submission Requirements

Approved June 4, 2001

Contact Department: Utility Planning

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#### Guiding Principle

- To define standards for drawing submissions to the City.

#### Purpose

- The purpose of this policy is to outline the minimum standards and requirements the City will accept in the submissions of work(s) and services.

#### Statement

- These procedures support submissions consistent with Bylaw 7900 - Subdivision, Development & Servicing. The City Engineer or designate will review each submission for conformance.



# Thank you







City of Kelowna  
1435 Water Street  
Kelowna, BC V1Y 1J4  
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# Council Policy

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## A. GENERAL

Drawings may combine various services on one plan but must be clear, readable and agreed upon by the City Engineer or designate prior to the acceptance of Issued for Construction drawings. Refer to the table below for requirements.

Drawing Submission Options			
	Combining Services	Color	Greyscale
Preliminary Designs	Yes	Yes	Yes
Detail Designs	Yes	Yes	Yes
Issued for Construction	Yes	Yes	Yes
Record Drawing	No	Yes	Yes

## B. DRAWING STANDARDS

Drawings shall clearly show the existing, proposed and to be abandoned locations of all utilities using offsets from property lines or boundaries of rights-of-way.

Dimensioning and "offset measurements" required by this policy may be minimized on the construction drawings. However detailed field measurements are required on the record drawings, for City records, in accordance with this policy.

Elevations shall be relative to geodetic datum. The horizontal coordinates shall be referenced to the UTM NAD 83 coordinate system. A minimum of one (1) reference bench mark with elevation shall be shown on each design drawing.

Chainage shall increase from left to right and from bottom to top on a drawing. North should be at the top or right side of a drawing. North arrow should be placed on the drawing as to not obstruct the design elements.

Where a City of Kelowna standard drawing exists, it shall be sufficient to refer to the appropriate drawing by reference number and date of issue. Where a standard drawing does not exist, or is unsuitable for a particular case, detail drawings shall be provided.

All drawings shall be signed and sealed by an appropriate Professional registered in British Columbia.  
(December, 2019)

**Sheet Sizes**

Drawing shall be submitted using the following standard sheet sizes (outside dimensions):

ANSI D 558.8 x 863.6 (22 x 34)

ANSI B 279.4 x 431.8 (11 x 17)

All drawing submissions must be produced using ANSI D paper size, unless mutually agreed otherwise. ANSI B paper size to be utilized as a fully scalable half-size plot (field reviews).

**Title Block**

The City A size block shell shall be used for all drawings. The title shall describe the contents of the drawing (e.g. key plan, road, etc.) and shall clearly indicate the location of the works by road name(s). Do not include developer name or legal descriptions in title area.

**Scales**

The following scales shall normally be used:

Location Plans	-	1:2500; 1:5000; 1:10000
Composite Plans	-	1:500
Details	-	1:100; 1:500; 1:75; 1:20; 1:10
Plan/Profile	-	Horizontal 1:500 or 1:250
	-	Vertical 1:25 or 1:50
Cross-Sections	-	Horizontal 1:100
	-	Vertical 1:25 or 1:50

**Dimension, Units and Text**

All Dimension must be shown in metric and maintain an accuracy of minimum of 2 decimals places. All text shall be Leroy font and maintain a ratio of 1:10 between the text height and printed line thickness.

- Minimum printed text height is 1.5mm
- Maximum printed text height is 5mm
- Standard text height is 2mm

**Legend**

The legend shall be contained on the City's standard sheet size.

**Media Submissions**

Drawings shall be submitted using the following media types:

- Preliminary or design drawings - paper and/or electronic (PDF)
- Record drawings - paper and/or electronic (PDF)
- Record drawings - AutoCAD drawing format and all digital files (i.e. AutoCAD)

Digital files submitted to the City of Kelowna must be in a current AutoCAD drawing file format. The information may be supplied on either CD, flash drive or other method. The submission must include all files required to create the project. Any AutoCAD objects used to create the drawing must remain intact and may not be exploded or modified.

**Drawing and Plotting Appearance**

The City's Civil3D template can be downloaded from the City's website. The template is based on AutoCAD conventions and plotted drawing appearance, including a title block. All drawing submission must follow the layering, symbology and line types as contained in the template.

The following information must be included on all drawings;

**Plan View;**

- a) Offset of pipelines from property lines.
- b) The Infrastructure, diameter, and material of pipe. (e.g. WAT 250mm PVC DR 25)
- c) Offset of service connections from nearest property line.
- d) The locations of manholes, clean-outs and services relating to property lines.
- e) Information on any curves or deflections, if applicable, to pipe design.
- f) Easements; existing and/or required. Reference applicable plan number on the drawing.
- g) Future works as required.
- h) The extent of work required of the City of Kelowna to make the connection(s) to existing live mains.
- i) The location of hydrants, valves, end of the main, services and other appurtenances tied to the nearest property line.
- j) Note the location of any abandoned infrastructure.

**Profile View;**

- a) Surface profiles (existing and design, if applicable) over proposed main.
- b) Infrastructure, Length, diameter, material and grade of pipe (e.g. WAT 84 m - 200 mm PVC DR 25 @ 1.15%).
- c) Profiles of invert and crown of pipes.
- d) Percent grades to two decimal places.
- e) Bedding, backfill and surface restoration requirements.
- f) Location, diameter, material and invert elevation of all crossing utilities (existing and abandoned).
- g) Profile only of any existing, proposed or abandoned infrastructure.
- h) storm, sanitary sewers, water and culverts.

## **C. REQUIRED DRAWINGS**

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Each set of drawings shall include the following drawings and shall be presented in the same order:

**Cover Sheet**

The cover sheet shall note the consultant's name and phone number, a description of the project, the City project number, legal description of the lands involved, a site location plan and a drawing index of all the drawings provided in the submission. The following statement must be contained on the cover sheet;

*"The Professional of record responsible for the design confirms that the drawing set provided complies with Bylaw 7900, Council Policy 265 and 266. Any discrepancies or deviations are noted below: "*

**Legend and General Notes**

The legend and note sheet shall contain the project legend and general project notes any specific notes must be contained on the appropriate drawing.

**Composite Plan**

The composite plan shall show the area being served with lot numbers, and all active and proposed Works and Services. Geodetic survey monuments will be shown.

**Removal Plan**

The removal plan shall show any infrastructure or objects that will be removed during the proposed Works and Services.

**Road Drawings (Plan/Profile)**

Both plan and profile stationing must be tied to a property line or road boundary. Drawings shall show width of road, road structure, width of shoulders and the offset of curb from property line.

Chainages of the B.C. and E.C. of horizontal curves shall be shown together with the delta angle, centreline radius, tangent length, and centreline arc length. Curb radii are not required if the centreline radius and road width are shown, except on curb returns at intersections and at the end of cul-de-sacs.

The percent grade to two (2) decimal places shall be shown on the profile, together with the following information on vertical curves:

(December, 2019)

- a) The chainage and elevations of B.C., E.C. and P.I.
- b) The external value, e.
- c) The length of vertical curve.
- d) The chainage and elevation of the low spot of sag curves.
- e) K value of vertical curvature (crest on sag).

Profiles are to show all relevant surface features including:

- a) Existing ground elevation along the centreline of proposed roadway and/or the edge of existing asphalt.
- b) Existing curbs, gutters and sidewalks.
- c) Elevation of private driveways, doorways, and sidewalks at property line, and any other relevant information.
- d) The designed gutter and/or centreline grade.

On super-elevated curves and crossfall sections, the drawings shall show a profile of each gutter with pertinent gutter elevations either on the profile or in tabular form. At all intersections, the drawing shall show a profile of each curb return with pertinent gutter elevations.

The profile shall be shown at true centreline length and projected above to the plan in as close a relationship as possible. The plan shall show the location of catch basins (using road chainage) and catch basin leads.

#### **Water Drawings**

##### **Plan and Profile**

The following information shall be shown:

The top half of a Plan/Profile sheet shall show the Plan view, and shall show the legal layout, with legal descriptions of all properties, the location of all sidewalks, catch basins, underground utilities such as sewer, storm, water, telephone, television, fibre, power, manholes, valves, hydrants, and all survey monuments, etc.

Drawings shall also show existing dwellings, fences, trees, hedges, unusual ground features, existing roads and driveways including the type such as asphalt, concrete or gravel.

Baselines and proposed works are to be referenced to legal corner(s) on each sheet. Dimensions of road allowances are to be shown on each sheet.

##### **Plan View:**

The following information shall be shown on the PLAN VIEW:

- a) Information as detailed under "General" and "Drawing Standards".
- b) The location of hydrants, valves, end of the main, services and other appurtenances dimensioned from the nearest property line.
- c) Size, type, make & model of pipes, valves and fittings. This information can be placed on drawing as note or as a table.

##### **Profile View:**

The following information shall be shown on the PROFILE:

- a) Information as detailed under "General" and "Drawing Standards".
- b) Invert elevations of fittings or other appurtenances.

#### **Storm Sewer and Sanitary Sewer Drawings**

Plan and Profile -infrastructure must be separated, refer to Section A.



The top half of a Plan/Profile sheet shall show the Plan view, and shall show the legal layout, with legal descriptions of all properties, the location of all sidewalks, catch basins, underground utilities such as sewer, water, telephone, television, power, manholes, valves, hydrants, and all survey monuments, etc.

Drawings shall also show existing dwellings, fences, trees, hedges, unusual ground features, existing roads and driveways including the type such as asphalt, concrete or gravel.

Baselines and proposed works are to be referenced to legal corner(s) on each sheet. Dimensions of road allowances are to be shown on each sheet.

The drawings shall show the structural details of all manholes and chambers, etc. not covered by standard drawings. Where the sanitary sewers and storm drains or other utilities are to be installed in a common trench, a typical cross-section showing vertical and horizontal distances between pipes and classes of pipe and bedding shall be shown.

#### **Plan View:**

The following information shall be shown on the PLAN VIEW:

- a) Information as detailed under "General" and "Drawing Standards".
- b) Unique Manhole identification numbers.
- c) For pipes servicing lots, inverts of connections at property line. Inverts to be "boxed in" for easy identification and dimensions from nearest property line.
- d) For pipes servicing lots, basement elevations on each house.
- e) For sanitary sewer, where service connections are required, location of existing septic tanks.
- f) For storm drainage, features such as ditches, culverts, streams, channels, etc.
- g) Size, type, make & model of pipes, valves, and fittings. This information can be placed on drawing as note or as a table for information to servicemen and to confirm parts are on approved products list. For pipes servicing lots, basement elevations on each house.
- h) A table including catch basin information, coordinates and rim elevations.

#### **Profile View:**

The following information shall be shown on the PROFILE (bottom of sheet):

- a) Information as detailed under "General" and "Drawing Standards".
- b) Invert elevations at both inlet and outlet of manholes.
- c) Designation of manhole stationing.
- d) Unique manhole identification number.
- e) For pipes servicing lots, basements elevation with symbols.
- f) For pipes servicing lots, service connection symbols for invert elevation at the property line.
- g) Rim elevations of proposed or adjusted manholes, as required.

#### **Lot Grading Plan**

Shall be at 1:500 scale and identified as per key plan system if more than one sheet is required. Plan shall note:

- a) The pre-development contour lines. This topography shall extend a minimum 30.0 m outside the development site;
- b) all existing corner lot elevations (un-circled);
- c) all proposed corner lot elevations (circled);
- d) The proposed building envelope with the Minimum Basement Elevation (MBE) noted;
- e) The slope of the lot (directional arrow), noting a minimum 1% grade on the lots;
- f) The minor (5 year return) storm sewer system with the flows noted per section and the accumulated flows from all upstream sections. Provision must be made for upstream development potential where applicable;
- g) The major (100 year return) system. The Consultant shall note wherever the major system is not in the pipe or the roadway, showing the routing and flows for the 100 year return storm;
- h) All swales proposed to affect the submitted Storm Water Management Plan;
- i) Indicate how the development proposal will affect adjacent lands. Attempts should be made to "meet" existing elevations along the development boundary;
- j) A legend noting all items proposed in the Storm Water Management Plan. Applicable "General Notes" should also be included.

#### **Storm Water Management Plan (SMP)**

- a) Site and surrounding area (400 m minimum outside development) showing roads and major features (1:2500 scale). A small location plan of the watershed is also to be included.

(December, 2019)

- b) Contours of existing ground (1.0 m intervals where slope <20%, 2.0 m >20%) for the site and surrounding area mentioned above.
- c) Major flood routing (1:100 year); show as arrows and indicate if in pipe or on surface show an "open" arrow for surface routes and the same arrow "shaded" for routes in pipes).
- d) Detention pond details, if applicable.
- e) Area, in hectares, of development and the total area of drainage basin.
- f) Directional arrows of flow within the site and on surrounding areas.
- g) Sub-catchment boundaries, coefficients and areas.
- h) Pipe system including size, grade, and minor and major flows (a table may be utilized).
- i) The subject development is to be highlighted.

### **Erosion and Sediment Control Plan**

This plan is to detail methods and procedures that will be used to prevent or minimize soil displacement and transport of sediment from the Development site. This is to include methods to prevent or minimize soil transport onto adjacent properties or onto existing roads adjacent to the site (i.e. tracking from vehicles). Preventative methods of soil displacement on the site are to be detailed. The drawing shall show the following:

- a) Existing contours of the site at an interval sufficient to determine drainage patterns.
- b) Final contours if the existing contours are significantly changed.
- c) Final drainage patterns/boundaries.
- d) Existing vegetation such as significant trees, shrubs, grass, and unique vegetation.
- e) Limits of clearing and grading.
- f) Erosion and sediment control measures (temporary and permanent) including locations, names and details, in accordance with "Best Management Practices for Erosion and Sediment Control - Upland Works, City of Kelowna" and "Land Development Guidelines for the Protection of Aquatic Habitat - DFO + BC MOE".
- g) Storm Drainage systems including drain inlets, outlets, pipes, and other permanent drainage facilities (swales, waterways, etc.).

The plan must have a narrative section describing the land, the disturbing activity and details of the methods used for controlling erosion and sedimentation. Include a description of the procedures for construction and maintenance of the control measures. Note the persons involved in maintenance and provide a maintenance schedule that is to be followed.

### **Street Lighting Plans**

Shall be a plan view (1:500) of the street lighting proposal. There shall be General Notes included on the Plan noting reference(s) to the Municipal Standards and Specifications and the appropriate design criteria. Generally, street lights shall be located at all intersections and within 1.0 m of the side property lines. Any street lighting plan(s) should be accompanied with the photometric calculations. All designs must be signed and sealed by a Professional Engineer qualified to do street light calculations.

### **Traffic Signal (Control Devices) Plans**

Traffic signal designs are highly specialized and will therefore be prepared, signed and sealed by a Professional Engineer qualified in this area of expertise.

Traffic signals will be designed in general accordance with Sections 402.6 of the Ministry of Transportation Electrical and Traffic Engineering Manual. Contrary to this manual the City uses NEMA phase designations as opposed to the Ministry movement designations. Traffic signal designs will also conform to the British Columbia Motor Vehicle Act and the Manual of Uniform Traffic Control Devices for Canada.

Traffic signal timing/coordination plans will typically be provided by the City. In the case where this work is to be provided by the Developer; a qualified traffic Professional Engineer with PTOE certification is retained by the Developer.

The following information shall be shown on the PLAN VIEW:

- a) Information as detailed under "General" and Drawing Standards".
- b) The plan will be at a scale of 1:200 with north arrow oriented at 0 degrees.
- c) Existing and proposed civil information including roadway, sidewalks, letdowns, underground utilities, signing and road markings.

- d) The designed signal including pole locations, controller location, conduits (power and communications), junction boxes, wiring/cabling, point of electrical service and any additional information required by the City.
- e) General notes.
- f) Existing signal equipment to be retained and/or removed.
- g) City colour code chart.
- h) Pole coordinate table.
- i) Signal display schematic.
- j) Intersection illuminance table.
- k) Loop detector coordinate table (if applicable).
- l) Image sensor table (if applicable)
- m) References to Supplementary Standard Drawings.
- n) A table to provide pole coordinates and top of base elevation.

The following information shall be shown on the ELEVATION VIEW:

- a) Information as detailed under “General” and Drawing Standards”.
- b) The plan will be at a scale of 1:75.
- c) Elevation and description for each signal pole including corresponding concrete base type, signal displays, luminaire, pushbuttons, signs and image sensor (if applicable).
- d) Pre-approved product list for applicable equipment to be supplied.
- e) References to Supplementary Standard Drawings.

#### **Street Signs and Road Markings Plans**

Provide a plan which clearly shows existing and proposed traffic road markings and signage. All details will be to MUTCD standards (Manual of Uniform Traffic Control Devices for Canada prepared by the National Committee on Uniform Traffic Control) unless otherwise accepted by the City. Additional reference to the Ministry of Transportation and Highways - Manual of Standard Traffic Signs and Pavement Markings may be used when specific signs are required that are not denoted in the MUTCD. The plan will be at a scale of 1:500.

The following information shall be shown on the Plan:

- a) Information as detailed under “General” and “Drawing Standards”.
- b) Existing and proposed roadway, sidewalks, letdowns, signing and road markings.
- c) Existing signing and road markings to be retained and/or removed.
- d) The designed road markings.
- e) The designed signing including overhead signs mounted on pole structures.
- f) The sign offset and method of installation as denoted on Supplementary Standard Drawings.

#### **Traffic Control Plan**

Detailed routes for traffic including vehicle, cyclist and pedestrians, construction traffic and Traffic Control on existing roads affected by construction shall be prepared and implemented in accordance with the current Traffic Control Manual for Work on Roadways, the Manual of Uniform Traffic Control Devices for Canada, all City of Kelowna Bylaws that pertain to Traffic Control and all WorkSafeBC regulations.

#### **Construction Details**

Show all proposals for construction which are not covered or specifically detailed in the City Standards and Specifications. Where there is a City Standard, it is expected to refer to the Drawing Number. It is not necessary to include or provide work(s) for which there is a Standard Drawing.

#### **Electrical, Gas and Communication Utilities**

Per appropriate authority (individual utilities may provide separate drawings).

#### **Road Cross-Section Plans**

Shall be scaled at 1:100 horizontal and 1:50 vertical and shall note the existing ground elevation, the proposed elevations of the road centreline, the curb and gutter (or road edge) and property lines. Cross-sections are required at 20.0 m intervals. The City Engineer may waive or reduce the number of sections required where the information is not beneficial. Additional sections may be required or requested where excessive cuts or fills are involved.

## D. DRAWING SUBMISSIONS

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The first complete design submission shall consist of:

- a) One complete electronic set (PDF);
- b) Two complete paper sets of drawings;
- c) Two lot grading plans;
- d) Soils report (to verify road structure design) (Soils reports shall be required on all new road construction design) in accordance with Subdivision, Development & Servicing Bylaw;
- e) Utility calculations for water, sanitary, storm sewer to confirm that designed is in accordance with Subdivision, Development & Servicing Bylaw;
- f) Owner/consulting engineering confirmation letter;
- g) Quality Control and Assurance Plans for:
  - 1.Design;
  - 2.Construction; and
  - 3.Record-keeping all in accordance with Schedule 3 of the Subdivision, Development & Servicing Bylaw.

Subsequent design submissions requiring changes to the previous submission shall consist of:

- a) One complete electronic set (PDF);
- b) Two complete sets of paper drawings;
- c) A complete construction cost estimate;
- d) All submissions subsequent to first submission shall have highlighted with yellow any changes made by the Design Engineer which are in addition to "Red Line" changes required by the City;
- e) Items "Red Lined" must be addressed by the Design Engineer. Failure to do so will result in submissions being returned.

The Issued for Construction submission shall consist of:

- a) One complete sealed electronic set (PDF).
- b) A complete construction cost estimate.
- c) Four complete hard copy sets of sealed drawings.
- d) Electronic digital files.

## E. CONSTRUCTION ESTIMATE

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The construction cost estimate shall be broken down in a format as approved by the City Engineer or designate. These items and costs will be reviewed and amended where or if necessary.

## F. RECORD DRAWINGS AND SERVICE INFORMATION

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Record drawings, new and decommissioned service connection cards, hydrant data sheets and construction estimate must be submitted to the City Engineer or designate. Record drawings shall include relevant construction and design information. Notes shall be modified to reflect actual construction. Any existing infrastructure that has been abandoned in place must remain on the drawing and be labeled accordingly. Any infrastructure removed during construction must be deleted from the record drawings.

AutoCAD data that is forwarded to the City by the Consultant must conform to the requirements and formats set out herein. The AutoCAD data submission must be same file that was used to generate the hard copy. Failure to comply will result in work being returned to the Consultant for correction at the Consultant's expense.

Service connection cards, Service disconnect cards and hydrant data sheets in the format provided by the City are to be forwarded to the City Engineer or designate at the time of submission of the record drawings. The service records shall clearly detail the location of all services. If connections are skewed to the property line, the connection shall be located at the main by showing the distances from property lines as well as located at the property line. The hydrant data sheets must be supplied for each hydrant and include fire flow data to confirm that they meet Bylaw 7900.

The following procedures shall be followed in the submission of "Record" drawings for municipal acceptance:

- a) Record drawings and service information must be submitted within 90 days of the issuance of substantial completion.
- b) Sealed record drawings and all information noted within section F. One marked-up set of the record prints may be returned to the Consultant for revisions.
- c) Drawings must be signed and sealed by an appropriate professional registered in British Columbia. The drawings must contain either no disclaimer or the following statement.

"I hereby give assurance that the new works shown on this drawing were inspected during construction and substantially reflect the installed works in all material aspect"

- a) Record drawings shall include the following drawings:
  - a. All drawings contained in the issued for construction set.
  - b. Design drawings not requiring "Record" but shall be included as paper prints for City records are:
  - c. Storm Water Management Plan.
  - d. Erosion and Sediment Control Plan.
  - e. Road cross sections.
- b) The Professional Engineer shall also submit the "Assurance of Professional Field Inspection and Compliance Form"
- c) Final inventory sheets of infrastructure installed in format as provided by the City Engineer or designate.

#### **Amendments**

RESOLUTION: R\_\_\_/19/12/09

REPLACING: R375/10/04/26; R1039/08/11/24; R59/99/01/25; R445/01/06/04



# CITY OF KELOWNA

## BYLAW NO. 11913

### Amendment No. 20 to Subdivision, Development and Servicing Bylaw No. 7900

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Subdivision, Development and Servicing Bylaw No. 7900 be amended as follows:

1. THAT **SCHEDULE 4 – DESIGN STANDARDS**, title page be amended by deleting the title for Section 3 that reads "**3. DRAINAGE**" and replace it with a new title that reads "**3. STORMWATER MANAGEMENT**";
2. AND THAT **SCHEDULE 4 – DESIGN STANDARDS, Section 1 – Water Distribution, 1.5 Fire Flows** be deleted that reads:

#### **"1.5 Fire Flows**

Fire flows shall be determined in accordance with the requirements of the current edition of "Water Supply for Public Fire Protection - A Guide to Recommended Practice", published by Fire Underwriters Survey.

The following minimum fire flows must be met for the noted zones under peak daily flow conditions (Table 1.5):

**Table 1.5 Minimum Fire Flow Requirements**

<b>Developments (without sprinklers)</b>	<b>Minimum Fire Flow</b>
Single Family & Two Dwelling Residential	60 L/s
Modular / Mobile Home	60 L/s
Three & Four Plex Housing	90 L/s
Apartments, Townhouses	150 L/s
Commercial	150 L/s
Institutional	150 L/s
Industrial	225 L/s

The Design shall not use a fire flow greater than those listed in Table 1.5 to design their onsite fire protection systems. The maximum available fire flow for site development is the lesser of the actual available fire flow at the service connection or the fire flows in Table 1.5.

Subdivisions and main extensions may utilize hydraulic information from water model as provided by the City.

Actual required fire flows shall be determined for all new developments.”

And replacing it with:

#### **“1.5 Fire Flows**

Fire flows are subject to the following minimum requirements (Table 1.5) for all offsite works.

**Table 1.5 Minimum Required Fire Flow by Zoning Designation**

<b>General Zoning Designation</b>	<b>Minimum Fire Flow*</b>
Single Family & Two Dwelling Residential	60 L/s
Modular / Mobile Home	60 L/s
Three & Four Plex Housing	90 L/s
Apartments, Townhouses	150 L/s
Commercial	150 L/s
Institutional	150 L/s
Industrial	225 L/s

\*Off-site fire flow requirements are calculated in accordance with the requirements of the current edition of "Water Supply for Public Fire Protection - A Guide to Recommended Practice", published by Fire Underwriters Survey.

Subdivisions and main extensions must utilize hydraulic information from water model results provided by the City.

Onsite requirements are defined during the Building Permit process:

- a) Fire flow requirements for structures are to be calculated based on the worst-case requirement consistent with Section 3.2.5.7 of the BC Building Code.
- b) Where a structure design includes an automated sprinkler system to NFPA 13 as per Section 3.2.5.12 of the BC Building Code, then:
  - i. The NFPA 13 fire flow result for the worst-case building shall be the fire flow requirement on site.
  - ii. Confirmation of meeting the NFPA 13 requirement must be provided to the City.
- c) The Owner or Developer must report to the City that the calculated fire flow does not exceed the minimum requirements for that zoning found in Table 1.5.”

3. AND THAT **SCHEDULE 4 – DESIGN STANDARDS**, Section 3 be deleted in its entirety and replaced with a new Section 3 – Stormwater Management as attached to and forming part of this bylaw as Appendix A;
4. AND THAT **SCHEDULE 5 – CONSTRUCTION STANDARDS, 2. STANDARD DRAWINGS**, be amended by deleting the standard detailed drawings for **MANHOLE REQUIREMENT FOR SERVICES DRAWING SS-S50 and IDF Curves – City of Kelowna (YLW) SS-S56** and replacing the standard detailed drawings for **MANHOLE REQUIREMENT FOR SERVICES DRAWING SS-S50 and IDF Curves – City of Kelowna (YLW) SS-S56** as attached to and forming part of this bylaw as Appendix B and C;
5. AND THAT **SCHEDULE 5 – CONSTRUCTION STANDARDS, 2. STANDARD DRAWINGS** be amended by adding a standard detailed drawing for **GROUNDWATER RECHARGE SUITABILITY MAP DRAWING SS-S58** as attached to and forming part of this bylaw as Appendix D;
6. This bylaw may be cited for all purposes as "Bylaw No.11913, being Amendment No. 20 to Subdivision, Development and Servicing Bylaw No. 7900."
7. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

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Mayor

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City Clerk

### **3 Stormwater Management**

#### **3.1 General**

#### **3.2 Stormwater Flow Control**

#### **3.3 On-Site Stormwater Management and Practice**

#### **3.4 Runoff Analysis**

#### **3.5 Site and Lot Grading**

#### **3.6 Minimum Building Elevations (MBE)**

#### **3.7 Rational Method**

#### **3.8 Hydrograph Method**

#### **3.9 Minor System Design**

#### **3.10 Major System Design**

#### **3.11 Runoff Controls**

#### **3.12 Outlet Controls**

#### **3.13 Drainage Pump Stations**

#### **3.14 Erosion and Sediment Control (ESC)**

### **3 Stormwater Management**

#### **3.1 General**

The City stormwater system integrates surface water flows collected through the City's infrastructure and the natural watercourses that flow into Okanagan Lake. Proper integrated stormwater management practice mitigates impacts with the goal of maintaining Okanagan Lake as a high quality water source, with an abundant water supply, and with a balanced ecosystem. While urban, agricultural and natural areas all benefit from Okanagan Lake, drainage impacts from our systems must be mitigated, as well as be resilient to flood hazard and a changing climate.

The presence of an existing stormwater management facility does not imply that there is adequate capacity to receive the design flow, nor does it imply the facility is necessarily acceptable to the City. Where required, stormwater facilities must be upgraded to accommodate the appropriate flow as specified in this standard.

##### **3.1.1 Outcomes**

With respect to stormwater, the City's goals are to:

- a) Improve and protect water quality from creek flows, outfalls and groundwater entering Okanagan Lake.
- b) Reduce the risk of health hazard, life, and damage to property and infrastructure from

- flooding, and provide strategies to attenuate peak flows and volumes.
- c) Preserve and protect aquatic and riparian habitat and provide opportunity for restoration.
  - d) Minimize risks to the Okanagan Lake drinking water source.
  - e) Increase the resiliency of our watersheds to climate change impacts.**

This stormwater management standard applies the latest Best Management Practices (BMP) and processes in use in British Columbia. New systems and development within the City are to use the practices described within this Section as a *minimum* standard.

All flows must be routed through sewer pipe, ditching, water courses, riparian areas, or road allowances with the required capacity and right of way access for operation and maintenance. The City requires that major system flows must be safely routed downstream to an adequately sized municipal drain or natural watercourse without impacting private property.

### 3.1.2 Regulations

Stormwater management designs must conform to this standard, City of Kelowna bylaws, regulations and policies; in addition to federal and provincial statutes where applicable. These include but are not limited to the following:

- Supplementary Design Criteria
- Existing Master Drainage Plans,
- Local Government Act
- Fisheries Act of BC
- Water Sustainability Act
- BC Water Act
- Navigable Waters Protection Act
- Canada Wildlife Act
- Migratory Birds Convention Act
- Dike Maintenance Act
- Standards and Best Practices for Instream Works (Canada/BC)
- Land Development Guidelines for the Protection of Aquatic Habitat (Canada/BC)
- Urban Runoff Quality Control Guidelines for British Columbia
- National Guide to Sustainable Municipal Infrastructure (Canada)
- Canadian Dam Association Dam Safety Guidelines

### 3.1.3 Climate Change

The City accepts that climate patterns are changing, and that its customers are impacted by creek flooding, lake rises, temperature fluctuations and fire. The design standards for infrastructure outlined in this bylaw are to be considered a minimum expectation. The City requires that design professionals consider impacts of climate change, through potential changing weather patterns or water levels when implementing a design; particularly in components where critical and long term design decisions are being made, or in areas where the consequence of failure is high.

To account for a changing climate, the capacity of storm works will include an additional 15 percent (15%) upward adjustment, and applied to the rainfall intensity curve stage (IDF) in



Section 3.7.2. This is consistent with recommendations in EGBC (2018): Legislated Flood Assessments in a Changing Climate in BC.

The design professional will be required to consider debris flow and flow management as a result of higher peak flows.

On larger projects, basin characteristics are required elements of the Stormwater Management Plan (See Section 3.2.1). Developers will need to anticipate this form of analysis as part of their overall cost strategy.

#### **3.1.4 Hillside Areas**

Hillside areas or areas of poor infiltration conditions have been identified by the City in Drawing **SS-S58**.

- a) For development in Hillside Areas, the City focus is on safe conveyance of water. Roof or site drainage must discharge directly to the storm system. This focus is to not allow infiltration to ground except for foundation drainage. Where storm drains are not available or not considered feasible, minor system designs (see 3.2.a below) will require a hydrogeological review provided by a qualified Professional (P.Eng. or P.Geo.) to ensure that site infiltration is possible while not exceeding pre-development conditions, not impacting slope stability or off-site seepage, or not directly impacting downhill properties. The terms of reference of the review must be confirmed by the City Engineer and approved as a condition for obtaining a Development Permit.
- b) For new development where Groundwater Recharge is designated **Not Suited**, the City will not permit minor systems (see Item 3.2a) to infiltrate to ground.

### **3.2 Stormwater Flow Control**

The City's Stormwater Management system consists of three main components:

- a) **The Minor System** consists of sewer pipes, gutters, catch basins, driveway culverts, open channels, watercourses and storm water management BMPs designed to capture, convey, treat or modify flows up to a 5-year return design event as directed by the City.
- b) **The Major System** consists of surface flood paths, roadways, roadway culverts, channels and storm water management facilities designed to capture, convey, treat or modify larger flows up to a 100-year return design event. A piped minor system may be enlarged or supplemented to accommodate major flows. Major roads and arterials, bridges and creek protection armouring are to be designed for the 1 in 200 year event. This is discussed further in Section 3.10.
- c) **The Natural System** consists of all natural lakes, rivers, creeks, streams and ephemeral drains that flow naturally downstream ultimately to Okanagan Lake. Natural system capacity and water quality can be impacted negatively by incoming Minor or Major systems.

#### **3.2.1 Stormwater Management Plan**

Stormwater Management Plans are required for all municipal development. A plan should include the following:

- a) Tributary areas in the catchment which identify existing and potential land uses or current development.

- b) References to applicable Area Stormwater Drainage Plans.
- c) Details indicating how the proposed site relates to the Master Plan and its recommendations. Contours at 0.5 m elevation intervals.
- d) Conceptual lot grading patterns.
- e) Existing watercourses, including environmental classifications and/ or fish presence information, if available.
- f) Layouts of existing and proposed drainage systems.
- g) Major flow paths to a municipal drain or natural watercourse without impacting private property.
- h) Proposed control features to meet the water quantity and quality targets identified in the applicable Master Plan
- i) Locations, sizes, design flows, volumes, and capacities of all existing and proposed works.
- j) Capacity assessment of receiving downstream works, or reference to the applicable Master Plan demonstrating adequate capacity. The City will provide the required stormwater area plans upon request.
- k) Minor and Major hydraulic grade line elevations on profiles for all proposed works.
- l) Proposed service connection locations and their associated minimum building elevations (MBE). Pre and post development flows both entering and leaving the subject lands.
  - i. Pre development is defined as the natural condition prior to any development changes, including those resulting from past development activities.
- m) The City may exempt plan requirements for development in rural or agricultural areas upon request or determination by the City Engineer.

### **3.3 On-Site Stormwater Management and Practice**

#### **3.3.1 Storm Effluent Limitations to City Storm System**

- a) For structures designed or constructed above the proven high groundwater table, intermittent stormwater pumping will be permissible to the City stormwater system where approved by the City Engineer. All operations and testing must be consistent with the requirements in Sanitary Sewer/Storm Drain Regulation Bylaw 6618.
- b) Where structures are designed or constructed below the proven high groundwater table, permanent groundwater pumping will not be permitted to discharge to the storm system. The City will approve designs that include provisions for eliminating groundwater penetration into the structure, while addressing buoyancy concerns. These design aspects must be reviewed and approved by the City Engineer.
- c) Refer to the latest BC Building code for drainage discharge requirements in parkades.

### 3.3.2 Water Quality

Whether water is routed through creeks, pipelines or infiltration into ground, the City will require consideration for treatment, emergency management and maintenance of the stormwater infrastructure and water quality. Stormwater designs on private property must meet or exceed minimum water quality guidelines prior to entering the City storm system. Water quality for a minor system flow (50% of the 1 in 2-year) must meet minimum BC Ministry of Environment Recreational Water Quality Guidelines and as per Sanitary Sewer/Storm Drain Regulation Bylaw 6618 .

### 3.3.3 Construction Sites

The City storm system can be used for temporary site water management provided the water quality exiting the property meets BC Ministry of Environment Recreational Water Quality Guidelines. This temporary use must be approved by the City prior to issuance of the Development Permit and/or Building Permit, following a confirmation of capacity within the downstream system, and adequacy of the quality of storm effluent. There must be no discharge to the sanitary sewer system.

### 3.3.4 High Density Residential, Commercial and Industrial Storm Systems

- a) A control manhole is to be installed within 3 metres of the property line, and downstream of any water quality enhancement system. The manhole will include provision for isolating runoff into the City Storm system.
- b) The City requires access to the structure in an emergency and inspection. An SROW is required. Provisions must be considered for response to emergency toxic spills on site. Any costs associated with emergency response are the responsibility of the property owner.
- c) Water quality enhancement systems such as oil/grit separators, fuel/water separator (where required), naturalized storm ponds or other approved systems are the responsibility of the site owner, and must be maintained on a regular basis. The City can request regular maintenance records.
- d) Minor system flows must meet water quality guidelines described above prior to discharging to a creek or city storm system.
- e) On industrial sites where perforated storm systems or dry wells are used, the design must include provisions to manage emergency spills on site and minimize groundwater impacts.

## 3.4 Runoff Analysis

Storm drainage design should be carried out using one or both of the following methods. Calculations are to be submitted with designs.

- a) **Rational Method:** To be used only for hydrologically simple and uniform areas with contributing area less than 10 Ha.
- b) **Hydrograph Method:** Applicable for all larger areas or more hydrologically complex catchments, or where stormwater management systems require more than basic conveyances. Use SWMM based models or approved equivalent to analyze these processes. Each model

must include a level of complexity dependent on the watershed and the hydrologic processes that need to be considered (e.g., detention, groundwater recharge and infiltration, evapotranspiration, continuous simulation, etc.).

For all modelling, use the rainfall Intensity Duration Frequency (IDF) curves found in standard drawing **SS-S56**. Both historical data as well as climate change information must be incorporated into the runoff analysis.

### **3.5 Site and Lot Grading**

Grading is to comply with the BC Building Code and the following:

- a) Swales and site drainage must be constructed to prevent ponding within lots, with runoff routed, where possible, to storm services in public streets or other appropriate stormwater management system for the site.
- b) Grade lots to drain to an approved City drainage system or roadway. Use 1% minimum grade. Grading directly to a natural drainage path must include adequate erosion control and water quality improvement measures.
- c) Avoid drainage across adjacent lots. Where cross-lot drainage is unavoidable, provide adequate measures such as channelling, swales, inlets or piped connections to direct flow appropriately. A statutory right of way in favour of the City or private easement is required for unobstructed access.
- d) Positive drainage is required for buildings and foundations.
- e) Set building elevations above the hydraulic grade line (HGL) of the major drainage system as per Minimum Building Elevations (MBE) guidelines below.

### **3.6 Minimum Building Elevations (MBE)**

The MBE applies to the elevation of the lowest floor slab in a building or the underside of the floor joists where the lowest floor is constructed over a crawl space. Crawl space is defined as the space between a floor and the underlying ground having a maximum height of 1.2 m to the underside of the joists and not used for the storage of goods or equipment damageable by flood waters.

The MBE is to be at least 0.60 m above the storm sewer service connection invert and 0.30 m above the major drainage system hydraulic grade line (HGL), whichever governs except where permissible on Hillside development where:

- foundation drains are disconnected from the storm main; or
- intermittent foundation pumping has backflow prevention.

For developments within close proximity to the Okanagan Lake shoreline, the MBE is elevation 343.66m. Further consideration shall be given to wind and wave action when setting the required MBE.

For sites near a watercourse where a floodplain elevation has been established through flood mapping, the MBE is to be a minimum of 300mm above the 200-year return period peak flood elevation or as per City of Kelowna Mill Creek Flood Plain Bylaw No. 10248. Where a flood elevation has not been established, setbacks are to be as per the Provincial guidelines or 1.5 metres above the natural boundary of any watercourse, lake, marsh or pond.

### 3.7 Rational Method

The Rational Method for calculation of peak flows is as follows:

$$Q = R A I N$$

Where:

- Q = Peak flow in cubic metres per second (m<sup>3</sup>/s)
- R = Runoff Coefficient (C) x Adjustment Factor (C<sub>AFs</sub>)
- A = Area of catchment in hectares (ha)
- I = Intensity of rainfall (mm/hr)
- N = 1/360

Factors for use in the Rational Formula are indicated below.

#### 3.7.1 Runoff Coefficients (C)

The following runoff coefficients are for use with the Rational Formula. These coefficients are for general application only. Design values are subject to verification by the designer and approval by the City. Higher values may be applicable in those areas which experience rainfall during the winter when the ground is frozen.

**Table 3.7.1 Runoff Coefficients (C)**

Land Use	Percent Impervious	C	
		Minor Storm (1:5 year)	Major Storm (1:100 Year)
Residential			
• Suburban Residential (Lots>0.4 ha)	20%	0.35	0.40
• Low Density (Single Family)	40%	0.50	0.55
• Medium (Multi-Units Detached)	65%	0.60	0.65
• High Density (Multi-Units Attached)	90%	0.85	0.90
Commercial	90%	0.85	0.90
Industrial	90%	0.85	0.90
Institutional (e.g. Schools)	80%	0.75	0.80
Parks/Grasslands	20%	0.20	0.30
Cultivated Fields	30%	0.30	0.40



### Runoff Coefficient Adjustment Factor ( $C_{AF}$ )

An adjustment factor is to be applied to the runoff coefficient to reflect variations in soil permeability and slope.

**Table 3.7.2 Runoff Coefficient - Soil Adjustment Factor ( $C_{AF}$ )**

Soil type and Slope	$C_{AF}$
Sandy soil with flat slope (up to 5%)	0.9
Sandy soil with steep slope (over 5%)	1.0
Clayey soil with flat slope (up to 5%)	1.0
Clayey soil with steep slope (over 5%)	1.1
Rock	1.1

Note: The above runoff coefficient adjustment factors are subject to verification by the designer. The product of C and  $C_{AF}$  can not exceed 1.0.

### 3.7.2 Rainfall Intensity (I)

Rainfall intensity for use in the Rational Method should be determined using the rainfall IDF curve in standard drawing **SS-S56** for the City of Kelowna. This curve was developed from the Atmospheric Environment Service recording station located at the Kelowna international Airport. To account for climate change, as noted in Section 3.1.3, **a 15 percent increase (15%)** will be applied to the intensity derived from the IDF curve. The duration is equal to the Time of Concentration ( $T_c$ ), as calculated below.

#### Time of Concentration ( $T_c$ )

The time of concentration is the time required for runoff to route from the most remote part of the catchment area under consideration to the design outlet node. The time of concentration can be calculated using the following formula:

$$T_c = T_i + T_t$$

Where:

$T_c$  = time of concentration (minutes)

$T_i$  = inlet or overland flow time (minutes)

$T_t$  = travel time in sewers, ditches, channels or watercourses (minutes).

#### Inlet or Overland Flow Time ( $T_i$ )

Typical inlet times for urban areas, assuming BMP's are not applied, are as follows:

- a) Single Family Lot 10 minutes

- |  |           |
|--|-----------|
| b) Multi-Family Lot                    | 8 minutes |
| c) Commercial/Industrial/Institutional | 5 minutes |

For relatively flat areas, the inlet time for larger areas can be calculated using the "Airport Method" as follows:

$$T_i = \frac{3.26 (1.1 - C) L^{0.5}}{S^{0.33}}$$

Where:

$T_i$  = inlet time (minutes)

$C$  = runoff coefficient (See above)

$L$  = travel distance (Maximum length = 300 m)

$S$  = slope of travel path (%)

### Travel Time

The travel time for routing in sewers, ditches, channels or watercourses can be estimated using the Modified Manning formula:

$$T_t = \frac{L n}{60 R^{0.667} S^{0.5}}$$

Where:

$T_t$  = travel time (minutes)

$L$  = length of flow path (m)

$n$  = Manning roughness coefficient:

0.050 Natural channels

0.030 Excavated ditches

0.013 Pipe and concrete lined channels.

$R$  = Hydraulic radius = Area/Wetted Perimeter (m)

$S$  = slope (m/m)

### 3.7.3 Design Summary Sheet

All design calculations are to be tabulated and shown on the design drawings, or in a report and summarized on design drawings.

## 3.8 Hydrograph Method

Analysis using the Hydrograph Method requires computer modeling capable of analyzing the hydrologic characteristics of the watershed and generating runoff hydrographs.

For City applications, SWMM based models are appropriate. The City of Kelowna must be consulted before selecting a more specialized software program.

### 3.8.1 Modelling Procedures

Modelling results are to be calibrated using observed historical rainfall and flow data from the

design watershed. Sensitivity of the model predictions to variations of key parameters should be tested and the findings used to develop a realistic and conservative model.

At a minimum, post-development hydrographs are to be generated at key points of the drainage system for a 5-year and 100 year design storm with durations of 1, 2, 6, 12, and 24 hours for each development condition. A different range of storm durations may be appropriate, subject to City approval. This will identify the critical storm event to be used in designing the system component. Note that the storm durations that generate the critical peak flow may be different from the durations that generate the critical storage volume.

Systems with a number of interconnected ponds or with restricted outlet flow capacity may require a more detailed analysis for sequential storm events or modelling with a continuous rainfall record.

Detailed designs should include hydraulic grade lines (HGLs) of the minor and major systems plotted on profiles of the minor system components and compared with MBE to demonstrate flood protection.

### **3.8.2 Submission of Modelling Results**

Modelling results are to be submitted to the City in a report or drawing containing at least the following information:

- a) Stormwater Control Plan as defined in Section 3.2,
- b) Name and version of modelling program(s)
- c) Parameters and simulation assumptions.
- d) Design precipitation details.
- e) Pre-development and post-development hydrographs.

## **3.9 Minor System Design**

The minor system includes all drainage works that collect, convey, detain, divert and intercept design storm runoff. The minor design event must be the 5-year design storm.

### **3.9.1 Pipe and Channel Capacity**

Use Manning's formula.

$$Q = \frac{A R^{0.667} S^{0.5}}{n}$$

Where:

A = Cross sectional area in m<sup>2</sup>

R = Hydraulic radius (area/wetted perimeter) in m

S = Slope of hydraulic grade line in m/m

n = Roughness coefficient:

0.013 for all smooth pipes.

0.024 for corrugated pipes and culverts.

### 3.9.2 Flow Velocities

#### a) Pipes/Culvert Flow

- i. Minimum design velocity for pipes flowing full or half full: 0.60 m/s.
- ii. Where grades are greater than 10%, measures are required to prevent pipe erosion and movement such as control structures and/or tie-backs and anchor blocks.
- iii. Where a storm sewer discharges into a watercourse, provide riprap bank protection and, if necessary, energy dissipation facilities. Avoid discharge perpendicular to stream flow.

b) Conveyance channels must be armoured and sized for a 1:100-year event. For riprap design chart see standard drawing **SS-S57**.

#### c) Road Ditches

- i. Maximum road ditch velocity is 0.5 m/s without armouring.
- ii. Ditch Inlets - Ditch inlets to storm sewers must include wing wall structures, safety grillage for large pipes (>600 mm diameter), debris screens and sedimentation basins.

### 3.9.3 Alignment

Except as indicated for Curved Sewers, horizontal and vertical alignments are to be straight lines between manholes.

### 3.9.4 Minimum Pipe Diameter

• Storm Sewers	250 mm
• Culverts crossing roads	450 mm
• Culverts crossing driveways	300 mm
• Catch Basin Leads	200 mm
• Double Catch Basin Leads	250 mm

Downstream pipe sizes are not to be reduced unless the downstream pipe is 600 mm diameter or larger and increased grade provides adequate capacity. Detailed hydraulic analysis is required. The maximum reduction is one standard pipe size.

### 3.9.5 Minimum Grade

Minimum grades of storm sewers are as required to obtain the minimum velocity of 0.6 m/s at design flow except for catch basin leads and service connections, for which minimum grades are as indicated in Section 3.9.12, Service Connections.

### 3.9.6 Curved Sewers

Where permitted by the City, horizontal and vertical curves may be formed using pipe joint deflections as follows:

- a) The radius of the curve is to be no less than the recommended manufacturer's minimum radius of curvature at a constant radius.
- b) Horizontal curves must be parallel to the centre line of road at a constant offset.
- c) Only one horizontal curve is permitted between manholes, unless the mainline is installed and appropriately anchored outside the road on a steep hill slope requiring multiple vertical curves.
- d) Where the pipe curve does not have a consistent offset from a road centre line, the offsets must be properly referenced on Record Drawings.
- e) Subject to City Engineer approval, curved storm sewer systems larger than 600 mm diameter may include deflections formed by mitred bends to a maximum mitre of 45°.

### **3.9.7 Depth**

The minimum depth of the sewer must be sufficient to provide all service connection piping with a minimum cover of 1.2m to the top of the service, anywhere within the finished right-of-way. In no instance shall the cover over the crown of the sewer main be less than 1.2m when installed in travelled areas. The depth of course can be reduced to 1.0m when installed outside of travelled areas.

- a) The maximum depth of cover must be 4.5m, except under special circumstances and with permission of the City Engineer.
- b) For catch basin leads, the minimum depth of cover is 0.90m.

### **3.9.8 Pipe Joints**

All pipe joints are to be watertight.

### **3.9.9 Perforated Storm Pipe**

- a) The City will only consider the installation of perforated storm sewers and/or dry wells to discharge water back to the ground where soil conditions, slope and water table elevation are suitable. The perforated pipe system design must be designed to provide surcharge conditions.
- b) Perforated pipes can only be installed in areas of the City described as "Possibly Suited" in the Groundwater Recharge Suitability Map in Standard Drawing **SS-S58** and confirmed by a hydro-geotechnical site investigation.

### **3.9.10 Manholes**

- a) Manholes are required at:
  - i. Every 150m or less.
  - ii. Every change of pipe size.
  - iii. Every change in grade, except on curvilinear pipe alignments.
  - iv. Every change in direction, except on curvilinear pipe alignments.
  - v. All terminal sections.



- vi. Every sewer main intersection.
- b) Placement of manholes in existing or future wheel paths must be avoided.
- c) Manhole sizes must be in accordance with the Standard Drawings: Manhole connection details as per MMCD S3 & S4, or City of Kelowna supplemental standard drawing **SS-S1a**.
- d) Hydraulics: Crown elevations of inlet sewers not lower than crown elevation of outlet sewer. When connecting a collector sewer main to a trunk sewer 300 mm or greater, the invert of the collector main must not connect lower than 0.75D (¾ of the pipe diameter).
- e) Minimum drop in invert elevations across manholes:
  - i. Straight run: 10 mm drop
  - ii. Deflections up to 45 degrees: 25 mm drop
  - iii. Deflections 45 to 90 degrees: 50 mm drop
- f) Drop manhole and ramp structures should be avoided where possible by steepening inlet sewers. Where necessary, provide drop structures as follows (table 3.9.10):

**Table 3.9.10 Drop Structures**

Invert Difference	Structure
Up to 0.45m	Inside Ramp
0.45 to 0.90 m	Outside Ramp
Greater than 0.90 m	Outside Drop*

\*Inside drop may be used if specifically approved by the City Engineer.

- g) Drop manholes and outside ramps must be installed in accordance with standard drawings.
- h) Hydraulic losses are to be calculated for manholes with significant change of grade or alignment. For high velocity flows, particularly for pipes 600 mm or larger, detailed analysis is required using the Froude number, or utilizing appropriate computer models. The Manning's equation should not be relied on for pipe slopes above 10%. For low to moderate velocities and smaller pipes, use the following formula:

$$H_L = k v^2 / 2g$$

Where:

$H_L$  = head loss (m)

$v$  = flow velocity entering junction (m/s)

$g$  = gravitational acceleration (9.81 m/s<sup>2</sup>)

$k$  = head loss coefficient (1.0 for channeled 90° bends and tees, to 1.5 without channelized benching)

Where benching is used, the minimum drops listed above are applicable for velocities below 1 m/s. Where flows exceed 1 m/s,  $H_L$  should be specifically computed and used as the drop across the junction.

### 3.9.11 Catch Basins

- a) Catch basins are required at regular intervals along roadways, at intersections and at low points to:
  - i. Prevent overflows to driveways, boulevards, sidewalks and private property.
  - ii. Avoid interference with crosswalks.
  - iii. Avoid low points in curb returns at intersections.
- b) Catch basin leads are minimum 200 mm diameter.
- c) Minimum grade of a catch basin lead is 1%.
  - i. Catch basin leads require a 0.9 m minimum cover. If 0.9 m is not available, design to protect from freezing and traffic loads; design calculations must be provided.
- d) Spacing is to provide sufficient inlet capacity to collect the entire minor flow or major flow, where required, into the sewer system.
- e) Local suppliers are required to provide rating curves for available catch basin grates. As a general rule, space catch basins to drain maximum impervious areas of:
  - i. 500 m<sup>2</sup> on roads with grades up to 4%,
  - ii. 400 m<sup>2</sup> on roads with grades greater than 4% at 100 m maximum.
- f) Lawn basins are required on boulevards and private properties where necessary to prevent ponding or flooding of sidewalks, boulevards, driveways, buildings and yards.
- g) Double or twinned catch basins must not be connected directly together, rather one basin will be wyed into the lead of the other. Maximum lead length to the mainline must be 30 meters and be minimum 250mm diameter. Each CB will have a trapping hood (standard drawing **SS-S54**).
- h) Double or twinned catch basins are to be provided at all sag points or sump locations as a minimum. Inlet calculations are required where the major storm needs to be accommodated, such as downhill cul-de-sacs or where there is potential for excessive ponding or overflow onto private property.
- i) Oversized grates and/or secondary emergency inlets must be considered where leaves and/or debris collection is anticipated.

### 3.9.12 Service Connections

Service connections to the City storm system are required for all multi-family, commercial, industrial and institutional land uses.

Single Family Residential service connections to the City Storm system are required in instances where site conditions do not provide for safe infiltration or dispersal of storm water on site. The safe use of infiltration is to be confirmed by a qualified Professional.

a) Service connection requirements:

- i. The minimum storm service diameter for any property is 150mm.
- ii. Inspection chambers (ICs) are required to be installed as per **SS-S7** and **SS-S9**. Where this is not possible, identify offset on the record drawings and service card. An IC is not required on residential connections where the service is less than 2.5 m long and connected directly into a manhole.
- iii. Refer to Drawing **SS-S50** for all service connection requirements to a storm mainline.
- iv. All storm services 200 mm and larger require a manhole either on the storm mainline or on the storm service at the property line. The service manhole must be offset from the property line a sufficient distance to ensure replacement will not impact private property.
- v. Flow control manholes are to be installed on the private side of the property line as per Drawing **SS-S55**.
- vi. Service connections are permitted into manholes as per Drawing **SS-S1a**.
- vii. Depth to be minimum 1.2 m.
- viii. Minimum grade from property line to storm sewer main is 2%.
- ix. Wye fittings are preferred for service connections into proposed City storm sewers. Insertable tees are permitted into 250mm or larger existing mains.

b) Roof Leaders (drains):

- i. Where permissible and not in Hillside Areas, roof water is expected to be contained on site as part of best management practices to meet requirements for pre-development storm rate. Acceptable best management practices include splash-pad onto green space, rain harvesting systems or appropriately sized rock pits where soil infiltration parameters permit.
- ii. Roof leaders are not permitted to be directed to any infiltration device or soak away pit near to or part of an engineered retaining wall or reinforced earth structure.
- iii. Roof leaders or inlets from downward sloping driveways in Hillside Areas must be connected to the City storm sewer.

c) Perimeters Drains

- i. Perimeter drains for buildings are required as per the British Columbia Building Code.
- ii. Discharge may be to the surface or a soak away pit.
- iii. Foundation perimeter drains are not permitted to be directed to any infiltration

device or soak away pit that impacts an engineered retaining wall or reinforced earth structure.

- iv. Foundation perimeter drains can be routed by gravity through a storm service to the storm sewer provided that:
  - the elevation of the basement/crawlspace floor is at least 600 mm above the MBE (Section 3.6), or
  - 600 mm above the anticipated or known high ground water table, or
  - 600 mm above the 100 year hydraulic grade line within the sewer main at that point, whichever is higher.
- v. Where a sump pump is required, a backflow prevention device must be installed as part of the mechanical configuration to prevent backflow into a basement from the City Storm sewer.
- vi. As per Section 3.3.1, permanent groundwater pumping is not permitted to City storm sewers.

#### **3.9.13 Perforated Sub-Drains**

Perforated subsurface drainage systems designed for the purpose of permanent groundwater level reduction are not permitted to be connected to the City Storm sewer system.

#### **3.9.14 Locations and Corridors**

Wherever possible, storm sewers and service connections should be located within the public road right of way. Side or rear yard easements should be avoided where possible. Where it can't be avoided, statutory right-of-ways will be required for permanent City access.

### **3.10 Major System Design**

The major drainage system includes all drainage pathways that convey, detain and/or intercept flows in excess of the capacity of the minor system. Its primary purpose is to provide flood protection for the 1:100 year return event. The major system generally includes surface flow paths such as ditches, swales, sewers, roadways, plus roadway culverts and watercourses.

#### **3.10.1 Surface Flow Routing**

All surface flows should have specially designed routes that are preserved and protected by right-of-ways and are accessible for maintenance. Design criteria include:

- a) HGL is to be at least 600 mm below the MBE of adjacent buildings.
- b) Maximum flow depth on roadways: 300 mm. Boulevards and intersecting driveway profiles will need to be set such that roadway surface flows are contained within the public right-of-way.
- c) One lane, or a 3.5 m width at the crown of each roadway, is to be free from flooding.
- d) Where a roadway is used as a major flow path, the road grades are to be designed to accommodate and control the flow at intersections.

- e) Flood routing is not permitted on to private property except in engineered flow channels or sewers protected in a statutory right-of-way.
- f) Overflow routes are required at all sags and low points in roadways and other surface flow routes.
- g) Major flood routes are required to exit down-slope in cul-de-sacs with Statutory Rights of Way established.

#### **3.10.2 Surface Flow Capacity**

Flow capacity of road surfaces and swales can be calculated using the Manning formula, presented in Section 3.9.2, Time of Concentration. Typical values of the Manning Roughness Coefficient "n" are:

- a) 0.018 for paved roadway
- b) 0.03 for grassed boulevards and swales
- c) 0.04 to 0.10 for irregular or treed channels.

Design detail is to include consideration of flow velocities and the potential requirement for erosion control measures. Ditches should be designed using a low n-value to determine velocity and provide the basis for stable channel design and a high n-value to determine ditch capacity and free board to prevent flooding or submergence of adjacent roadway subgrades.

#### **3.10.3 Piped System**

As noted in Section 3.2.1, the minor drainage system may be enlarged or supplemented to accommodate major flows in special circumstances. Modifications to the design criteria must be included in Stormwater Management Plan. Design considerations include:

- a) Provision of adequate inlets to accommodate major flows. Capacity calculations are to be provided in the Stormwater Management Plan.
- b) The requirement for surface overflow routes at potential surface ponding locations.
- c) Flow depth and velocity.
- d) Where applicable, design in accordance with minor drainage system guidelines.

#### **3.10.4 Culverts and Bridges**

The following service levels are to be used for design:

<b>Road Class</b>	<b>Design Flood Frequency for Bridges and Culverts</b>
Arterial and Collector	1:200 Year Flood
Local	1:100 Year Flood + provision for overflow if on major channel



The fishery value (aquatic classification) of the watercourse will establish the design requirements for the crossing. Particular designs will apply if fish passage is needed. Approvals are required under the BC Water Act and the Federal Fisheries Act, and may be required under the federal Navigable Waters Protection Act.

Culvert design is to be in accordance with the procedures outlined in an applicable design manual including but not limited to:

- a) American Concrete Pipe Association - Concrete Pipe Design Manual
- b) Corrugated Steel Pipe Institute - Handbook of Steel Drainage and Highway Construction Products.
- c) Standards and Best Practices for In-stream Works - Culverts, Province of British Columbia and DFO.

Inlet and outlet protection is required for all major system culverts. Design considerations are to include inlet control and outlet control conditions, energy dissipation and erosion control measures.

The City requires all municipal channel culverts 500mm or greater to be constructed with headwalls, end-walls and safety grillage as per Standard Drawings.

#### **3.10.5 Watercourses**

Natural watercourses are integral components of both the major drainage system and the ecological system. Riparian areas are to be preserved and/or enhanced to sustain habitat for aquatic and other wildlife as well as convey storm runoff.

Increases in peak storm flows and volumes to major watercourses and receiving waters shall be minimized. Consideration must be given to fish bearing streams and to streams presently at capacity.

Designers must consider all federal, provincial and municipal laws, regulations and guidelines noted above, and must obtain comments and approvals from the appropriate agencies.

### **3.11 Runoff Controls**

Runoff controls are required to meet the objectives indicated previously. The controls may include:

#### **3.11.1 Detention Storage**

Detention storage is used to capture and store water on site to assure that storm releases are limited to the pre-development release rate for a 1 in 5 year storm. Drainage Basin Plans are available upon request to the City Engineer.

As a guideline, detention storage is not required on any lands west of Richter Street between Bernard Avenue to the north and Wardlaw Avenue to the South unless approved by the City Engineer. Where peak flow rates or volumes are increased and will cause detrimental impacts, provisions for downstream improvements must be provided in order to mitigate the impacts.

Detention storage options and design guidelines include the following:

### **3.11.2 Parking Lot Storage**

- a) Requires detailed lot grading design to ensure proper drainage, pedestrian safety and convenience, and major flow paths .
- b) Maximum ponding depth: 300 mm outside vehicle stalls, 150 mm within vehicle stalls, however, also with consideration to frequency of ponding and impact to users of the parking lot.

### **3.11.3 Underground Storage**

- a) Facilities include tanks and oversized pipes, with outlet controls.
- b) Tanks, fencing and graded slopes to be constructed off-line and on-site.
- c) Cross sections and inlet and outlet locations should be designed to minimize maintenance requirements.
- d) Structural design to accommodate traffic loads and groundwater pressure.
- e) Maintenance access provisions required.

### **3.11.4 Dry Detention Ponds**

- a) Intended to provide storage only during severe storm events.
- b) May be on-line or off-line, although off-line is preferred. Fencing and graded slopes required.
- c) May accommodate active recreational uses.
- d) Overflow elevations to be coordinated with MBEs.
- e) Emergency overflow spillway to be constructed for 1:100yr storm event.
- f) Design details, other than discharge rates should be in accordance with current technologies as outlined in Land Development Guidelines for Protection of Aquatic Habitat (Canada/BC).
- g) Provide warning signage indicating facility is a stormwater detention structure subject to flooding or rapid water level changes. Signs to be posted at all public access points or road frontages.

### **3.11.5 Wet Detention Ponds**

- a) Intention is to provide on-line detention storage and maintain a permanent minimum water levels.
- b) Catchment area must be large enough to provide sufficient base flow to ensure wet storage and is sustained without becoming stagnant (based on local hydrologic characteristics).
- c) Generally located off-site, and must include fencing and graded slopes on-site.
- d) Can provide a public amenity within a passive park.
- e) Overflow elevations to be coordinated with MBEs.
- f) Design details, other than discharge rates, should be in accordance with current

technologies as outlined in Land Development Guidelines for the Protection of Aquatic Habitat (Canada/BC), and related documents.

- g) Provide warning signage indicating facility is a stormwater detention structure subject to flooding or rapid water level changes. Signs to be posted at all public access points or road frontages.

### 3.11.6 Subsurface Disposal / Infiltration Systems

- a) These systems are intended to promote stormwater retention and groundwater recharge.
- b) Suitable for high permeability soils with low groundwater elevation. Geotechnical investigation is required.
- c) Design details should be in accordance with current technologies as outlined in Infiltration systems guidelines in Land Development Guidelines for the Protection of Aquatic habitat (Canada/BC), and related documents.
- d) Stormwater infiltration basins planned for Hillside Areas must be designed by a qualified Professional with experience in hydrogeology. The design must be reviewed and confirmed by the City Engineer. See Section 3.1.4.

### 3.12 Outlet Controls

Outlet controls for storage facilities may be designed using the standard orifice and weir equations:

Orifice Equation:

$$Q = C A (2 g h)^{0.5}$$

Where:

Q = release rate (m<sup>3</sup>/s)

C = orifice coefficient (0.62 for sharp or square edge, 0.85 for rounded edge)

A = area of orifice (m<sup>2</sup>)

g = gravitational acceleration (9.81 m/s<sup>2</sup>)

h = net head on orifice (m)

Weir Equation:

$$Q = CLH^{1.5}$$

Where:

Q = release rate (m<sup>3</sup>/s)

C = weir coefficient

L = effective length of weir crest (m)

H = net head on weir crest (m)

Larger storage facilities are to include provisions for discharges at rates greater than the design release rate (i.e., major storm event and emergency conditions). Rapid drawdown of the water

level may be necessary for emergency purposes or to restore the available storage to accommodate subsequent storm events. Simple reducers are permitted on smaller facilities.

Orifices shall be fixed and designed to pre-development outflow rate. Adjustable mechanisms such as slide gates or removable orifice plates are not permitted unless approved by the City Engineer.

Design of inlet and outlet structures is to include consideration of energy dissipation and erosion control. Safety grates are required over all inlet and outlet openings larger than 500 mm diameter. Locks for access hatches are required.

The following is an introductory list of some runoff controls focused on water quality treatment.

- a) Bio-filtration Swales and Constructed Wetlands
- b) Intended to provide bio-filtration and sediment removal.
- c) May be designed to provide on-line detention storage as well as quality treatment.
- d) May be located on-site or off-site.
- e) Qualified professional required for design.
- f) Design requires consideration of climatic conditions.

#### **3.12.1 Oil and Grit Separators**

Oil and Grit Separators are required:

- a) On site with parking for 50 or more vehicles (does not apply to parkades).
- b) On all industrial zoned properties, unless it can be proven that there is no risk of storm water contamination.
- c) Supplier design details are required.

Design criteria for Oil and Grit Separators must include:

- a) Devices must have a current Canadian Environmental Technology Verification (ETV) or ISO 14034 ETV verification.
- b) A target Total Suspended Solids removal of 60% of the ETV Particle Size Distribution.
- c) Performance predictions for all proposed units.
- d) A maintenance plan and commitment from all Owners. This will be included in the business license renewal.
- e) A location on-site, including a Statutory Right of Way or covenant on title should the City need to inspect the unit.

#### **3.12.2 Oil/Water Separators**

- a) Required for gas stations, vehicle service areas and storage areas for highway vehicles and construction equipment.
- b) Design details in accordance with current technologies as outlined in Urban Runoff Quality Control Guidelines for British Columbia.

### **3.13 Drainage Pump Stations**

Drainage pump stations are not commonly used in the City. Where drainage pumping is required, the designer must review the design concept and proposed guidelines with the City, submit a pre-design report and obtain approval of the City before proceeding with design. At a minimum, the pre-design report should include the following:

- a) Delineated catchment area map
- b) Estimated flows and HGL
- c) Pump station location
- d) Connection to existing infrastructure.

### **3.14 Erosion and Sediment Control (ESC)**

All construction projects in the City require an Erosion and Sediment Control (ESC) Plan approved by the City. Storm water runoff from construction sites commonly contains significantly higher contaminant concentrations than storm water from developed sites. Poor construction practices and lack of attention to detail are contributors to sediment transport, in turn impacting both downstream infrastructure, aquatic habitats and Okanagan Lake.

Erosion and Sediment Control will be managed as a separate process with a cost identified as a separate line item in the development planning process

The following policies will be administered:

- a) No Person may cause, or permit another Person to cause, sediment or sediment-laden water to discharge into the storm system, with concentrations greater than 75 milligrams per litre (ppm) of total suspended solids (TSS). A sample measuring greater than 60 nephelometric turbidity units (NTU) will be the trigger point where the sample must also be sent to the lab for analysis.
- b) A Security Deposit for ESC Works equal to 3% of the Consulting Engineer's opinion of probable costs of civil earthworks and infrastructure will be added to the Servicing Agreement.
  - i. The Security Deposit submitted is to secure the full and proper compliance with the provisions of the By-law. In the event, that the Owner, Developer, or Person Responsible has not complied with the provisions of this By-law, the necessary funds from the security deposit may be drawn down, at the City's option, and the money used either by the City or its agents to protect the storm system from sediment or sediment-laden water in adherence with the terms and conditions of this By-law. Notwithstanding, the City is under no obligation to initiate or complete remedial works in or under the Land.
  - ii. If the amount of the security deposit is insufficient for the City to complete the ESC Facilities, the Owner and Developer jointly and severally will pay any deficiency to the City on demand.
- c) The Owner must retain a Qualified Professional (P.Eng, RPBio, P.Ag, ASCT, CPESC, CISEC or CESCL) responsible for inspecting and monitoring the ESC Facilities weekly and after any rain event which exceeds the intensity of 25mm of total rainfall depth in a 24-hour period. All records and data must be made available to the City upon request. Should a site be determined



to be non-compliant, the Professional will be responsible for submitting notification and presenting a remediation plan to the City within two days of the event.

- d) The ESC will include a construction plan and site management plan. ESC features must be installed before any clearing, excavation, or soils mobilization takes place.
- e) The fundamental approaches to effective ESC include:
  - i. reduce clearing and grading and preserve natural vegetation as much as possible;
  - ii. phase construction to limit soil exposure at any one time, particularly in wet seasons;
  - iii. stabilize exposed soils as quickly as possible, whether temporary or permanent;
  - iv. protect slopes and cuts;
  - v. prepare the site to limit soil tracked off-site by haul vehicles;
  - vi. sweep off-site streets when dirt is tracked;
  - vii. filter runoff water before it leaves the site;
  - viii. install filters or barriers to protect downstream drains and inlets;
  - ix. adjust ESC plan to suit changing weather and construction phasing;
  - x. assess ESC practices after rain event; and
  - xi. maintain the works throughout construction.

Ideally, practices and features are put in place to prevent erosion from occurring in the first place, but realistically some degree of erosion and sediment transport will occur. When it does, other practices and features are to intercept and capture the sediment before reaching vulnerable areas. As such, the following sub-sections introduce ESC practices in two core categories; erosion control and sediment control.

#### **3.14.1 Erosion Control**

Rainfall and wind can aggressively displace and transport soil, although rainfall tends to be the more damaging in BC climates. The soil composition has a significant bearing on its erosion potential. The first line of defense is to either maintain or provide protective cover to the soil. Ideally, natural vegetative cover is maintained for areas that do not need to be disturbed. Where soils do need to be exposed or stockpiled, temporary covers should be applied when rainfall events are imminent.

For exposed site areas, straw mulch is the most common form and can be effective with low cost. However, it is commonly not applied thick enough or replenished frequently enough. It is important that a uniform blanket be provided and refreshed as the straw decays or is displaced. For the most part, bare soil should not be visible.

For steeper slopes, or for areas exposed and inactive for considerable time, manufactured erosion control blankets may be most appropriate. There are many products available and local suppliers should be consulted for the selection of the appropriate one. While they have a higher purchase cost, with proper selection and installation they will provide longer and more effective service with far less maintenance than straw mulch.

For soil stockpiles, poly tarps should be applied when the stockpile is inactive, including short overnight periods if there is any threat of precipitation. If inactive for considerable time, other measures such as temporary seeding, mulching, or matting may be considered.

Once disturbance to an area is complete, permanent cover practices should be established as soon as possible. Top dressing the area with topsoil having high organic content in itself can be a significant benefit; a minimum of 100 mm should be applied for purposes of erosion control. Greater depth is often required to meet landscape growing medium and hydrologic management needs. Sodding, broadcast seeding, hydro-seeding, and drill seeding are acceptable methods to re-establish a blanket of vegetative.

Aside from maintaining good quality ground cover, there are a number of other techniques that can be applied as erosion control, including the following, but not necessarily limited to those below. They should be selected based upon the specific conditions and requirements of the site.

Construction of stable haul roads for transport vehicles coming and going from the site is required.

At a minimum, haul roads include 200 mm of a coarse granular running surface, but strong consideration for underlying filter fabric, and potentially geogrid reinforcing in weak soils, should be given;

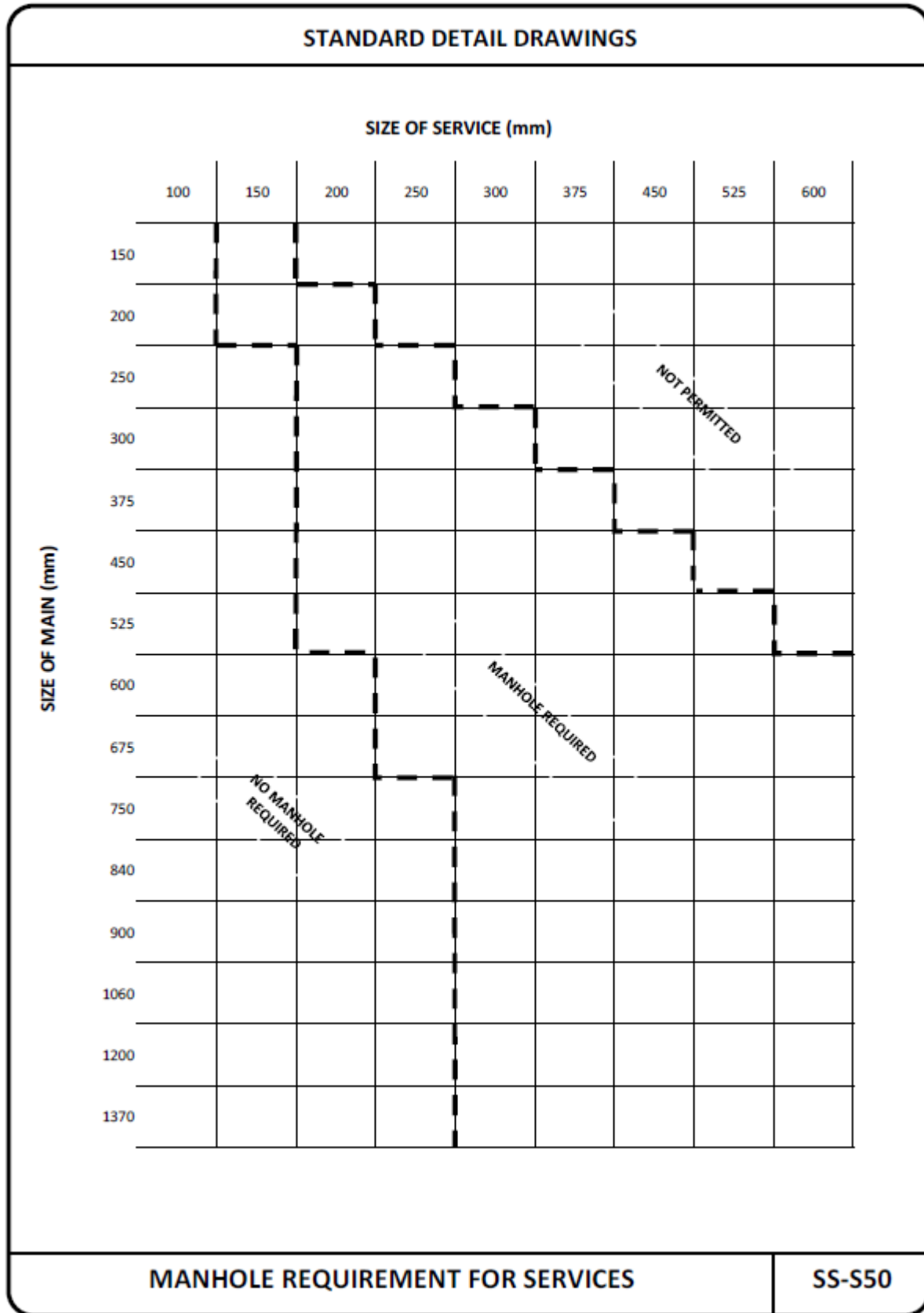
- a) Intercept trenches on the upstream edges of the working area to redirect runoff;
- b) Terracing steeper slopes;
- c) Scarifying the soil surface;
- d) Bio-engineered protection of very steep slopes;
- e) Rip-rap with appropriate underlying filter.

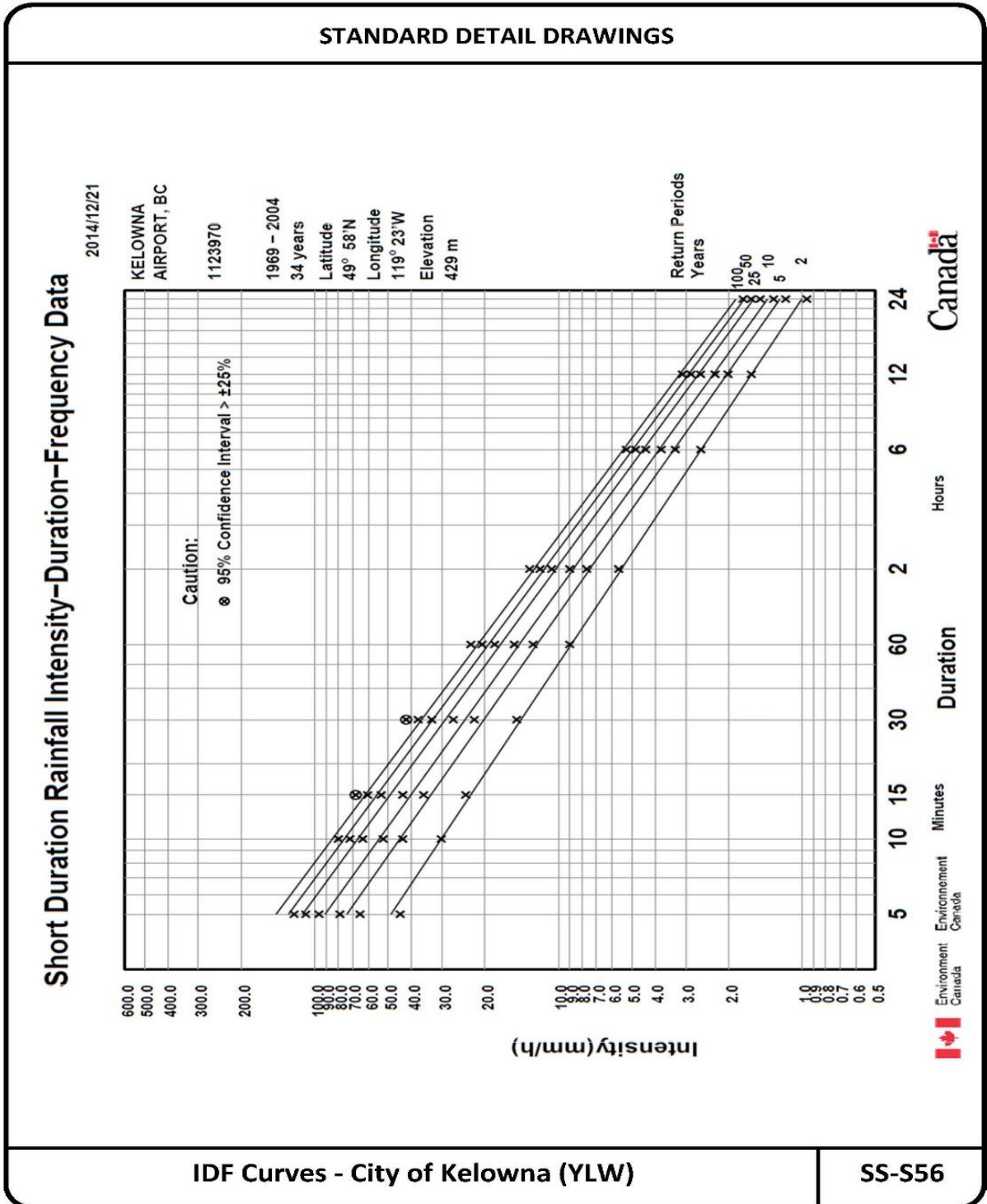
#### **3.14.2 Sediment Control**

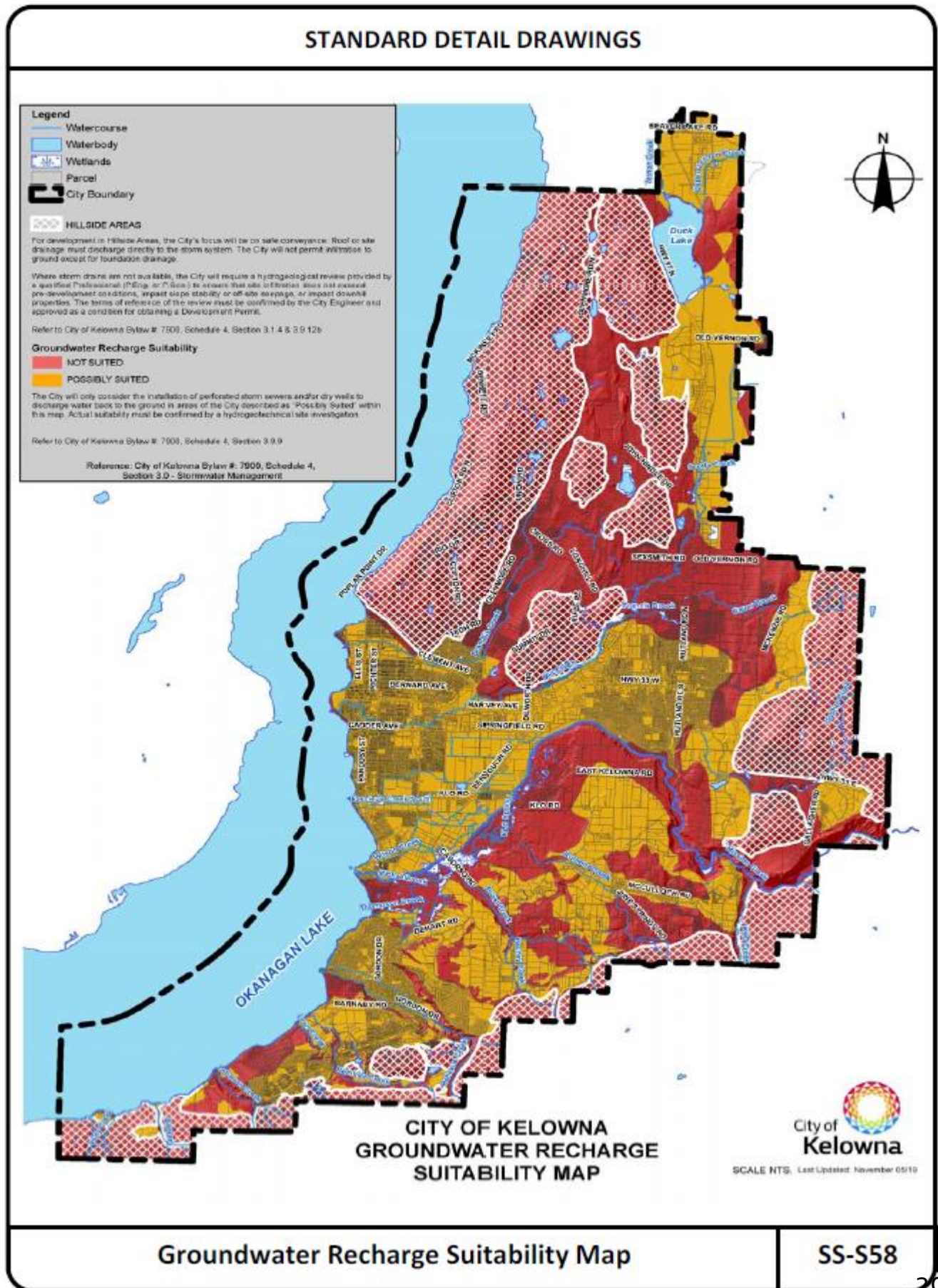
Silt fences can be an effective barrier to contain soil, but are not an effective filter of sediment laden runoff. Their permeability is insufficient to allow water to pass through, and therefore more commonly act as a dam which is then often undermined or circumvented by the flow of water. When used appropriately as a soil containment barrier, they must be sufficiently installed and maintained. Design criteria include: stakes should be > 7.5cm in diameter and > 1.5m long and driven > 40cm into the ground; stakes should be < 2.4m apart unless wire backing is used; and bottom should be buried in a trench > 20cm.

- a) Storm drains and catch basins potentially receiving site runoff are to be protected with filters.
- b) Straw bales and gravel berms are to be used within flow paths to slow water and promote trapping of coarse sediment. Note that these are less effective for fine sediment.
- c) Dust control is required at all times.
- d) Soil transport from vehicles coming and going from the site must be controlled. Where a wheel wash facility is constructed, wash water must be appropriately contained and treated prior to release off-site.

- e) Sediment ponds (or basins) are generally applied to larger construction sites (> 2 hectares) to settle suspended sediments larger than 0.02mm. The outlet should consist of a perforated riser pipe with a gravel jacket. Internal gravel baffles are to be installed to create individual cells to reduce velocities and prevent short circuiting of flow to the outlet. As a design guideline, ponds should be sized to accommodate 125 m<sup>3</sup>/ha of site area. Of this volume, at least 20% should be dedicated to a forebay. The remainder, as a permanent pool, should measure 1.3-1.8m in average depth, and not exceed 2.4m.
- f) Sediment traps are similar to sediment ponds, but designed for small sites. Generally fed by swales, these facilities are located on the low-side of the site to receive site runoff water and allow settling of solids before discharge off-site.









# CITY OF KELOWNA

## BYLAW NO. 11955

### **Housing Agreement Authorization Bylaw – Mission Group Rentals Ltd., Inc. No. BC1151526 454 – 464 West Avenue**

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Whereas pursuant to Section 483 of the *Local Government Act*, a local government may, by bylaw, enter into a housing agreement.

Therefore, the Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. The Municipal Council hereby authorizes the City of Kelowna to enter into a Housing Agreement with West Avenue - Mission Group Rentals Ltd., Inc. No. BC1151526 for the lands known as Lot A District Lot 14 ODYD Plan EPP92146 located on West Avenue, Kelowna, B.C., a true copy of which is attached to and forms part of this bylaw as Schedule "A".
2. The Mayor and City Clerk are hereby authorized to execute the attached agreement as well as any conveyances, deeds, receipts or other documents in connection with the attached agreement.
3. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this 2<sup>nd</sup> day of December, 2019.

Adopted by the Municipal Council of the City of Kelowna this

---

Mayor

---

City Clerk

**PURPOSE-BUILT RENTAL HOUSING AGREEMENT**

THIS AGREEMENT dated for reference August 20<sup>th</sup>, 2019 affects:

**LEGAL DESCRIPTION OF PROPERTY SUBJECT TO THE AGREEMENT:**

*LOT A, DISTRICT LOT 14, OSOYOOS DIVISION YALE DISTRICT, PLAN EPP92146*

("Land")

And is

**BETWEEN: WEST AVENUE – MSSION GROUP RENTALS LTD**

1000-1631 Dickson Avenue  
Kelowna, B.C.  
V1Y 0B5

("Owner")

**AND:**

**CITY OF KELOWNA**, a local government incorporated pursuant to the *Community Charter* and having its offices at 1435 Water Street, Kelowna, B.C. V1Y 1J4

("City")

**GIVEN THAT:**

- A. The Owner has applied to the City for rezoning of the Lands to permit the construction of a housing complex that will include purpose-built rental housing units, as defined in this Agreement, on certain lands more particularly described in this Agreement;
- B. The City may, pursuant to section 483 of the *Local Government Act*, enter into an agreement with an owner of land that includes terms and conditions regarding the occupancy, tenure, and availability of the housing units on the land or construction on land;
- C. The Owner and the City wish to enter into this Agreement to provide for purpose-built rental housing on the terms and conditions set out in this Agreement, and agree that this Agreement is a housing agreement under s. 483 of the *Local Government Act*; and
- D. The City has, by bylaw, authorized the execution of this Agreement and the Owner has duly authorized the execution of this Agreement;

This Agreement is evidence that in consideration of \$1.00 paid by the City to the Owner (the receipt of which is acknowledged by the Owner) and in consideration of the promises exchanged below, the City and Owner agree, as a housing agreement between the Owner and the City under s. 483 of the *Local Government Act*, as follows:

## ARTICLE 1 INTERPRETATION

### 1.1 Definitions -

"Caregiver" means an individual who provides assistance with the performance of the personal functions and activities necessary for daily living that a person is unable to perform efficiently for himself or herself;

"City" means the City of Kelowna;

"Dwelling Unit" means accommodation providing sleeping rooms, washrooms, and no more than one kitchen, intended for domestic use, and used or intended to be used permanently or semi-permanently for a Household. This use does not include a room in a hotel or a motel.

"Household" means

- (a) a person;
- (b) two or more persons related by blood, marriage, or adoption; or associated through foster care, all living together in one dwelling unit as a single household using common cooking facilities;
- (c) a group of not more than five persons, including boarders, who are not related by blood, marriage, or adoption, or associated through foster care, all living together in one dwelling unit as a single household using common cooking facilities; or
- (d) a combination of (b) and (c), provided that the combined total does not include more than 3 persons unrelated by blood, marriage or adoption or associated through foster care; all living together in one dwelling unit as a single household using common cooking facilities.

In addition, a household may also include up to one Caregiver or nanny;

"Land" means the land described herein;

"LTO" means the Kamloops Land Title Office or its successor;

"Official Community Plan" means the City of Kelowna Official Community Plan Bylaw No. 10500, or its successor bylaw;

"Owner" means the registered owner of the Lands from time to time and any parcels into which the Lands are subdivided;

"Purpose-Built Rental Housing" means a Dwelling Unit that is intended to be used for rental housing; and

"Tenancy Agreement" means a tenancy agreement as defined in, and subject to, the *Residential Tenancy Act*.

**1.2 Interpretation** - In this Agreement:

- (a) reference to the singular includes a reference to the plural, and vice versa, unless the context requires otherwise;
- (b) article and section headings have been inserted for ease of reference only and are not to be used in interpreting this Agreement;
- (c) reference to a particular numbered section or article, or to a particular lettered Schedule, is a reference to the correspondingly numbered or lettered article, section or Schedule of this Agreement;
- (d) if a word or expression is defined in this Agreement, other parts of speech and grammatical forms of the same word or expression have corresponding meanings;
- (e) the word "enactment" has the meaning given in the *Interpretation Act* on the reference date of this Agreement;
- (f) reference to any enactment includes any regulations, orders or directives made under the authority of that enactment;
- (g) reference to any enactment is a reference to that enactment as consolidated, revised, amended, re-enacted or replaced, unless otherwise expressly provided;
- (h) the provisions of s. 25 of the *Interpretation Act* with respect to the calculation of time apply;
- (i) time is of the essence;
- (j) all provisions are to be interpreted as always speaking;
- (k) reference to a "party" is a reference to a party to this Agreement and to their respective successors, assigns, trustees, administrators and receivers;
- (l) reference to a "day", "month", "quarter" or "year" is a reference to a calendar day, calendar month, calendar quarter or calendar year, as the case may be, unless otherwise expressly provided;
- (m) the definitions given in the City of Kelowna Zoning Bylaw No. 8000, or its successor bylaw, and the Official Community Plan apply for the purposes of this Agreement; and
- (n) any act, decision, determination, consideration, consent or exercise of discretion by a party, or other person, as provided in this Agreement will be performed, made or exercised acting reasonably.

**1.3 Purpose of Agreement** - The Owner and the City agree that:

- (a) this Agreement is intended to serve the public interest by providing for occupancy of a certain number of Dwelling Units, of the kinds provided for in this Agreement, that are in demand in the City of Kelowna but that are not readily available;
- (b) damages are not an adequate remedy to the City in respect of any breach of this Agreement by the Owner, such that the Owner agrees the City should be entitled to an order for specific performance, injunction or other specific relief respecting any breach of this Agreement by the Owner.

**ARTICLE 2  
HOUSING AGREEMENT AND LAND USE RESTRICTIONS**

**2.1 Land Use Restrictions** - The Owner and the City hereby covenant and agree as follows:

- (a) The Land will be used only in accordance with this Agreement;
- (b) The Owner will design, construct and maintain one or more buildings providing 48 Dwelling Units as Purpose-Built Rental Housing
- (c) The Owner acknowledges that the City will not support applications to stratify the building(s) on the Land, thereby allowing the identified Purpose-Built Rental Housing Dwelling Units to be sold independently of each other, for a period of ten (10) years from the date of this Agreement.

**ARTICLE 3  
HOUSING AGREEMENT AND TRANSFER RESTRICTIONS**

**3.1 Purchaser Qualifications** - The City and the Owner agree as follows:

- (a) the Owner will not sell or transfer, or agree to sell or transfer, any interest in any building containing Purpose-Built Rental Housing Dwelling Units on the Land other than a full interest in the fee simple title to an agency or individual that will continue to ensure that the Purpose-Built Rental Housing Dwelling Units are available in accordance with this Agreement.

**3.2 Use and Occupancy of Purpose-Built Rental Housing Dwelling Unit** - The Owner agrees with the City as follows:

- (a) the Owner will rent or lease each Purpose-Built Rental Housing Dwelling Unit on the Land in accordance with the *Residential Tenancy Act*, and in no event may the Owner itself occupy a Purpose-Built Rental Housing Dwelling Unit or use the Purpose-Built Rental Housing Dwelling Unit for short-term vacation accommodation; and
- (b) the Owner will deliver a copy of the Tenancy Agreement for each Purpose-Built Rental Housing Dwelling Unit to the City upon demand.

**ARTICLE 4  
GENERAL**

**4.1 Notice of Housing Agreement** - For clarity, the Owner acknowledges and agrees that:

- (a) this Agreement constitutes a housing agreement entered into under s. 483 of the *Local Government Act*;

- (b) the City is requiring the Owner to file a notice of housing agreement in the LTO against title to the Land;
- (c) once such a notice is filed, this Agreement binds all persons who acquire an interest in the Land;
- (d) in the event the parties agree to release this Agreement from the title of the Land, which may not occur before the tenth (10<sup>th</sup>) anniversary of the date of this Agreement, the Owner will repay the City for 100% of the amount of the rental grant received from the City. Such repaid funds will be directed to the City's Housing Opportunities Reserve Fund.

**4.2 No Effect On Laws or Powers** - This Agreement does not

- (a) affect or limit the discretion, rights, duties or powers of the City under any enactment or at common law, including in relation to the use or subdivision of land,
- (b) impose on the City any legal duty or obligation, including any duty of care or contractual or other legal duty or obligation, to enforce this Agreement,
- (c) affect or limit any enactment relating to the use or subdivision of land, or
- (d) relieve the Owner from complying with any enactment, including in relation to the use or subdivision of land.

**4.3 Management** – The Owner covenants and agrees that it will furnish good and efficient management of the Dwelling Units and will permit representatives of the City to inspect the Dwelling Units at any reasonable time, subject to the notice provisions of the *Residential Tenancy Act*. The Owner further covenants and agrees that it will maintain the Dwelling Units in a satisfactory state of repair and fit for habitation and will comply with all laws, including health and safety standards applicable to the Land. Notwithstanding the foregoing, the Owner acknowledges and agrees that the City, in its absolute discretion, may require the Owner, at the Owner's expense, to hire a person or company with the skill and expertise to manage the Dwelling Units.

**4.4 Notice** - Any notice which may be or is required to be given under this Agreement will be in writing and either be delivered or sent by facsimile transmission. Any notice which is delivered is to be considered to have been given on the first day after it is dispatched for delivery. Any notice which is sent by fax transmission is to be considered to have been given on the first business day after it is sent. If a party changes its address or facsimile number, or both, it will promptly give notice of its new address or facsimile number, or both, to the other party as provided in this section.

**4.5 Agreement Runs With the Land** - Every obligation and covenant of the Owner in this Agreement constitutes both a contractual obligation and a covenant granted by the Owner to the City in respect of the Land and this Agreement burdens the Land and runs with it and binds the Owner's successors in title and binds every parcel into which it is consolidated or subdivided by any means, including by subdivision or by strata plan under the *Strata Property Act*.

**4.6 Limitation on Owner's Obligations** - The Owner is only liable for breaches of this Agreement that occur while the Owner is the registered owner of the Land.

**4.7 Release** – The Owner by this Agreement releases and forever discharges the City and each of its elected



officials, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors, and assigns, from and against all claims, demands, damages, actions, or causes of action by reason of or arising out of advice or direction respecting the ownership, lease, operation or management of the Land or the Dwelling Units which has been or at any time after the commencement of this Agreement may be given to the Owner by all or any of them. This clause will survive the termination of this Agreement.

- 4.8 **Joint Venture** – Nothing in this Agreement will constitute the Owner as the agent, joint venturer, or partner of the City or give the Owner any authority to bind the City in any way.
- 4.9 **Waiver** - An alleged waiver of any breach of this Agreement is effective only if it is an express waiver in writing of the breach. A waiver of a breach of this Agreement does not operate as a waiver of any other breach of this Agreement.
- 4.10 **Further Acts** - The Owner will do everything reasonably necessary to give effect to the intent of this Agreement, including execution of further instruments.
- 4.11 **Severance** - If any part of this Agreement is held to be invalid, illegal or unenforceable by a court having the jurisdiction to do so, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.
- 4.12 **Equitable Remedies** – The Owner acknowledges and agrees that damages would be an inadequate remedy for the City for breach of this Agreement and that the public interest strongly favours specific performance, injunctive relief (mandatory or otherwise), or other equitable relief, as the only adequate remedy for a default under this Agreement.
- 4.13 **No Other Agreements** - This Agreement is the entire agreement between the parties regarding its subject and it terminates and supersedes all other agreements and arrangements regarding its subject.
- 4.14 **Amendment** - This Agreement may be discharged, amended or affected only by an instrument duly executed by both the Owner and the City.
- 4.15 **Enurement** - This Agreement binds the parties to it and their respective successors, heirs, executors and administrators. Reference in this Agreement to the "City" is a reference also to the elected and appointed officials, employees and agents of the City.
- 416 **Deed and Contract** - By executing and delivering this Agreement each of the parties intends to create both a contract and a deed executed and delivered under seal.

IN WITNESS WHEREOF the parties hereunto have executed this Agreement on the date and year first above written.

SIGNED, SEALED & DELIVERED in  
the presence of:

  
Signature of Witness

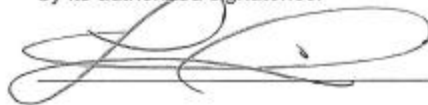
ARPAN KANDOLA  
Print Name

1000-1631 DICKSON AVENUE, KELOWNA  
Address

DEVELOPMENT COORDINATOR  
Occupation

"OWNER"

by its authorized signatories:



LUKE TURRI  
Print Name:

WEST AVENUE - MISSION GROUP RENTALS LTD.

\_\_\_\_\_  
Print Name:

SIGNED, SEALED & DELIVERED in  
the presence of:

\_\_\_\_\_  
Signature of Witness

\_\_\_\_\_  
Print Name

\_\_\_\_\_  
Address

\_\_\_\_\_  
Occupation

CITY OF KELOWNA

by its authorized signatories:

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

# CITY OF KELOWNA

## BYLAW NO. 11956

### **Housing Agreement Authorization Bylaw Whitworth Holdings Ltd., Inc. No. BC1059455 4119 Lakeshore Road**

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Whereas pursuant to Section 483 of the *Local Government Act*, a local government may, by bylaw, enter into a housing agreement.

Therefore, the Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. The Municipal Council hereby authorizes the City of Kelowna to enter into a Housing Agreement with Whitworth Holdings Ltd., Inc. No. BC1059455 for the lands known as Lot 3 Section 6 Township 26 ODYD Plan 4912 Except Plan EPP93973 located on Lakeshore Road, Kelowna, B.C., a true copy of which is attached to and forms part of this bylaw as Schedule "A".
2. The Mayor and City Clerk are hereby authorized to execute the attached agreement as well as any conveyances, deeds, receipts or other documents in connection with the attached agreement.
3. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this 2<sup>nd</sup> day of December, 2019.

Adopted by the Municipal Council of the City of Kelowna this

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Mayor

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City Clerk

**PURPOSE-BUILT RENTAL HOUSING AGREEMENT**

THIS AGREEMENT dated for reference November 18, 2019 affects:

**LEGAL DESCRIPTION OF PROPERTY SUBJECT TO THE AGREEMENT:**

Lot 3 Section 6 Township 26 ODYD Plan 4912

("Land")

And is

**BETWEEN: Whitworth Holdings Ltd.**

with offices at 103, 1054 Ellis Street, Kelowna, British Columbia, V1Y 1Z1

("Owner")

**AND:**

**CITY OF KELOWNA**, a local government incorporated pursuant to the *Community Charter* and having its offices at 1435 Water Street, Kelowna, B.C. V1Y 1J4

("City")

GIVEN THAT:

- A. The Owner has applied to the City for rezoning of the Lands to permit the construction of a housing complex that will include purpose-built rental housing units, as defined in this Agreement, on certain lands more particularly described in this Agreement;
- B. The City may, pursuant to section 483 of the *Local Government Act*, enter into an agreement with an owner of land that includes terms and conditions regarding the occupancy, tenure, and availability of the housing units on the land or construction on land;
- C. The Owner and the City wish to enter into this Agreement to provide for purpose-built rental housing on the terms and conditions set out in this Agreement, and agree that this Agreement is a housing agreement under s. 483 of the *Local Government Act*; and
- D. The City has, by bylaw, authorized the execution of this Agreement and the Owner has duly authorized the execution of this Agreement;

This Agreement is evidence that in consideration of \$1.00 paid by the City to the Owner (the receipt of which is acknowledged by the Owner) and in consideration of the promises exchanged below, the City and Owner agree, as a housing agreement between the Owner and the City under s. 483 of the *Local Government Act*, as follows:

## ARTICLE 1 INTERPRETATION

### 1.1 Definitions -

"Caregiver" means an individual who provides assistance with the performance of the personal functions and activities necessary for daily living that a person is unable to perform efficiently for himself or herself;

"City" means the City of Kelowna;

"Dwelling Unit" means accommodation providing sleeping rooms, washrooms, and no more than one kitchen, intended for domestic use, and used or intended to be used permanently or semi-permanently for a Household. This use does not include a room in a hotel or a motel.

"Household" means

- (a) a person;
- (b) two or more persons related by blood, marriage, or adoption; or associated through foster care, all living together in one dwelling unit as a single household using common cooking facilities;
- (c) a group of not more than five persons, including boarders, who are not related by blood, marriage, or adoption, or associated through foster care, all living together in one dwelling unit as a single household using common cooking facilities; or
- (d) a combination of (b) and (c), provided that the combined total does not include more than 3 persons unrelated by blood, marriage or adoption or associated through foster care; all living together in one dwelling unit as a single household using common cooking facilities.

In addition, a household may also include up to one Caregiver or nanny;

"Land" means the land described herein;

"LTO" means the Kamloops Land Title Office or its successor;

"Official Community Plan" means the City of Kelowna Official Community Plan Bylaw No. 10500, or its successor bylaw;

"Owner" means the registered owner of the Lands from time to time and any parcels into which the Lands are subdivided;

"Purpose-Built Rental Housing" means a Dwelling Unit that is intended to be used for rental housing; and

"Tenancy Agreement" means a tenancy agreement as defined in, and subject to, the *Residential Tenancy Act*.

### 1.2 Interpretation - In this Agreement:

- (a) reference to the singular includes a reference to the plural, and vice versa, unless the context requires otherwise;
- (b) article and section headings have been inserted for ease of reference only and are not to be used in interpreting this Agreement;
- (c) reference to a particular numbered section or article, or to a particular lettered Schedule, is a reference to the correspondingly numbered or lettered article, section or Schedule of this Agreement;
- (d) if a word or expression is defined in this Agreement, other parts of speech and grammatical forms of the same word or expression have corresponding meanings;
- (e) the word "enactment" has the meaning given in the *Interpretation Act* on the reference date of this Agreement;
- (f) reference to any enactment includes any regulations, orders or directives made under the authority of that enactment;
- (g) reference to any enactment is a reference to that enactment as consolidated, revised, amended, re-enacted or replaced, unless otherwise expressly provided;
- (h) the provisions of s. 25 of the *Interpretation Act* with respect to the calculation of time apply;
- (i) time is of the essence;
- (j) all provisions are to be interpreted as always speaking;
- (k) reference to a "party" is a reference to a party to this Agreement and to their respective successors, assigns, trustees, administrators and receivers;
- (l) reference to a "day", "month", "quarter" or "year" is a reference to a calendar day, calendar month, calendar quarter or calendar year, as the case may be, unless otherwise expressly provided;
- (m) the definitions given in the City of Kelowna Zoning Bylaw No. 8000, or its successor bylaw, and the Official Community Plan apply for the purposes of this Agreement; and
- (n) any act, decision, determination, consideration, consent or exercise of discretion by a party, or other person, as provided in this Agreement will be performed, made or exercised acting reasonably.

**1.3 Purpose of Agreement** - The Owner and the City agree that:

- (a) this Agreement is intended to serve the public interest by providing for occupancy of a certain number of Dwelling Units, of the kinds provided for in this Agreement, that are in demand in the City of Kelowna but that are not readily available;
- (b) damages are not an adequate remedy to the City in respect of any breach of this Agreement by the Owner, such that the Owner agrees the City should be entitled to an order for specific performance, injunction or other specific relief respecting any breach of this Agreement by the Owner.



**ARTICLE 2  
HOUSING AGREEMENT AND LAND USE RESTRICTIONS**

**2.1 Land Use Restrictions** - The Owner and the City hereby covenant and agree as follows:

- (a) The Land will be used only in accordance with this Agreement;
- (b) The Owner will design, construct and maintain one or more buildings providing 26 Dwelling Units as Purpose-Built Rental Housing
- (c) The Owner acknowledges that the City will not support applications to stratify the building(s) on the Land, thereby allowing the identified Purpose-Built Rental Housing Dwelling Units to be sold independently of each other, for a period of ten (10) years from the date of this Agreement.

**ARTICLE 3  
HOUSING AGREEMENT AND TRANSFER RESTRICTIONS**

**3.1 Purchaser Qualifications** - The City and the Owner agree as follows:

- (a) the Owner will not sell or transfer, or agree to sell or transfer, any interest in any building containing Purpose-Built Rental Housing Dwelling Units on the Land other than a full interest in the fee simple title to an agency or individual that will continue to ensure that the Purpose-Built Rental Housing Dwelling Units are available in accordance with this Agreement.

**3.2 Use and Occupancy of Purpose-Built Rental Housing Dwelling Unit** - The Owner agrees with the City as follows:

- (a) the Owner will rent or lease each Purpose-Built Rental Housing Dwelling Unit on the Land in accordance with the *Residential Tenancy Act*, and in no event may the Owner itself occupy a Purpose-Built Rental Housing Dwelling Unit or use the Purpose-Built Rental Housing Dwelling Unit for short-term vacation accommodation; and
- (b) the Owner will deliver a copy of the Tenancy Agreement for each Purpose-Built Rental Housing Dwelling Unit to the City upon demand.

**ARTICLE 4  
GENERAL**

**4.1 Notice of Housing Agreement** - For clarity, the Owner acknowledges and agrees that:

- (a) this Agreement constitutes a housing agreement entered into under s. 483 of the *Local Government Act*;
- (b) the City is requiring the Owner to file a notice of housing agreement in the LTO against title to the

Land;

- (c) once such a notice is filed, this Agreement binds all persons who acquire an interest in the Land;
- (d) in the event the parties agree to release this Agreement from the title of the Land, which may not occur before the tenth (10<sup>th</sup>) anniversary of the date of this Agreement, the Owner will repay the City for 100% of the amount of the rental grant received from the City. Such repaid funds will be directed to the City's Housing Opportunities Reserve Fund.

**4.2 No Effect On Laws or Powers** - This Agreement does not

- (a) affect or limit the discretion, rights, duties or powers of the City under any enactment or at common law, including in relation to the use or subdivision of land,
- (b) impose on the City any legal duty or obligation, including any duty of care or contractual or other legal duty or obligation, to enforce this Agreement,
- (c) affect or limit any enactment relating to the use or subdivision of land, or
- (d) relieve the Owner from complying with any enactment, including in relation to the use or subdivision of land.

**4.3 Management** – The Owner covenants and agrees that it will furnish good and efficient management of the Dwelling Units and will permit representatives of the City to inspect the Dwelling Units at any reasonable time, subject to the notice provisions of the *Residential Tenancy Act*. The Owner further covenants and agrees that it will maintain the Dwelling Units in a satisfactory state of repair and fit for habitation and will comply with all laws, including health and safety standards applicable to the Land. Notwithstanding the foregoing, the Owner acknowledges and agrees that the City, in its absolute discretion, may require the Owner, at the Owner's expense, to hire a person or company with the skill and expertise to manage the Dwelling Units.

**4.4 Notice** - Any notice which may be or is required to be given under this Agreement will be in writing and either be delivered or sent by facsimile transmission. Any notice which is delivered is to be considered to have been given on the first day after it is dispatched for delivery. Any notice which is sent by fax transmission is to be considered to have been given on the first business day after it is sent. If a party changes its address or facsimile number, or both, it will promptly give notice of its new address or facsimile number, or both, to the other party as provided in this section.

**4.5 Agreement Runs With the Land** - Every obligation and covenant of the Owner in this Agreement constitutes both a contractual obligation and a covenant granted by the Owner to the City in respect of the Land and this Agreement burdens the Land and runs with it and binds the Owner's successors in title and binds every parcel into which it is consolidated or subdivided by any means, including by subdivision or by strata plan under the *Strata Property Act*.

**4.6 Limitation on Owner's Obligations** - The Owner is only liable for breaches of this Agreement that occur while the Owner is the registered owner of the Land.

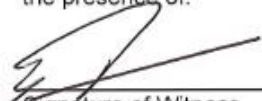
**4.7 Release** – The Owner by this Agreement releases and forever discharges the City and each of its elected officials, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors, and assigns, from and against all claims, demands, damages,

actions, or causes of action by reason of or arising out of advice or direction respecting the ownership, lease, operation or management of the Land or the Dwelling Units which has been or at any time after the commencement of this Agreement may be given to the Owner by all or any of them. This clause will survive the termination of this Agreement.

- 4.8 **Joint Venture** – Nothing in this Agreement will constitute the Owner as the agent, joint venturer, or partner of the City or give the Owner any authority to bind the City in any way.
- 4.9 **Waiver** - An alleged waiver of any breach of this Agreement is effective only if it is an express waiver in writing of the breach. A waiver of a breach of this Agreement does not operate as a waiver of any other breach of this Agreement.
- 4.10 **Further Acts** - The Owner will do everything reasonably necessary to give effect to the intent of this Agreement, including execution of further instruments.
- 4.11 **Severance** - If any part of this Agreement is held to be invalid, illegal or unenforceable by a court having the jurisdiction to do so, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.
- 4.12 **Equitable Remedies** – The Owner acknowledges and agrees that damages would be an inadequate remedy for the City for breach of this Agreement and that the public interest strongly favours specific performance, injunctive relief (mandatory or otherwise), or other equitable relief, as the only adequate remedy for a default under this Agreement.
- 4.13 **No Other Agreements** - This Agreement is the entire agreement between the parties regarding its subject and it terminates and supersedes all other agreements and arrangements regarding its subject.
- 4.14 **Amendment** - This Agreement may be discharged, amended or affected only by an instrument duly executed by both the Owner and the City.
- 4.15 **Enurement** - This Agreement binds the parties to it and their respective successors, heirs, executors and administrators. Reference in this Agreement to the "City" is a reference also to the elected and appointed officials, employees and agents of the City.
- 4.16 **Deed and Contract** - By executing and delivering this Agreement each of the parties intends to create both a contract and a deed executed and delivered under seal.

IN WITNESS WHEREOF the parties hereunto have executed this Agreement on the date and year first above written.

SIGNED, SEALED & DELIVERED in  
the presence of:

  
\_\_\_\_\_  
Signature of Witness

Cameron J. Costley  
\_\_\_\_\_  
Print Name

#103 - 1054 Elliot Street  
\_\_\_\_\_  
Address Kelowna BC V1Y 1Z1

\_\_\_\_\_  
Occupation

"OWNER"

by its authorized signatories:

  
\_\_\_\_\_

KATE STEWART  
\_\_\_\_\_  
Print Name:

\_\_\_\_\_  
Print Name:

SIGNED, SEALED & DELIVERED in  
the presence of:

\_\_\_\_\_  
Signature of Witness

\_\_\_\_\_  
Print Name

\_\_\_\_\_  
Address

\_\_\_\_\_  
Occupation

CITY OF KELOWNA

by its authorized signatories:

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

# CITY OF KELOWNA

## BYLAW NO. 11958

### **Housing Agreement Authorization Bylaw Culos Development (1996) Inc., Inc. No. BC1099204 1165 Sutherland Avenue**

---

Whereas pursuant to Section 483 of the *Local Government Act*, a local government may, by bylaw, enter into a housing agreement.

Therefore, the Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. The Municipal Council hereby authorizes the City of Kelowna to enter into a Housing Agreement with Culos Development (1996) Inc., Inc. No. BC1099204 for the lands known as Lot A District Lot 137 ODYD Plan EPP88875 located on Sutherland Avenue, Kelowna, B.C., a true copy of which is attached to and forms part of this bylaw as Schedule "A".
2. The Mayor and City Clerk are hereby authorized to execute the attached agreement as well as any conveyances, deeds, receipts or other documents in connection with the attached agreement.
3. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this 2<sup>nd</sup> day of December, 2019.

Adopted by the Municipal Council of the City of Kelowna this

---

Mayor

---

City Clerk

**PURPOSE-BUILT RENTAL HOUSING AGREEMENT**

THIS AGREEMENT dated for reference \_\_\_\_\_ affects:

**LEGAL DESCRIPTION OF PROPERTY SUBJECT TO THE AGREEMENT:**

Proposed Strata Lots #6 to #41 ( inclusive ) of the subdivision of:

Lot "A" , District Lot 137, Osoyoos Division Yale District, Plan EPP88875

("Land")

And is

**BETWEEN:** CULOS DEVELOPMENT (1996) INC.  
#106 - 1449 St. Paul Street,  
Kelowna, B.C. V1Y2E5

("Owner")

**AND:**

**CITY OF KELOWNA**, a local government incorporated pursuant to the *Community Charter* and having its offices at 1435 Water Street, Kelowna, B.C. V1Y 1J4

("City")

**GIVEN THAT:**

- A. The Owner has applied to the City for rezoning of the Lands to permit the construction of a housing complex that will include purpose-built rental housing units, as defined in this Agreement, on certain lands more particularly described in this Agreement;
- B. The City may, pursuant to section 483 of the *Local Government Act*, enter into an agreement with an owner of land that includes terms and conditions regarding the occupancy, tenure, and availability of the housing units on the land or construction on land;
- C. The Owner and the City wish to enter into this Agreement to provide for purpose-built rental housing on the terms and conditions set out in this Agreement, and agree that this Agreement is a housing agreement under s. 483 of the *Local Government Act*; and
- D. The City has, by bylaw, authorized the execution of this Agreement and the Owner has duly authorized the execution of this Agreement;

This Agreement is evidence that in consideration of \$1.00 paid by the City to the Owner (the receipt of which is acknowledged by the Owner) and in consideration of the promises exchanged below, the City and Owner agree, as a housing agreement between the Owner and the City under s. 483 of the *Local Government Act*, as follows:



## ARTICLE 1 INTERPRETATION

### 1.1 Definitions -

"Caregiver" means an individual who provides assistance with the performance of the personal functions and activities necessary for daily living that a person is unable to perform efficiently for himself or herself;

"City" means the City of Kelowna;

"Dwelling Unit" means accommodation providing sleeping rooms, washrooms, and no more than one kitchen, intended for domestic use, and used or intended to be used permanently or semi-permanently for a Household. This use does not include a room in a hotel or a motel.

"Household" means

- (a) a person;
- (b) two or more persons related by blood, marriage, or adoption; or associated through foster care, all living together in one dwelling unit as a single household using common cooking facilities;
- (c) a group of not more than five persons, including boarders, who are not related by blood, marriage, or adoption, or associated through foster care, all living together in one dwelling unit as a single household using common cooking facilities; or
- (d) a combination of (b) and (c), provided that the combined total does not include more than 3 persons unrelated by blood, marriage or adoption or associated through foster care; all living together in one dwelling unit as a single household using common cooking facilities.

In addition, a household may also include up to one Caregiver or nanny;

"Land" means the land described herein;

"LTO" means the Kamloops Land Title Office or its successor;

"Official Community Plan" means the City of Kelowna Official Community Plan Bylaw No. 10500, or its successor bylaw;

"Owner" means the registered owner of the Lands from time to time and any parcels into which the Lands are subdivided;

"Purpose-Built Rental Housing" means a Dwelling Unit that is intended to be used for rental housing; and

"Tenancy Agreement" means a tenancy agreement as defined in, and subject to, the *Residential Tenancy Act*.

**1.2 Interpretation - In this Agreement:**

- (a) reference to the singular includes a reference to the plural, and vice versa, unless the context requires otherwise;
- (b) article and section headings have been inserted for ease of reference only and are not to be used in interpreting this Agreement;
- (c) reference to a particular numbered section or article, or to a particular lettered Schedule, is a reference to the correspondingly numbered or lettered article, section or Schedule of this Agreement;
- (d) if a word or expression is defined in this Agreement, other parts of speech and grammatical forms of the same word or expression have corresponding meanings;
- (e) the word "enactment" has the meaning given in the *Interpretation Act* on the reference date of this Agreement;
- (f) reference to any enactment includes any regulations, orders or directives made under the authority of that enactment;
- (g) reference to any enactment is a reference to that enactment as consolidated, revised, amended, re-enacted or replaced, unless otherwise expressly provided;
- (h) the provisions of s. 25 of the *Interpretation Act* with respect to the calculation of time apply;
- (i) time is of the essence;
- (j) all provisions are to be interpreted as always speaking;
- (k) reference to a "party" is a reference to a party to this Agreement and to their respective successors, assigns, trustees, administrators and receivers;
- (l) reference to a "day", "month", "quarter" or "year" is a reference to a calendar day, calendar month, calendar quarter or calendar year, as the case may be, unless otherwise expressly provided;
- (m) the definitions given in the City of Kelowna Zoning Bylaw No. 8000, or its successor bylaw, and the Official Community Plan apply for the purposes of this Agreement; and
- (n) any act, decision, determination, consideration, consent or exercise of discretion by a party, or other person, as provided in this Agreement will be performed, made or exercised acting reasonably.

**1.3 Purpose of Agreement - The Owner and the City agree that:**

- (a) this Agreement is intended to serve the public interest by providing for occupancy of a certain number of Dwelling Units, of the kinds provided for in this Agreement, that are in demand in the City of Kelowna but that are not readily available;

- (b) damages are not an adequate remedy to the City in respect of any breach of this Agreement by the Owner, such that the Owner agrees the City should be entitled to an order for specific performance, injunction or other specific relief respecting any breach of this Agreement by the Owner.

## **ARTICLE 2 HOUSING AGREEMENT AND LAND USE RESTRICTIONS**

### **2.1 Land Use Restrictions** - The Owner and the City hereby covenant and agree as follows:

- (a) The Land will be used only in accordance with this Agreement;
- (b) The Owner will design, construct and maintain one or more buildings providing **36 Dwelling Units** as Purpose-Built Rental Housing
- (c) The Owner acknowledges that the City will not support applications to stratify the building(s) on the Land, thereby allowing the identified Purpose-Built Rental Housing Dwelling Units to be sold independently of each other, for a period of ten (10) years from the date of this Agreement.

## **ARTICLE 3 HOUSING AGREEMENT AND TRANSFER RESTRICTIONS**

### **3.1 Purchaser Qualifications** - The City and the Owner agree as follows:

- (a) the Owner will not sell or transfer, or agree to sell or transfer, any interest in any building containing Purpose-Built Rental Housing Dwelling Units on the Land other than a full interest in the fee simple title to an agency or individual that will continue to ensure that the Purpose-Built Rental Housing Dwelling Units are available in accordance with this Agreement.

### **3.2 Use and Occupancy of Purpose-Built Rental Housing Dwelling Unit** - The Owner agrees with the City as follows:

- (a) the Owner will rent or lease each Purpose-Built Rental Housing Dwelling Unit on the Land in accordance with the *Residential Tenancy Act*, and in no event may the Owner itself occupy a Purpose-Built Rental Housing Dwelling Unit or use the Purpose-Built Rental Housing Dwelling Unit for short-term vacation accommodation; and
- (b) the Owner will deliver a copy of the Tenancy Agreement for each Purpose-Built Rental Housing Dwelling Unit to the City upon demand.

## **ARTICLE 4 GENERAL**

**4.1 Notice of Housing Agreement** - For clarity, the Owner acknowledges and agrees that:

- (a) this Agreement constitutes a housing agreement entered into under s. 483 of the *Local Government Act*;
- (b) the City is requiring the Owner to file a notice of housing agreement in the LTO against title to the Land;
- (c) once such a notice is filed, this Agreement binds all persons who acquire an interest in the Land;
- (d) in the event the parties agree to release this Agreement from the title of the Land, which may not occur before the tenth (10<sup>th</sup>) anniversary of the date of this Agreement, the Owner will repay the City for 100% of the amount of the rental grant received from the City. Such repaid funds will be directed to the City's Housing Opportunities Reserve Fund.

**4.2 No Effect On Laws or Powers** - This Agreement does not

- (a) affect or limit the discretion, rights, duties or powers of the City under any enactment or at common law, including in relation to the use or subdivision of land,
- (b) impose on the City any legal duty or obligation, including any duty of care or contractual or other legal duty or obligation, to enforce this Agreement,
- (c) affect or limit any enactment relating to the use or subdivision of land, or
- (d) relieve the Owner from complying with any enactment, including in relation to the use or subdivision of land.

**4.3 Management** – The Owner covenants and agrees that it will furnish good and efficient management of the Dwelling Units and will permit representatives of the City to inspect the Dwelling Units at any reasonable time, subject to the notice provisions of the *Residential Tenancy Act*. The Owner further covenants and agrees that it will maintain the Dwelling Units in a satisfactory state of repair and fit for habitation and will comply with all laws, including health and safety standards applicable to the Land. Notwithstanding the foregoing, the Owner acknowledges and agrees that the City, in its absolute discretion, may require the Owner, at the Owner's expense, to hire a person or company with the skill and expertise to manage the Dwelling Units.

**4.4 Notice** - Any notice which may be or is required to be given under this Agreement will be in writing and either be delivered or sent by facsimile transmission. Any notice which is delivered is to be considered to have been given on the first day after it is dispatched for delivery. Any notice which is sent by fax transmission is to be considered to have been given on the first business day after it is sent. If a party changes its address or facsimile number, or both, it will promptly give notice of its new address or facsimile number, or both, to the other party as provided in this section.

**4.5 Agreement Runs With the Land** - Every obligation and covenant of the Owner in this Agreement constitutes both a contractual obligation and a covenant granted by the Owner to the City in respect of the Land and this Agreement burdens the Land and runs with it and binds the Owner's successors in title and binds every parcel into which it is consolidated or subdivided by any means, including by subdivision or by strata plan under the *Strata Property Act*.

- 4.6 Limitation on Owner's Obligations** - The Owner is only liable for breaches of this Agreement that occur while the Owner is the registered owner of the Land.
- 4.7 Release** – The Owner by this Agreement releases and forever discharges the City and each of its elected officials, officers, directors, employees and agents, and its and their heirs, executors, administrators, personal representatives, successors, and assigns, from and against all claims, demands, damages, actions, or causes of action by reason of or arising out of advice or direction respecting the ownership, lease, operation or management of the Land or the Dwelling Units which has been or at any time after the commencement of this Agreement may be given to the Owner by all or any of them. This clause will survive the termination of this Agreement.
- 4.8 Joint Venture** – Nothing in this Agreement will constitute the Owner as the agent, joint venturer, or partner of the City or give the Owner any authority to bind the City in any way.
- 4.9 Waiver** - An alleged waiver of any breach of this Agreement is effective only if it is an express waiver in writing of the breach. A waiver of a breach of this Agreement does not operate as a waiver of any other breach of this Agreement.
- 4.10 Further Acts** - The Owner will do everything reasonably necessary to give effect to the intent of this Agreement, including execution of further instruments.
- 4.11 Severance** - If any part of this Agreement is held to be invalid, illegal or unenforceable by a court having the jurisdiction to do so, that part is to be considered to have been severed from the rest of this Agreement and the rest of this Agreement remains in force unaffected by that holding or by the severance of that part.
- 4.12 Equitable Remedies** – The Owner acknowledges and agrees that damages would be an inadequate remedy for the City for breach of this Agreement and that the public interest strongly favours specific performance, injunctive relief (mandatory or otherwise), or other equitable relief, as the only adequate remedy for a default under this Agreement.
- 4.13 No Other Agreements** - This Agreement is the entire agreement between the parties regarding its subject and it terminates and supersedes all other agreements and arrangements regarding its subject.
- 4.14 Amendment** - This Agreement may be discharged, amended or affected only by an instrument duly executed by both the Owner and the City.
- 4.15 Enurement** - This Agreement binds the parties to it and their respective successors, heirs, executors and administrators. Reference in this Agreement to the "City" is a reference also to the elected and appointed officials, employees and agents of the City.
- 416 Deed and Contract** - By executing and delivering this Agreement each of the parties intends to create both a contract and a deed executed and delivered under seal.

IN WITNESS WHEREOF the parties hereunto have executed this Agreement on the date and year first above written.

SIGNED, SEALED & DELIVERED in )  
the presence of: )

\_\_\_\_\_  
Signature of Witness )

\_\_\_\_\_  
Print Name )

\_\_\_\_\_  
Address )

\_\_\_\_\_  
Occupation )

CULOS DEVELOPMENT (1996) INC.  
by its authorized signatory:

\_\_\_\_\_  


Michael A. Culos  
Print Name: Michael A. Culos  
President

SIGNED, SEALED & DELIVERED in )  
the presence of: )

\_\_\_\_\_  
Signature of Witness )

\_\_\_\_\_  
Print Name )

\_\_\_\_\_  
Address )

CITY OF KELOWNA  
by its authorized signatories:

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk