City of Kelowna
Regular Council Meeting
AGENDA

Monday, November 18, 2019

6:00 pm

UBCO Okanagan, University Theatre Administration Building

1138 Alumni Avenue

Kelowna, BC V1V 1V7

Pages

1. Call to Order

I would like to acknowledge that we are gathered today on the traditional, ancestral, unceded territory of the syilx/Okanagan people.

This meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

2. Confirmation of Minutes 4 - 9

Regular PM Meeting - November 4, 2019

3. Public In Attendance

3.1 Final Report - Kelowna 2019 55+ BC Games 10 - 16

To provide Council with information on the results and outcomes of the Kelowna 2019 55+ BC Games

4. Development Application Reports & Related Bylaws

4.1 Harvey 757, Z19-0037 - Harsheen and Gursher Pannu 17 - 41

The Mayor to invite the Applicant, or Applicant's Representative, to come forward.

To consider a Staff recommendation to NOT rezone the subject property from the RU6 – Two Dwelling Housing zone to the RM4 – Transitional Low Density Multiple Housing zone.

4.2 Hollywood Rd 150, Z19-0109 (BL11953) - Lexington Enterprises Ltd. 42 - 45

To consider an application to rezone the subject property from the C4 - Urban Centre Commercial zone to the C4rcs- Urban Centre Commercial (retail cannabis sales) zone to allow for a retail cannabis sales establishment.

4.3 Hollywood Rd 150, BL11953 (Z19-0109) - Lexington Enterprises Ltd. 46 - 46

To give first reading to Bylaw No. 11953 in order to rezone the subject property from the C4 - Urban Centre Commercial zone to the C4rcs- Urban Centre Commercial (retail cannabis sales) zone.

4.4 Sadler Rd 145, Hwy 33 E 180 & 190, Z18-0117 (BL11957) - Studio 33 Properties Ltd., Inc No.BC1137489 47 - 76

To consider an application to rezone the subject properties from the RU1 – Large Lot Housing and RU6 – Two Dwelling Housing zones to the C4 – Urban Centre Commercial zone to facilitate the development of Multiple Dwelling Housing.

4.5 Sadler Rd 145, Hwy 33 E 180 & 190, BL11957 (Z18-0117) - Studio 33 Properties Ltd., Inc No.BC1137489 77 - 77

To give first reading to Bylaw No. 11957 in order to rezone the subject properties from the RU1 – Large Lot Housing zone and RU6 – Two Dwelling Housing zone to the C4-Urban Centre Commercial zone.

4.6 Houghton Rd 595, DP19-0182 - 562957 B.C. LTD. INC. NO. 0562957 78 - 90

To consider the form and character of an addition and façade improvements to an existing commercial building.

5. Bylaws for Adoption (Development Related)

5.1 Maple St 1869, BL11499 (Z17-0045) - Gregory and Linda Bauer 91 - 91

To adopt Bylaw No. 11499 in order to rezone the subject property from the RU1 - Large Lot Housing zone to the RU1c - Large Lot Housing with Carriage House zone.

5.2 Gramiak Rd 535, BL11949 (Z19-0062) - Barry and Doris Marciski 92 - 92

To adopt Bylaw No 11949 in order to rezone the subject property from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone.

6. Non-Development Reports & Related Bylaws

6.1 Transportation Master Plan Options Development 93 - 134

To provide Council with an overview of the projects, policies and programs (collectively referred to as "options") under consideration for the Transportation Master Plan.

6.2 Community Trends Report 2019 - Impacts of the Sharing Economy 135 - 164

To introduce "Sharing Economy Impacts: 2019 Community Trends Report".

6.3 **Okanagan Rail Trail Status Update** 165 - 167

To provide the elected leaders of the participating jurisdictions of the Okanagan Rail Trail Committee (ORTC) with a report on the status of the Okanagan Rail Trail and the activities of the Committee.

7. **Bylaws for Adoption (Non-Development Related)**

7.1 **BL11954 - Amendment No. 35 to Airport Fees Bylaw 7892** 168 - 171

To adopt Bylaw No. 11954 to amend the City of Kelowna Airport Fees Bylaw No. 7892.

8. **Mayor and Councillor Items**

9. **Termination**

City of Kelowna
Regular Council Meeting
Minutes

Date:	Monday, November 4, 2019
Location:	Council Chamber City Hall, 1435 Water Street
Members Present	Mayor Colin Basran, Councillors, Ryan Donn, Gail Given, Brad Sieben, Mohini Singh, Luke Stack and Loyal Wooldridge
Members Absent	Councillor Maxine DeHart and Charlie Hodge
Staff Present	Acting City Manager, Derek Edstrom; City Clerk, Stephen Fleming, Urban Planning & Development Policy Manager, Laura Bentley*; Planner Specialist, Jocelyn Black*; Airport Director, Sam Samaddar*; Senior Airport Finance & Corporate Services Manager, Shayne Drydal*; Parking Services Manager, Dave Duncan*; Real Estate Department Manager Johannes Saufferer*; Partnership Manager, Sandra Kochan*; Cultural Services Manager, Christine McWillis*; Recreation Technician, Melina Moran*; Community & Neighbourhood Services Manager, Mariko Siggers*; Divisional Director, Active Living & Culture, Jim Gabriel*; Divisional Director, Infrastructure, Alan Newcombe*; Integrated Transportation Department Manager, Rafael Villarreal*; Active Transportation Coordinator, Matt Worona*; Legislative Coordinator (Confidential), Arlene McClelland

(* Denotes partial attendance)

1. Call to Order

Mayor Basran called the meeting to order at 1:34 p.m.

I would like to acknowledge that we are gathered today on the traditional, ancestral, unceded territory of the syilx/Okanagan people.

Mayor Basran advised that the meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

2. Confirmation of Minutes

Moved By Councillor Wooldridge/Seconded By Councillor Donn

R1018/19/11/04 THAT the Minutes of the Regular Meetings of October 28, 2019 be confirmed as circulated.

Carried

DRAFT

3. Development Application Reports & Related Bylaws

3.1 Supplemental Report - Pandosy St. 1636-1652, Z19-0100 TA19-0013 - ALM888 Ventures Ltd., Inc. No. BC1089095

Moved By Councillor Wooldridge/Seconded By Councillor Given

R1019/19/11/04 THAT Council receives for information the Supplemental Report of the Legislative Services department dated November 4, 2019, regarding Rezoning Application No. Z19-0100 and Text Amendment No. TA19-0013, located at 1636-1652 Pandosy Street.

Carried

3.2 Pandosy St 1636-1652, BL11959 (TA19-0013) - ALM888 Ventures Ltd, Inc. No. BC1089095

Moved By Councillor Singh/Seconded By Councillor Wooldridge

R1020/19/11/04 THAT Bylaw No. 11959 be read a first time.

Carried

3.3 Pandosy St 1636-1652, BL11960 (Z19-0100) - ALM888 Ventures Ltd, Inc. No. BC1089095

Moved By Councillor Singh/Seconded By Councillor Wooldridge

R1021/19/11/04 THAT Bylaw No. 11960 be read a first time.

Carried

4. Bylaws for Adoption (Development Related)

4.1 Glenwood Ave 455, BL11914 (Z19-0038) - Mark Procknow

Moved By Councillor Wooldridge/Seconded By Councillor Singh

R1022/19/11/04 THAT Bylaw No. 11914 be adopted.

Carried

4.2 Summit Dr 1932, BL11944 (TA19-0014) - Summit Real Estate Holdings Ltd., Inc. No. BC1098449

Moved By Councillor Given/Seconded By Councillor Wooldridge

R1023/19/11/04 THAT Bylaw No. 11944 be adopted.

Carried

4.3 Summit Dr 1932, BL11945 (Z19-0070) - Summit Real Estate Holdings Ltd., Inc. No. BC1098449

Moved By Councillor Stack/Seconded By Councillor Sieben

R1024/19/11/04 THAT Bylaw No. 11945 be adopted.

Carried

DRAFT

4.4 Abbott St 1884, BL11947 (Z19-0065) - William James Feist and Treena June Harley

Moved By Councillor Sieben/Seconded By Councillor Stack

R1025/19/11/04 THAT Bylaw No. 11947 be adopted.

Carried

5. Non-Development Reports & Related Bylaws

5.1 Kelowna International Airport - Fees Bylaw Amendment

Staff:
- Displayed a PowerPoint Presentation summarizing the reasons for the increase in Airport fees and responded to questions from Council.

Moved By Councillor Sieben/Seconded By Councillor Stack

R1026/19/11/04 THAT Council receive for information the report of the Senior Airport Finance and Corporate Services Manager dated November 4, 2019;

AND THAT Council support Kelowna International Airport's Soaring Beyond 2.5 Million Passengers Airport Improvement Fee Capital Program;

AND FURTHER THAT Bylaw No. 11954 being Amendment No. 35 to the City of Kelowna Airport Fees Bylaw 7982 be advanced for reading consideration.

Carried

5.2 BL11954 - Amendment No. 35 to Airport Fees Bylaw 7892

Moved By Councillor Sieben/Seconded By Councillor Stack

R1027/19/11/04 THAT Bylaw No. 11954 be read a first, second and third time.

Carried

5.3 Chapman Parkade Secure Parking Area Budget Adjustment

Staff:
- Displayed a PowerPoint Presentation providing rationale to increase the budget for the Chapman Parkade secured parking project and responded to questions from Council.

Moved By Councillor Given/Seconded By Councillor Singh

R1028/19/11/04 THAT Council receives, for information, the report from the Manager, Parking Services dated November 4, 2019, with respect to a budget adjustment for the "Chapman Parkade – Secure Parking Area" project;

AND FURTHER THAT the 2019 Financial Plan be amended to increase the approved 2019 capital budget for "Chapman Parkade – Secure Parking Area" from $80,000 to $198,000, funded from the Downtown Parking Reserve to allow for completion of the revised project.

Carried

DRAFT

5.4 Complimentary Saturday Parking in December 2019

Staff:
- Provided comments regarding the complimentary Saturday parking in December for the downtown area.

Moved By Councillor Wooldridge/Seconded By Councillor Stack

R1029/19/11/04 THAT Council receives, for information, the report from the Manager, Parking Services dated November 4, 2019 with respect to Complimentary Saturday On-Street Parking in December 2019;

AND THAT Council approves waiving on-street parking fees in the Downtown area on the four (4) Saturdays in December 2019.

Carried

5.5 Community Amenity Space - 350 Doyle Avenue

Staff:
- Displayed a PowerPoint Presentation outlining the potential establishment of a creative hub in the community amenity space as part of the redevelopment of 350 Doyle Avenue and the request for grant funding.
- Responded to questions from Council.

Moved By Councillor Donn/Seconded By Councillor Given

R1030/19/11/04 THAT Council receives, for information, the report from the Partnership Manager dated November 4, 2019, regarding the potential establishment of a creative hub in a community amenity space to be built as part of the redevelopment of real property located at 350 Doyle Avenue in Kelowna;

AND THAT Council approves a request to the Department of Canadian Heritage Canada Cultural Spaces Fund for a grant of up to $35,000 representing 50 per cent of consulting costs to determine the feasibility of the establishment of a creative hub in the community amenity space at 350 Doyle Avenue in Kelowna, and the Partnership Manager is authorized to sign the grant application on behalf of the City;

AND FURTHER THAT Council directs staff to report back regarding the feasibility of the establishment of a creative hub in the community amenity space to be built as part of the redevelopment of real property located at 350 Doyle Avenue in Kelowna.

Carried

5.6 Civic Awards Council Policy

Staff:
- Displayed a PowerPoint Presentation outlining the changes to some Civic award categories and Council Policy changes and responded to questions from Council.

Moved By Councillor Donn/Seconded By Councillor Wooldridge

R1031/19/11/04 THAT Council receives, for information, the report from Active Living & Culture, dated November 4th, 2019, that introduces the Council Policy and Steering Committee Terms of Reference for the Annual Civic & Community Awards;

AND THAT Council endorse the Civic & Community Award Council Policy No. 382 as outlined in the report dated November 4th, 2019;

DRAFT

AND THAT Council endorse the Civic & Community Award Steering Committee Terms of Reference as outlined in the report dated November 4th, 2019;

AND FURTHER THAT Council rescind Policy No. 256 regarding Naming of Awards "In Memory" and include this information in Policy No. 382.

Carried

5.7 Framework for Bikeshare Regulation

Staff:
- Displayed a PowerPoint Presentation providing an update on the City's Bikeshare Permit Program and transport options and responded to questions from Council.

Moved By Councillor Donn/Seconded By Councillor Singh

R1032/19/11/04 THAT Council receives for information, the report from the Integrated Transportation Department, dated November 4, 2019 regarding a progress report on the Bikeshare Permit Program and contractual obligations.

Carried

Moved By Councillor Sieben/Seconded By Councillor Stack

R1033/19/11/04 THAT Council direct staff to report back on the implications on amending the Parks Bylaw to restrict e-scooters on the waterfront pathways from the City Park Pedestrian Tunnel to the Rotary Marshes.

Carried
Mayor Basran, Councillors Donn and Given - Opposed

6. Resolutions

6.1 Draft Resolution, re: Resolution to Hold Meeting at Another Location

Moved By Councillor Singh/Seconded By Councillor Wooldridge

R1034/19/11/04 THAT the Regular PM Council Meeting scheduled for Monday, November 18, 2019 be at another location other than City Hall, being the University of British Columbia Okanagan Campus, University Theatre (ADM 026) Administration building,1138 Alumni Avenue, Kelowna, B.C., starting at 6:00 p.m.

Carried

7. Bylaws for Adoption (Non-Development Related)

7.1 BL11951 - Amendment No. 35 to Traffic Bylaw No. 8120

Moved By Councillor Stack/Seconded By Councillor Donn

R1035/19/11/04 THAT Bylaw No. 11951 be adopted.

Carried

DRAFT

8. Mayor and Councillor Items

Mayor Basran
- Provided condolences to the family of Captain Joe Kolar on his recent passing and acknowledged his long career with the Kelowna Fire Department.
- Recognized next Monday is Remembrance Day and encouraged residents to attend one of the two Ceremonies in City Park and the Rutland Lions Park.

Councillor Wooldridge
- Spoke to their attendance at the Offleashed Gala benefiting Kelowna's SPCA.
- Spoke to the Okanagan College's fundraising campaign that began last week for a state of the art new Health Sciences Centre.

Councillor Given
- Tourism Kelowna is welcoming the International Indigenous Tourism Conference from November 12 to 14th.

9. Termination

This meeting was declared terminated at 3:59 p.m.

Mayor Basran

City Clerk

/acm

DRAFT

Report to Council

Date: November 18, 2019

To: City Manager

From: Doug Nicholas, Sport & Event Services Manager

Subject: Final Report – Kelowna 2019 55+ BC Games

Department: Sport & Event Services

Recommendation:

THAT Council receives, for information, the report from the Sport & Event Services Manager dated November 18, 2019, regarding the Kelowna 2019 55+ BC Games and the dissolution of the local Games Society.

Purpose:

To provide Council with information on the results and outcomes of the Kelowna 2019 55+ BC Games

Background:

Kelowna recently hosted the 2019 55+ BC Games from September 10-14, 2019. Over 4,000 participants from across the Province, supported by over 1,500 volunteers, came together in Kelowna to take part in the festivities. Led by the local Board of Directors, the Kelowna Games were a great success that provided an amazing Active for Life opportunity to our community, a key objective of our recently adopted Community Sport Plan. As the largest 55+ BC Games ever held in the province, the economic impact was significant and created an impressive community legacy of sport in Kelowna.

The board is now working on the final responsibilities of the Society before dissolution, including: auditing the financial statements, reporting to the BC Senior Games Society and determining the usage of the legacy funds. David Graham, President of the Kelowna 2019 BC Games Society, will provide a summary of the results and outcomes from the Games.

Internal Circulation:
Jim Gabriel, Divisional Director, Active Living & Culture
Lisa Ruether, Communications Advisor

Financial/Budgetary Considerations:
On May 25, 2015, Council committed to the Games a cash contribution of $60,000 and in-kind support of services and facilities with a deemed value of $55,000

Considerations not applicable to this report:
Legal/Statutory Authority:
Legal/Statutory Procedural Requirements:
Existing Policy:
Personnel Implications:
External Agency/Public Comments:
Communications Comments:
Alternate Recommendation:

Submitted by:
Doug Nicholas, Sport & Event Services Manager

Approved for inclusion:
Jim Gabriel, Divisional Director, Active Living & Culture

Attachments:
Kelowna 55+ BC Games Report – November 2019

cc: Jim Gabriel, Divisional Director, Active Living & Culture

55+ BC Games

September 10 – 14

Successful Games

- 31 Sports/Games
- 4200 Participants
- 1500 Volunteers
- Ceremonies and events

Games Support

- City of Kelowna
- 55+ BC Games Society
- Board/Volunteers
- Community

City of Kelowna

55+ BC Games
For Life, Sport and Friends

Community Legacy

- Economic impact
- Hosting capacity
- Community pride/accomplishment
- Active living, 55+ opportunities

Financial legacy contribution of $60,000

Next steps:

- Audit
- Legacy
- Dissolve

REPORT TO COUNCIL

Date:	November 18, 2019		
To:	Council		
From:	City Manager		
Department:	Development Planning (AC)		
Application:	Z19-0037	**Owner:**	Gursher Pannu & Harsheen Pannu
Address:	757 Harvey Ave	**Applicant:**	New Town Services Inc. (Jesse Alexander)
Subject:	Rezoning Application		
Existing OCP Designation:	MRM – Multiple Unit Residential (Medium Density)		
Existing Zone:	RU6 – Two Dwelling Housing		
Proposed Zone:	RM4 – Transitional Low Density Multiple Housing		

1.0 Recommendation

That Rezoning Application No. Z19-0037 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 1, Block 1, District Lot 138, ODYD, Plan 7117, located at 757 Harvey Ave, Kelowna, BC from the RU6 – Two Dwelling Housing zone to the RM4 – Transitional Low Density Multiple Housing zone, **NOT** be considered by Council.

2.0 Purpose

To consider a Staff recommendation to NOT rezone the subject property from the RU6 – Two Dwelling Housing zone to the RM4 – Transitional Low Density Multiple Housing zone.

3.0 Development Planning

The Official Community Plan (OCP) designates the subject property as MRM – Multiple Unit Residential (Medium Density) with the vision of achieving larger apartment style buildings under the RM5 zone. This land use vision is evident along the Harvey Ave corridor in this area, specifically with the Murano, Cambridge House, NOW Canada, Dorchester, and Central Green buildings. Council-directed growth expectations fit the vision of creating significant density within large apartment buildings along the Harvey Ave / Highway 97 corridor because of its proximity to downtown, commercial services, employment, transit, cycling routes, parks, and schools.

The proposed application for the subject property undermines this vision as the applicant is proposing to use the less intensive RM4 zone on the proposed lot. While the RM4 zone is technically aligned with the MRM future land use designation, it is not being utilized for the intended purpose with this application. The purpose of the RM4 zone is:

> "to provide a zone primarily for low-rise low-density apartment housing with urban services as a transition between low and medium density developments."

Therefore, the RM4 zone should be used with smaller urban infill apartment situations (3 storeys) within transitional areas between MRM and MRL designations. This project has a Floor Area Ratio (FAR) of 0.68 which is less than the RM3 zone maximum of 0.75. This means the proposal could have fit within the RM3 zone but the RM3 zone does not fit the intended land use for the area as MRM style developments and would have required an OCP amendment. Staff are recommending against the rezoning proposal in order to facilitate lot consolidation and larger developments along the Harvey Ave corridor. Due to the age of the buildings between Harvey Ave and Saucier Ave, Staff believe lot consolidation is viable and would recommend against smaller single lot developments with lower density.

4.0 Proposal

4.1 Project Description

The applicant has applied for a rezoning and development permit application to facilitate an 8-unit ground-oriented 2-bedroom townhouse development. The building is 3 storeys in height and is oriented towards Harvey Ave, with vehicular access limited to and from the rear lane only. The existing driveway from the highway would be decommissioned and removed. Parking is provided within garages (2 stalls/unit) with a visitor spot allocated as a surface stall. Two of the units have a double garage, with the 6 remaining dwelling units configured in a tandem parking format. Staff are currently tracking one variance to increase overall site coverage and would provide comprehensive comments on the form & character within the Development Permit report should Council support the rezoning application.

4.2 Site Context

The subject property is located just east of the Downtown Urban Centre fronting on Harvey Avenue between Richter Street and Ethel Street. The site is currently zoned RU6, has a future land use designation of MRM – Multiple Unit Residential (Medium Density), and the existing single-family dwelling was built in the 1940s. The surrounding neighbourhood has a combination of older single family residential, multiple dwelling housing, and commercial uses.

4.3 Public Notification

The applicant has met Council Policy No. 367 (Development Notification Policy) by mailing notices to all properties within a 50 metre radius.

Subject Property Map: 757 Harvey Ave

5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Chapter 1: Introduction

Goal 1: Contain Urban Growth. Reduce greenfield urban sprawl and focus growth in compact, connected and mixed-use (residential and commercial) urban and village centres.

Goal 2: Address Housing Needs of All Residents. Address housing needs of all residents by working towards an adequate supply of a variety of housing.

Chapter 5: Development Process

Objective 5.3 Focus development to designated growth areas.

> *Policy .2 Compact Urban Form.* Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Objective 5.22 Ensure context sensitive housing development.

> *Policy .11 Housing Mix.* Support a greater mix of housing unit size, form and tenure in new multi-unit residential and mixed use developments.
>
> *Policy .13 Family Housing.* Support housing alternatives for families when single detached housing is too costly, including features that are important to families such as: outdoor space, direct access to grade, workshop space, larger units, safe design, and neighborhood characteristics (e.g.: location and amenities).

6.0 Technical Comments

6.1 Development Engineering Department
See attached memorandum dated Jan 10, 2019.

6.2 Ministry of Transportation
No direct access to any portion of the subject property via Highway 97 shall be maintained (it is noted that the site plan is proposing all access via the rear lane). Physical removal of the existing drop curb/driveway letdown along subject property's frontage on Highway 97 must be completed.

7.0 Application Chronology

Date of Application Received:	December 22, 2018
Date Public Consultation Completed:	July 2019

8.0 Alternate Recommendation

THAT Rezoning Application No. Z19-0023 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 1, Block 1, District Lot 138, ODYD, Plan 7117, located at 757 Harvey Ave, Kelowna, BC from the RU6 – Two Dwelling Housing zone to the RM4 – Transitional Low Density Multiple Housing zone, be considered by Council;

AND THAT the Zone Amending Bylaw be forwarded to a Public Hearing for further consideration.

AND FURTHER THAT final adoption of the Zone Amending Bylaw be subsequent to the outstanding conditions of approval as set out in Attachment 'A' attached to the Report from the Development Planning Department dated November 18, 2018.

Report prepared by:	Adam Cseke, Planner Specialist
Reviewed by:	Laura Bentley, Urban Planning & Development Policy Manager
Reviewed by:	Terry Barton, Development Planning Department Manager

Attachments:

Attachment 'A' - Development Engineering Memo dated Jan 10th 2019
Attachment 'B' – Applicant's Rationale & Initial Architectural Drawing Package

CITY OF KELOWNA

MEMORANDUM

ATTACHMENT A
This forms part of application
Z19-0037
Planner Initials AC
City of Kelowna
DEVELOPMENT PLANNING

Date: January 10, 2019 (*Revised Oct.-24-19*)

File No.: Z19-0037

To: Community Planning (AC)

From: Development Engineering Manager (JK)

Subject: 757 Harvey Ave RU6 to RM4

The Development Engineering Department has the following comments and requirements associated with this rezoning application. The road and utility upgrading requirements outlined in this report will be a requirement of this development. The Development Engineering Technologist for this project is Jason Angus

1. <u>Domestic Water and Fire Protection</u>

a) Property 757 Harvey Ave is currently serviced with a 19mm-diameter water service. The developer's consulting mechanical engineer will determine the domestic, fire protection requirements of this proposed development and establish hydrant requirements and service needs. The applicant will arrange for the removal and disconnection of the existing services and the installation of one new larger service at the applicants cost.

b) A water meter is mandatory for this development and must be installed inside the building on the water service inlet as required by the City Plumbing Regulation and Water Regulation bylaws. The developer or building contractor must purchase the meter from the City at the time of application for a building permit from the Inspection Services Department, and prepare the meter setter at his cost. Boulevard landscaping, complete with underground irrigation system, must be integrated with the on-site irrigation system

c) The developer must obtain the necessary permits and have all existing utility services disconnected prior to removing or demolishing the existing structures. The City of Kelowna water meter contractor must salvage existing water meters, prior to building demolition. If water meters are not salvaged, the developer will be invoiced for the meters.

2. <u>Sanitary Sewer</u>

Our records indicate that these properties are currently serviced with a 100mm-diameter sanitary sewer service. The applicant's consulting mechanical engineer will determine the requirements of the proposed development and establish the service needs. Only one service will be permitted for this development. If required, the applicant will arrange for the removal and disconnection of the existing service and the installation of one new larger service at the applicants cost.

3. <u>Storm Drainage</u>

The developer must engage a consulting civil engineer to provide a storm water management plan for this site which meets the requirements of the City Subdivision

Development and Servicing Bylaw 7900. The storm water management plan must also include provision of lot grading plans, minimum basement elevations (MBE), if applicable, and provision of a storm drainage service and recommendations for onsite drainage containment and disposal systems

4. Road Improvements

(a) Harvey Ave has been upgraded to a urban standard however, the existing driveway let-down will need to be removed and new sidewalk and BLVD will be constructed. Due to the likelihood of larger services for this development, the entire sidewalk fronting this development will need to be re-constructed.

(b) The Laneway fronting this development will need to be constructed to a SS-C7 standard.

4. Road Dedication and Subdivision Requirements

(a) Grant Statutory Rights of Way if required for utility services.

(b) If any road dedication or closure affects lands encumbered by a Utility right-of-way (such as Hydro, TELUS, Gas, etc.) please obtain the approval of the utility. Any works required by the utility as a consequence of the road dedication or closure must be incorporated in the construction drawings submitted to the City's Development Manager.

(c) Laneway dedication is needed for this development. Due to safety reasons and access issue to this property, a minimum of *3.0m* will need to be dedicated as laneway right-of-way.

5. Development Permit and Site Related Issues

a) Direct the roof drains into on-site rock pits or splash pads.

b) The vehicle access to this site must be from a fully constructed 6.0m laneway. MOTI will not allow access to Harvey for this development.

6. Electric Power and Telecommunication Services

The electrical and telecommunication services to this building must be installed in an underground duct system, and the building must be connected by an underground service. It is the developer's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for these services, which would be at the applicant's cost.

7. Design and Construction

(a) Design, construction supervision and inspection of all off-site civil works and site servicing must be performed by a Consulting Civil Engineer and all such work is subject to the approval of the City Engineer. Drawings must conform to City standards and requirements.

(b) Engineering drawing submissions are to be in accordance with the City's "Engineering Drawing Submission Requirements" Policy. Please note the number of sets and drawings required for submissions.

(c) Quality Control and Assurance Plans must be provided in accordance with the Subdivision, Development & Servicing Bylaw No. 7900 (refer to Part 5 and Schedule 3).

ATTACHMENT A
This forms part of application
Z19-0037
Planner Initials AC
City of Kelowna
DEVELOPMENT PLANNING

(d) A "Consulting Engineering Confirmation Letter" (City document 'C') must be completed prior to submission of any designs.

(e) Before any construction related to the requirements of this subdivision application commences, design drawings prepared by a professional engineer must be submitted to the City's Works & Utilities Department. The design drawings must first be "Issued for Construction" by the City Engineer. On examination of design drawings, it may be determined that rights-of-way are required for current or future needs.

8. Servicing Agreement for Works and Services

(a) A Servicing Agreement is required for all works and services on City lands in accordance with the Subdivision, Development & Servicing Bylaw No. 7900. The applicant's Engineer, prior to preparation of Servicing Agreements, must provide adequate drawings and estimates for the required works. The Servicing Agreement must be in the form as described in Schedule 2 of the bylaw.

(b) Part 3, "Security for Works and Services", of the Bylaw, describes the Bonding and Insurance requirements of the Owner. The liability limit is not to be less than $5,000,000 and the City is to be named on the insurance policy as an additional insured.

9. Administration Charge

An administration charge will be assessed for processing of this application, review and approval of engineering designs and construction inspection. The administration charge is calculated as (3.5% of Total Off-Site Construction Cost plus GST).

9. Survey, Monument and Iron Pins

If any legal survey monuments or property iron pins are removed or disturbed during construction, the developer will be invoiced a flat sum of $1,200.00 per incident to cover the cost of replacement and legal registration. Security bonding will not be released until restitution is made.

10. Geotechnical Report

As a requirement of this application the owner must provide a geotechnical report prepared by a Professional Engineer qualified in the field of hydro-geotechnical survey to address the following:

(a) Area ground water characteristics.

(b) Site suitability for development, unstable soils, etc.

(c) Drill and / or excavate test holes on the site and install pisometers if necessary. Log test hole data to identify soil characteristics, identify areas of fill if any. Identify unacceptable fill material, analyse soil sulphate content, identify unsuitable underlying soils such as peat, etc. and make recommendations for remediation if necessary.

(d) List extraordinary requirements that may be required to accommodate construction of roads and underground utilities as well as building foundation designs.

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City of Kelowna
DEVELOPMENT PLANNING

(e) Additional geotechnical survey may be necessary for building foundations, etc.

11. <u>Bonding and Levy Summary</u>

(a) Bonding

(i) Offsite improvements **TBD**

James Kay, P. Eng.
Development Engineering Manager
JA

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DEVELOPMENT PLANNING

NEW TOWN

Proposal for Rezoning, Development Permit, and Development Variance Permit

757 Harvey Avenue, Kelowna BC

Introduction

This application is for rezoning, DP, and DVP to accommodate an 8-unit infill townhome enclave located at 757 Harvey Avenue, Kelowna BC.

Site Context

The subject site consists of a single lot that is currently zoned RU6. The property is within the Medium Density Residential Land Use Designation as prescribed by the City of Kelowna Official Community Plan. The lot is currently occupied by a single-family home that was built in the 1940's.

Site Location

Existing Home and Street Interface

ATTACHMENT B
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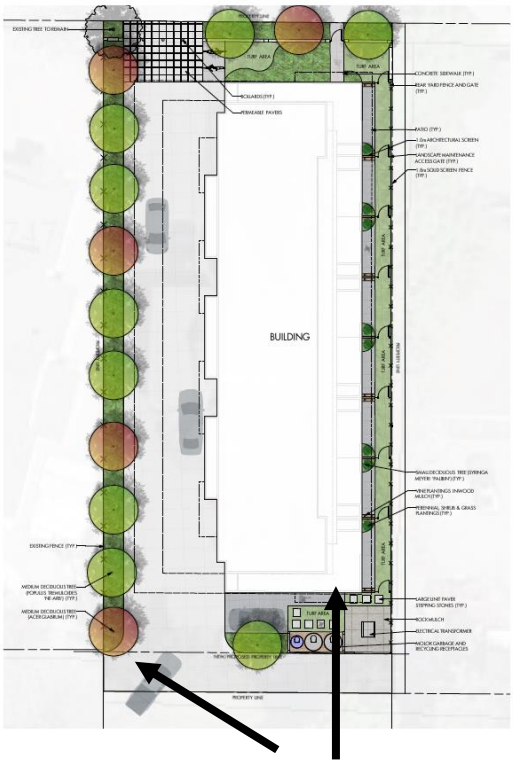
Overview

This application for Development Permit is for a single building consisting of 8 ground-oriented 2-bedroom townhome units. The building itself is 3 storeys (9.73m) in height and is oriented towards Harvey Ave. The existing driveway from the highway will be only accessible for emergency vehicles. There will be bollards placed at the existing driveway to prevent regular traffic from entering and exiting the proposed development from Harvey Ave. Access to the proposed development will be from the rear lane. Parking is provided within garages (2 stalls/unit) with a visitor spot allocated as a surface stall.

Existing home and highway access to be removed



Proposed building and access through dedicated rear lane

Rezoning to RM4

The subject site is located near the corner of Harvey Ave and Richter St. The proposed project requires a rezoning to *RM4 – Transitional Low Density Housing* from *RU6 – Two Dwelling Housing*. Rezoning this property to allow for higher density is consistent with the OCP.

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Form and Character

Simple yet strong geometry of this modern 8 unit development offers 2 bedroom units on two types of floorplans nested within compact efficient building.

Both end units provide double garage on first level with generous entry interacting with Harvey Ave on the north side or facing the treed yard on the south side. The second level is an open-plan concept housing the kitchen, dining and living spaces with a full bathroom and secondary bedroom. The third level has a generous master opening into a large corner sun deck with large walk-in closet and en suite.

The inner units with tandem garages on the main level have as well open-plan concept kitchen, dining and living spaces on second level and tucked away secondary bedroom and bathroom. The third level offers large master with walk-in closet and en-suite leading to a large exterior sun deck.

Large focus was given to private open space belonging to the residents. All units come with a ground floor back yard with architectural and landscape screening to assure privacy and all units are equipped with large sun decks facing east catching the morning sun and making the most of the Okanagan climate.

Durable materials like textured Ceraclad Cast Stripe Panel or custom color Hardi panels were selected for this centrally located development. The subtle color combination of grey, white and black will be accented by cultured stone veneer in color shades reminiscent of arid earth tones.

Site Coverage – Variance

A single variance is requested to vary site coverage of buildings, parking, and driveways from 60% to 69.1%. Although the site coverage of the building alone is within the RM4 allowance (37.1%), the combined site coverage of building/parking/driveways is above the 60% limit. To help mitigate this coverage, robust landscape plantings will be provided with a total of 16 medium sized deciduous trees. Various shrubs, perennials, grasses and vines will round out the planting treatment for the remainder of the ground level and rooftop patio level.

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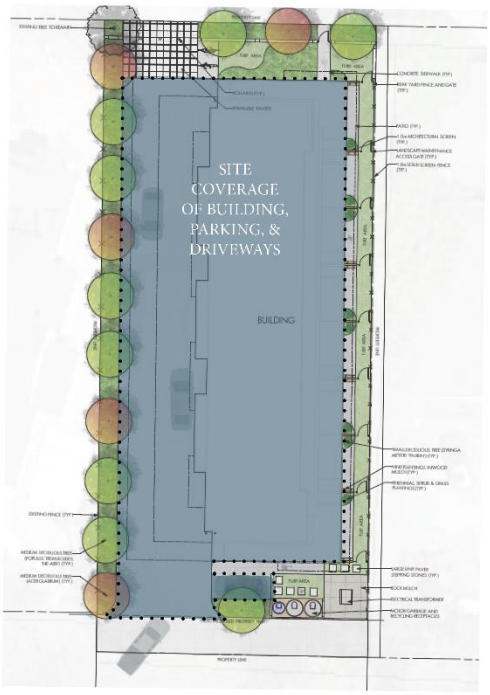

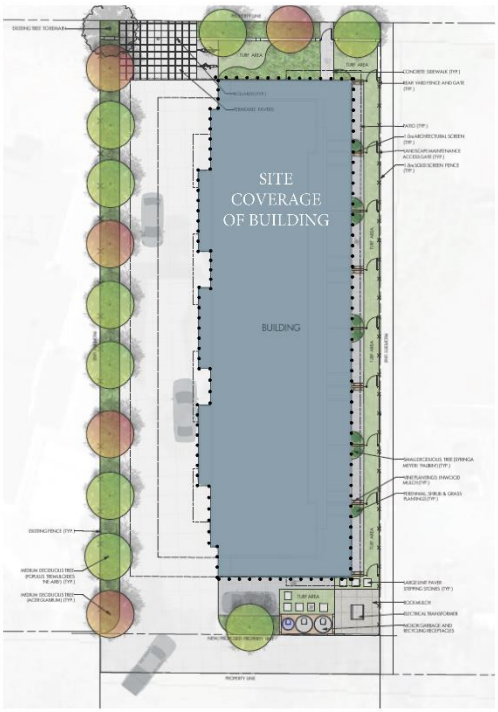

Rocky Mountain Maple
(7 Provided)

Prairie Gold Aspen
(9 Provided)

Dwarf Korean Lilac
(8 Provided)

Summary

The proposed development is consistent with the City's goal for increasing density in existing neighborhoods and complies with the Official Community Plan. The applicant kindly requests support from Staff and Council for this infill housing project.

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DEVELOPMENT PLANNING

HARVEY TOWNHOUSES

RE-ISSUED FOR DEVELOPEMENT PERMIT, OCTOBER 22, 2019

ATTACHMENT B
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Z19-0037
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City of Kelowna
DEVELOPMENT PLANNING

ARCHITECTURAL DRAWING LIST:

NEW TOWN ARCHITECTURE & ENGINNERING INC.
1464 St. Paul Street Kelowna, B.C., V1Y 2E6
Keith Funk, MCIP, PIBC, RPP
ph: 250 860 8185, fax: 250 860 0985
keithf@newtownservices.net

NEW TOWN ARCHITECTURE & ENGINNERING INC.
1464 St. Paul Street Kelowna, B.C., V1Y 2E6
Lenka Algerova, Building Design Technologist
ph: 250 860 8185, fax: 250 860 0985
lenka@newtownservices.net

A0.00	COVER SHEET & DRAWING LIST
A1.00	ZONING & CODE ANALYSIS
A1.01	RENDERINGS
A1.02	RENDERINGS
A2.00	SITE PLAN
A3.01	LEVEL 1
A3.02	LEVEL 2 & 3
A4.00	MATERIALS
A4.01	BUILDING ELEVATIONS
A5.01	BUILDING SECTIONS

CIVIL DRAWING LIST:

NEW TOWN ARCHITECTURE & ENGINNERING INC.
1464 St. Paul Street Kelowna, B.C., V1Y 2E6
Jacob Paul, EIT
ph: 250 860 8185, fax: 250 860 0985
jacob@newtownservices.net

STORMWATER MANAGEMENT STRATEGY

LANDSCAPE DRAWING LIST:

OUTLAND DESIGN LANDSCAPE ARCHITECTURE
303-590 KLO Road Kelowna BC V1Y 7S2
Fiona Barton MBCSLA CSLA
ph: 250.868.9270
fiona@outlanddesign.ca

L1/2	CONCEPTUAL LANDSCAPE PLAN
L2/2	WATER CONSERVATION / IRRIGATION PLAN

NOT FOR CONSTRUCTION

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA

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Report all errors and omissions to the Architect.

Seal

Revision

No	Date	Description
1	18-12-18	ISSUED FOR DP
2	19-06-21	RE-ISSUED FOR DP
3	19-09-30	RE-ISSUED FOR DP
4	19-10-22	RE-ISSUED FOR DP

NEW TOWN SERVICES
250 · 860 · 8185
www.newtownservices.net

project title
HARVEY TOWNHOUSES

project address
757 HARVEY AVE, KELOWNA, BC

project no. 4074

file no.

drawing title
COVER SHEET & DRAWING LIST

designed L. A.
scale
drawn L. A.
checked K.F.
drawing no. **A0.00**
plotted 22/10/2019 2:48:56 PM

HARVEY TOWNHOUSES

ADDRESS:

757 Harvey Avenue, Kelowna, BC, V1Y 6E2

LEGAL DESCRIPTION:

PLAN KAP7117 LOT 1 BLOCK 1 DISTRICT LOT 138

GRADES:

EXISTING AVERAGE - FLAT | PROPOSED AVERAGE - FLAT

NUMBER OF BUILDINGS:

1 BUILDING (8 UNITS)

ZONING ANALYSIS:

EXISTING	PROPOSED
ZONING:	
RU6	RM4

RM4 ZONING REQUIREMENTS:

REQUIRED	PROPOSED

SITE DETAILS

SITE AREA (m^2)	
900 m^2	1112.7m^2
SITE WIDTH (m)	
30.0 m	21.79 m
SITE DEPTH (m)	
30.0 m	50.78 m
MAX SITE COVERAGE FOR BUILDINGS (%):	
50%	37.6%
MAX SITE COVERAGE INCL. PARKING & DRIVEWAYS (%):	
60%	72.7% (VARIANCE REQ'D)

DEVELOPMENT REGULATIONS

TOTAL NUMBER & TYPES OF UNITS:	
8 UNITS	8x 2BR UNITS
FLOOR AREA (GFA/NFA):	
MAX 934.7m^2 BASED ON 0.84 FAR	L1 GFA FOR SITE COVERAGE = 412.8 m^2 NFA FOR FAR = 759 m^2
FLOOR AREA RATIO (FAR):	
0.65 + 0.19 FOR COVERED PARKING SPACES	0.68
BUILDING HEIGHT:	
3 STOREYS OR 13m	3 STOREYS / 9.73m
SETBACKS:	
FRONT NORTH: 4.5m (6.0m over 2 storeys)	4.5m (6.0m)
SIDE EAST: 2.3m (4.5m over 2 storeys)	7.7m
SIDE WEST: 2.3m (4.5m over 2 storeys)	2.3m (4.5m)
REAR SOUTH: 7.5m (9.0m over 2 storeys)	7.5m (9.0m)
NUMBER OF PARKING STALLS / LOADING SPACES:	
ROW STACKED HOUSING - 1.5 / 2BR / 8 UNITS 12 STALLS + 1 VISITOR PARKING STALL / 8 UNITS	17 STALLS
13 PARKING SPACES	
PRIVATE OPEN SPACE AREA:	
25.0 m^2 per home w/ more than 1 bedroom x 8 = 200 m^2	ROOF DECKS 138.8 m^2 OPEN SPACE 300.2 m^2

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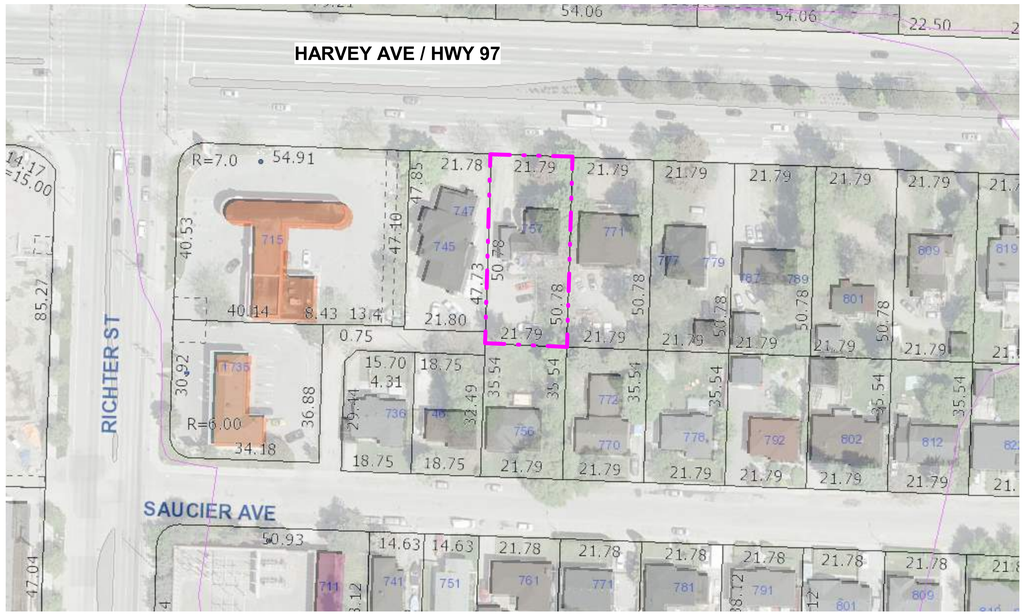

KEY PLAN

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project title
HARVEY TOWNHOUSES

project address
757 HARVEY AVE, KELOWNA, BC

project no. 4074
file no.

drawing title
ZONING & CODE ANALYSIS

designed L.A. | scale
drawn L.A.
checked K.F.
drawing no. **A1.00**
plotted 22/10/2019 2:48:56 PM

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project address
757 HARVEY AVE,
KELOWNA, BC

project no. 4074

file no.

drawing title
RENDERINGS

designed L. A.
scale
drawn L. A.
checked K.F.
drawing no. A1.01
plotted 22/10/2019 2:48:56 PM

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project address
757 HARVEY AVE,
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project no. 4074

file no.

drawing title
RENDERINGS

designed L. A.
scale
drawn L. A.
checked K.F.
drawing no. A1.02
plotted 22/10/2019 2:48:57 PM

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project title
HARVEY TOWNHOUSES

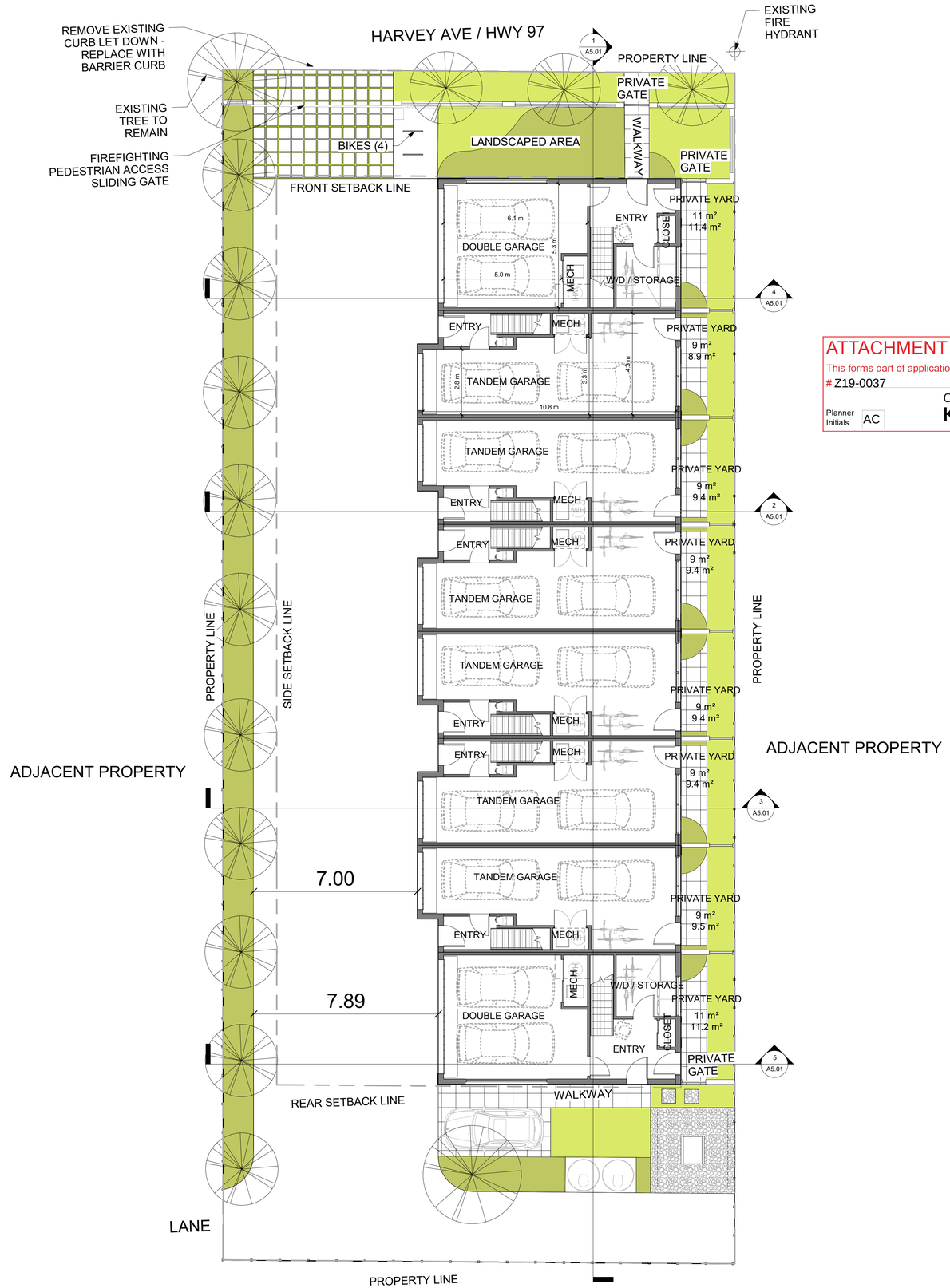
project address
757 HARVEY AVE, KELOWNA, BC

project no. 4074

file no.

drawing title
SITE PLAN

designed L. A.
scale 1 : 100
drawn L. A.
checked K. F.
drawing no. **A2.00**
plotted 22/10/2019 2:49:01 PM



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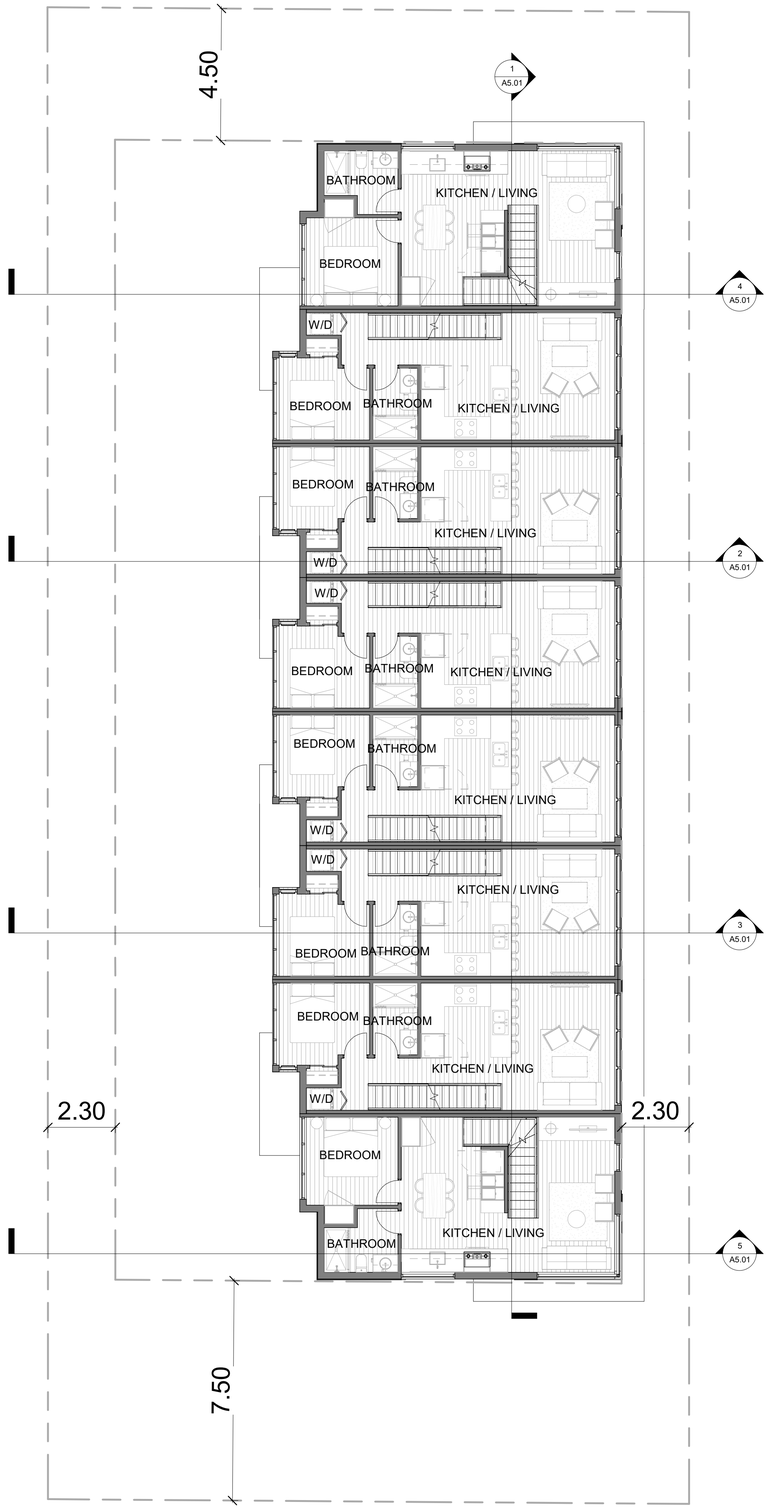
project address
757 HARVEY AVE, KELOWNA, BC

project no. 4074

file no.

drawing title
LEVEL 1

designed L. A.
scale 1/8" = 1'-0"
drawn L. A.
checked K. F.
drawing no. A3.01
plotted 22/10/2019 2:49:04 PM



1 LEVEL 2
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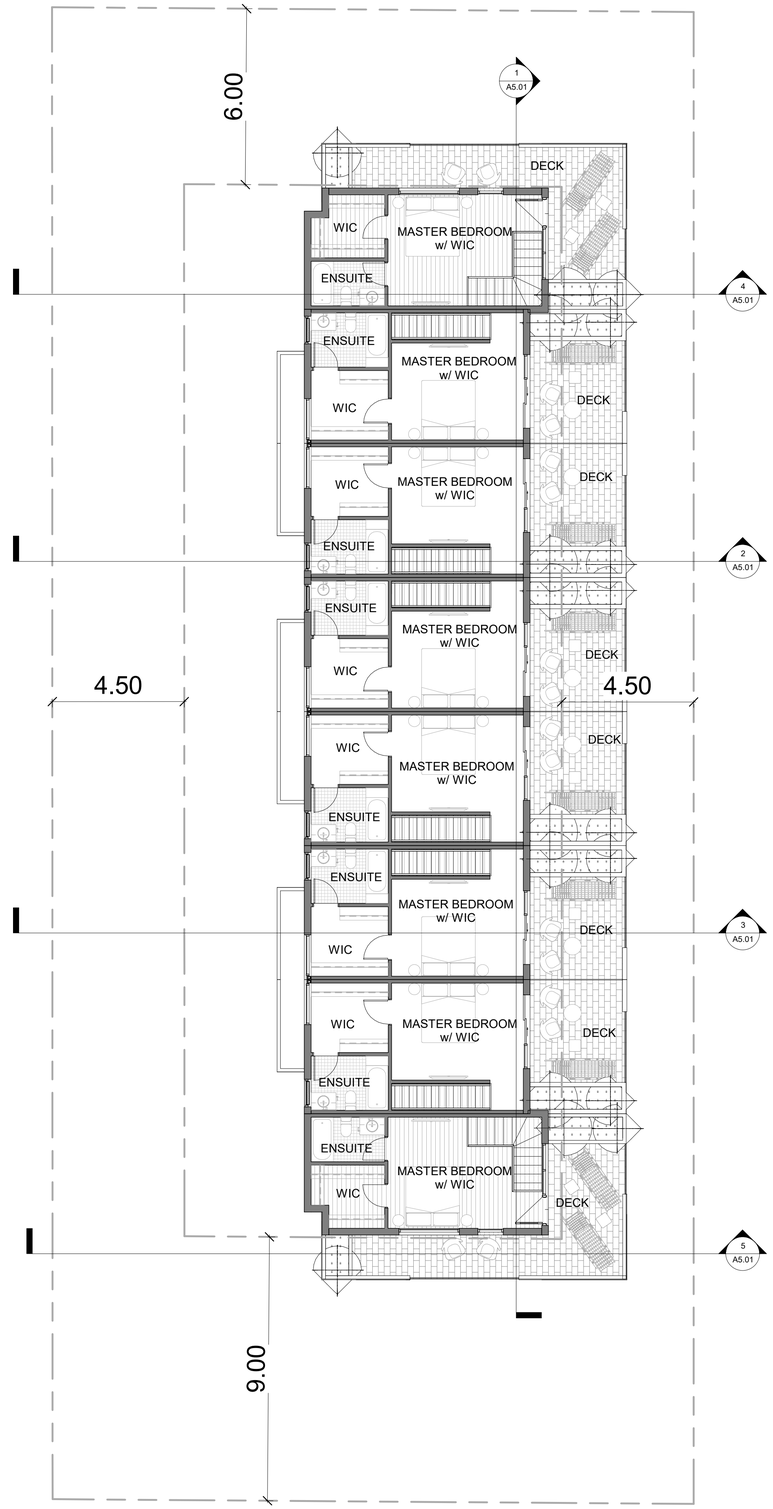

2 LEVEL 3
1/8" = 1'-0"

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project address
757 HARVEY AVE, KELOWNA, BC

project no. 4074

file no.

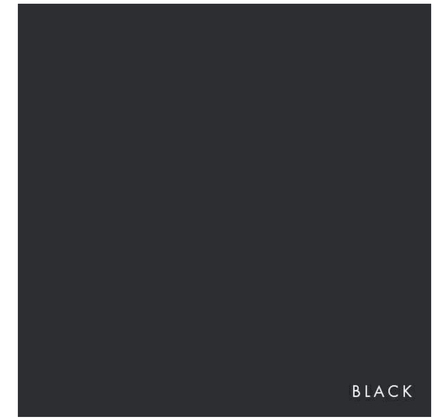
drawing title
LEVEL 2 & 3

designed L. A.
scale 1/8" = 1'-0"
drawn L. A.
checked K.F.
drawing no. A3.02
plotted 22/10/2019 2:49:11 PM

HARDIE REVEAL PANEL (SMOOTH) W/ PREFIN. ALU BREAK SHAPE (COLOR TO MATCH) & PAINTED EXP. FASTENERS; **COLOR CAVIAR SW 6990 (#1)**

CULTURED STONE VENEER **(#3)**

PRE-FINISHED ALU PANEL;
COLOR BLACK (#5)

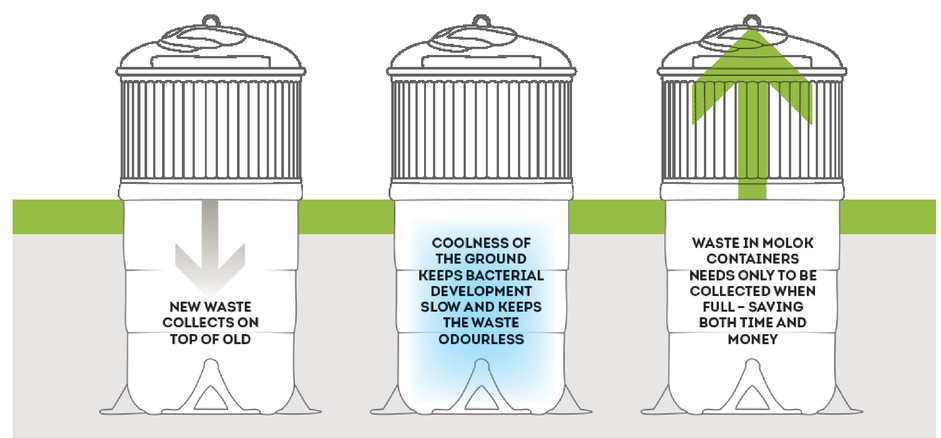


HARDIE REVEAL PANEL (SMOOTH) W/ EASY TRIM REVEAL & PAINTED EXP. FASTENERS; **COLOR PURE WHITE SW 7005 (#2)**

CERACLAD CAST STRIPE PANEL;
COLOR CHARCOAL NH31215U (#4)

WINDOWS - CLEAR VISION GLASS
W/ BLACK FRAME **(#6)**

MOLOK GARBAGE CONTAINERS (#14)



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Z19-0037
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DEVELOPMENT PLANNING
Planner Initials AC

project title
HARVEY TOWNHOUSES

project address
757 HARVEY AVE, KELOWNA, BC

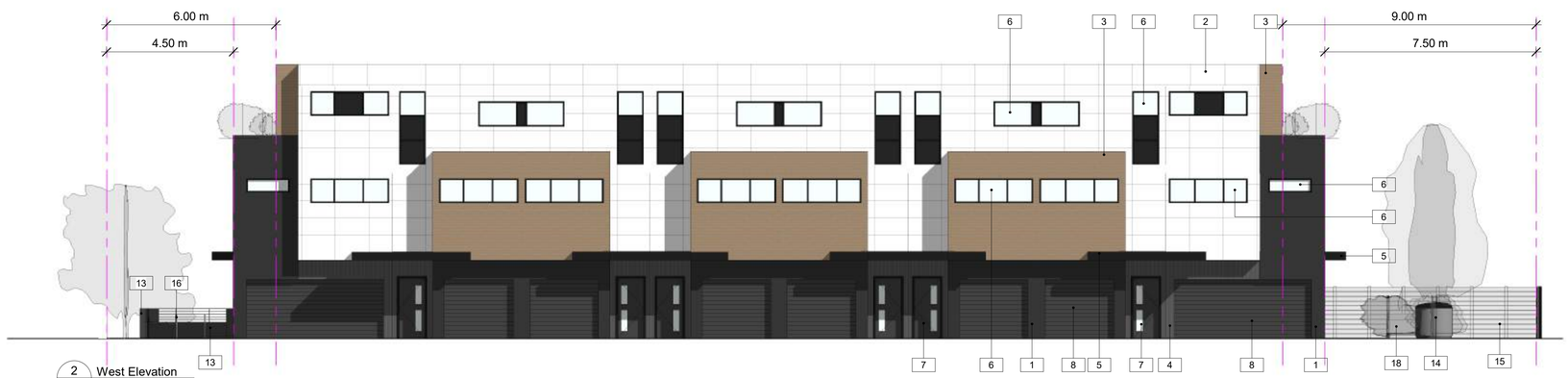
project no. 4074

file no.

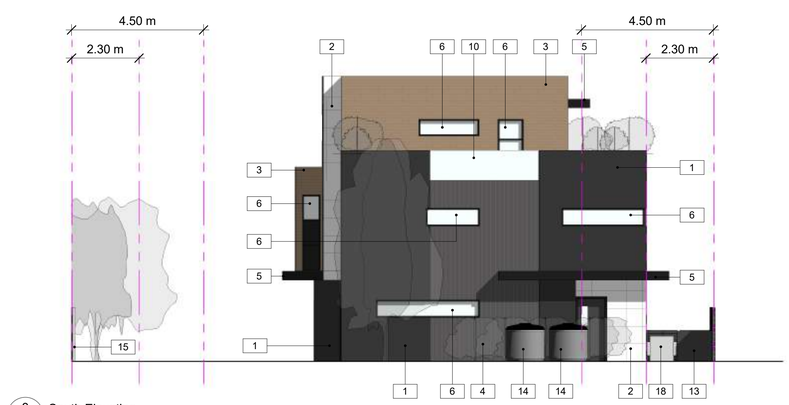
drawing title
MATERIALS

designed L. A.
scale
drawn L. A.
checked K. F.
drawing no. **A4.00**
plotted 22/10/2019 2:49:12 PM

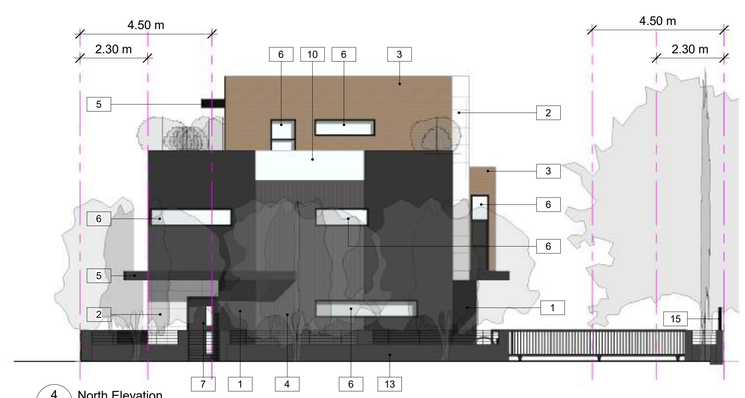
1 East Elevation
1/8" = 1'-0"



2 West Elevation
1/8" = 1'-0"



3 South Elevation
1/8" = 1'-0"



4 North Elevation
1/8" = 1'-0"

MATERIAL KEYNOTE LEGEND

1. HARDIE REVEAL PANEL (SMOOTH) W/ PREFIN. ALU BREAK SHAPE (COLOR TO MATCH) & PAINTED EXP. FASTENERS; **COLOR CAVIAR SW 6990**
2. HARDIE REVEAL PANEL (SMOOTH) W/ EASY TRIM REVEAL & PAINTED EXP. FASTENERS; **COLOR PURE WHITE SW 7005**
3. CULTURED STONE VENEER
4. CERACLAD CAST STRIPE PANEL; **COLOR CHARCOAL NH31215U**
5. PRE-FINISHED ALU FLASHING (COLOR BLACK)
6. WINDOW W/ BLACK FRAME (W/ SPANDREL PANEL AS PER ELEVATIONS)
7. EXTERIOR DOOR W/ BLACK FRAME
8. OVERHEAD GARAGE DOOR
9. SLIDING PATIO DOOR W/ BLACK FRAME
10. GLASS GUARDRAIL
11. GLAZED SWING PATIO DOOR
12. EXTERIOR BACKYARD DOOR
13. ARCHITECTURAL PATIO PRIVACY WALL W/ GATES
14. MOLOK GARBAGE CONTAINERS
15. 1.8m SOLID SCREEN FENCE
16. BIKE RACKS
17. SLIDING GATE
18. TRANSFORMER

NOT FOR CONSTRUCTION

ALL CONTRACTORS ARE REQUIRED TO PERFORM THEIR WORK AND SUPPLY THEIR PRODUCTS IN COMPLIANCE WITH ALL BUILDING CODES AND LAWS OF THE PROVINCE OF BRITISH COLUMBIA

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This drawing must not be scaled

Verify all dimensions and datums prior to commencement of work.

Report all errors and omissions to the Architect.

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Seal

Revision

No	Date	Description
1	18-12-18	ISSUED FOR DP
2	19-06-21	RE-ISSUED FOR DP
3	19-09-30	RE-ISSUED FOR DP
4	19-10-22	RE-ISSUED FOR DP

ATTACHMENT B
This forms part of application
Z19-0037
Planner Initials AC
City of Kelowna
DEVELOPMENT PLANNING

project title
HARVEY TOWNHOUSES

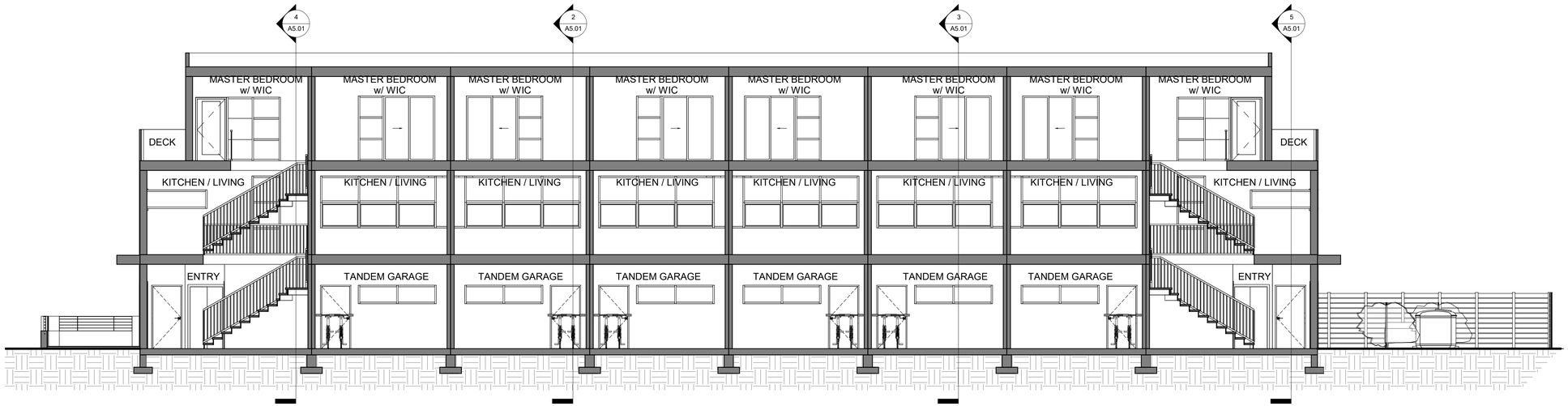
project address
757 HARVEY AVE,
KELOWNA, BC

project no. 4074

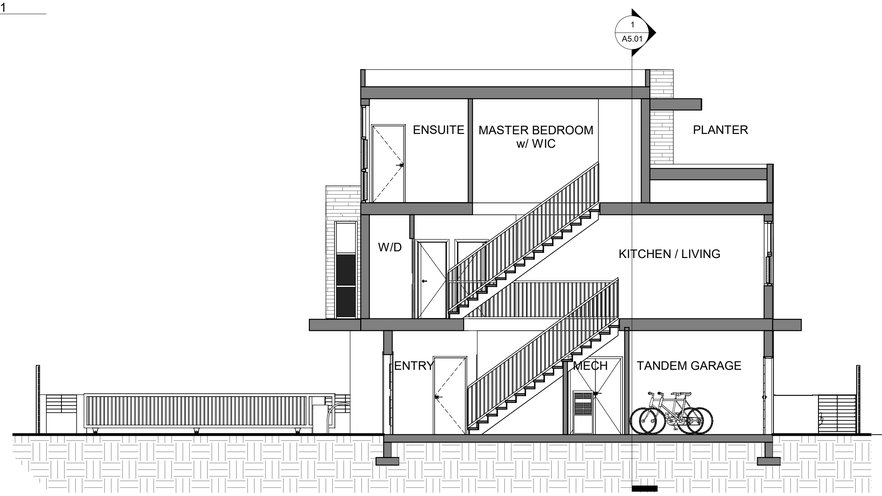
file no.

drawing title
BUILDING ELEVATIONS

designed L. A.
scale As indicated
drawn L. A.
checked K. F.
drawing no. **A4.01**
plotted 22/10/2019 2:49:28 PM



1 A3.01 Section 1
1 : 75



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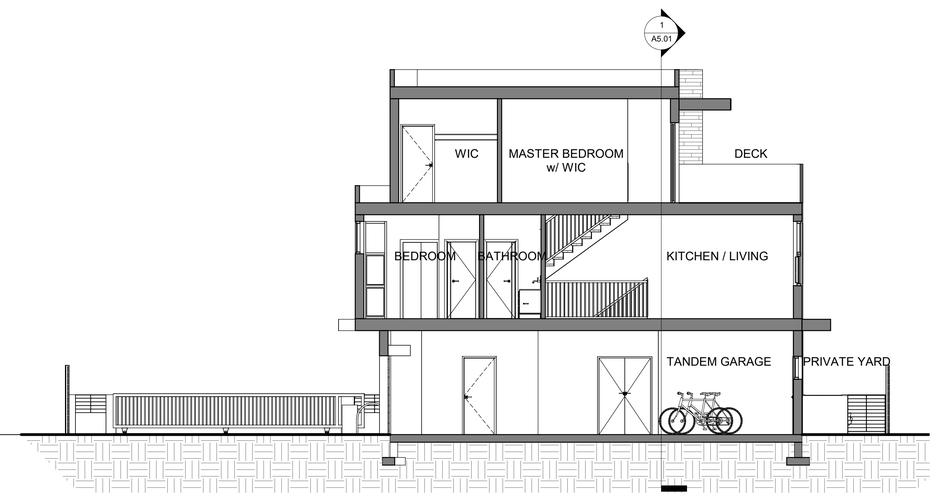

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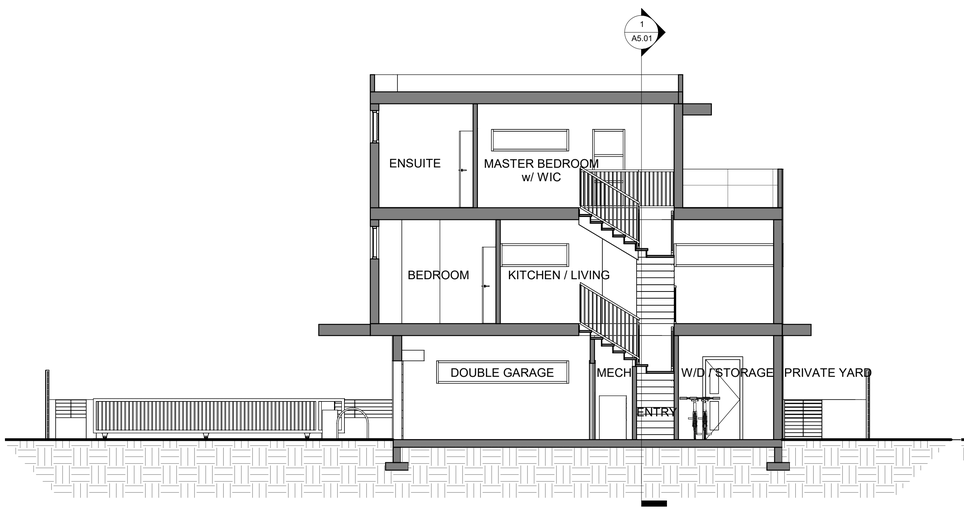

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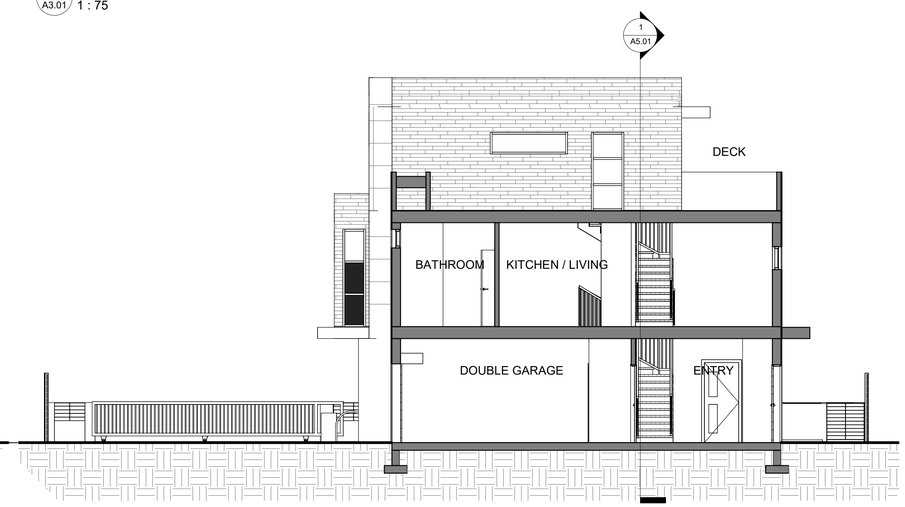

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1 : 75

NOT FOR CONSTRUCTION

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file no.

drawing title
BUILDING SECTIONS

designed L. A.
scale 1 : 75
drawn L. A.
checked K.F.
drawing no. A5.01
plotted 22/10/2019 2:49:35 PM

OUTLAND DESIGN
LANDSCAPE ARCHITECTURE

Friday July 5, 2019

New Town Architecture & Engineering Inc
1464 St. Paul Street
Kelowna BC V1Y 2E6
Attn: Lenka Aligerova, Building Design Technologist
Tel: (250) 860 8185
Email: lenka@newtownservices.net

Re: Proposed 757 Harvey Avenue, Kelowna, BC Development – Preliminary Cost Estimate for Bonding

Dear Lenka:

Please be advised of the following preliminary cost estimate for bonding of the proposed landscape works shown in the Proposed 757 Harvey Avenue conceptual landscape plan dated 19.07.05;

- 209 square metres (2,250 square feet) of improvements = $ 29,068.00

This preliminary cost estimate is inclusive of trees, shrubs, turf, mulch, topsoil, irrigation, & bike racks.

You will be required to submit a performance bond to the City of Kelowna in the amount of 125% of the preliminary cost estimate. Please do not hesitate to contact me with any questions about the landscape plan.

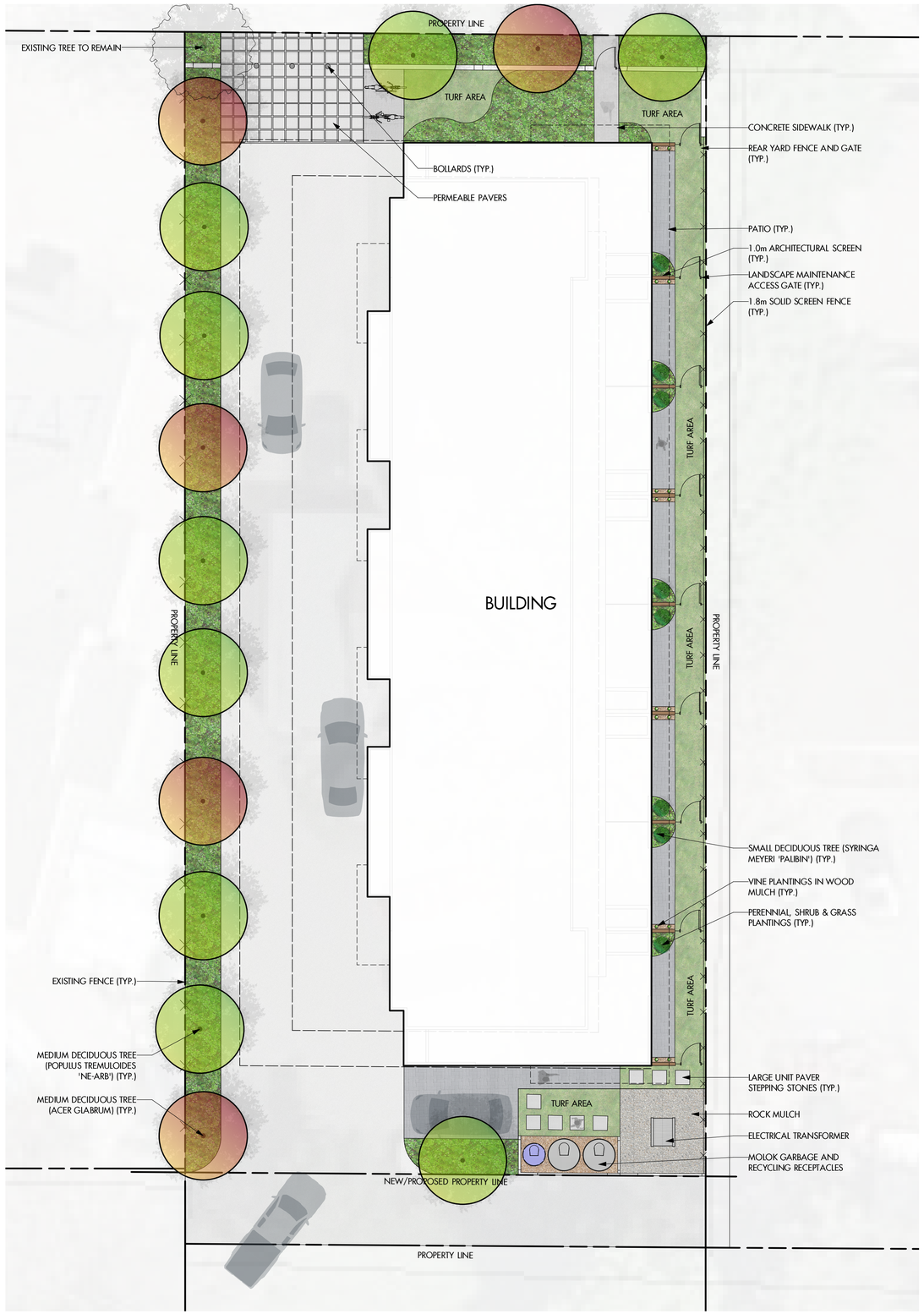
Best regards,

Fiona Barton

Fiona Barton, MBCSLA, CSLA
as per
Outland Design Landscape Architecture

ATTACHMENT B
This forms part of application
Z19-0037
City of Kelowna
DEVELOPMENT PLANNING
Planner Initials AC

303-590 KLO Road, Kelowna, BC, V1Y 7S2 P 250.868.9270
outlanddesign.ca



ATTACHMENT B
This forms part of application
Z19-0037
Planner Initials AC
City of Kelowna
DEVELOPMENT PLANNING

OUTLAND DESIGN
LANDSCAPE ARCHITECTURE
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Kelowna, BC V1Y 7S2
T (250) 868-9270
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NOTES

1. PLANT MATERIAL AND CONSTRUCTION METHODS SHALL MEET OR EXCEED C.L.N.A. STANDARDS.

2. ALL SOFT LANDSCAPE AREAS SHALL BE WATERED BY A FULLY AUTOMATIC TIMED UNDERGROUND IRRIGATION SYSTEM.

3. TREE AND SHRUB BEDS TO BE DRESSED IN A MINIMUM 75mm WOOD MULCH. DO NOT PLACE WEED MAT UNDERNEATH TREE AND SHRUB BEDS.

4. TREE AND SHRUB BEDS TO RECEIVE A MINIMUM 300mm DEPTH TOPSOIL PLACEMENT.

5. TURF AREAS FROM SOD SHALL BE NO. 1 GRADE GROWN FROM CERTIFIED SEED OF IMPROVED CULTIVARS REGISTERED FOR SALE IN B.C. AND SHALL BE TOLERANT OF DROUGHT CONDITIONS. A MINIMUM OF 150mm DEPTH OF GROWING MEDIUM IS REQUIRED BENEATH TURF AREAS. TURF AREAS SHALL MEET EXISTING GRADES AND HARD SURFACES FLUSH.

PLANT LIST

BOTANICAL NAME	COMMON NAME	QTY	SIZE / SPACING & REMARKS
TREES			
ACER GLABRUM	ROCKEY MOUNTAIN MAPLE	5	6cm CAL
POPULUS TREMULOIDES 'NE-ARB'	PRAIRIE GOLD ASPEN	9	6cm CAL
SYRINGA MEYERI 'PALIBIN'	DWARF KOREAN LILAC	8	4cm CAL. TOP GRAFTED
SHRUBS			
BUXUS 'GREEN GEM'	GREEN GEM BOXWOOD	18	#02 CONT. / 1.0m O.C. SPACING
CORNUS STOLONIFERA 'FARROW'	ARCTIC FIRE DOGWOOD	18	#02 CONT. /1.0m O.C. SPACING
EUONYMUS ALATUS 'SELECT'	FIRE BALL BURNING BUSH	35	#02 CONT. /1.5m O.C. SPACING
HYDRANGEA ARBORESCENS 'ABETWO'	INCREDIBALL HYDRANGEA	18	#01 CONT. /1.0m O.C. SPACING
PINUS MUGO 'MOPS'	MOPS MUGO PINE	35	#02 CONT. /1.5m O.C. SPACING
RIBES ALPINUM	ALPINE CURRANT	18	#01 CONT. /1.0m O.C. SPACING
PERENNIALS, GRASSES & VINES			
ARTEMISIA STELLERANA 'SILVER BROCADE'	SILVER BROCADE ARTEMISIA	15	#01 CONT. /0.6m O.C. SPACING
ASTER FRIKARTII 'MONCH'	FRIKART'S ASTER	10	#01 CONT. /0.75m O.C. SPACING
CALAMAGROSTIS ACUTIFLORA 'KARL FOERSTER'	FOERSTER'S FEATHER REED GRASS	5	#01 CONT. /1.0m O.C. SPACING
ERYNGIUM PLANUM 'BLUE DIAMOND'	BLUE DIAMOND SEA HOLLY	10	#01 CONT. /0.75m O.C. SPACING
LONICERA BROWNII 'DROPMORE SCARLET'	DROPMORE SCARLET HONEYSUCKLE	16	#01 CONT. /AS PER DRAWINGS
PANICUM VIRGATUM 'HANSE HERMS'	RED SWITCH GRASS	15	#01 CONT. /0.6m O.C. SPACING
PEROVSKIA ATRIPLICIFOLIA 'LONGIN'	LONGIN RUSSIAN SAGE	5	#01 CONT. /1.0m O.C. SPACING
PENNISETUM ALOPECUROIDES 'HAMELN'	DWARF FOUNTAIN GRASS	10	#01 CONT. /0.75m O.C. SPACING
SALVIA NEMEROSA 'SENSATION ROSE'	SENSATION ROSE SALVIA	15	#01 CONT. /0.6m O.C. SPACING
THYMUS PSEUDOLANGINOSUS	WOOLY THYME	15	#01 CONT. /0.6m O.C. SPACING

N

PROJECT TITLE

757 HARVEY AVENUE

Kelowna, BC

DRAWING TITLE

CONCEPTUAL LANDSCAPE PLAN

ISSUED FOR / REVISION

1	19.07.05	Review
2		
3		
4		
5		

PROJECT NO	18-121
DESIGN BY	FB
DRAWN BY	MC/WC
CHECKED BY	FB
DATE	JUL. 05, 2019
SCALE	1:100
PAGE SIZE	24'x36'

SEAL

BRITISH COLUMBIA SOCIETY OF LANDSCAPE ARCHITECTS
REGISTERED MEMBER
FIONA BARTON
334

DRAWING NUMBER

L1/2

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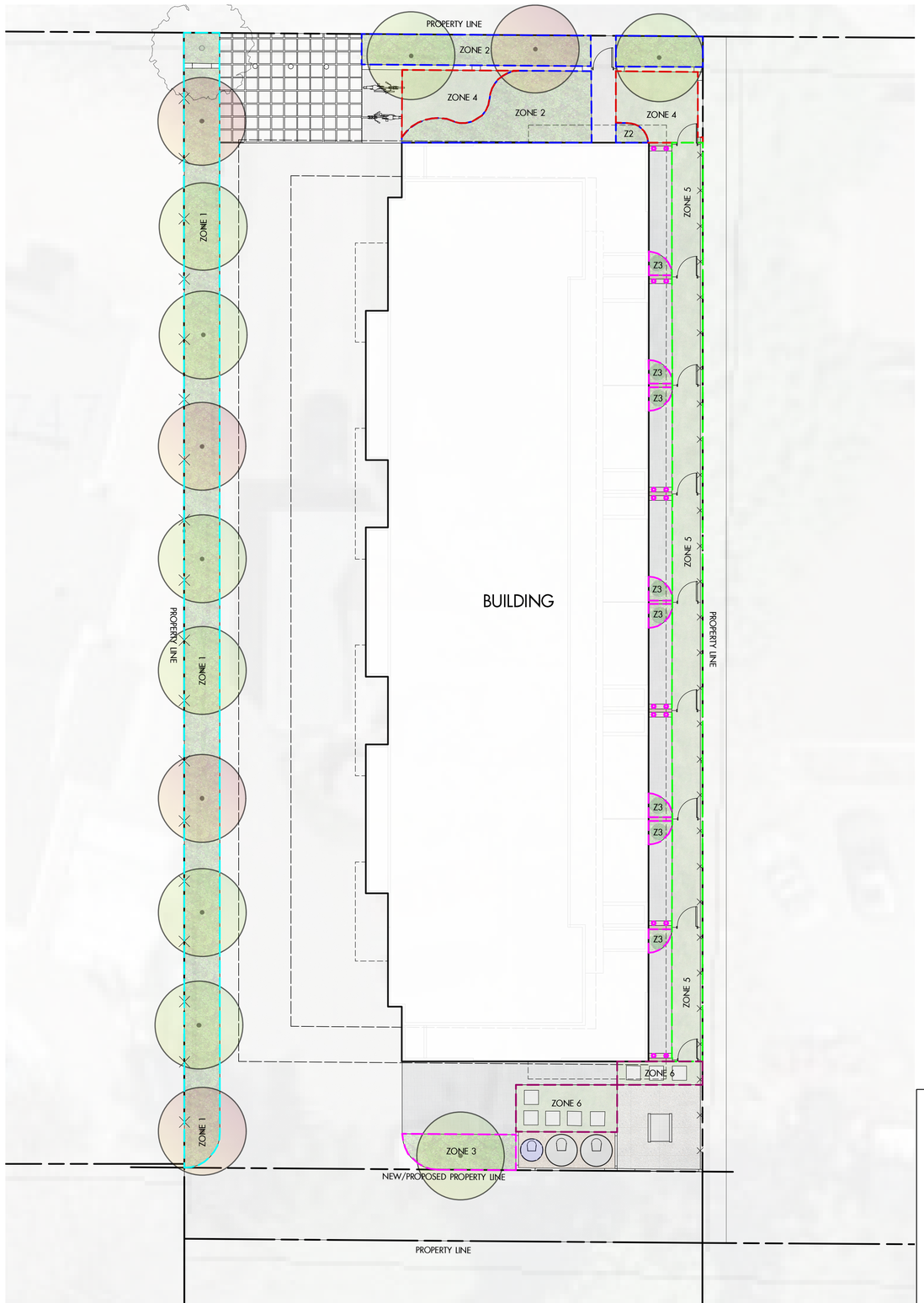

0 5 10 15 20 25 M

WATER CONSERVATION CALCULATIONS

LANDSCAPE MAXIMUM WATER BUDGET (WB) = 210 cu.m. / year
ESTIMATED LANDSCAPE WATER USE (WU) = 210 cu.m. / year
WATER BALANCE = 30 cu.m. / year
*REFER ATTACHED IRRIGATION APPLICATION FOR DETAILED CALCULATIONS

IRRIGATION NOTES

1. IRRIGATION PRODUCTS AND INSTALLATION METHODS SHALL MEET OR EXCEED THE REQUIREMENTS OF THE WATER USE REGULATION BYLAW NO. 10480 AND THE SUPPLEMENTARY SPECIFICATIONS IN THE CITY OF KELOWNA BYLAW 7900 (PART 6, SCHEDULE 5).

2. THE IRRIGATION SYSTEM SHALL MEET THE REQUIREMENTS, REGULATIONS, AND BYLAWS OF THE WATER PURVEYOR.

3. THE IRRIGATION SYSTEM SHALL BE EQUIPPED WITH AN APPROVED BACKFLOW PREVENTION DEVICE, WATER METER, AND SHUT OFF VALVE LOCATED OUTSIDE THE BUILDING ACCESSIBLE TO THE CITY.

4. AN APPROVED SMART CONTROLLER SHALL BE INSTALLED. THE IRRIGATION SCHEDULING TIMES SHALL UTILIZE A MAXIMUM ET VALUE OF 7" / MONTH (KELOWNA JULY ET), TAKING INTO CONSIDERATION SOIL TYPE, SLOPE, AND MICROCLIMATE.

5. DRIP LINE AND EMITTERS SHALL INCORPORATE TECHNOLOGY TO LIMIT ROOT INTRUSION.

6. IRRIGATION SLEEVES SHALL BE INSTALLED TO ROUTE IRRIGATION LINES UNDER HARD SURFACES AND FEATURES.

7. IRRIGATION PIPE SHALL BE SIZED TO ALLOW FOR A MAXIMUM FLOW OF 1.5m /SEC.

8. A FLOW SENSOR AND MASTER VALVE SHALL BE CONNECTED TO THE CONTROLLER AND PROGRAMMED TO STOP FLOW TO THE SYSTEM IN CASE OF AN IRRIGATION WATER LEAK.

IRRIGATION LEGEND

ZONE #1: HIGH EFFICIENCY SUBSURFACE DRIP IRRIGATION FOR MODERATE WATER USE PLANTING AREAS
TOTAL AREA: 71 sq.m.
MICROCLIMATE: WEST EXPOSURE, PARTIALLY SHADED BY TREES
ESTIMATED ANNUAL WATER USE: 39 cu.m.

ZONE #2: HIGH EFFICIENCY SUBSURFACE DRIP IRRIGATION FOR MODERATE WATER USE PLANTING AREAS
TOTAL AREA: 34 sq.m.
MICROCLIMATE: EAST EXPOSURE, PARTIALLY SHADED BY TREES
ESTIMATED ANNUAL WATER USE: 19 cu.m.

ZONE #3: HIGH EFFICIENCY SUBSURFACE DRIP IRRIGATION FOR MODERATE WATER USE PLANTING AREAS
TOTAL AREA: 13 sq.m.
MICROCLIMATE: SOUTHEAST EXPOSURE, PARTIALLY SHADED BY TREES
ESTIMATED ANNUAL WATER USE: 7 cu.m.

ZONE #4: LOW VOLUME POP-UP SPRAYHEADS FOR TURF AREA
TOTAL AREA: 18 sq.m.
MICROCLIMATE: WEST EXPOSURE, PARTIALLY SHADED BY TREES
ESTIMATED ANNUAL WATER USE: 26 cu.m.

ZONE #5: LOW VOLUME POP-UP SPRAYHEADS FOR TURF AREA
TOTAL AREA: 50 sq.m.
MICROCLIMATE: EAST EXPOSURE
ESTIMATED ANNUAL WATER USE: 71 cu.m.

ZONE #6: LOW VOLUME POP-UP SPRAYHEADS FOR TURF AREA
TOTAL AREA: 12 sq.m.
MICROCLIMATE: SOUTH EAST EXPOSURE, PARTIALLY SHADED BY TREES
ESTIMATED ANNUAL WATER USE: 17 cu.m.

OUTLAND DESIGN
LANDSCAPE ARCHITECTURE

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Kelowna, BC V1Y 7S2
T (250) 868-9270
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N

PROJECT TITLE

757 HARVEY AVENUE

Kelowna, BC

DRAWING TITLE

WATER CONSERVATION / IRRIGATION PLAN

ISSUED FOR / REVISION

1	19.07.05	Review
2		
3		
4		
5		

PROJECT NO	18-121
DESIGN BY	FB
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334

DRAWING NUMBER

L2/2

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REPORT TO COUNCIL

Date:	November 18, 2019		
To:	Council		
From:	City Manager		
Department:	Development Planning Department (JB)		
Application:	Z19-0109	**Owner:**	Lexington Enterprises Ltd., Inc. No. 381759
Address:	150 Hollywood Rd S	**Applicant:**	Zen Canna Retail Corp.
Subject:	Rezoning Application		

Existing Zone: C4- Urban Centre Commercial

Proposed Zone: C4rcs- Urban Centre Commercial (retail cannabis sales)

1.0 Recommendation

THAT Rezoning Application No. Z19-0109 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of all land shown on the Strata Plan KAS2403, located at 150 Hollywood Rd S, Kelowna, BC from the C4- Urban Centre Commercial zone to the C4rcs- Urban Centre Commercial zone (Retail Cannabis Sales) zone, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND FURTHER THAT if the Rezoning Bylaw is adopted, Council direct Staff to send a recommendation to the British Columbia Liquor and Cannabis Regulation Branch that they support issuance of a non-medical cannabis retail store license for this legal lot with the following comments:

- The proposed location meets local government bylaw requirements and as such, no negative impact is anticipated;
- The views of the residents were captured during a public hearing process for the rezoning of the property and Council meeting minutes summarizing those views are attached; and
- Local government recommends that the application be approved because of compliance with local regulations and policies.

2.0 Purpose

To consider an application to rezone the subject property from C4- Urban Centre Commercial to C4rcs- Urban Centre Commercial (retail cannabis sales) to allow for a retail cannabis sales establishment.

3.0 Development Planning

Development Planning Staff recommend support for the rezoning application to allow for a retail cannabis sales establishment on the subject property.

Should Council support the proposed Rezoning Bylaw, the property would be rezoned to a retail cannabis sales subzone, and Staff would send a recommendation to the British Columbia Liquor and Cannabis Regulation Branch indicating support for issuance of a non-medical cannabis retail store license for this property.

The application meets the Zoning Bylaw No. 8000 and there are no variances being requested. Further, the applicant has confirmed the completion of public notification in accordance with Council Policy No. 367.

4.0 Proposal

4.1 Project Description

A retail cannabis sales establishment is proposed in an existing ground-floor commercial retail unit.

4.2 Site Context

The property is located in the Rutland Urban Centre and has a Future Land Use Designation of MXR- Mixed Use (Residential/Commercial). The surrounding area is comprised of commercial and service commercial uses along the Hwy 33 W corridor.

Specifically, adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	C4- Urban Centre Commercial	Food primary, retail stores general
East	C4- Urban Centre Commercial	Retail stores, general
South	RU1- Large Lot Housing	Single Dwelling Housing
West	RU1- Large Lot Housing	Single Dwelling Housing

Subject Property Map:

5.0 Application Chronology

Date of Application Received: July 7, 2019
Date Public Consultation Completed: October 9, 2019

Report prepared by: Jocelyn Black, Planner Specialist

Reviewed by: Laura Bentley, Urban Planning & Development Policy Manager

Approved for Inclusion: Terry Barton, Development Planning Department Manager

Attachments:

Attachment A: Site Plan

Zen Canna Store – #150 Hollywood Rd. – Site Plan

The proposed location is separated by a two-rated fire wall, constructed by dry-wall and steel studs.

The Zen Canna Retail Cannabis store has its own entrances on the front and back of the building shown on the right, outlined in red.

No access between units, full height walls separating. The Retail Cannabis store is approximately **1,572** square feet.

= Man Door

= Overhead Loading / Garage Door

= Zen Canna Store

Site Plan Legend

110
- Taco Time
- Beautiful Nails

130 - KFC

150
- Specialty Bakery
- Quantum Games
- Zen Canna
- Pizza Factory
- Zaab Thai Restaurant

170 – Old Town Farm Market

190
- Lorenzo's Barbershop
- Johnny's Meat and Deli

250 – Residential Complex

720
- Esso Gas Station
- Skogie's Willow Park Car Wash

160 – Tim Hortons

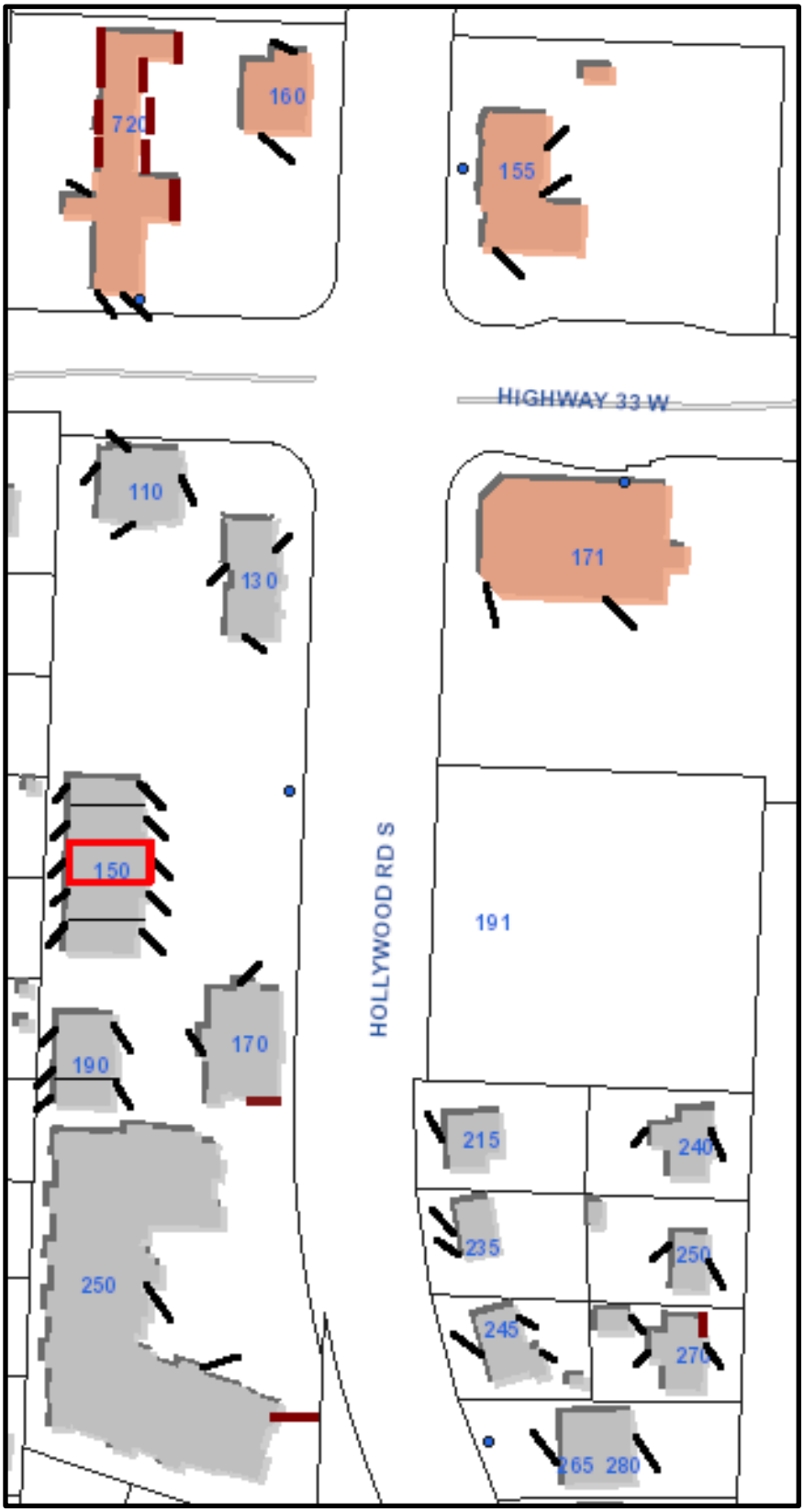
155 – McDonalds

171 – Dollarama

191 – Currently empty lot

Residential – Houses -

215, 240, 235, 250, 245, 270, 265, 280



ATTACHMENT A

This forms part of application

Z19-0109

Planner Initials JB

City of Kelowna DEVELOPMENT PLANNING

CITY OF KELOWNA

BYLAW NO. 11953
Z19-0109 – 150 Hollywood Rd S

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of all land shown on Strata Plan KAS2403, located on Hollywood Rd S, Kelowna, BC from the C4-Urban Centre Commercial zone to the C4rcs - Urban Centre Commercial zone (Retail Cannabis Sales) zone.

2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Approved under the Transportation Act this

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL

Date:	November 18, 2019		
To:	Council		
From:	City Manager		
Department:	Development Planning		
Application:	Z18-0117	**Owner:**	Studio 33 Properties Lt., Inc. No. BC1137489
Address:	145 Sadler Rd, 180 & 190 Hwy 33 E.	**Applicant:**	Pacific West Architecture Inc.
Subject:	Rezoning Application		

1.0 Recommendation

THAT Rezoning Application No. Z18-0117 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lots 1, 2 & 3 Section 26 Township 26 ODYD Plan 10045, located at 180 & 190 Hwy 33 E and 145 Sadler Rd, Kelowna, BC from the RU1 – Large Lot Housing zone and RU6 – Two Dwelling Housing zone to the C4 – Urban Centre Commercial zone, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Development Planning Department dated November 18, 2019;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered in conjunction with Council's consideration of a Development Permit and a Development Variance Permit for the subject properties.

2.0 Purpose

To consider an application to rezone the subject properties from the RU1 – Large Lot Housing and RU6 – Two Dwelling Housing zones to the C4 – Urban Centre Commercial zone to facilitate the development of Multiple Dwelling Housing.

3.0 Development Planning

The applicant is requesting to rezone the properties from RU1 – Large Lot Housing and RU6 – Two Dwelling Housing zones to the C4 – Urban Centre Commercial zone. Development Planning Staff are supportive of the application to facilitate the development of mixed-use multi-family housing on the three subject properties. The development site is located within the Rutland Urban Centre at the intersection of Highway 33 E and Sadler Road. The parcel has a Walk Score of 80 – Very Walkable (most errands can be accomplished on foot) and a transit score of 40 – Some Transit (there are a few nearby public transportation options). The

development is close to many nearby amenities including parks, restaurants, shops and recreational opportunities in the immediate area.

To fulfill Council Policy No. 367 for 'Zoning Major' applications, the applicant held a public information session on October 4, 2019 at Whisk Cake Company Bakery located at 203 Rutland Rd N. The open house was held from 4:00 – 8:00 p m. The session was advertised in the Kelowna Capital News on Friday, September 20, 2019. The applicant also contacted all neighbours within a 50 m radius of the subject parcels.

4.0 Proposal

4.1 Background

A Rezoning application was received by the City on December 10, 2018. Upon application, it was indicated to the applicant and owners that the property was listed on the Kelowna Heritage Registry. As the applicant team was unaware of this, the Rezoning application was paused to allow the owners time to determine how best to move the project forward.

The Kelowna Heritage Register is the official listing of properties within the community that have been identified as having heritage value. Properties on the Heritage Registry are not necessarily Heritage Designated properties. The registry does not provide long-term heritage protection and the development potential of a site is not restricted and the owner is entitled to develop the property in accordance with the permitted uses of the property's existing zoning. Buildings can be altered and may even be demolished, though the City is able to pause new development in order to explore options with the owner to attempt to preserve the heritage value of the asset.

On August 16, 2018, a Heritage Register Removal Request was presented to the Heritage Advisory Committee by Staff on behalf of the owners. The owners enlisted Katie Cummer, a registered heritage professional, to provide a Heritage Assessment and Evaluation of the site located at 180 Hwy 33 E known as Sproule Farm House. The Committee chose not to support the request for removal of the home from the Heritage Registry and urged the project team to find a way to retain and incorporate the Sproule Farm House into the proposed development plans.

Staff have had numerous meetings with the project team to discuss the many options they have presented and feel the current proposal is a suitable solution which allows for the redevelopment of the site and preserves the heritage value of the Sproule Farm House.

4.2 Project Description

The applicant is proposing to construct a 5-storey 90-unit multi-family development with two commercial units at-grade along Hwy 33 E. Two levels of structured parking provide 91 parking stalls which are accessed along the north property line of the site. A conceptual site plan has been submitted to show the proposed layout of the building and parking arrangement. The proposal will require a variance to the building height and number of storeys. Should Council support the Rezoning application, a form and character development permit and development variance permit would come before Council for consideration.

As proposed, the Sproule Farm House would be relocated from its current location to the south east corner of the project site. Due to the deterioration of the house, only two of the four facades would be retained and incorporated into the commercial CRU. This will include a prominent bay window and covered front porch. Further details on how this will be accomplished will be provided to Council upon consideration of the Development Permit.

4.3 Site Context

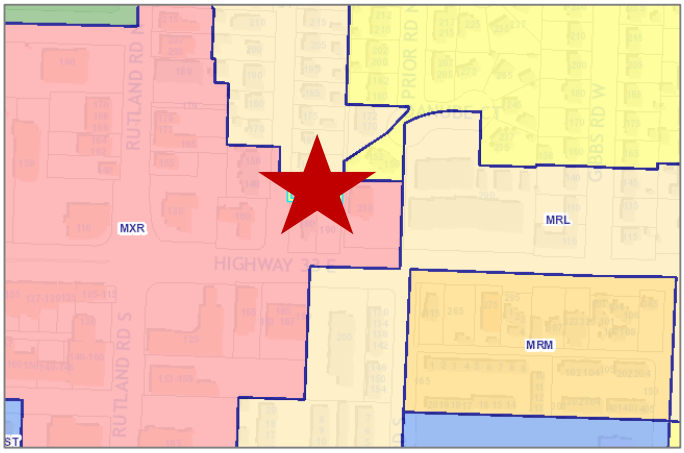
The three subject properties are located within the Rutland Urban Centre at the intersection of Sadler Rd and Hwy 33 E. The project is within the Permanent Growth Boundary and is on City services with water service provided by the Rutland Waterworks District (RWD).

Specifically, adjacent land uses are as follows:

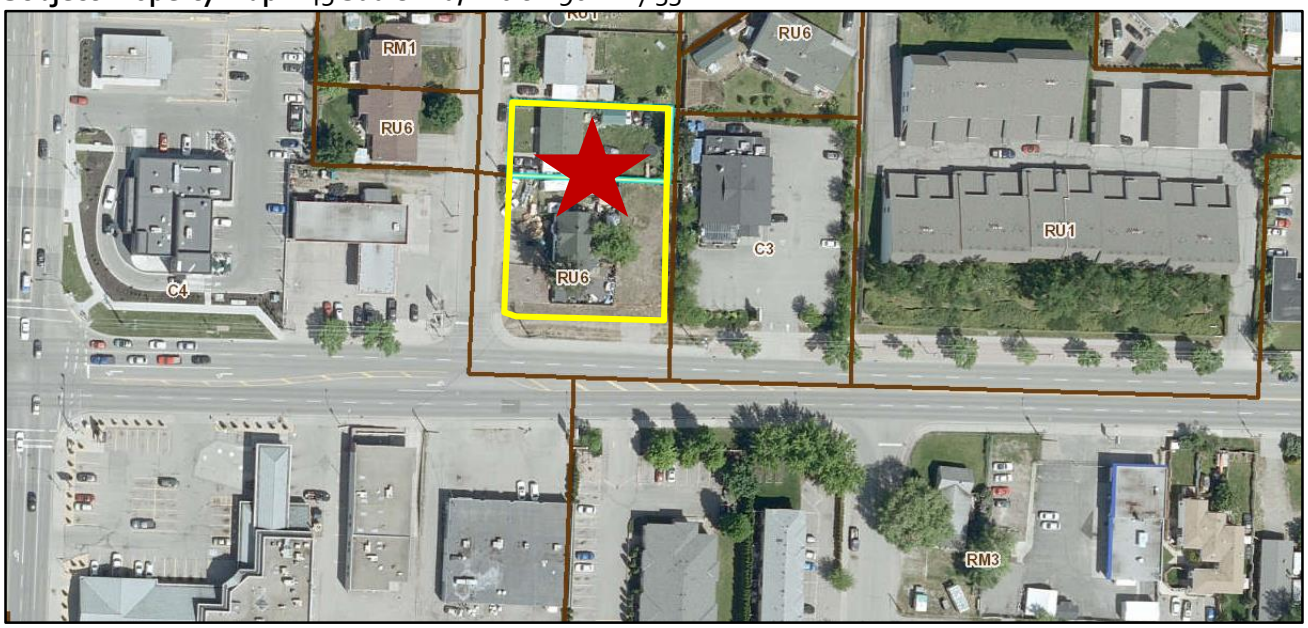
Orientation	Zoning	Land Use
North	RU1 – Large Lot Housing	Single Detached Dwellings
East	C3 – Community Commercial	Liquor Primary (Post Haus Pub)
South	C4 – Urban Centre Commercial RM3 – Low Density Multiple Housing	Various Commercial Businesses Apartment Housing
West	C4 – Urban Centre Commercial	Automotive Repair Shop

Context Map:

Future Land Use:



Subject Property Map: 145 Sadler Rd, 180 & 190 Hwy 33 E.



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Chapter 1: Introduction

Goals for a Sustainable Future:

> Contain Urban Growth – Reduce greenfield urban sprawl and focus growth in compact, connected and mixed-use (residential and commercial) urban and village centres.

Chapter 5: Development Process

Objective 5.3 Focus development to designated growth areas

> *Policy .1 Permanent Growth Boundary.* Establish a Permanent Growth Boundary as identified on Map 4.1 and Map 5.2. The City of Kelowna will support development of properties outside the PGB for more intensive use only to the extent permitted as per the OCP Future Land Use designations in place as of initial adoption of OCP Bylaw 10500, except for Agri-Business designated sites or as per Council's specific amendment of this policy. The PGB may be reviewed as part of the next major OCP update.
>
> *Policy .2 Compact Urban Form.* Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75-100 people and/or jobs per ha located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Objective 5.22 Ensure context sensitive housing development

> *Policy .7 Healthy Communities.* Through current zoning regulations and development processes, foster healthy, inclusive communities and a diverse mix of housing forms, consistent with the appearance of the surrounding neighbourhood.
>
> *Policy .11 Housing Mix.* Support a greater mix of housing unit size, form and tenure in multi-unit residential and mixed-use developments.

6.0 Application Chronology

Date of Application Received:	December 10, 2018
Date Public Consultation Completed:	October 4, 2019

Report prepared by:	Lydia Korolchuk, Planner II
Reviewed by:	Laura Bentley, Urban Planning & Development Policy Manager
Approved for Inclusion:	Terry Barton, Development Planning Department Manager

Attachments:

Schedule A: Development Engineering Memo

Attachment A: Conceptual Drawing Package

Attachment B: Heritage Assessment & Evaluation

CITY OF KELOWNA

MEMORANDUM

SCHEDULE A
This forms part of application
Z18-0177
Planner Initials LK
City of Kelowna
DEVELOPMENT PLANNING

Date: Jan 03, 2019

File No.: Z18 - 0117

To: Urban Planning Management (LK)

From: Development Engineering Manager (JK)

Subject: 145 Sadler Rd., 180 190 Hwy 33 E RU1, RU6 to C4

The Development Engineering Branch has the following comments and requirements associated with this rezoning application. The road and utility upgrading requirements outlined in this report will be requirements of this development.

The Development Engineering Technologist for this project is Andy Marshall.

1. Domestic Water and Fire Protection

a) This development is within the service area of the Rutland Water District (RWD). The developer is required to make satisfactory arrangements with the RWD for these items. All charges for service connection and upgrading costs, as well as any costs to decommission existing services are to be paid directly to RWD.

b) The developer must obtain the necessary permits and have all existing utility services disconnected prior to removing or demolishing the existing structures.

2. Sanitary Sewer

Our records indicate that 145 Sadler Rd. is currently serviced with a 100mm-diameter sanitary sewer service and 180 and 190 Hwy 33 E are currently each serviced with a 150mm-diameter sanitary sewer service. The applicant's consulting mechanical engineer will determine the requirements of the proposed development and establish the service needs. Only one service will be permitted for this development. The applicant, at his cost, will arrange for the removal and disconnection of the existing services not required and the installation of one new larger service if required.

3. Storm Drainage

a) The developer must engage a consulting civil engineer to provide a storm water management plan for the site which meets the requirements of the City Subdivision Development and Servicing Bylaw 7900. The storm water management plan must also include provision of lot grading plans, minimum basement elevations (MBE), if applicable, and recommendations for onsite drainage containment and disposal systems.

b) On site drainage systems for the site will be reviewed and approved by Engineer when site servicing design is submitted.

4. Road Improvements

a) Sadler Rd. will require upgrading to an urban standard (SS-R5 modified - to be determined at design) along the full frontage of the subject property which includes curb and gutter, sidewalk, street lighting, landscape boulevard, storm drainage system, pavement removal and replacement and relocation or adjustment of utility appurtenances if required to accommodated upgrading construction. Access shall be from Sadler.

b) Hwy 33 will require upgrading of sidewalk, boulevard and trees to match existing frontage to the east. Access shall be removed.

5. Subdivision and Dedication

a) Approximately 2.44m of road dedication is required along the entire frontage of Sadler Road.

b) Approximately 5.2m of road dedication is required along the entire frontage of Hwy 33.

c) Corner Rounding of 6m radius is required at the intersection.

d) If any road dedication or closure affects lands encumbered by a Utility right-of-way (such as Hydro, Telus, Gas, etc.) please obtain the approval of the utility. Any works required by the utility as a consequence of the road dedication or closure must be incorporated in the construction drawings submitted to the City's Development Manager.

6. Electric Power and Telecommunication Services

a) All proposed service connections are to be installed underground. It is the developer's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for these services, which would be at the applicant's cost.

b) Re-locate existing utilities, where necessary.

7. Geotechnical Report

As a requirement of this application the owner must provide a geotechnical report prepared by a Professional Engineer qualified in the field of hydro-geotechnical survey to address the following:

(a) Area ground water characteristics.

(b) Site suitability for development, unstable soils, etc.

(c) Drill and / or excavate test holes on the site and install pisometers if necessary. Log test hole data to identify soil characteristics, identify areas of fill if any. Identify unacceptable fill material, analyse soil sulphate content, identify unsuitable underlying soils such as peat, etc. and make recommendations for remediation if necessary.

(d) List extraordinary requirements that may be required to accommodate construction of roads and underground utilities as well as building foundation designs.

(e) Additional geotechnical survey may be necessary for building foundations, etc.

8. Design and Construction

(a) Design, construction supervision and inspection of all off-site civil works and site servicing must be performed by a Consulting Civil Engineer and all such work is subject to the approval of the City Engineer. Drawings must conform to City standards and requirements.

(b) Engineering drawing submissions are to be in accordance with the City's "Engineering Drawing Submission Requirements" Policy. Please note the number of sets and drawings required for submissions.

(c) Quality Control and Assurance Plans must be provided in accordance with the Subdivision, Development & Servicing Bylaw No. 7900 (refer to Part 5 and Schedule 3).

(d) A "Consulting Engineering Confirmation Letter" (City document 'C') must be completed prior to submission of any designs.

(e) Before any construction related to the requirements of this subdivision application commences, design drawings prepared by a professional engineer must be submitted to the City's Development Engineering Department. The design drawings must first be "Issued for Construction" by the City Engineer. On examination of design drawings, it may be determined that rights-of-way are required for current or future needs

9. Servicing Agreements for Works and Services

(a) A Servicing Agreement is required for all offsite works and services on City lands in accordance with the Subdivision, Development & Servicing Bylaw No. 7900. The applicant's Engineer, prior to preparation of Servicing Agreements, must provide adequate drawings and estimates for the required works. The Servicing Agreement must be in the form as described in Schedule 2 of the bylaw.

(b) Part 3, "Security for Works and Services", of the Bylaw, describes the Bonding and Insurance requirements of the Owner. The liability limit is not to be less than $5,000,000 and the City is to be named on the insurance policy as an additional insured.

10. Other Engineering Comments

(a) Provide all necessary Statutory Rights-of-Way for any utility corridors as required.

James Kay

James Kay, P. Eng.
Development Engineering Manager

agm

Zoning Analysis Table

CRITERIA	C4 ZONE REQUIREMENTS	PROPOSAL
	Development Regulations	
Lot Area	460 m^2	2260 m2 (24,323 SF)
	1300 m2 if there is no abutting lane	
Lot Depth	30.0 m	57. 3m
Lot Width	13.0 m	39.4m
	40.0 m if there is no abutting lane	
Height	15.0 m/ 4 Storeys	17.5 m/ 5 Storeys
Front Yard (West)	0.0 m	0.92 m
Side Yard (North)	4.5m/half height of building where the site abuts an RU1, RU2, RU3, or RU4 zone	8.8 m
	2.0 m where the site abuts other residential zone	
Side Yard (South)	0.0 m	0.0m
Rear Yard (East)	0.0 m	0m
	6.0m where abutting a residential zone	
Site coverage	75%	71%
Floor Area (Net)		
Floor Area (Gross)		3222.1m^2 (34,682SF)
FAR	1.3(Parking space are provided totally bequeath habitable of a principal building +0.2)	1.43
	Parking Regulations	
Minimun Parking Requierments	Total Required=100	91Parking Stalls (9 parking stalls will be pay- in lieu)
	Full size at 50%	46
Ratio of Parking Stalls	Medium size at 40% Max	36
	Small size at 10% Max	9
Minimun Drive Aisle Width	7.0 m	7.0 m
Setback (Parking)	1.5 m	1.5m
	Other Regulations	
Loading	1 per 1,900 m^2 GFA	N/A
Minimun Bicycle Parking Requirements	Apartment: Class 1: 0.5 Per DU Class 2: 0.1 Per DU	Class 1: 50 Class 2: 10
	Commercial: Class 1: 0.2 Per 100m^2 GLA or 1 Per 10 Employees, Class 2: 0.6 Per 100m^2 GLA	Class 1: 1 Class 2: 3

Unit Breakdown

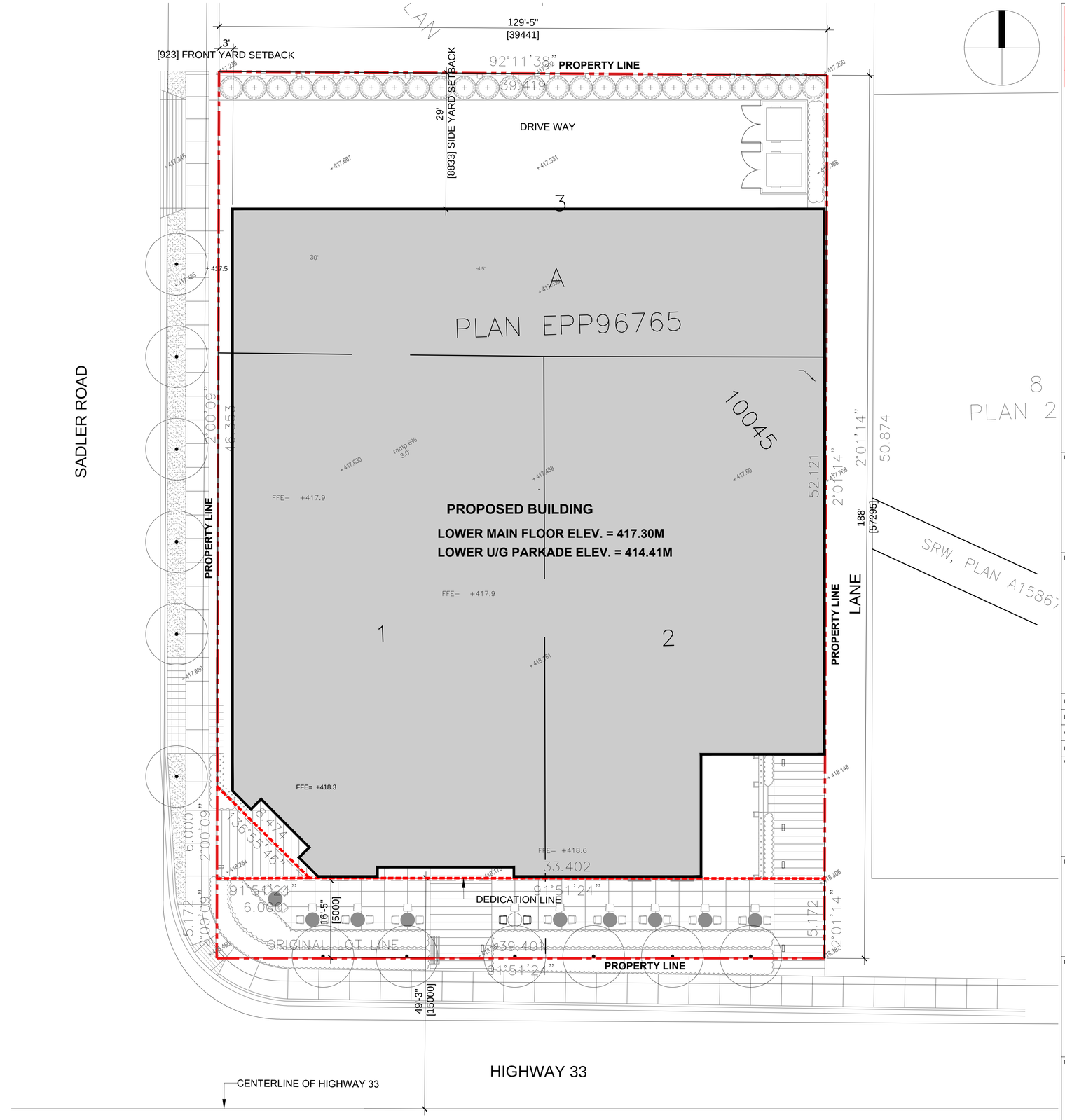
			Unit Area	Total Area
Unit	Unit type	*No. of Unit*	(ft.²)	(ft.²)
Unit A	Micro Suite	47	312	14664
Unit B	Micro Suite	25	312	7800
Unit C	1 Bedroom	1	505	505
Unit D	1 Bedroom	3	430	1290
Unit E	1 Bedroom	3	747	2241
Unit F	1 Bedroom	3	410	1230
Unit G	1 Bedroom	4	496	1984
Unit H	1 Bedroom	4	473	1892
Commercial				3076
Total		90		34682

PRIVATE OPEN SPACE (M^2)

	REQUIRED(M^2)	PROPOSED (M^2)
6m^2 STALL / MICRO SUITE		353
10m^2 STALL / ONE BEDROOM UNIT		154
15m^2 STALL /TWO BEDROOM UNIT		23
ROOF PATIO		425
COMMON ROOM		50
LOCKER ROOM		105
TOTAL	662	1110

PARKING NUMBER

	REQUIRED	PROPOSED
Residentical Units	82	75
Commercial	13	11
Visitor Parking	5	5
TOTAL	100	91



1 SITE PLAN
Scale: 3/32"= 1'-0"

PWA
pacific west architecture

1200 West 73rd Ave (Airport Square)
Suite 1100
Vancouver B.C. V6P 6G5

Office: 604 267 7072
Fax: 604 267 7056
Email: info@pwaarchitecture.com
www.pwaarchitecture.com

ATTACHMENT A
This forms part of application
Z18-0177
Planner Initials LK
City of Kelowna DEVELOPMENT PLANNING

REVISIONS
1

ISSUES		DATE
8		
7		
6		
5		
4	REISSUED FOR REZONING AND DP	OCT.2 2019
3	REISSUED FOR REZONING AND DP	MAY 22 2019
2	REISSUED FOR REZONING AND DP	MAY 1 2018
1	ISSUED FOR REZONING AND DP	FEB.6 2018

PROJECT NUMBER	A125
DRAWN BY	WW
CHECKED BY	PY
DATE CHECKED	
CONSULTANT	

PROJECT
MIXED-USE DEVELOPMENT
HWY 33 & SADLER KELOWNA,B.C.

DRAWING TITLE
SITE PLAN AND STATISTICS

DRAWING No.
A1.01

1 RENDERING
N.T.S.

PWA

pacific west architecture

1200 West 73rd Ave (Airport Square)
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Fax: 604 267 7056
Email: info@pwaarchitecture.com
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ATTACHMENT A
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REVISIONS
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1	ISSUED FOR REZONING AND DP	FEB.6 2018

PROJECT NUMBER	A125
DRAWN BY	WW
CHECKED BY	PY
DATE CHECKED	
CONSULTANT	

PROJECT
MIXED-USE DEVELOPMENT

HWY 33 & SADLER KELOWNA,B.C.

DRAWING TITLE
RENDERINGS

DRAWING No.
A6.03

1 RENDERING
N.T.S.

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architecture

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ATTACHMENT A
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City of Kelowna
DEVELOPMENT PLANNING

REVISIONS
1

ISSUES		DATE
8		
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2	REISSUED FOR REZONING AND DP	MAY 1 2018
1	ISSUED FOR REZONING AND DP	FEB.6 2018

PROJECT NUMBER A125
DRAWN BY WW
CHECKED BY PY
DATE CHECKED
CONSULTANT

PROJECT
MIXED-USE DEVELOPMENT

HWY 33 & SADLER KELOWNA,B.C.

DRAWING TITLE
STREET VIEW-HERITAGE BUILDING RENDERING

DRAWING No.
A6.02

ATTACHMENT B
This forms part of application
Z18-0177
Planner Initials LK
City of Kelowna
DEVELOPMENT PLANNING

Cummer Heritage Consulting

Katie Cummer, PhD CAHP
707, 838 Broughton Street
Victoria, BC, V8W 1E4
(778) 678 1913

May 13, 2019

City of Kelowna
1435 Water Street
Kelowna, BC
V1Y 1J4

RE: Heritage-related brief in relation to the proposed redevelopment of 180 Hwy 33 E

This letter provides my professional perspective on the proposed redevelopment of the Sproul Farm House and its heritage-related design going forward. I was the heritage consultant who conducted the re-assessment of the site in July 2018 and helped present to the City of Kelowna Heritage Committee in August 2018 about de-registering the house from Kelowna's Heritage Register. I am now working with the team to help ensure their updated proposal respects the heritage place as much as possible, while still allowing their new development to provide needed amenities for the community.

The house located at 180 Hwy 33 E, known as the Sproul Farm House on the Heritage Register, is a one and a half storey wood frame construction dating from 1906. Formally recognized in 2001, it was then listed on the Canadian Register of Historic Places in 2009. As articulated in my original assessment of the structure and as stated at the Heritage Committee Meeting, I have some doubts as to the original assessment of the site, which was conducted during a time of more limited research capabilities. It looks as though some of the articulated significance (specifically Enoch Mugford's supposed 55 years' association with the place) was misattributed to this structure from another one formally on the site and already since demolished (Cummer 2018 pp. 4-8). No matter the inaccuracy, this is not to say that the site is entirely insignificant, simply that it is perhaps not as significant as originally assessed.

No matter the historical associations, it is one of the few remaining early 20th century structures of the Rutland area and among a rare stock of pre-World War 1 housing. However, being one of the oldest surviving structures does not necessarily give a place greater significance. Tangibly, the structure is in fairly poor condition, having not been particularly well maintained, and with a number of changes and updates over the years that have compromised the integrity of the building. However, that is not to say that the Sproul Farm House is unsalvageable or without significance. In fact, its intangible elements seem to be a key importance of the place. In particular, its location and its historical associations with the pioneering Sproul brothers and the prominent local builder M.J. Curts. These are important intangible elements of the building that are worth celebrating and promoting, and which can continue to be done through thorough documentation, thoughtful on-site interpretation and selective preservation of the historic structure and fabric.

In particular, preserving two of its façades (the western and southern facing ones; the most prominent ones) a key character defining element can be preserved, specifically its form,

which "is representative of the straightforward, vernacular farmhouses of the day, one-and-one-half storeys high with a gabled roof facing the street, gabled dormers on the side, and a broad porch" (City of Kelowna). Moving the house to the southeast corner of the property and rotating it 45 degrees, allows the most prominent sides to be preserved and better showcased from the main thoroughfare. This provides a valuable reminder of the former streetscape and allows the house to be foregrounded rather than hidden in its current location at the northwest corner, behind and beside the proposed new development. It is also proposed to re-open the porch and make this a usable outdoor space once more and to ensure the iconic bay window continues to be a functioning window, rather than a false one. Looking through the Character Defining Elements (CDEs) listed in the Statement of Significance for the place, through this proposed functional facadism, the vast majority of the CDEs can be restored and preserved:

> ~~- Several mature trees in front and side yards~~ *As outlined in my original assessment through comparative aerial photography, unfortunately, most of the mature trees on-site were cut down between 2012 and 2017 (Cummer 2018, p. 11). It has been articulated to me that the two remaining trees on-site appear unwell and are a potential hazard going forward. Perhaps good to have an arborist to conduct an on-site inspection to confirm this, but if hazardous it seems defensible and logical to have them removed, despite their significance.*
> - Residential form, scale and massing, expressed by 1 and 1/2 storey height and rectangular plan
> - Medium-pitch gable roof with 2 secondary cross-gables
> - Street elevation has full-width open porch with repetitive, evenly-spaced painted wood columns
> ~~- Corbelled brick chimney~~ *Considering the positioning of the chimney in the middle of its roof, in preserving the two façades it may be difficult to retain this Character Defining Element, however, it will be thorough documented prior to removal.*
> - Narrow V-joint horizontal wood siding
> - Wood shingles in upper part of main gables
> - 1-over-1 double-hung wood sash windows on the upper floor, with plain wood trim
>
> (City of Kelowna)

Of course, in the heritage conservation field, facadism is a somewhat polarizing approach (Vancouver Heritage Foundation 2013). There are those who vehemently oppose its use and others that understand that compromise is sometimes needed, particularly when a structure cannot be preserved in full (Bargery 2005). There are numerous modern, Canadian examples where a compromise has been needed on account of a building's condition, such as the current project in the provincial capital with the Customs House site redevelopment in Victoria, BC. There are and will continue to be critics of this approach, but unfortunately, sometimes pragmatism is needed over idealism.

In the case of this project, in preserving the two façades, one could argue that the majority of what has been visible from the street for over a century is being preserved and allowing for the continuation of this community landmark. It is also providing the opportunity for certain key elements to be restored, in particular, the characteristic wood siding and shingles as well as the porch, which was closed in at some point in the last fifteen years. If anything, this project is allowing the Sproul Farm House to be refreshed and given a new lease of life. This is particularly the case if the developers are able to incorporate the porch into a functional, usable space for the community as an attachment to an interior space, allowing this Character Defining Element to be restored and accessible to the public for the first time in its history. As eloquently stated by Robert Bargery, the former Head of Policy and Research at the Commission for Architecture and the Built Environment in the UK:

> If the facade really is all that can stay, we might insist on the new building being properly related to and integrated with the retained facade, correctly-placed cross-walls included. Facadism works least well when windows evidently lead through to nothing, when their lack of relationship to anything behind them is betrayed by mirror glass, or blanked-out windows, or even a view of the sky. It is an unhappy compromise, but perhaps in future we should seek to avoid it by keeping more, not less, of the historic building.
>
> (Bargery 2005)

I hope this helps to provide some context from a professional perspective in relation to the proposed redevelopment of 180 Hwy 33 E. If you have any further questions or would like me to clarify anything, please feel free to contact me at kcummer@gmail.com.

Thank you for your time and take care.

Sincerely,

Katie Cummer, PhD CAHP
Principal, Cummer Heritage Consulting

References

Bargery, Robert. "The Ethics of Facadism: Pragmatism Versus Idealism." *The Building Conservation Directory*, 2005. http://www.buildingconservation.com/articles/facadism/facadism.htm

City of Kelowna. "Heritage Register: Sproul Farm House." Online resource: https://www.kelowna.ca/our-community/arts-culture-heritage/heritage/heritage-register/sproul-farm-house.

Cummer, Katie. *Heritage Assessment and Evaluation: 180 Hwy 33 E, Kelowna BC*. Victoria, BC: Cummer Heritage Consulting, 2018.

Vancouver Heritage Foundation. "Facadism as a Heritage Strategy." *Spacing Vancouver*, March 26, 2013. http://spacing.ca/vancouver/2013/03/26/facadism-as-a-heritage-strategy/

ATTACHMENT B
This forms part of application
Z18-0177
Planner Initials LK
City of Kelowna
DEVELOPMENT PLANNING

CHC
Cummer Heritage Consulting

Heritage Assessment and Evaluation

180 Hwy 33 E, Kelowna BC
July 17, 2018

Background Information
Neighbourhood: Rutland
Address: 180 Hwy 33 E
Plan: 10045*; Lot: 1
*Known as such from 1959 onwards. Prior to that it was known as: Plan 5971 from 1953; Plan 4033 from 1947; Plan 2773 from 1940; and Plan 2478 from 1936
Lot Size: 0.21 Acres
Property Type: P – Typical Property
Date of completion: 1906
Builder: M.J. Curts

Introduction

This report is a heritage assessment and evaluation of the wood frame construction located at 180 Hwy 33 E (Figs. 1 and 2). Please note that due to the age of the building and its location, there were fewer resources available than is typically desired for such an evaluation. A thorough search was conducted of: various City of Kelowna Departmental records, Kelowna Public Archives, City of Vancouver Archives, the Vancouver Public Library, the Victoria Public Library, the BC Archives and the Library and Archives Canada, however, some key documents were not found, such as the building permit information, building site plans and any fire insurance maps. Therefore, this assessment is conducted based on the information available. Please note that due to this shortage of information, it appears that the original Statement of Significance conducted on the site may have mistaken certain information about this place, as discussed in greater detail below.

Fig. 1: Aerial view of 180 Hwy 33 E, outlined in red, and its immediate surroundings. (Source: City of Kelowna Map Viewer, 2017)

Katie Cummer, PhD CAHP – Cummer Heritage Consulting (CHC)

Fig. 2: Street view of 180 Hwy 33 E taken from the northwest, along Sadler Road. It is believed this is a similar view of the house as illustrated in the 1906 historical photograph featured in Fig. 7 below. (Source: Google Street View, 2017)

Overview

The one and a half storey structure located at 180 Hwy 33 E is a very early 20th century wood frame construction occupying a corner lot along Hwy 33 E and Sadler Road (Figs. 3 to 6). Often referred to as the Sproul Farm House, it was built in an area known as Rutland, named after John Hope Rutland who "is reputed to have brought the first irrigation system in the district into operation" (Rutland Centennial Committee 1958, p. 23).

Figs. 3 and 4: Southern facing view (left), taken from Hwy 33 E and western facing view (right), taken from Sadler Road. (Source: John Douglas)

Figs. 5 and 6: Northern facing view (left) and eastern facing view (right) of 180 Hwy 33 E. (Source: John Douglas)

Today, Rutland is one of the larger neighbourhoods of Kelowna, but, historically, this was a separate rural town that did not become a part of the Greater Kelowna area until 1973, contributing to the scarcity of materials available both on the region and this building specifically. Despite this shortage of information, it is clear that the development of this area is connected to the irrigation network that was introduced and the subsequent growth of numerous orchards, a key feature of the area's reputation and economy (Kelowna Museum 2005, p. 118). In fact, the first orchard grown on the Rutland Estate surrounded the Sproul Farm House (Rutland Centennial Committee 1958, p. 22), as illustrated in Fig. 7 below.

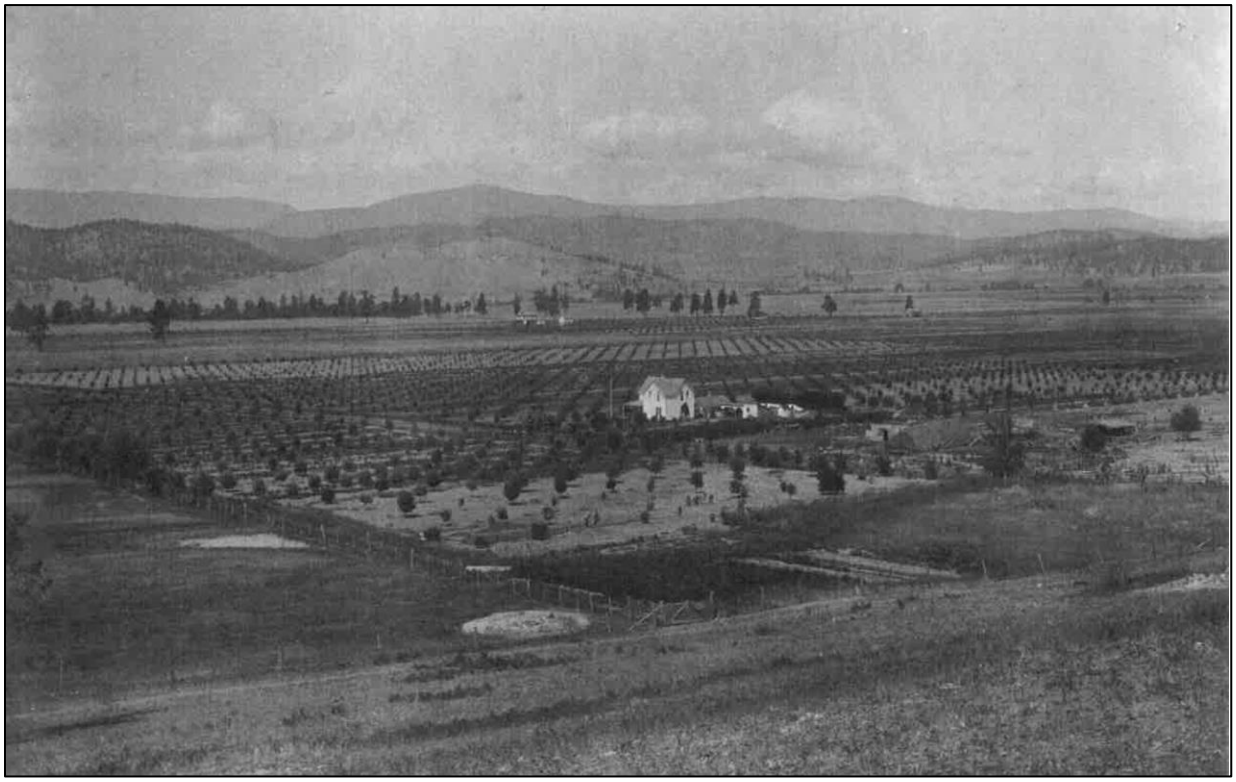

Fig. 7: Historical view of "Sproule's Orchard, Kelowna," c. 1905 (note the archival description states c. 1905, however, the Statement of Significance states it was built in 1906. It is unclear where that date was obtained. This is one of the information discrepancies, addressed below). (Source: Kelowna Museum Archives, 3119)

The namesake of this Farm House, Samuel Sproul and his brother Robert, were among the early pioneers to this area and are recorded to have "planted the first orchards from Hardie's store to the foot of the benches" (Rutland Centennial Committee 1958, p. 31). As stated in this property's Statement of Significance: "They came to the Black Mountain area from the United States with the 'covered wagon' group about 1893" (City of Kelowna). It goes on to say that they purchased the land in 1904 and commissioned the prominent builder, M.J. Curts to build their farm house (*ibid*.). M.J. (Michael 'Johnny') Curts was an important and prolific builder for the area in the early 20th century period. In fact, a collection of buildings from the Kelowna area that were designed and/or built by him are on the Canadian Register of Historic Places (Table 1).

Table 1: Consolidated list of Kelowna buildings associated with M.J. Curts that are recognised on the Canadian Register of Historic Places (Source: Canada's Historic Places)

Building Name	Location	Construction Date
W.D. Walker House	4464 Lakeshore Road	1904
Sproul Farm House	180 Hwy 33 E	1906
C. Martin House	1441 Richter Street	1907
Courier Building	1580 Water Street	1908
Old Post Office	274 Bernard Avenue	1908
W.J. Marshall House	1869 Marshall Street	1908
OK Loan & Investment Building	280 Bernard Avenue	1909
Second Mallam House	4852-4856 Lakeshore Road	1910
Belgo House	1590-1640 Belgo Road	1912
Renfrew House	504 Keith Road	1913
The Raymer Block	289-299 Bernard Avenue	1917
Adams House	1998 Abbott Street	1922

It is important to note that it is unclear where the Statement of Significance obtained its information and how accurate the dating or builder information is. For example, without the building permit information it is hard to confirm these details. There are also concerns with regards to the accuracy of the information contained in the SoS, as its latter history appears to be incorrect. Based on the SoS content, the following is an outline of its later history.

Despite being named after Samuel Sproul, he only owned and occupied the land for less than a decade, selling "the house and the orchard in 1914 to Enoch Mugford (1880-1969)" (City of Kelowna). Enoch Mugford, on the other hand, is said to have occupied the house with his family for 55 years, until his death in 1969 (*ibid*.). Enoch Mugford was an important and prominent figure for Rutland, involved in community affairs and part of the group who helped to set up the Black Mountain Irrigation District (BMID) in 1920 to improve the water supply to Rutland. Mugford was part of the BMID's first Board of Trustees, resigning in 1922 to become its Superintendent until his retirement in 1950 (Rutland Centennial Committee 1958, pp. 101-104).

Having said this, there appears to be some confusion as to Enoch Mugford's use and occupancy of this place. It is possible that Enoch Mugford's association was in fact with a different house belonging to Samuel Sproul, not in fact this structure at 180 Hwy 33 E. As stated in the book *Down Memory Lane Rutland*:

> Sam Sproul bought a twenty-four acre block near the foot of McKenzie Hill. His brother Robert Sproul bought several parcels of the young orchard. Sam built a house on the property. (In 1914 Sam sold his house and property to Enoch Mugford.)
>
> Joe Rich Road ran along the south boundary of the property.
>
> Later in 1906, Sam Sproul had M.J. Curts build a high gable roof with a large gable dormer house for his daughter Lillian who married Ernest Dudgeon. This building has a bay window with decorative centre window and flat roof on the south side. The windows have a wide trim with a lug sill. The front entrance (facing south to Hwy 33) is set back with a covered veranda. (House on the corner of Sadler Road and Hwy 33).
>
> McLeod's lived in this house then one of the Schneider families rented the house. Joe Horning bought the house in summer of 1945. After Joe Horning sold the remainder of the property, Hank and Anita Funk bought the house and land. The Mussel family rented the home and many others followed. The Funks still own the house and property in 2008, as a rental home.
> (Vielvoye and Senger 2008, p. 297)

The above suggests that there were in fact at least two houses on the Sproul land and that Enoch Mugford did indeed buy a property from Samuel Sproul in 1914, however, it seems it was not the house located at 180 Hwy 33 E, but instead another. The excerpt above, addressing the daughter's house, seems to be describing the appearance and location of the captioned study site. The key details are: gable roof; bay window; front entrance (facing Hwy 33) with veranda; corner house at Sadler and Hwy 33; and eventually bought by the Funks.

There are, however, a few challenges with regards to this publication worth noting. Unfortunately, there is no reference list or bibliography to it, so it is difficult to confirm its information as well. There is also a potential error in its facts. As stated in the *History of the District of Rutland, B.C. 1858-1958*, Lillian Sproul was in fact the daughter of Robert Sproul: "The Sam Sproul home was purchased by Mr. and Mrs. E. Mugford, Sr., who are still residing there. One of Bob Sproul's daughters, Lillian, married Ernest Dudgeon, and the former home is now occupied by Mr. and Mrs. Joe Horning and family (Rutland Centennial Committee 1958, p. 32).

It is worth noting that in this 1958 publication, Enoch Mugford and his wife are stated as still residing in the Sam Sproul house, however by 1962, as confirmed by the "Canada, Voters List," the Mugfords had left the house and moved into an elderly care facility, located at Apt 15, 1469 Bertram Road (Okanagan Regional Library 1962). This means that, no matter what, Enoch Mugford did not occupy the house until his death in 1969, as stated in the Statement of Significance (City of Kelowna). It is also important to note that, despite the authors' error above with regards to the Sproul daughter, one of the authors of this 2008 book is the Granddaughter of Mr. Horning and has memories of being in his house at 180 Hwy 33 E (personal communication, 2018). This ownership and occupation is also further confirmed through a 1959 subdivision plan of the area (see Fig. 8 below), as well as a family photograph from the Hornings clearly taken at the house (Fig. 9).

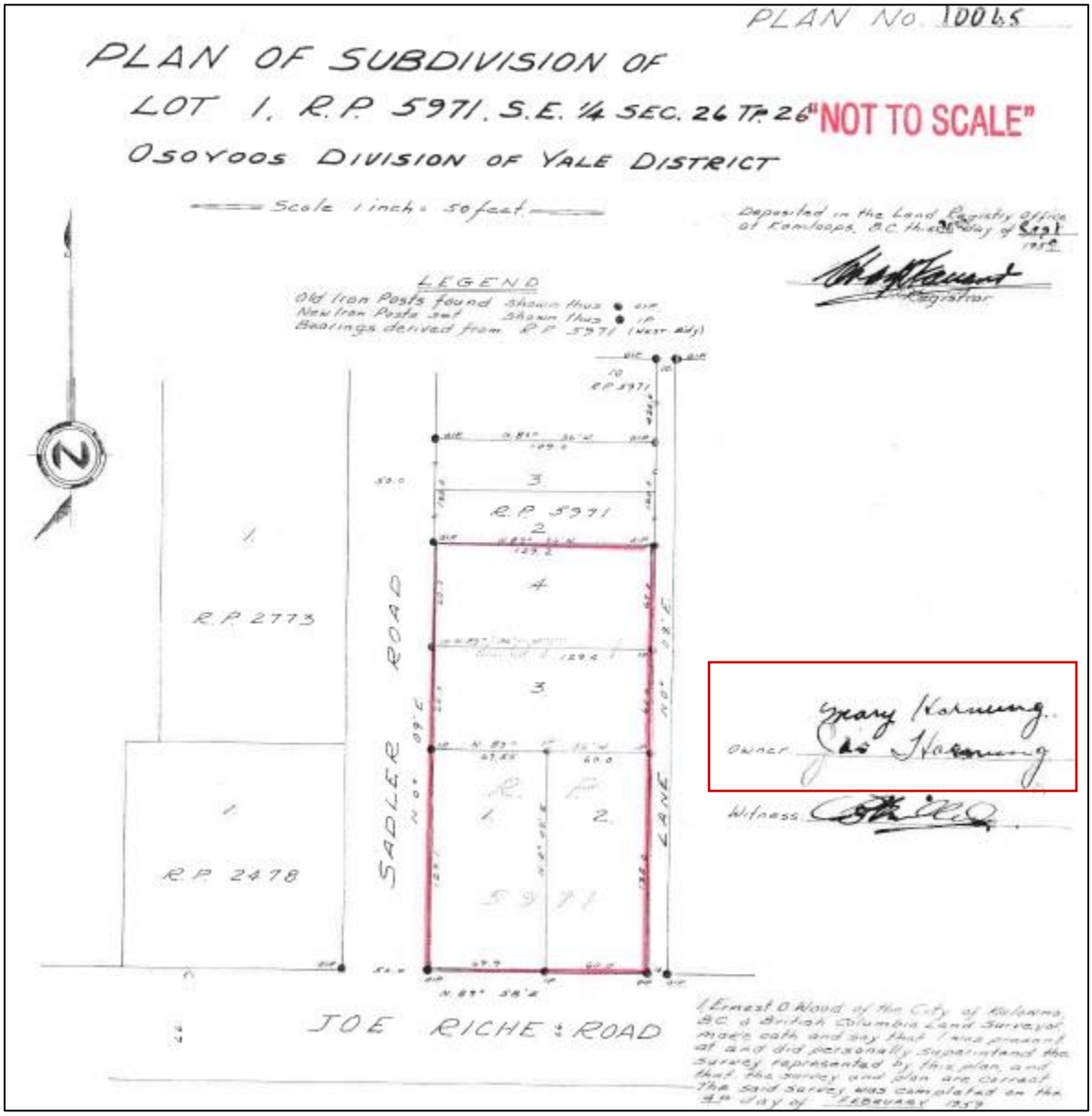

Fig. 8: Plan of Subdivision of Lot 1. R.P. 5971, 1959. Note the reference to the current numbering system, Plan 10045, and the ownership of "Joe Horning," outlined in red. (Source: City of Kelowna Registry, Plan No. 10045)

Fig. 9: Photograph of the Horning family occupying 180 Hwy 33 E in 1946, taken from the SE corner. The house is recognisable through its architectural features, such as the protruding entryway, the corner of the gable roof and the open porch on the far left of the photograph, in addition to showing one of the iconic trees. (Source: Vielvoye and Senger 2008, p. 239)

In the book *Down Memory Lane Rutland*, it goes on to explain more about Joseph Horning:

> Joseph Hornung (changed name to Horning in Rutland) was born in Saskatchewan. He met and married Mary Flegel. In June 1945, they moved to Rutland, B.C. They bought a house on Joe Rich Road (now is Hwy 33) with about twenty acres of land that was associated with Sam Sproul; a huge two storey home with a balcony surrounding half the house, built in 1906 by M.J. Curts, a prominent builder, for Sproul's daughter who married a Dudgeon. The house was bought by the McLeod's and later rented to the Schneider family.
>
> … Joseph's first job was at the Rutland Sawmill as steam Engineer. In the 1960-1970s, Joseph became a Water Bailiff for Black Mountain Irrigation…Mary worked for Rowcliffe Cannery for many years and then at Brown Brother's Book Binding, which is now the Kelowna Book Bindery.
>
> Joseph was one person instrumental in Black Knight TV coming to Rutland, along with Alvin Angus. A Radio Repair Shop from Spokane ran a wire and transmitted to this area. Joseph was asked to put a TV in his house for free viewing and to let other people watch.
>
> They split up their property to make lots in 1952. John and Julia Ottenbreit bought a good portion of this land at the north end by Mugford Road. Joseph had to provide a short road and supplied water to Husche's from their well. Later the rest was sub-divided and that was when the barn was taken down.
>
> Mary's parents (Frank and Rose Flegel) bought a small lot across from the Ottenbreit home on Sadler Road, and their house from Moyer Road was moved to the existing lot. Joe and Mary built a new house next to the Heritage house [see Fig. 10 below].

> Later, son Al built between his parents and grandparents places. The old house was rented to the Mussel family and later Henry Funk bought the house (lives at Carr's Landing) has rented it to various people over the years. It is still rented at present time.
>
> (Vielvoye and Senger 2008, p. 239-240)

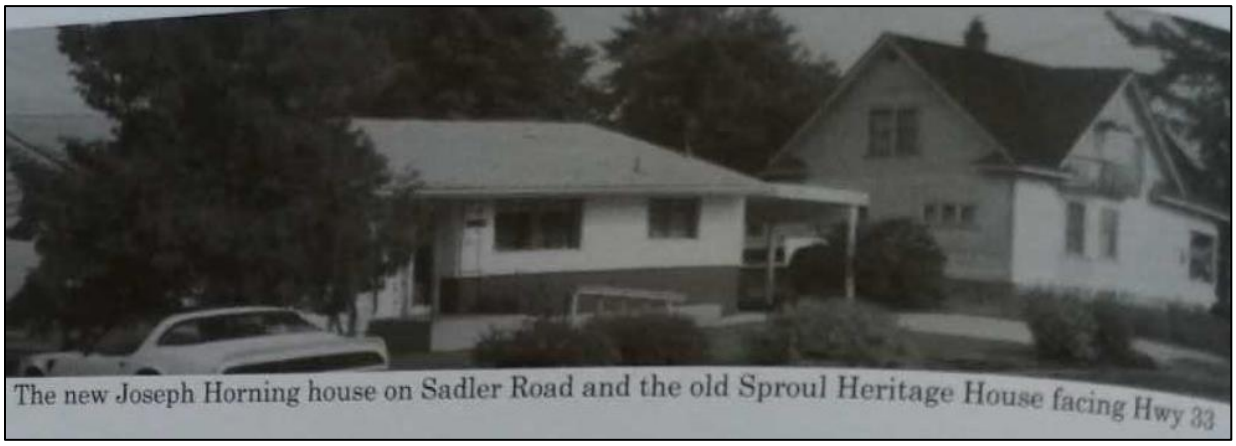

Fig. 10: Photograph of the "new Joseph Horning house" and the "old Sproul Heritage House", taken from the NW. (Source: Vielvoye and Senger 2008, p. 240)

These details outlined above, in particular the subdivision of the lot and the various renters and owners, are confirmed from the records at the Land Title Office as well. Such as, "Title No. 211141F, (1959) Lots 1, (2,3,4) Plan 10045 R.O. 'Hornung, Joseph & Mary'"; "R.P. 107172E, 14/2/1964 - Charles Henry John Mussell & Hilary Jean Josephine Mussell (Joint Tenants)"; and "B267373, 7/11/67 - Henry Funk" (Land Title Office 1959, 1964 and 1967). Although difficult to confirm, the description above could suggest that perhaps the actual Mugford residence was located further north on the original lot and could explain why Mugford Road is named "Mugford."

Either way, from the above, it seems fairly clear that Enoch Mugford did not live at 180 Hwy 33 E, as originally attributed in the Statement of Significance. No matter though, the late 20th century marks an important transition for the house and the surrounding area. For most of the early and mid-20th century, the area of Rutland continued to be rural and modest. However, in the 1960s and into the 1970s, a number of changes began to take place, specifically a move towards more commercialisation in the area, compared to its rural past (City of Kelowna). This resulted in a series of boundary extensions for Kelowna. This was particularly the case following the development of the Orchard Park Shopping Centre in 1971 (Simpson 2011, p. 220) and the eventual amalgamation of the areas of Benvoulin, Glenmore, Okanagan Mission and Rutland in 1973 (Surtees 1989, p. 75) (Fig. 11).

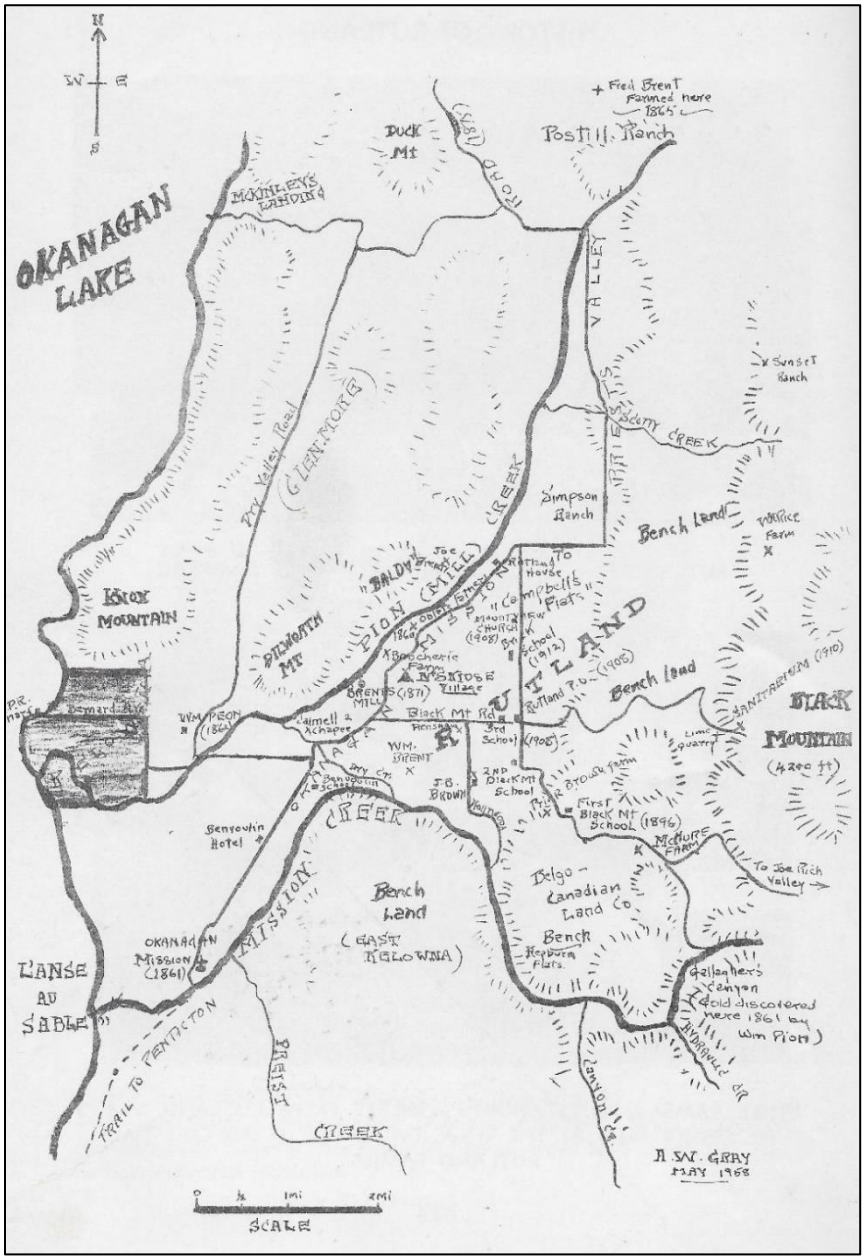

Fig. 11: Map of the Rutland area, May 1958. The areas to be amalgamated with Kelowna (Benvoulin, Glenmore, Okanagan Mission and Rutland) are distinctly visible. (Source: Rutland Centennial Committee 1958, p. 128)

In 1971, it appears the house occupying 180 Hwy 33 E shifted from being a single-family dwelling to being used for commercial purposes, beginning with its use by the excavating firm of H.R. Funk (City of Kelowna); the same Hank Funk who is said to have purchased the house from Joe Horning (Vielvoye and Senger 2008, p. 297). In the Statement of Significance, it is stated that this change "illustrates the business development of 'downtown' Rutland" taking place in the latter part of the 20th century (City of Kelowna). Today, the house is mixed use, being used commercially with a section of the house being used as a bridal gown store, called *Bubbles N Bells*, with residential tenants also occupying the house, including the proprietor of the aforementioned store.

Katie Cummer, PhD CAHP – Cummer Heritage Consulting (CHC)

Character Defining Elements and Current Condition

As recognised in the Statement of Significance composed for the designation of the Sproul Farm House in 2001, followed by its listing on the Canadian Register in 2009 (Canada's Historic Places), the following were considered the Character Defining Elements (CDEs) of this place:

- Several mature trees in front and side yards
- Residential form, scale and massing, expressed by 1 and 1/2 storey height and rectangular plan
- Medium-pitch gable roof with 2 secondary cross-gables
- Street elevation has full-width open porch with repetitive, evenly-spaced painted wood columns
- Corbelled brick chimney
- Narrow V-joint horizontal wood siding
- Wood shingles in upper part of main gables
- 1-over-1 double-hung wood sash windows on the upper floor, with plain wood trim

(City of Kelowna)

Many of these elements are still intact today, although there has been some deterioration and lack of maintenance of these features, as visible in comparing two photographs of the house; one from 2005 and another from 2017 (Figs. 12 and 13).

Figs. 12 and 13: Comparative views of 180 Hwy 33 E, 2005 (left) and 2017 (right). (Source: City of Kelowna 2005 and Google Street View 2017).

In comparing these, it is important to note that two CDEs, in particular, have been altered. The first being the porch. As outlined above, the "street elevation has full-width open porch with repetitive, evenly-spaced painted wood columns." Between 2005 and 2017, this was closed in without permission and is a feature no longer visible. Although this alteration is reversible, the second altered CDE is not. In comparing Figs. 12 and 13 above, in particular the area to the left of the house, as well as Figs. 14 to 16 below, the first CDE, "several mature trees in front and side yards," have been irreversibly removed (Figs. 14 to 16).

Figs. 14, 15 and 16: Comparative aerial views of 180 Hwy 33 E, 2006 (top left), 2012 (top right) and 2017 (bottom). (Source: City of Kelowna Map Viewer 2006, 2012 and 2017).

Concluding Remarks

The building at 180 Hwy 33 E, the Sproul Farm House, is one of the few remaining early 20th century structures of the Rutland area. Looking at the Heritage Register of Kelowna, and the Rutland neighbourhood in particular, there are only a handful of recognised buildings. Of these, the Sproul Farm House appears to be the oldest and is among a rare stock of pre-World War 1 housing in the Rutland area (City of Kelowna b). However, being of an old age does not necessarily give a place greater significance. Tangibly, the structure itself has certainly seen better days. A number of changes over the years have

compromised the integrity of the building and it has not been particularly well maintained. There are other recognised houses in the area that are in better condition, with greater integrity, such as the Willis Schell House at 1024 Rutland Road or the Dudgeon Farm House on Leathead Road. However, that is not to say that the Sproul Farm House is unsalvageable or without significance. In fact, its intangible elements seem to be a key importance of the place. In particular, its location and its association with the pioneers, the Sproul brothers. These are important intangible elements of the building, worth celebrating and promoting.

That being said, it is important to note that the inaccurate association of this building with Enoch Mugford, does influence the significance of this place. Considering these were important elements to the original assessment and its associated Statement of Significance, one could argue that its significance is perhaps somewhat less.

Based on the above overview and the data available, the following is the evaluation for 180 Hwy 33 E, following the City of Kelowna's "Kelowna Heritage Register Evaluation Criteria, May 2012."

EVALUATION

Address 180 Hwy 33 E, Kelowna, BC Local Area Rutland

Date of Construction: 1906

A. ARCHITECTURAL HISTORY			**E**	**VG**	**G**	**F/P**
1	Style/Type	**VG: A very good example of a style or type; and a good example of a style or type that is notably early in Kelowna.** *180 Hwy 33 E is a representative example of the straightforward vernacular farmhouses built in the Rutland area and is notably early, being one of the few surviving examples constructed in the early 20th century.*	35	**18**	12	**0**
2	Design	**G: A design which incorporates several special aesthetic or functional attributes.** *Although a modest design, there are a few noteworthy attributes: such as its residential form, scale and massing; its medium-pitch gable roof with secondary cross-gables; its corbelled brick chimney; its narrow V-joint horizontal wood siding; its wood shingles in the upper part of the main gables; and its 1-over-1 double-hung wood sash windows on the upper floor, with plain wood trim.*	30	15	**10**	0
3	Construction	**F/P: An example of no particular significance.**	15	8	5	**0**
4	Designer/Builder	**G: An architect, designer, engineer and/or builder of some importance to building development in the city, province or nation.** *The prolific local builder, M.J. Curts, is said to have built the Sproul Farm House, in addition to other important Kelowna buildings.* *If this is not accurate, this should be F/P: 0.*	15	8	**5**	0
		(Maximum 40)		**28-33**		

Katie Cummer, PhD CAHP – Cummer Heritage Consulting (CHC)

B. CULTURAL HISTORY

1	Historical Association	**G: Connected with a person, group, institution, event or activity that is of moderate importance.** *180 Hwy 33 E is connected to a group of early pioneers to the area, specifically the Sproul brothers, Samuel and Bob, who helped tend one of the first orchards on the Rutland Estate.*	35	18	**12**	0
2	Historical Pattern	**G: A building that provides strong evidence of an historical pattern of local area importance.** *The Sproul Farm House at 180 Hwy 33 E connects to the historical establishment of the Rutland area and its orchards. It is also one of the earliest surviving examples of farm houses in the area.*	30	15	**10**	0
		(Maximum 35)			**22**	

C. CONTEXT

1	Landscape/Site	**G: A landscape which includes one or two important features which are directly related to the building's style, design and history; and an altered but recognizable historical relationship between a building's site and its immediate urban environment or related geographic features.** *The corner lot of 180 Hwy 33 E and the surviving mature trees are of particular importance to the landscape. Much of the surrounding area has dramatically changed from its rural past, although the broader terrain (the topography, hills and mountains) remains.*	15	8	**5**	0
2	Neighbourhood	**G: A building which is not part of a contiguous group of similar style, type or age, but is in an area of compatible use.** *The neighbourhood surrounding 180 Hwy 33 E is no longer rural farmland as was the case historically nor is it a part of a contiguous group of a similar style, type or age. However, it is in an area of compatible use in that it is surrounded by similar residential houses with some commercial use in the vicinity as well.*	20	10	**6**	0
3	Visual/Symbolic	**F/P: A building of no landmark or symbolic significance.**	25	13	8	**0**
		(Maximum 25)		**11**		
		Subtotal		**61-66**		

D. INTEGRITY & CONDITION

F/P: A building with alterations which greatly detract from the style, design, construction or character.	0	-5	-8	**-15**

The lack of on-going maintenance has impacted the condition of the building, which includes the possible growth of moss and/or mold, the deterioration of the defining corbelled brick chimney as well as general decay of its materials, paints and finishes. There have also been a number of alterations and additions to the building over time. It appears that, at a minimum, the following major/minor alterations have been carried out:

- *Late 2000s/early 2010s: additional outdoor shower stall added to the eastern side of the building*
- *Late 2000s/early 2010s: porch enclosed*
- *Late 2000s/early 2010s: railing installed on the western facing upper floor window, possibly for use as a planter, if not as a full balcony*
- *Mid-2010s: a number of the mature trees on site were cut down*

Please note that there was likely additional work done on the house, however, those listed above are what are discernible from the available records and correspondence.

		TOTAL	**46-51**
Evaluation Date	**July 17, 2018**	**Classification Group**	**B**

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CITY OF KELOWNA

BYLAW NO. 11957
Z18-0117 – 145 Sadler Road and 180 & 190 Highway 33 East

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 1 Section 26 Township 26 ODYD Plan 10045, located on Hwy 33 E, Kelowna, BC from the RU6 – Two Dwelling Housing zone to the C4 – Urban Centre Commercial zone;

2. AND THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 2 Section 26 Township 26 ODYD Plan 10045, located on Hwy 33 E, Kelowna, BC from the RU6 – Two Dwelling Housing zone to the C4 – Urban Centre Commercial zone;

3. AND FURTHER THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 3 Section 26 Township 26 ODYD Plan 10045, located on Sadler Rd, Kelowna, BC from the RU1 – Large Lot Housing zone to the C4 – Urban Centre Commercial zone;

4. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Approved under the Transportation Act this

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL

Date:	November 18, 2019		
To:	Council		
From:	City Manager		
Department:	Development Planning		
Application:	DP19-0182	**Owner:**	562957 B.C. Ltd. Inc. No. 0562957
Address:	595 Houghton Road	**Applicant:**	562957 B.C. Ltd. Inc. No. 0562957
Subject:	Development Permit Application		
Existing OCP Designation:	MXR – Mixed Use (Residential / Commercial)		
Existing Zone:	C4 – Urban Centre Commercial		

1.0 Recommendation

THAT Council authorizes the issuance of Development Permit No. DP19-0182 for Lot 5 Section 26 Township 26 ODYD Plan 29795, located at 595 Houghton Road, Kelowna, BC subject to the following:

1. The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";
2. The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";
3. Landscaping to be provided on the land be in accordance with Schedule "C";
4. The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect;

AND THAT the applicant be required to complete the above noted conditions of Council's approval of the Development Permit Application in order for the permits to be issued;

AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

2.0 Purpose

To consider the form and character of an addition and façade improvements to an existing commercial building.

3.0 Development Planning

Development Planning staff are supportive of the proposed addition and façade improvements to the existing commercial building located on the subject property. As indicated in the attached Revitalization Development Permit Guidelines Checklist (Attachment B), this application is in alignment with numerous design guidelines including:

- Use appropriate architectural features and detailing of buildings and landscapes to define area character;
- Convey a strong sense of authenticity through high quality urban design that is distinctive of Kelowna;
- Provide for a scale and massing of buildings that promotes an enjoyable living, pedestrian, working, shopping and service experience; and
- Create open, architecturally-pleasing and accessible building facades to the street.

Overall, this proposal will result in improvements to the existing building and additional landscaping and screening along Houghton Road.

4.0 Proposal

4.1 Project Description

This application proposes a 111 m^2 addition to the east side of an existing commercial building occupied by an animal clinic. The proposed addition is to accommodate additional veterinary staff and to provide more storage space. In addition to adding floor area, the applicant is also proposing updates to the façade of the building including a new finish on the existing brick, cedar siding accents, and new modern signage.

As part of this proposal, the garbage storage bins have been moved towards the north of the site near the entrance on Houghton Road. Locating the garbage bins to this location is intended to minimize conflicts with the adjacent multi-family residential building to the east. To reduce the streetscape impacts of having the garbage storage bins closer to Houghton Road, the applicant is proposing to use Molok containers that are to be screened by signage and landscaping.

4.2 Site Context

The subject property is located on the southeast corner of the Hollywood Road North and Houghton Road intersection. The subject property is centrally located within the Rutland Urban Centre. Adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	RU6 – Two Dwelling Housing	Two Dwelling Housing
East	RM5 – Medium Density Multiple Housing	Multiple Dwelling Housing
South	C4 – Urban Centre Commercial	Health Services
West	RM5 – Medium Density Multiple Housing	Multiple Dwelling Housing

Subject Property Map: 595 Houghton Road

4.3 Zoning Analysis Table

Zoning Analysis Table		
CRITERIA	**C4 ZONE REQUIREMENTS**	**PROPOSAL**
Existing Lot/Subdivision Regulations		
Min. Lot Area	460 m²	1862 m²
Min. Lot Width	40.0 m	±32 m
Min. Lot Depth	30.0 m	55.5 m
Development Regulations		
Max. Floor Area Ratio	1.0	0.19
Max. Site Coverage (buildings)	75%	19%
Max. Height	15.0 m / 4 storeys	4.6 m / 1 storey
Min. Front Yard (north)	0.0 m	±33 m
Min. Side Yard (west)	0.0 m	5.2 m
Min. Side Yard (east)	2.0 m	4.9 m
Min. Rear Yard (south)	0.0 m	5.9 m
Other Regulations		
Min. Parking Requirements	8	14
Min. Bicycle Parking	2 class I / 3 class II	2 class I / 3 class II
Min. Loading Space	1	1

5.0 Application Chronology

Date of Application Received: September 6, 2019

Report prepared by: Arlene Janousek, Planner 1
Reviewed by: Laura Bentley, Urban Planning & Development Policy Manager
Approved for Inclusion: Terry Barton, Development Planning Department Manager

Attachments:

Attachment A: Draft Development Permit DP19-0182
Attachment B: Revitalization Development Permit Guidelines Checklist

Development Permit
DP19-0182

This permit relates to land in the City of Kelowna municipally known as

595 Houghton Road

and legally known as

Lot 5 Section 26 Township 26 ODYD Plan 29795

and permits the land to be used for the following development:

ATTACHMENT A
This forms part of application
DP19-0182
Planner Initials AJ
City of Kelowna
DEVELOPMENT PLANNING

Animal Clinics, Minor

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

Date of Council Decision:

Decision By: Council

Development Permit Area: Revitalization Development Permit Area

Existing Zone: C4 – Urban Centre Commercial

Future Land Use Designation: MXR – Mixed Use (Residential / Commercial)

This is NOT a Building Permit.

In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

NOTICE

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner: 562957 B.C. Ltd. Inc. No. 0562957

Applicant: 562957 B.C. Ltd. Inc. No. 0562957

_____ _____

Terry Barton
Community Planning Department Manager
Planning & Development Services

Date

1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

2. CONDITIONS OF APPROVAL

a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";

b) The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";

c) Landscaping to be provided on the land be in accordance with Schedule "C"; and

d) The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect.

This Development Permit is valid for two (2) years <u>from the date of approval</u>, with no opportunity to extend.

3. PERFORMANCE SECURITY

As a condition of the issuance of this Permit, Council is holding the security set out below to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Developer and be paid to the Developer or his or her designate if the security is returned. The condition of the posting of the security is that should the Developer fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the Municipality may use enter into an agreement with the property owner of the day to have the work carried out, and any surplus shall be paid over to the property own of the day. Should the Developer carry out the development permitted by this Permit within the time set out above, the security shall be returned to the Developer or his or her designate. There is filed accordingly:

a) An Irrevocable Letter of Credit **OR** certified cheque in the amount of **$14,989.06**

Before any bond or security required under this Permit is reduced or released, the Developer will provide the City with a statutory declaration certifying that all labour, material, workers' compensation and other taxes and costs have been paid.

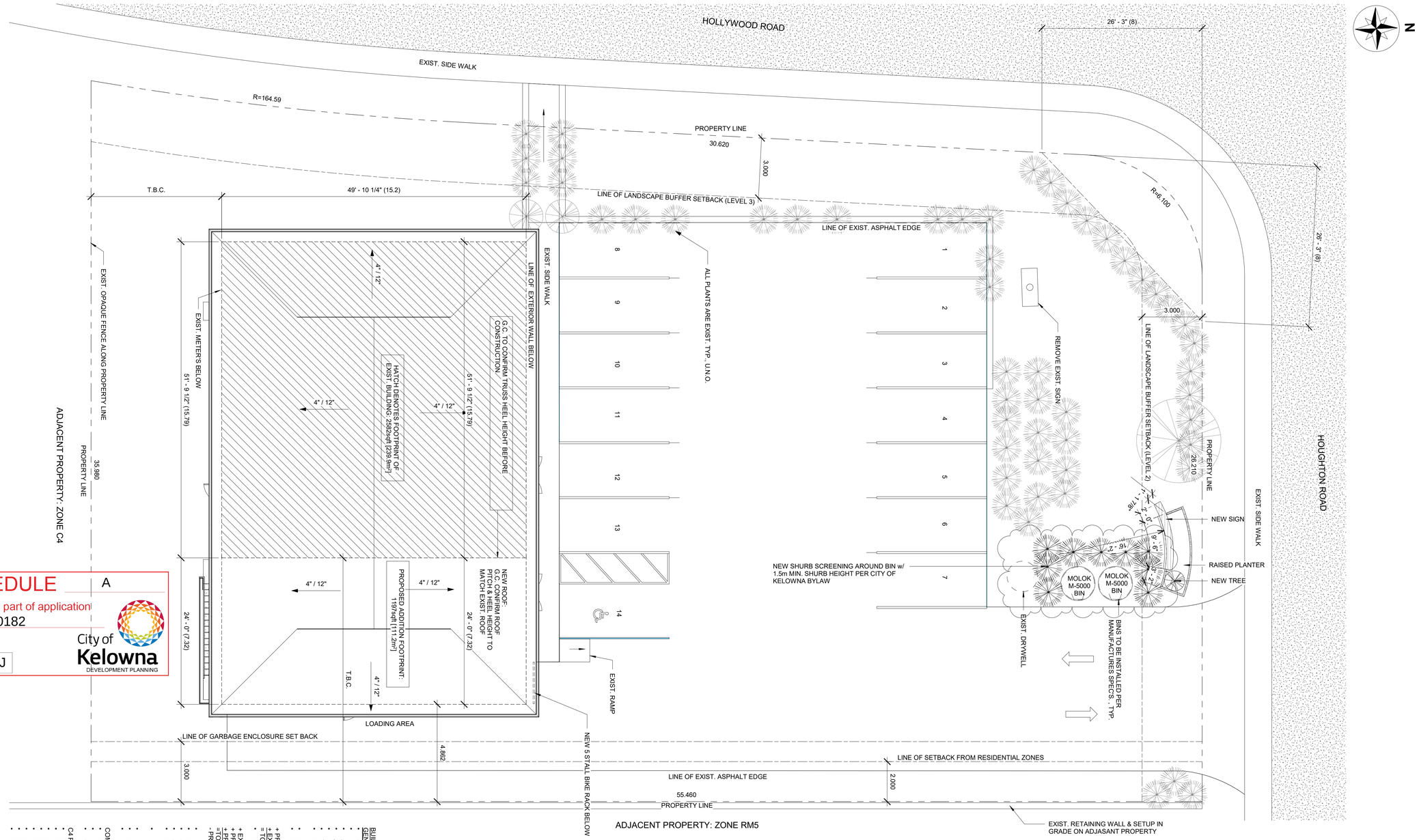
4. INDEMNIFICATION

Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:

a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

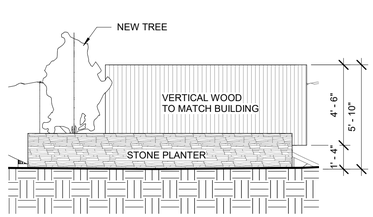
The PERMIT HOLDER is the <u>CURRENT LAND OWNER</u>.
Security shall <u>ONLY</u> be returned to the signatory of the
Landscape Agreement or their designates.



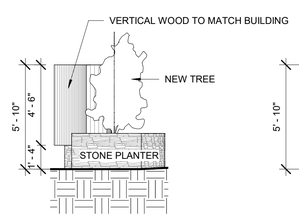
1 SITE PLAN
1/8" = 1'-0"

SCHEDULE A
This forms part of application
DP19-0182
Planner Initials AJ
City of Kelowna
DEVELOPMENT PLANNING

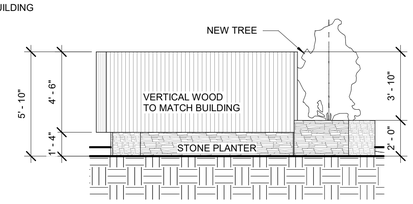
BUILDING NOTES
GENERAL ZONING AND SITE INFORMATION
CIVIC ADDRESS: 595 HOUGHTON RD.
LEGAL: PLAN 29795, LOT 5
ZONING: C4 - COMMERCIAL
AUTHORITY: CITY OF KELOWNA
BUILDING TYPE: PART 9
TOTAL LOT AREA: 19,965sqft [1854.8m²]
MAXIMUM PARCEL COVERAGE: 75%
EXISTING BUILDING FOOTPRINT: 2582sqft [239.9m²]
EXISTING % LOT COVERAGE: 12.9%
+ PROPOSED ADDITION FOOTPRINT: 1197sqft [111.2m²]
+ EXISTING BUILDING FOOTPRINT: 2582sqft [239.9m²]
= TOTAL PROPOSED SITE COVERAGE: 3779sqft [351.1m²]
PROPOSED % LOT COVERAGE: 18.9%
+ EXISTING FLOOR AREA: 2582sqft [239.9m²]
+ PROPOSED ADDITION FLOOR AREA: 1197sqft [111.2m²]
+ PROPOSED BASEMENT FLOOR AREA: 6728sqft [63.0m²]
= TOTAL PROPOSED FLOOR AREA (GFA): 4457sqft [414.1m²]
- PROPOSED FLOOR AREA RATIO (FAR): 0.19 (NOT INCL. BASEMENT)
FRONT YARD SETBACK: 0.0m
REAR YARD SETBACK: 0.0m
SIDE YARD SETBACK: ADJACENT RESIDENTIAL ZONES: 2.0m
SIDE YARD SETBACK FOR FLANKING STREET: 0.0m
MAXIMUM HEIGHT OF BUILDING: 15.0m (4 STOREYS)
MAX. FLOOR AREA RATIO IS 1.0
PARKING SETBACK FROM A STREET/FRONT: 2.0m
PARKING SETBACK FROM A SIDE/REAR: 1.5m
GARBAGE ENCLOSURE SETBACK FROM A RESIDENTAIL ZONE: 3.0m
COMMERCIAL PARKING %
FULL SIZE STALLS (6.0m x 2.5m): 50%
MEDIUM SIZE STALLS (4.8m x 2.3m): 40%
COMPACT/MOTORCYCLE (3.4m x 2.6m): 10%
C4 PARKING STALL CALC'S.
REQUIRED PARKING STALLS: 1.75/100m² GFA x 406.7m² GFA = 8 STALLS
REQUIRED LOADING AREAS: 1 NEW + 1 EXIST = 2 AREA
REQUIRED COMMERCIAL SPACE: 1.4/100m² GFA x 406.7m² GFA = 1 AREA
PROVIDED LOADING AREAS: 1 EXIST + 0 NEW = 1 AREA
REQUIRED CLASS I BICYCLE PARKING: 1 PER 10 EMPLOYEES = 2 STALLS
REQUIRED CLASS II BICYCLE PARKING: 0.6/100m² GFA x 406.7m² GFA = 3 STALLS
PROVIDED CLASS I BICYCLE PARKING: 0.6/100m² GFA = 2 STALLS
PROVIDED CLASS II BICYCLE PARKING: 0.6/100m² GFA = 3 STALLS



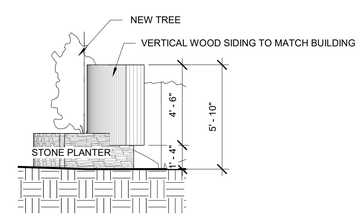
2 SIGN NORTH
1/4" = 1'-0"



3 SIGN EAST
1/4" = 1'-0"



4 SIGN SOUTH
1/4" = 1'-0"



5 SIGN WEST
1/4" = 1'-0"

Blue Vision
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'BRINGING YOUR VISIONS TO LIFE.'

Blue Vision
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3448 Cougar Rd
West Kelowna, BC
V4T 2G9
250.864.6666
blue.vision@hotmail.com

Issue Schedule

Issue Number	Description	Date
1	Prelim. Layout Review	01.08.19
2	Permit	16.08.19
3	Permit	03.09.19
4	Bin Revision	02.10.19

E.F.R. Construction Management
Ed Ruff
870 Toovey Rd.
Kelowna,BC V1X 6R1
250.862.7593
ERuff@shaw.ca

Rutland Pet Hospital Addition

595 Houghton Road
Kelowna, BC

SITE PLAN

SCALE:
AS NOTED

DRAWN BY:	CHECKED BY:
DS	BV
DATE: 02.10.19	PROJECT: 2019-044

A1.0

WALL LEGEND

	FW1	FROST/FOUNDATION WALL CONSTRUCTION: 1. CONTINUOUS PARGING &/OR FLASHING ABV. GRADE 2. SPRAY ON DAMPPROOFING 3. 8" REINFORCED CONCRETE WALL 4. 1½" RIGID INSULATION 5. 2x4 STUDS @ 24" o/c FILLED w/ R14 BATT INSULATION 6. 6 MIL. POLY. VAPOUR BARRIER 7. ½" G.W.B. NOTE: -CONCRETE TO BE 32 MPA -PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.
	FW2	FROST/FOUNDATION WALL CONSTRUCTION: 1. CONTINUOUS PARGING &/OR FLASHING ABV. GRADE 2. SPRAY ON DAMPPROOFING 3. 10" REINFORCED CONCRETE WALL 4. 1½" TYPE 2 EPS RIGID INSULATION 5. 2x4 STUDS @ 24" o/c FILLED w/ R14 BATT INSULATION 6. 6 MIL. POLY. VAPOUR BARRIER 7. ½" G.W.B. NOTE: -CONCRETE TO BE 32 MPA -PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.
	FW3	FROST/FOUNDATION WALL CONSTRUCTION: 1. CONTINUOUS PARGING &/OR FLASHING ABV. GRADE 2. SPRAY ON DAMPPROOFING 3. 10" REINFORCED CONCRETE WALL 4. 3"(R15.3) XPS RIGID INSULATION MIN. 48" DEPTH OR T/O FTG. 5. BACKFILL NOTE: -CONCRETE TO BE 32 MPA -50% RIGID INSULATION THERMAL BRAEAK AT SLAB ON GRADE
	W1-2	W1 TO W2 REFER TO EXTERIOR WALL ASSEMBLIES (NO HATCH)
	W3	PLUMBING/MECH. PARTITIONS (HEX HATCHED): 1. 2x6 STUDS @ 24" o/c (SPF#2 OR BETTER) 2. 1/2" G.W.B. (BOTH SIDES) NOTE: PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.
		UNTAGGED - NON-LOAD BEARING PARTITIONS (NO HATCH): 1. 2x4 STUDS @ 24" o/c (SPF#2 OR BETTER) 2. 1/2" G.W.B. (BOTH SIDES)
		GREY & UNTAGGED - EXISTING & TO REMAIN (NO HATCH): 1. REFER TO EXISTING PLANS FOR STRUCTURE
		DASHED & UNTAGGED - REMOVED EXISTING WALL(NO HATCH): 1. REFER TO EXISTING PLANS FOR STRUCTURE

ROOF CONSTRUCTION

TYPICAL ROOF CONSTRUCTION:
1. ROOFING FINISH PER ELEVATIONS
2. 7/16" OSB ROOF SHEATHING (c/w H-CLIPS)
3. ENGINEERED ROOF TRUSSES OR RAFTERS PER MANUF'RS SPEC.
4. INSULATION STOPS (AS REQ'D)
5. R50 BLOWN CELLULOSE
6. 6 MIL. POLY. AS VAPOR BARRIER
7. G.W.B. HANGERS TO MATCH EXIST. SERVICE CAVITY
8. 1/2" G.W.B. CEILING

NOTE: PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.

EAVE CONSTRUCTION:
ALUMINUM VENTED SOFFITS
5" ALUMINUM EAVESTROUGH RECESSED INTO ENGINEERED TRUSSES
(DRAIN TO SLOPE TO WATER LEADER THROUGH SOFFIT)
EAVESTROUGH SCREEN
- PROVIDE ICE DAMN EAVE PROTECTION MEMBRANE
- ATTIC VENTILATION TO BE 1/300 OF INSULATED AREA

WALL CONSTRUCTION

REFER TO EXTERIOR WALL ASSEMBLIES OR WALL LEGEND

FLOOR CONSTRUCTION

F1 - TYPICAL FLOOR CONSTRUCTION:
1. FLOOR FINISH AS PER OWNER/CONTRACTOR
2. MAX. 2" CONCRETE TOPPING
3. 3/4" T&G OSB SUBFLOOR (GLUED & SCREWED)
4. FLOOR JOISTS AS PER FLOOR PLAN
5. 1/2" G.W.B. (**INTERIOR EXPOSURE ONLY**)

NOTE: INSULATE ALL JOISTS SPACES @ EXTERIOR PERIMETER WALLS

BEAMS & STRUCTURAL FRAMING:
AS PER PLAN OR AS REQUIRED BY FLOOR/TRUSS SUPPLIER

FOUNDATION CONSTRUCTION

REFER TO STRUCTURAL DRAWINGS AS REQ'D.

FROST WALL CONSTRUCTION:
1. CONTINUOUS PARGING &/OR FLASHING ABV. GRADE
2. DRAIN MAT
3. SPRAY ON DAMPPROOFING
4. 8" OR 10" PER PLAN CONCRETE WALL c/w 2 ROWS 10mm (10M) REBAR TOP & BOTTOM, 10mm (10M) REBAR @ 24" o/cVERT. & HORIZ.
5. R15 RIGID INSULATION w/ 50% @ SLAB

NOTE: CONCRETE TO BE 32 MPA

FOOTING CONSTRUCTION:
1. 16"x8" CONCRETE STRIP FOOTING
2. c/w 2"x4" KEYWAY OR 10M DOWELS @ 24" o/c
3. 2 ROWS 16mm DIA. (15M) REBAR REINFORCEMENT.
4. FOOTING TO BE PLACED ON UNDISTURBED NATIVE SOIL OR ENGINEERED SOIL.
5. U/S OF FOOTING TO BE A MINIMUM OF 2'-0" BELOW GRADE (FROST DEPTH).

NOTE: CONCRETE TO BE 32 MPA

DAMPPROOFING:
1. ASPHALT EMULION APPLIED TO OUTSIDE OF CONCRETE WALL TO GRADE LEVEL
2. CONTINUOUS 4"ø PERFORATED DRAIN PIPE AROUND FOOTING
3. 6" CRUSHED ROCK (DRAIN ROCK)

NOTE: ALL SNAP TIES TO BE TARRED

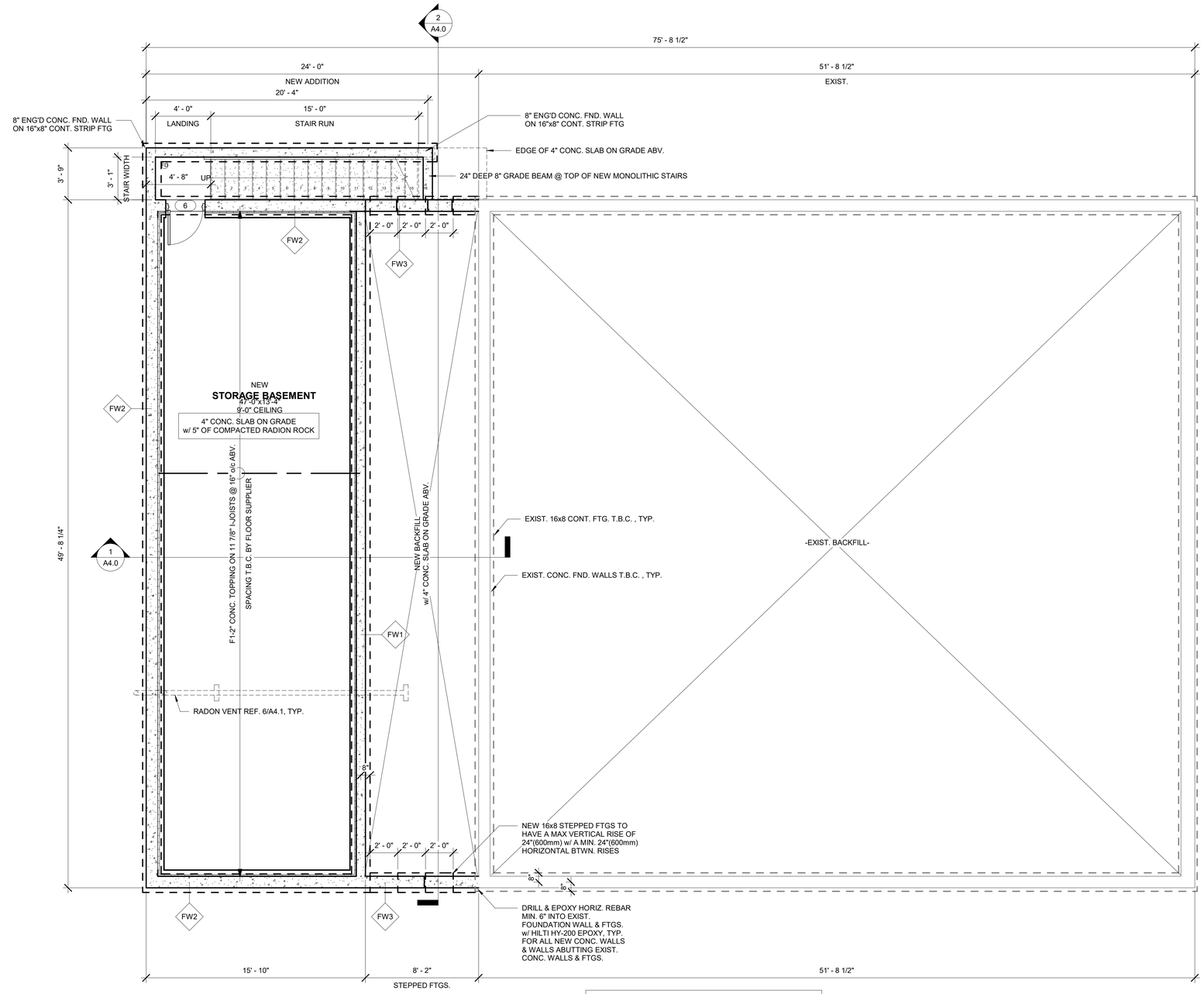
SLAB CONSTRUCTION

S1 - FLOOR SLAB:
1. 4" CONCRETE SLAB
2. 6X6-10/10 W.W.M. OR 10mm (10M) REBAR @ 24" o/c e/w
3. 6 MIL. POLY VAPOUR BARRIER
4. 5" MIN. COMPACTED GRANULAR BASE

NOTE: CONCRETE TO BE 32MPA
- TIE TO FOUNDATION/ GRADE BEAM w/ 10mm DOWELS

DRIVEWAY/SIDEWALKS:
4" CONCRETE SLAB
10mm REBAR @ 24" o/c e/w

NOTE: CONCRETE TO BE 25MPA
- TIE TO FOUNDATION/ GRADE BEAM w/ 10mm DOWELS
- PILES AS NOTED ABOVE



PROPOSED BASEMENT FLOOR AREA: 678sqft [63.0m²]

1 BASEMENT & FOUNDATION PLAN
1/4" = 1'-0"

N

DOOR SCHEDULE					
Door Type	Count	Type	Width	Height	Comments
1	1	72"x82"- Exterior- NEW Double Swing Curtain Wall Door	6' - 0"	6' - 10"	To Match Existing Curtain Wall Double Door (Size to be confirmed by G.C. before ordering/construction)
2	2	36" x 80" - Exterior - New Single Swing Door	3' - 0"	6' - 8"	
3	12	36" x 80" - Interior - New Single Swing Door	3' - 0"	6' - 8"	
4	1	36" x 80" - Interior - New Single Pocket Door	3' - 0"	6' - 8"	
5	4	30" x 80" - Interior - New Dog Kennel Door	2' - 6"	6' - 8"	
6	1	30" x 80" - Exterior - New Single Swing Door	2' - 6"	6' - 8"	

Grand total: 21

WINDOW SCHEDULE					
Window Type	Count	Type	Width	Height	Comments
1	1	48"x48" - Double Casement Window	4' - 0"	4' - 0"	

Grand total: 1

SCHEDULE A
This forms part of application
DP19-0182
City of Kelowna
DEVELOPMENT PLANNING
Planner Initials AJ

GENERAL CONSTRUCTION NOTES:
1. 1/2 WALL STAIR GUARDS TO BE 42" HIGH OR AS PER PLAN.
2. PROVIDE MIN 2" FRONT ENTRY STEP.
3. ALL BATHROOMS TO BE MECHANICALLY VENTILATED TO PROVIDE AT LEAST 1 AIR CHANGE PER HOUR.
4. 20"x28" INSULATED/WEATHER STRIPPED ATTIC ACCESS HATCH W/ INSULATION SHAFT
5. PROVIDE 2" (MIN.) CLEARANCE AROUND MECHANICAL FLUES TO ALL COMBUSTIBLE CONSTRUCTION.
6. PROVIDE FLASHING ABOVE EXTERIOR DOORS AND WINDOW OPENINGS WHERE REQUIRED
7. NON-HARDENING EXTERIOR CAULKING COMPOUND TO BE USED OVER & AROUND ALL EXTERIOR OPENINGS / IE. SERVICE ELECT., ETC.
8. ALL EXTERIOR/ INTERIOR LOAD BEARING LINTELS TO BE 2-PLY 2x10 U.N.O.
9. ALL EXTERIOR SIDING/ STUCCO FINISHES TO BE 8" MIN. ABOVE FINISHED GRADE
10. ALL WORKMANSHIP AND MILLWORK TO CONFORM TO THE STANDARDS OF GOOD BUILDING PRACTICES, & CURRENT LOCAL BUILDING CODES WITH ALL RESPECTS
11. WHERE THERE IS A DISCREPANCY BETWEEN THE PLANS AND THE BUILDING CODE, THE BUILDING CODE SHALL TAKE PRECEDENCE.

Blue Vision Drafting & Design
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Drafting & Design
3448 Cougar Rd
West Kelowna, BC
V4T 2G9
250.864.6666
blue.vision@hotmail.com

Issue Schedule

Issue Number	Description	Date
1	Prelim. Layout Review	01.08.19
2	Permit	16.08.19
3	Permit	03.09.19

E.F.R. Construction Management
Ed Ruff
870 Toovey Rd.
Kelowna,BC V1X 6R1
250.862.7593
ERuff@shaw.ca

Rutland Pet Hospital Addition

595 Houghton Road
Kelowna, BC

BASEMENT CONSTRUCTION / FOUNDATION PLAN

SCALE:
AS NOTED

DRAWN BY:	CHECKED BY:
DS	BV
DATE: 03.09.19	PROJECT: 2019-044

A3.0

WALL LEGEND

	FW1	FROST/FOUNDATION WALL CONSTRUCTION: 1. CONTINUOUS PARGING &/OR FLASHING ABV. GRADE 2. SPRAY ON DAMPPROOFING 3. 8" REINFORCED CONCRETE WALL 4. 1½" RIGID INSULATION 5. 2x4 STUDS @ 24" o/c FILLED w/ R14 BATT INSULATION 6. 6 MIL. POLY. VAPOUR BARRIER 7. ½" G.W.B. NOTE: -CONCRETE TO BE 32 MPA -PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.
	FW2	FROST/FOUNDATION WALL CONSTRUCTION: 1. CONTINUOUS PARGING &/OR FLASHING ABV. GRADE 2. SPRAY ON DAMPPROOFING 3. 10" REINFORCED CONCRETE WALL 4. 1½" TYPE 2 EPS RIGID INSULATION 5. 2x4 STUDS @ 24" o/c FILLED w/ R14 BATT INSULATION 6. 6 MIL. POLY. VAPOUR BARRIER 7. ½" G.W.B. NOTE: -CONCRETE TO BE 32 MPA -PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.
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	W1-2	W1 TO W2 REFER TO EXTERIOR WALL ASSEMBLIES (NO HATCH)
	W3	PLUMBING/MECH. PARTITIONS (HEX HATCHED): 1. 2x6 STUDS @ 24" o/c (SPF#2 OR BETTER) 2. 1/2" G.W.B. (BOTH SIDES) NOTE: PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.
		UNTAGGED - NON-LOAD BEARING PARTITIONS (NO HATCH): 1. 2x4 STUDS @ 24" o/c (SPF#2 OR BETTER) 2. 1/2" G.W.B. (BOTH SIDES)
		GREY & UNTAGGED - EXISTING & TO REMAIN (NO HATCH): 1. REFER TO EXISTING PLANS FOR STRUCTURE
		DASHED & UNTAGGED - REMOVED EXISTING WALL(NO HATCH): 1. REFER TO EXISTING PLANS FOR STRUCTURE

ROOF CONSTRUCTION

TYPICAL ROOF CONSTRUCTION:
1. ROOFING FINISH PER ELEVATIONS
2. 7/16" OSB ROOF SHEATHING (c/w H-CLIPS)
3. ENGINEERED ROOF TRUSSES OR RAFTERS PER MANUF'RS SPEC.
4. INSULATION STOPS (AS REQ'D)
5. R50 BLOWN CELLULOSE
6. 6 MIL. POLY. AS VAPOR BARRIER
7. G.W.B. HANGERS TO MATCH EXIST. SERVICE CAVITY
8. 1/2" G.W.B. CEILING

NOTE: PROVIDE TYPE 'X' GYPSUM IN WASHROOMS AND GARAGE.

EAVE CONSTRUCTION:
ALUMINUM VENTED SOFFITS
5" ALUMINUM EAVESTROUGH RECESSED INTO ENGINEERED TRUSSES
(DRAIN TO SLOPE TO WATER LEADER THROUGH SOFFIT)
EAVESTROUGH SCREEN
- PROVIDE ICE DAMN EAVE PROTECTION MEMBRANE
- ATTIC VENTILATION TO BE 1/300 OF INSULATED AREA

WALL CONSTRUCTION

REFER TO EXTERIOR WALL ASSEMBLIES OR WALL LEGEND

FLOOR CONSTRUCTION

F1 - TYPICAL FLOOR CONSTRUCTION:
1. FLOOR FINISH AS PER OWNER/CONTRACTOR
2. MAX. 2" CONCRETE TOPPING
3. 3/4" T&G OSB SUBFLOOR (GLUED & SCREWED)
4. FLOOR JOISTS AS PER FLOOR PLAN
5. 1/2" G.W.B. (**INTERIOR EXPOSURE ONLY**)

NOTE: INSULATE ALL JOISTS SPACES @ EXTERIOR PERIMETER WALLS

BEAMS & STRUCTURAL FRAMING:
AS PER PLAN OR AS REQUIRED BY FLOOR/TRUSS SUPPLIER

FOUNDATION CONSTRUCTION

REFER TO STRUCTURAL DRAWINGS AS REQ'D.

FROST WALL CONSTRUCTION:
1. CONTINUOUS PARGING &/OR FLASHING ABV. GRADE
2. DRAIN MAT
3. SPRAY ON DAMPPROOFING
4. 8" OR 10" PER PLAN CONCRETE WALL c/w 2 ROWS 10mm (10M) REBAR TOP & BOTTOM, 10mm (10M) REBAR @ 24" o/cVERT. & HORIZ.
5. R15 RIGID INSULATION w/ 50% @ SLAB

NOTE: CONCRETE TO BE 32 MPA

FOOTING CONSTRUCTION:
1. 16"x8" CONCRETE STRIP FOOTING
2. c/w 2"x4" KEYWAY OR 10M DOWELS @ 24" o/c
3. 2 ROWS 16mm DIA. (15M) REBAR REINFORCEMENT.
4. FOOTING TO BE PLACED ON UNDISTURBED NATIVE SOIL OR ENGINEERED SOIL.
5. U/S OF FOOTING TO BE A MINIMUM OF 2'-0" BELOW GRADE (FROST DEPTH).

NOTE: CONCRETE TO BE 32 MPA

DAMPPROOFING:
1. ASPHALT EMULION APPLIED TO OUTSIDE OF CONCRETE WALL TO GRADE LEVEL
2. CONTINUOUS 4"ø PERFORATED DRAIN PIPE AROUND FOOTING
3. 6" CRUSHED ROCK (DRAIN ROCK)

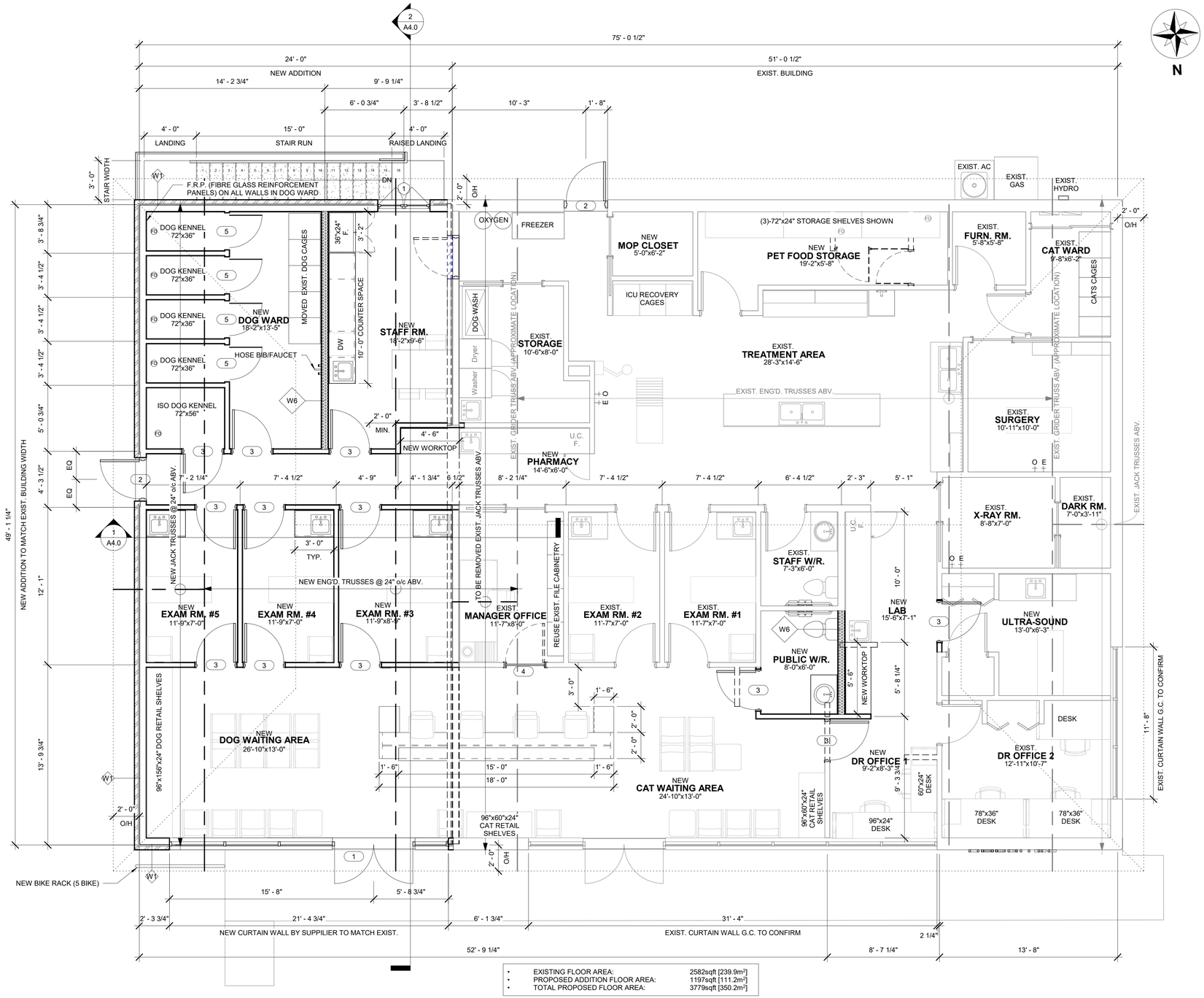
NOTE: ALL SNAP TIES TO BE TARRED

SLAB CONSTRUCTION

S1 - FLOOR SLAB:
1. 4" CONCRETE SLAB
2. 6X6-10/10 W.W.M. OR 10mm (10M) REBAR @ 24" o/c e/w
3. 6 MIL. POLY VAPOUR BARRIER
4. 5" MIN. COMPACTED GRANULAR BASE

NOTE: CONCRETE TO BE 32MPA
- TIE TO FOUNDATION/ GRADE BEAM w/ 10mm DOWELS

DRIVEWAY/SIDEWALKS:
4" CONCRETE SLAB
10mm REBAR @ 24" o/c e/w
NOTE: CONCRETE TO BE 25MPA
- TIE TO FOUNDATION/ GRADE BEAM w/ 10mm DOWELS
- PILES AS NOTED ABOVE



- EXISTING FLOOR AREA: 2582sqft [239.9m²]
- PROPOSED ADDITION FLOOR AREA: 1197sqft [111.2m²]
- TOTAL PROPOSED FLOOR AREA: 3779sqft [350.2m²]

1 MAIN FLOOR CONSTRUCTION PLAN
1/4" = 1'-0"

DOOR SCHEDULE

Door Type	Count	Type	Width	Height	Comments
1	1	72"x82"- Exterior- NEW Double Swing Curtain Wall Door	6' - 0"	6' - 10"	To Match Existing Curtain Wall Double Door (Size to be confirmed by G.C. before ordering/construction)
2	2	36" x 80" - Exterior - New Single Swing Door	3' - 0"	6' - 8"	
3	12	36" x 80" - Interior - New Single Swing Door	3' - 0"	6' - 8"	
4	1	36" x 80" - Interior - New Single Pocket Door	3' - 0"	6' - 8"	
5	4	30" x 80" - Interior - New Dog Kennel Door	2' - 6"	6' - 8"	
6	1	30" x 80" - Exterior - New Single Swing Door	2' - 6"	6' - 8"	

Grand total: 21

WINDOW SCHEDULE

Window Type	Count	Type	Width	Height	Comments
1	1	48"x48" - Double Casement Window	4' - 0"	4' - 0"	

Grand total: 1

SCHEDULE A
This forms part of application
DP19-0182
Planner Initials AJ
City of Kelowna
DEVELOPMENT PLANNING

GENERAL CONSTRUCTION NOTES:
1. 1/2 WALL STAIR GUARDS TO BE 42" HIGH OR AS PER PLAN.
2. PROVIDE MIN 2" FRONT ENTRY STEP.
3. ALL BATHROOMS TO BE MECHANICALLY VENTILATED TO PROVIDE AT LEAST 1 AIR CHANGE PER HOUR.
4. 20"x28" INSULATED/WEATHER STRIPPED ATTIC ACCESS HATCH W/ INSULATION SHAFT
5. PROVIDE 2" (MIN.) CLEARANCE AROUND MECHANICAL FLUES TO ALL COMBUSTIBLE CONSTRUCTION.
6. PROVIDE FLASHING ABOVE EXTERIOR DOORS AND WINDOW OPENINGS WHERE REQUIRED
7. NON-HARDENING EXTERIOR CAULKING COMPOUND TO BE USED OVER & AROUND ALL EXTERIOR OPENINGS / IE. SERVICE ELECT., ETC.
8. ALL EXTERIOR/ INTERIOR LOAD BEARING LINTELS TO BE 2-PLY 2x10 U.N.O.
9. ALL EXTERIOR SIDING/ STUCCO FINISHES TO BE 8" MIN. ABOVE FINISHED GRADE
10. ALL WORKMANSHIP AND MILLWORK TO CONFORM TO THE STANDARDS OF GOOD BUILDING PRACTICES, & CURRENT LOCAL BUILDING CODES WITH ALL RESPECTS
11. WHERE THERE IS A DISCREPANCY BETWEEN THE PLANS AND THE BUILDING CODE, THE BUILDING CODE SHALL TAKE PRECEDENCE.

Blue Vision Drafting & Design
'BRINGING YOUR VISIONS TO LIFE.'

Blue Vision
Drafting & Design
3448 Cougar Rd
West Kelowna, BC
V4T 2G9
250.864.6666
blue.vision@hotmail.com

Issue Schedule

Issue Number	Description	Date
1	Prelim. Layout Review	01.08.19
2	Permit	16.08.19
3	Permit	03.09.19

E.F.R. Construction Management
Ed Ruff
870 Toovey Rd.
Kelowna,BC V1X 6R1
250.862.7593
ERuff@shaw.ca

Rutland Pet Hospital Addition

595 Houghton Road
Kelowna, BC

MAIN FLOOR CONSTRUCTION PLAN

SCALE:
AS NOTED

DRAWN BY:	CHECKED BY:
DS	BV
DATE: 03.09.19	PROJECT: 2019-044

A3.1

EXTERIOR FINISH SCHEDULE	
A	ASPHALT SHINGLES (TO MATCH EXIST.)
B	BRICK FACADE (TO MATCH EXIST.)
C	VERTICAL CEDAR SIDING
D	NEW CURTAIN WALL (TO MATCH EXIST. T.B.C. BY G.C.)
E	ALUMINUM FLASHING ABOVE WINDOWS & DOORS
F	1x8 FASCIA (TO MATCH EXIST.)
G	ALUMINUM SOFFIT (VENTED)
H	ALUMINUM EAVESTROUGH (TO MATCH EXIST.)
J	STEELGUARD RAIL (MIN. 3'-6" HEIGHT)
K	BIKE RACK (5 BIKE SIZE)
L	NEW 4" CONCRETE SLAB ON GRADE
M	GABLE ROOF VENT TO MATCH EXIST.
N	HAND RAIL (3'-0" HEIGHT)
P	PARGING OVER CONCRETE

NOTES:
- ALL FINISHINGS TO BE NEW MATERIALS.
- EXTERIOR NOTES TYPICAL

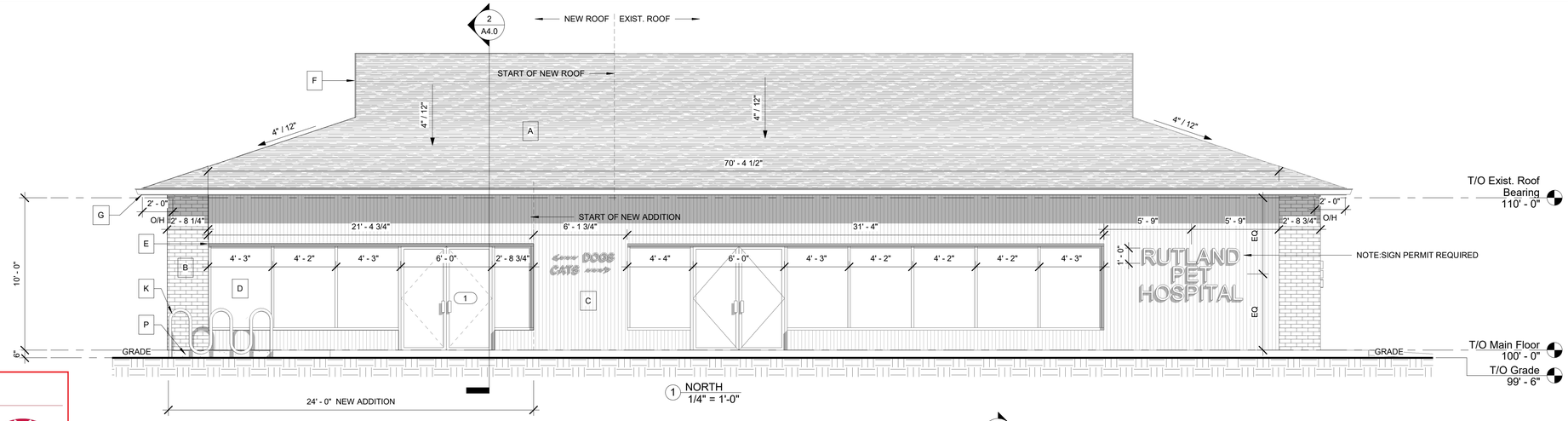
SCHEDULE B

This forms part of application

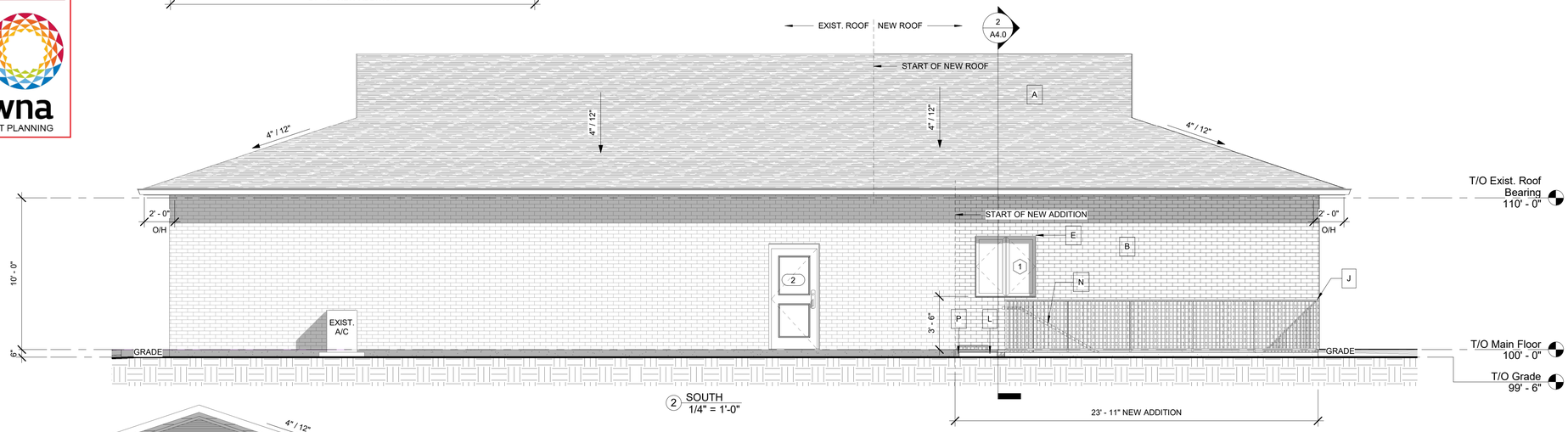
DP19-0182

City of Kelowna
DEVELOPMENT PLANNING

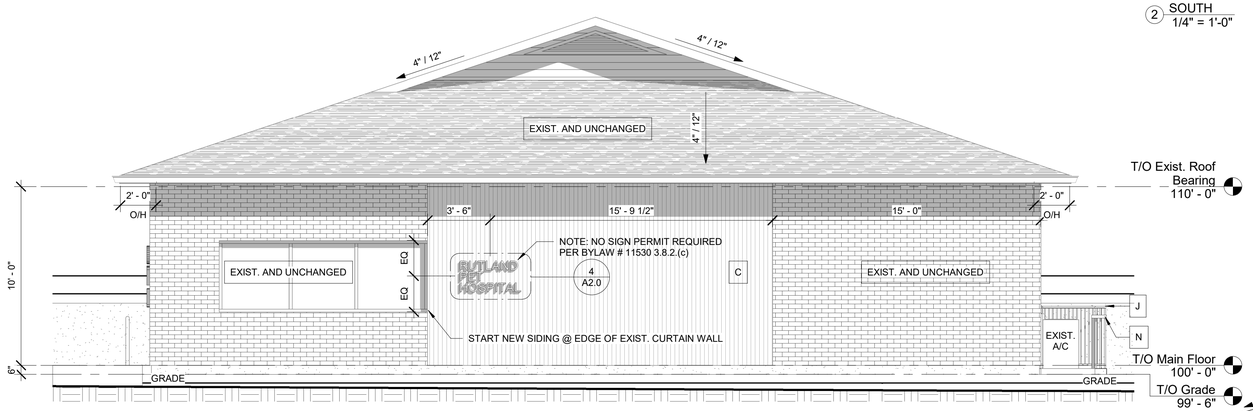
Planner Initials AJ



1 NORTH
1/4" = 1'-0"

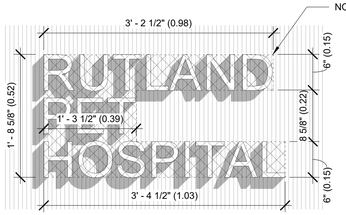


2 SOUTH
1/4" = 1'-0"

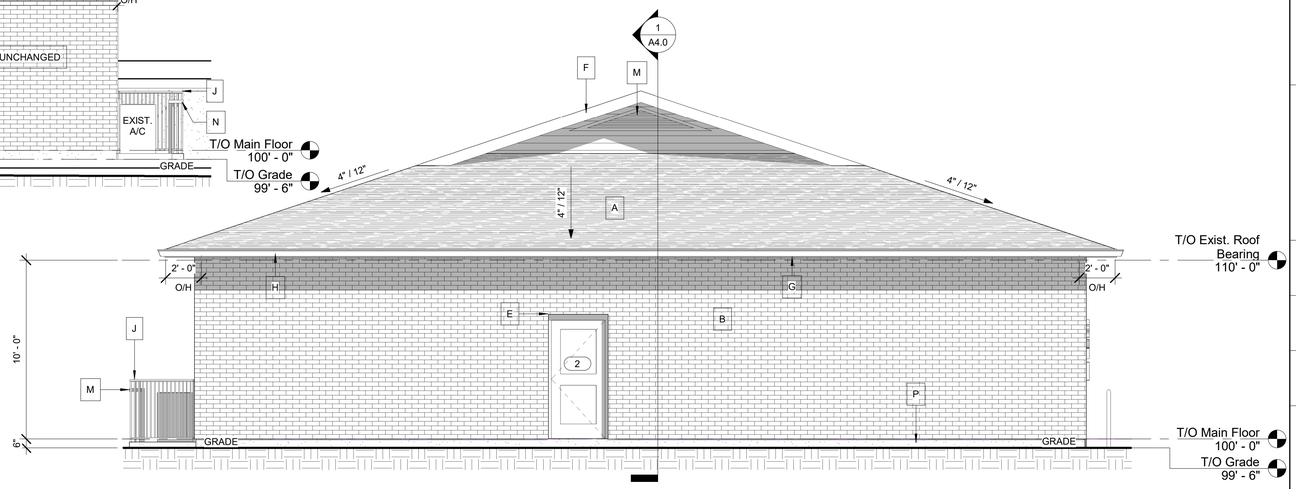


3 WEST
1/4" = 1'-0"

NO SIGN PERMIT REQUIRED PER BYLAW # 2011530 3.8.2.(c).(ii)



4 WEST SIGN CALLOUT
1" = 1'-0"



5 EAST
1/4" = 1'-0"

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E.F.R. Construction Management
Ed Ruff
870 Toovey Rd.
Kelowna,BC V1X 6R1
250.862.7593
ERuff@shaw.ca

Rutland Pet Hospital Addition

595 Houghton Road
Kelowna, BC

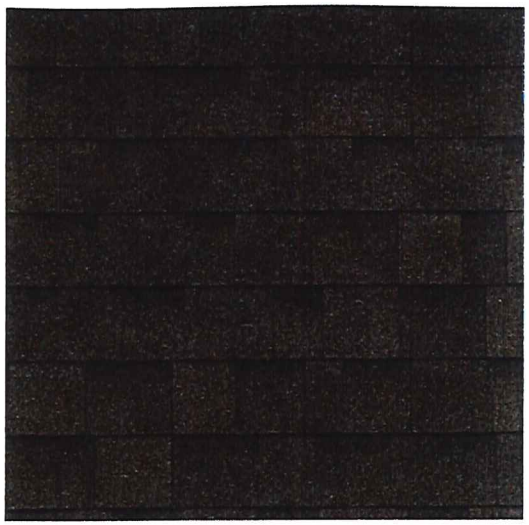
ELEVATIONS

SCALE:
AS NOTED

DRAWN BY:	CHECKED BY:
DS	BV
DATE: 03.09.19	PROJECT: 2019-044

A2.0

Proposed Material and Colours
For 595 Houghton Road



Iko Cambridge Driftwood Shingle

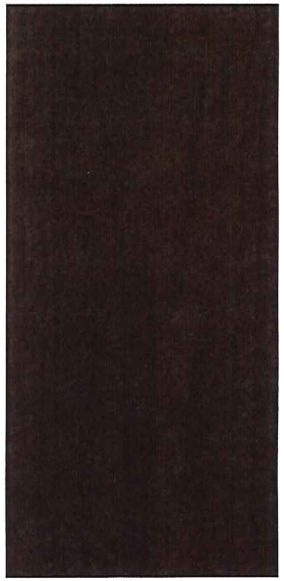
Matches existing shingles

Existing Coloured Bricks

Cedar Siding For Accents

Existing Door and Windows
New ones to match

Spraynet Brick Stain
Colour Kahki



Behr
Transparent Wood Stain
Colour Padre Brown

Cedar Semi

SCHEDULE B

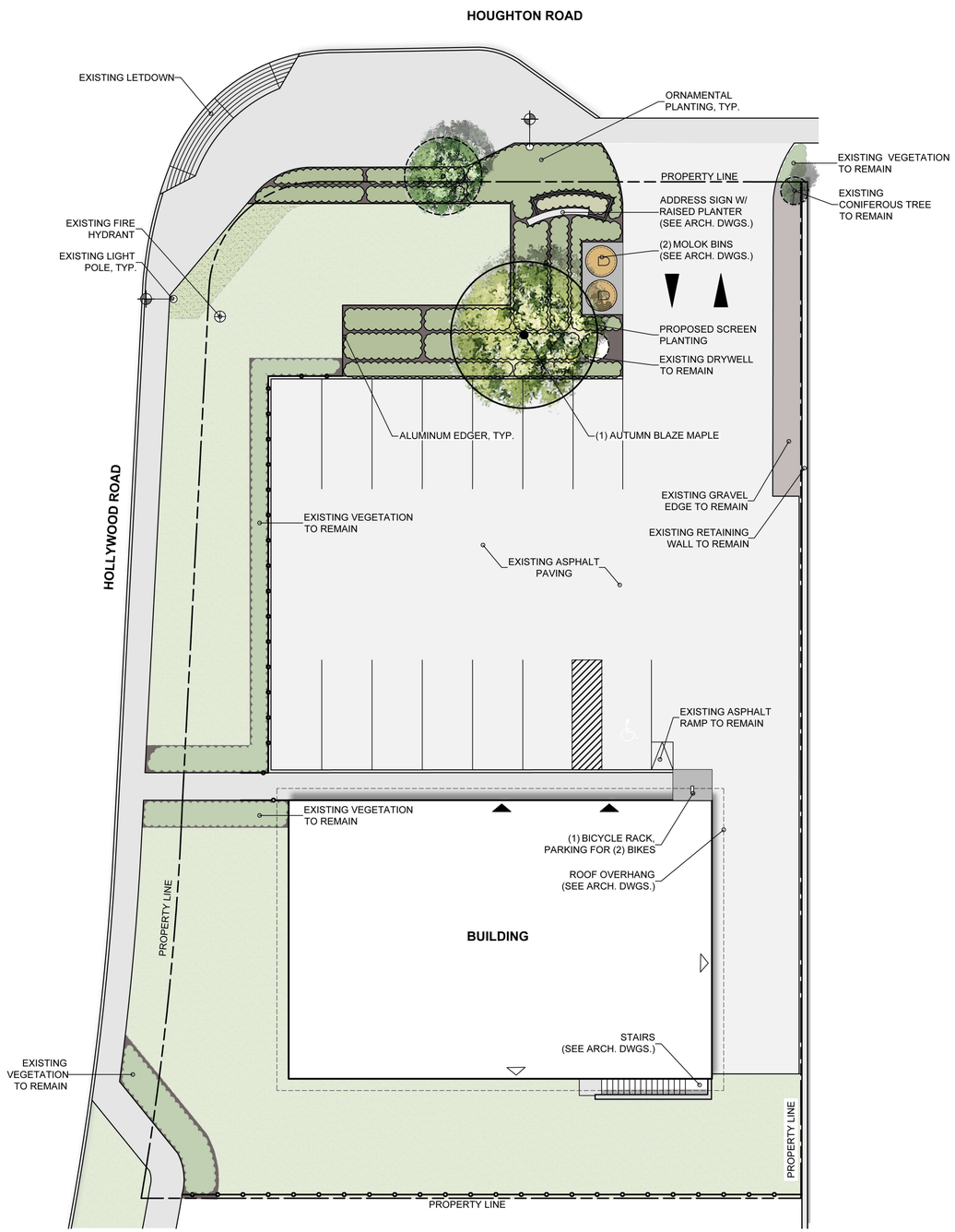
This forms part of application

DP19-0182

City of Kelowna
DEVELOPMENT PLANNING

Planner Initials AJ

/VOLUMES/BENCH/400 – PROJECTS/2019/19-021_RUTLANDPETHOSPITAL_SCREENINGPLAN/440 – PRODUCTION/442 – DRAWINGS/00-CURRENT/00_19-021_CONCEPT_LAYOUTS_M.DWG



DEVELOPMENT PERMIT NOTES:

A PLANT MATERIALS AND CONSTRUCTION METHODS SHALL CONFORM TO MINIMUM STANDARDS ESTABLISHED IN THE LATEST EDITION OF THE CANADIAN LANDSCAPE STANDARDS, PUBLISHED BY C.L.N.A. AND C.S.L.A. AS WELL AS THE CITY OF KELOWNA LANDSCAPE STANDARDS IN BYLAW 7900.
B THE LANDSCAPE DESIGN DESIGNATED HEREIN IS CONCEPTUAL BUT REFLECTS THE MINIMUM CITY OF KELOWNA FORM AND CHARACTER REQUIREMENTS
C PLANT MATERIAL SELECTIONS ARE CONCEPTUAL ONLY. FINAL PLANTING SELECTIONS MAY VARY DEPENDING UPON AVAILABILITY AT THE TIME OF CONSTRUCTION.
D TREES SHALL BE INSTALLED IN DEFINED SOIL PITS OR PLANTING BED AREAS. ADEQUATE SOIL VOLUME SHALL BE PROVIDED BASED ON THE SPECIFIED TREE SPECIES AND LOCATION.
E ORNAMENTAL SHRUB, GRASS AND PERENNIAL CLUSTERS ARE TO BE PLACED WITHIN DEFINED PLANTING BEDS. ALL PLANTING BEDS SHALL HAVE A MIN. OF 450mm (18") IMPORTED GROWING MEDIUM AND 75mm (3") OF COMPOSTED MULCH OR APPROVED EQUAL.
F TURF AREAS SHALL BE LOW WATER USE 'NO. 1 PREMIUM' SOD WITH A MIN. OF 150mm (6") IMPORTED GROWING MEDIUM
G A HIGH EFFICIENCY IRRIGATION SYSTEM SHALL BE INSTALLED FOR ALL ORNAMENTAL LANDSCAPE AREAS AND SHALL CONFORM TO THE CITY OF KELOWNA'S IRRIGATION STANDARDS IN BYLAW 7900.

LEGEND:

PROPOSED DECIDUOUS TREE
EXISTING DECIDUOUS TREE TO REMAIN
CONCRETE PAD
EXISTING CONCRETE SIDEWALK TO REMAIN
PROPOSED SODDED LAWN
EXISTING SODDED LAWN TO REMAIN
EXISTING CHAINLINK FENCE TO REMAIN
MAIN BUILDING ENTRY / EXIT
SECONDARY BUILDING ENTRY / EXIT
VEHICULAR ENTRY / EXIT

PLANT LIST:

TREES

Botanical Name	Common Name	Size/Spacing	Root
Acer x freemanii 'Jeffersred'	Autumn Blaze maple	6cm Cal.	B&B

SHRUBS

Botanical Name	Common Name	Size/Spacing	Root
Berberis thunbergii 'Bailone'	Ruby Carousel barberry	#01 Cont./1.0m O.C.	Potted
Cornus sericea 'Kelseyi'	Kelsey dogwood	#03 Cont./0.75m O.C.	Potted
Paxistima canbyi	Cliff green	#01 Cont./1.2m O.C.	Potted
Philadelphus lewisii 'Snowbelle'	Snowbelle mockorange	#05 Cont./1.0m O.C.	Potted
Pinus mugo 'Slowmound'	Dwarf mugo pine	#03 Cont./0.75m O.C.	Potted
Spiraea betulifolia 'Tor'	Tor birchleaf spirea	#02 Cont./1.0m O.C.	Potted
Taxus media 'Hicksii'	Hick's yew	#05 Cont./1.0m O.C.	Potted

PERENNIALS

Botanical Name	Common Name	Size/Spacing	Root
Eutrochium dubium 'Little Joe'	Little Joe pye weed	#01 Cont./0.9m O.C.	Potted
Rudbeckia fulgida 'Goldsturm'	Goldsturm coneflower	#01 Cont./0.6m O.C.	Potted
Sedum spectabile 'Autumn Fire'	Autumn Fire stonecrop	#01 Cont./0.5m O.C.	Potted

GRASSES

Botanical Name	Common Name	Size/Spacing	Root
Miscanthus sinensis 'Gracillimus'	Maiden hair grass	#01 Cont./1.2m O.C.	Potted
Calamagrostis brachytricha	Korean feather reed grass	#01 Cont./0.75m O.C.	Potted

0 1 2 3 4 5 10 15m
SCALE: 1:150

NORTH

NOT FOR CONSTRUCTION

SCHEDULE C
This forms part of application
DP19-0182
City of Kelowna
DEVELOPMENT PLANNING
Planner Initials AJ

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REVISIONS / ISSUED:

NO.	DATE	DESCRIPTION
1	OCT 29/19	ISSUED FOR DP APPLICATION

BENCH
| 4-1562 Water Street, Kelowna BC V1Y 1J7 |
| t 250 860 6778 |

CLIENT:
RUTLAND PET HOSPITAL
KELOWNA, B.C.

PROJECT:
RUTLAND PET HOSPITAL SCREENING PLAN
KELOWNA, B.C.

SHEET TITLE
LANDSCAPE PLAN

DESIGN BY	LS
DRAWN BY	YY
CHECKED BY	XS
PROJECT NO.	19-021
SCALE	1:150

SHEET NO.
L-1

Revitalization Development Permit Area

Consideration has been given to the following guidelines as identified in Section 14.B. of the City of Kelowna Official Community Plan relating to Revitalization Development Permit Areas:

REVITALIZATION DEVELOPMENT PERMIT AREA	YES	NO	N/A
Relationship to the Neighbourhood and Street			
Does the proposal maintain the established or envisioned architectural character of the neighbourhood?	✓		
Do developments adjacent to non-revitalization areas create an appropriate transition?			✓
Are spaces for pedestrian friendly amenities, such as street furniture, included on site?		✓	
Is the ratio of streetwall height to street width less than 0.75:1?	✓		
Does the building frontage occupy the entire length of the street, without drive aisles or other dead zones?		✓	
Building Design			
Are architectural elements aligned from one building to the next?			✓
Are the effects of shadowing on public areas mitigated?			✓
Are doors or windows incorporated into at least 75% of street frontage?		✓	
Do proposed buildings have an identifiable base, middle and top?			✓
Are windows, entrances, balconies and other building elements oriented towards surrounding points of interest and activity?	✓		
Are architectural elements such as atriums, grand entries and large ground-level windows used to reveal active interior spaces?	✓		
Are buildings designed with individual entrances leading to streets and pathways rather than with mall style entrances and internal connections?			✓
For multiple unit residential projects, is ground level access for first storey units provided?			✓
Are buildings finished with materials that are natural, local, durable and appropriate to the character of the development?	✓		
Are prohibited materials such as vinyl siding, reflective or non-vision glass, plastic, unpainted or unstained wood, and concrete block not used in the design?	✓		
Are stucco and stucco-like finishes omitted as a principal exterior wall material?	✓		
Are vents, mechanical rooms/equipment and elevator penthouses integrated with the roof or screened with finishes compatible with the building's design?			✓
View Corridors			
Are existing views preserved and enhanced?			✓
Vehicular Access and Parking			
Are at-grade and above-grade parking levels concealed with façade treatments?			✓
Are garage doors integrated into the overall building design?			✓

REVITALIZATION DEVELOPMENT PERMIT AREA	YES	NO	N/A
Are pedestrian entrances more prominent features than garage doors and vehicle entrances?	✓		
Is surface parking located to the rear of the building or interior of the block?		✓	
Are truck loading zones and waste storage areas screened from public view?	✓		
Do parking lots have one shade tree per four parking stalls?		✓	
Are pedestrian connections provided within and between parking lots?			✓
Are driving, parking, pedestrian and cycling areas distinguished through changes in colour or pattern of paving materials?			✓
Signage			
Is signage design consistent with the appearance and scale of the building?	✓		
Are corporate logos on signs complimentary to the overall building character?	✓		
Is signage lighting minimized?	✓		

CITY OF KELOWNA

BYLAW NO. 11499
Z17-0045 - 1869 Maple Street

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 1, District Lot 14, ODYD, Plan 2683 located on Maple Street, Kelowna, B.C., from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing 1 with Carriage House zone.

2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 16th day of October, 2017.

Considered at a Public Hearing on the 7th day of November, 2017.

Read a second and third time by the Municipal Council this 7th day of November, 2017.

Approved under the Transportation Act this 15th day of November, 2017.

_____Audrie Henry_____
(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

_____
Mayor

_____
City Clerk

CITY OF KELOWNA

BYLAW NO. 11949

Z19-0062 – 535 Gramiak Road

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 4 Section 22 Township 26 Osoyoos Division Yale District Plan 29137, located on Gramiak Road, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone;

2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 21st day of October, 2019.

Considered at a Public Hearing on the 5th day of November, 2019.

Read a second and third time by the Municipal Council this 5th day of November, 2019.

Approved under the Transportation Act this 6th day of November, 2019.

Blaine Garrison

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

Report to Council

Date:	November 18, 2019
To:	Council
From:	City Manager
Subject:	Transportation Master Plan: Options Development
Department:	Integrated Transportation

Recommendation:

THAT Council receive for information, the report from the Strategic Transportation Planning Manager dated, November 18, 2019 regarding draft options for the Transportation Master Plan (TMP).

Purpose:

To provide Council with an overview of the projects, policies and programs (collectively referred to as "options") under consideration for the Transportation Master Plan.

Background:

Coordination with Other Plans: Development of the TMP is occurring in coordination with development of the 2040 Official Community Plan (OCP) and the 2040 Servicing Plan and Financing Strategy. These plans are being developed in parallel using Imagine Kelowna as a foundation, and will work together to support our growing City, while minimizing future challenges. While population growth will necessitate substantial future investment to maintain Kelowna's quality of life, the 2040 OCP endorsed Growth Scenario will help mitigate future costs by focusing transportation and infrastructure investments in locations that benefit a high number of people and yield strong returns on investment. The TMP is working to identify the transportation investments that will be needed to service the OCP endorsed Growth Scenario between now and 2040.

Project Timeline:
Development of the Transportation Master Plan – our Kelowna as we Move was launched in 2018 and is being developed in five phases. Phase 1 began by developing a vision and goals for the Transportation Master Plan (TMP), derived from Imagine Kelowna and presented to the public during spring 2018.

TMP vision:
"Kelowna will be a city with vibrant urban centres where people and places are conveniently connected by diverse transportation options that help us transition from our car-centric culture"

Phase 2 involved coordination with the 2040 OCP to identify an endorsed Growth Scenario and assess the future impacts to the transportation network as a result of that scenario. Phase 2 concluded with the

publication of an Existing and Future Conditions Technical Report, which was presented to Council on August 12, 2019, and is available on the project website at: kelowna.ca/transportation2040. The Existing and Future Conditions Technical Report helped to provide a comprehensive understanding of Kelowna's existing and projected future transportation system performance, challenges and opportunities, for all modes, in 2040 under the endorsed Growth Scenario. The report found that, even though the endorsed growth scenario will help to reduce the amount of driving *per person* in the future, still the total amount of driving (and associated traffic congestion) will substantially increase if all of Kelowna's future residents continue to drive as much as they do today.

To keep Kelowna moving, the report noted that it will be necessary to shift as many future trips as possible to transportation modes that can move more people through the same amount of space (such as walking, biking, transit, carpooling, and personal electric mobility devices). This will help prioritize road space for trips that must be made by driving, while giving Kelowna residents more choices for getting around. In total, 30 future challenges and opportunities[1] were identified based on the review of existing and future conditions. Each of the 30 challenges and opportunities were used to identify potential options for consideration in the TMP (in conjunction with public input), which is the focus of Phase 3.

TMP Development Timeline

This report serves to launch Phase 3 of the TMP, which includes the development and evaluation of potential projects, policies and programs (collectively referred to as "options") for consideration in the TMP. An overview of the option development, screening and refinement process is provided, as well as

[1] See Chapter 4 of the TMP Existing and Future Conditions Technical Report available online at: kelowna.ca/transportation2040

a description of the upcoming public engagement. The report ends with a description of next steps, including the option evaluation process.

TMP Option Development

Options for consideration in the TMP were identified from a wide variety of sources, including existing City plans and policies, staff analysis of existing and future conditions, and input from stakeholders and the public.

- For existing plans and polices, several plans[2] were reviewed to identify projects, policies and programs that have yet to be implemented and are still relevant. Additionally, projects, policies and programs that are being considered as part of the Okanagan Gateway Transportation Study, and the Regional Transportation Plan (both of which are currently underway) will also be considered in the TMP.
- For staff analysis, two approaches were taken to identify options for consideration in the TMP. The first approach was to identify options that address problems – both current problems and those anticipated based on future traffic flows, as identified by staff through the review of existing and future conditions. The second approach was to review the TMP vision and then identify the options needed to achieve that vision by 2040. Options identified through both processes were included in the options "long list."
- For input from stakeholders and the public, ideas for options were gathered using numerous techniques and methods throughout the entire TMP planning process. These include the recent Kelowna 2040 Neighborhood Expo engagement and online interactive map, as well as stakeholder interviews held in spring 2019[3], a joint OCP/TMP Community Stakeholder Workshop held in winter 2018[4], and the TMP Phase 1 Vision and Goals engagement held in spring 2018. Ideas for projects, policies and programs provided from the public and stakeholders during all these engagements were included in the options "long list" under consideration.

Kelowna 2040 Public Engagement

As part of the fall 2019 Kelowna 2040 Neighborhood Expo joint public engagement for the OCP, TMP and 20 Year Servicing Plan and Financing Strategy, residents had the opportunity to review the findings of the TMP Existing and Future Conditions Technical Report and share ideas for options they would like to see considered in the TMP. The public was invited to place pins on a map (both in-person and via an online interactive map) to share ideas for walking, biking, transit, driving, shared mobility and other types of improvements. In total, 156 people visited the map and 65 different contributors provided 242 option

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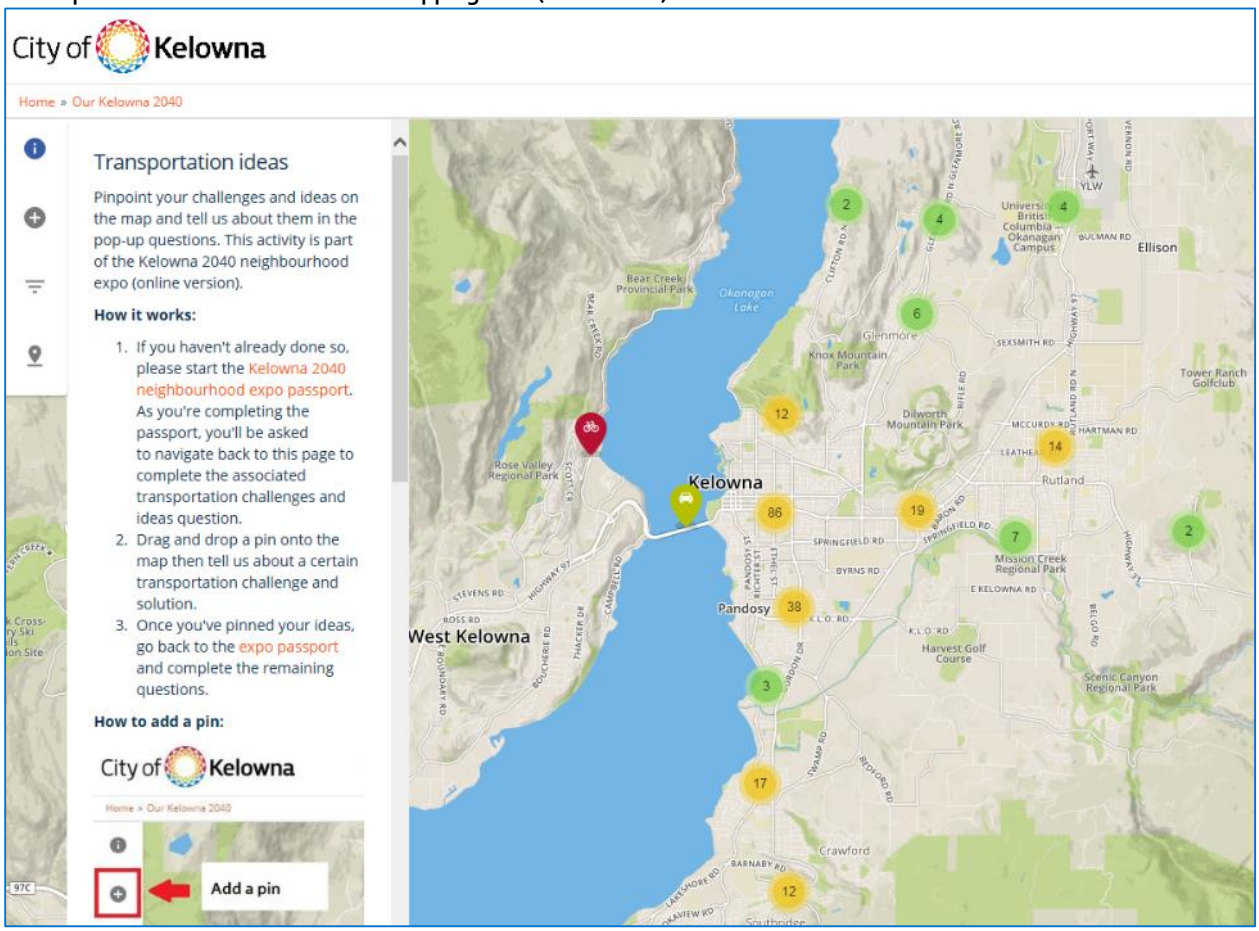
[2] *Plans reviewed included the 10-Year Capital Plan (2019 – 2028), Capri-Landmark Urban Centre Plan (2019), Central Okanagan Transit Future Action Plan (2018), Community Climate Action Plan (2018), Kelowna's Pedestrian and Bicycle Master Plan (2016), Urban Centres Roadmap (2016), Kelowna's Community for All Plan (2016), Hospital Area Plan (2016), and the Central Okanagan Clean Air Strategy (2015).*

[3] *Joint RTP/TMP stakeholder interviews were held with representatives from Interior Health, RCMP, Emergency Health Services, Kelowna Fire Department, Kelowna General Hospital, School District 23, the BC Trucking Association, Okanagan College, the Urban Development Institute, the Smarter Growth Partnership, University of British Columbia Central Okanagan (UBCO), Kelowna International Airport, and the Ministry of Transportation and Infrastructure.*

[4] *The joint OCP/TMP Community Stakeholder Workshop was held Dec 6, 2018. The following organizations received an invitation to attend the workshop: Kelowna Chamber of Commerce, Downtown Kelowna Business Association, Pandosy Village Business Association, Uptown Rutland Business Association, Tourism Kelowna, the Urban Development Institute, School District 23, Okanagan College, UBCO, Interior Health, Westbank First Nation, Okanagan Indian Band, BC Transit, Ministry of Transportation and Infrastructure, Canadian Homebuilders Association, and the Central Okanagan Economic Development Commission.*

ideas. Option ideas provided by the public during this engagement were incorporated into the list of options being considered for the TMP. A full summary of the Kelowna 2040 Neighborhood Expo engagement is being prepared and will be provided to Council in a separate report from the Policy and Planning Department, anticipated in December.

TMP Option Ideas: Online Interactive Mapping Tool (Screenshot)



Option Screening and Sorting

In total, over 700 individual option ideas were identified. To prepare the options for the next phase of engagement, the options were screened to remove redundancies and then bundled for evaluation. For example, individual ideas for sidewalk improvements were bundled into a sidewalk capital program option, or individual ideas for the same road segment were bundled into a roadway improvement package for that road. This process resulted in over 400 options for evaluation.

Next, the options were sorted into nine categories. The categories allow for the options to be categorized both by mode and by function. This recognizes that while some options fit neatly into modal categories (e.g. a new bikeway, or a transit exchange) others are more complex as they would provide improvements for multiple modes at once along the same corridor (e.g. a roadway expansion that also provides bicycle, transit, and pedestrian improvements).

Descriptions of each category are provided below and in Attachment 1:

- **Maintenance and Renewal (all modes):** This category includes investments to maintain the quality and safety of existing infrastructure, such as repaving roads, snow clearing, repairing sidewalks, repairing bus stops, street sweeping, and landscaping.
- **Education and Incentive Programs**: Not all investments in transportation involve building new infrastructure. This category includes policies and programs focused on reducing future traffic congestion. Examples include incentives for walking, biking, and transit; education campaigns and events; and working with major employers to encourage teleworking and flexible work hours; among others.
- **Shared Mobility and New Technology:** Emerging technologies such as ride-hailing (eg. Uber & Lyft) and shared vehicles (eg. carshare, bikeshare, etc.) are changing how people get around. This category includes investments in programs and infrastructure to help establish new transportation options while managing their impacts.
- **Neighborhood Streets:** The OCP endorsed Growth Scenario focuses much of Kelowna's future growth in the Core Area. Many of the neighbourhood streets in these areas lack basic pedestrian infrastructure and drainage. This category includes investments that would improve the attractiveness and walkability of residential streets, through adding sidewalks, crossing improvements, lighting, traffic calming and street trees.
- **Biking:** As more residents move to the Core Area, more future trips will be within biking distance. Providing a safe and well-connected network of bikeways would make it easier and more convenient for people of all ages and abilities to get around by bike.
- **Transit:** This category includes investments focused on improving Kelowna's transit service, including options to increase service hours as well as infrastructure (eg. bus stops, exchanges, dedicated transit lanes). This category also includes an option to study the feasibility of a frequent transit connection between the Downtown Kelowna, Pandosy, and Capri-Landmark Urban Centres, where future trips between these destinations are anticipated to more than double by 2040. The study would look at various alignment alternatives as well as potential transit technologies[5].
- **Multimodal Urban Corridors:** Recognizing that the mix of transportation demand in our Urban Centres and Core Area will change as these areas grow, options in this category will help to rebalance our urban transportation corridors to better accommodate convenient travel by multiple modes, balancing the needs of people walking, biking, taking transit and driving.
- **Road Improvements and Connections:** The long-term vision from Imagine Kelowna is to give people more options to get around besides driving. Still, cars and trucks will play a vital role in Kelowna for the foreseeable future. This category includes projects focused on increasing the safety, efficiency, and capacity of Kelowna's road network for vehicles.
- **Clement / Highway 33 Extension:** This category refers to the project idea of extending Clement from Spall east to either Highway 33 or McCurdy. Options for this project include simply protecting a corridor for future use, constructing an arterial with at-grade intersections, or constructing a freeway-style corridor with grade-separated intersections.

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[5] Note that the potential for higher order transit along Harvey Avenue (which is owned by the Ministry of Transportation and Infrastructure) is being discussed separately, as part of the Regional Transportation Plan.

It is important to note that each category includes projects that provide benefits for multiple TMP goals. For example, projects that improve safety can be found primarily in the following categories: Road Improvements and Connections, Maintenance and Renewal, Neighborhood Streets, Multimodal Urban Corridors, Biking, and Education and Incentives. Projects that improve travel choices can be found primarily in the Shared Mobility and New Technology, Education and Incentives, Neighborhood Streets, Multimodal Urban Corridors, Biking, and Transit categories.

Phase 3 Public Engagement

To share the options with the public and collect feedback, residents will be invited to build their own TMP using an online budget allocator tool. This approach will allow residents to review the options and identify their preferred levels of investment (basic, medium or high) for each category, with a "business as usual" transportation budget provided as a reference. Descriptions of the investment levels associated with each category are provided in Attachment 1. Information regarding the outcomes and budget implications associated with each selection will be presented. Overall, the intent of the engagement exercise is to put residents in the planner's seat and allow them to grapple with key trade-offs relating to transportation benefits and costs.

One of the key issues the tool will allow residents to explore is that to achieve the Imagine Kelowna and TMP vision by 2040 and align with the 2040 OCP endorsed Growth Scenario, changes will be needed in how the City invests in transportation. Our world is changing; how the City prioritizes and allocates budget for transportation to meet the needs of residents today, will not necessarily meet the needs of residents tomorrow. The TMP will consider new ways of allocating the transportation budget to maximize return on investment towards effectively servicing the endorsed OCP Growth Scenario and achievement of Imagine Kelowna and the TMP Vision.

The nine categories in the tool have investment package options that can make major shifts toward achievement of the TMP Vision, or work in the opposite direction. As residents use the tool, they will have the opportunity to dial investment levels up or down in each category, to show their allocation preferences. Additionally, respondents will not be confined to the "business as usual" budget; rather they will be able to increase or decrease the transportation budget to select their preferred investment packages, while being informed of trade-offs associated with property taxation, alignment with the endorsed 2040 OCP Growth Scenario, and the pace at which Imagine Kelowna and the TMP Vision are achieved.

In addition to the budget allocator tool, the TMP Phase 3 public engagement activities will include in-person events, a stakeholder workshop, targeted student outreach, and a promotional and educational campaign to help increase response rates. Residents will be able to provide input from November 18 - December 6. In tandem, a statistically significant telephone survey on transportation in Kelowna will be conducted in mid-late November. Engagement details can be accessed at kelowna.ca/transportation2040.

Technical Evaluation

To determine how well the options perform against the TMP Vision and Goals, a technical evaluation will be conducted. The options will be evaluated according to their policy alignment, and their benefits and costs will be assessed, seeking to maximize return on investment. Staff will conduct the evaluation using a Multiple Accounts Evaluation (MAE) framework and the regional travel demand model.

Next Steps:
Moving forward, staff will review the results of the technical evaluation and use the information to build a comprehensive recommended package of transportation projects, policies and programs to service the endorsed 2040 OCP Growth Scenario. The recommended suite of options will be informed by the public engagement activities and will be brought to Council for consideration in spring 2020. This will be considered in tandem with the ongoing refinement of the draft OCP Future Land Use Map to ensure alignment with the endorsed Growth Strategy.

Once Council has endorsed the package of options for inclusion in the TMP, the project team will develop an implementation strategy that will include project phasing, costs and funding, in coordination with the 20 Year Servicing Plan and Financial Strategy. This will be compiled into a draft Transportation Master Plan and circulated for public comment prior to bringing the TMP to Council for adoption. Final adoption of the Transportation Master Plan is anticipated in summer 2020. The recommendations of the Transportation Master Plan will be coordinated with future updates of the 10-Year Capital Plan and Infrastructure Plan.

Internal Circulation:
Divisional Director, Infrastructure
Divisional Director, Planning & Development Services
Communications
Development Planning
Development Services
Policy & Planning
Infrastructure Operations
Utility Services

Considerations not applicable to this report:
Legal/Statutory Authority
Legal/Statutory Procedural Requirements
Financial/Budgetary Considerations
External Agency/Public Comments

Submitted by: M. VanZerr, Strategic Transportation Planning Manager

Reviewed and approved by: R. Villarreal, Integrated Transportation Department Manager

Approved for inclusion: Alan Newcombe, Divisional Directors, Infrastructure

Attachment 1 – TMP Options and Investment Packages
Attachment 2 – TMP Options Development Presentation

Our Kelowna as we Move: 2040 Transportation Master Plan

Option Categories and Investment Packages

Over 400 potential options have been identified for consideration as part of Transportation Master Plan. To more easily evaluate and engage on the options, they have been sorted into nine categories and bundled into basic, medium, and high investment packages. For the most part, the investment packages are additive. For example, the 'highest investment package' includes all the options in both the 'medium' and 'basic' investment packages. The options described in this memo have been incorporated into an online budget allocator tool, which will be available to the public from Nov 19th – Dec 6th at kelowna.ca/transportation2040. The tool will allow residents to select their preferred investment package for each category, informed by anticipated outcomes and costs.

One of the key issues the tool will allow residents to explore is that to achieve the Imagine Kelowna and TMP vision by 2040 and align with the 2040 OCP endorsed Growth Scenario, changes will be needed in how the City invests in transportation. Our world is changing; how the City prioritizes and allocates budget for transportation to meet the needs of residents today, will not necessarily meet the needs of residents tomorrow. The TMP will consider new ways of allocating the transportation budget to maximize return on investment towards effectively servicing the endorsed OCP Growth Scenario and achievement of Imagine Kelowna and the TMP Vision.

The nine categories in the tool have investment package options that can make major shifts toward achievement of the TMP Vision, or work in the opposite direction. As residents use the tool, they will have the opportunity to dial investment levels up or down in each category, to show their allocation preferences. Additionally, respondents will not be confined to the "business as usual" budget; rather they will be able to increase or decrease the transportation budget to select their preferred investment packages, while being informed of trade-offs associated with property taxation, alignment with the endorsed 2040 OCP Growth Scenario, and the pace at which Imagine Kelowna and the TMP Vision are achieved.

The estimated costs of each investment package are presented in the tool as an average annual budget for the City over the next twenty years and does not include funding from senior governments[1]. Where applicable, the budget allocator tool will provide links to maps that provide more information about the projects in each investment package.

TMP Option Categories and Investment Packages

1. **Maintenance and Renewal (all modes):** Before spending money on new projects, it is important to consider the financial requirements of maintaining the City's existing transportation infrastructure. The investments in this category maintain the condition of existing infrastructure, such as repaving roads, fixing potholes, repairing sidewalks, landscaping, street sweeping, and snow clearing.

    - Basic Investment Package: The City maintains current levels of spending on maintaining existing roads, sidewalks, pathways, snow clearing and sweeping. Due to the existing infrastructure deficit and the demands of a growing population, this level of investment is not sufficient and will result in a deteriorating condition of our transportation infrastructure by 2040.

[1] Projects that are contingent upon grant funding, may not be implemented.

- Medium Investment Package: The City increases the budget for renewal and maintenance to eliminate the infrastructure deficit and keep pace with the City's aging infrastructure. The condition of the City's existing transportation network and the frequency of maintenance activities, such as sweeping and snow clearing, would remain about the same as today. In addition, the City would undertake a study to identify opportunities to provide better snow clearing of the City's bicycle network.

- Highest Investment Package: The City increases the budget for renewal and maintenance to a level that both keeps up with the pace of population growth and provides better service than today. This would result in additional road resurfacing, sidewalk repairs, and more frequent sweeping.

2. **Education and Incentive Programs:** Not all investments in transportation involve building new infrastructure. This category includes policies and programs focused on reducing congestion, which typically have a strong return on investment.

    - Basic Investment Package: The City maintains current levels of spending for programs like Bike to Work Week, bike training for kids, and transit pass programs, etc. Reductions in future traffic congestion would be minimal.

    - Medium Investment Package: The City does everything in the Basic Package and adds funding for current programs and some new programs. Examples of new programs include working with major employers to provide more discounted transit passes, better trip-end facilities, and to encourage teleworking and staggered work hours. Additional programs would include better bicycle and pedestrian education and training programs citywide. This package would help reduce future traffic congestion.

    - Highest Investment Package: The City does everything in the Basic and Medium Packages and also takes on a more significant role in funding and delivering education and incentive programs. Examples of new programs in this package include exploring partnerships to increase school busing, a new multimodal pass program, and an individualized trip planning program. This package would do the most to help reduce future traffic congestion.

3. **Shared Mobility and New Technology:** Emerging technologies such as ride-hailing (eg. Uber & Lyft) and shared vehicles (eg. carshare, bikeshare, etc.) are changing how people get around. This category includes investments in programs and infrastructure to help establish new transportation options while managing their impacts.

    - Basic Investment Package: The City maintains current levels of funding. The City works to attract investment from shared mobility operators and identifies ways to maximize the benefits of future technology change. This would result in some additional travel options and a basic degree of readiness for the future.

    - Medium Investment Package: The City does everything in the Basic Package and also provides funding incentives to expand and improve shared mobility options. The City actively prepares for ride-hailing and develops a Curbside Management Plan. This would result in better quality travel options for more people and a good degree of readiness for future technology change.

- Highest Investment Package: The City does everything in the Basic and Medium Investment packages and also invests in infrastructure, such as mobility hubs, to help extend the reach of transit by making it easier for people to get to/from transit and their final destination using carshare, bikeshare, or ride-hailing. This would result in the most travel options available to people and the highest degree of readiness for the future.

4. **Neighbourhood Streets:** The OCP endorsed Growth Scenario focuses much of Kelowna's future growth in the Core Area. Many of the neighbourhood streets in these areas lack basic pedestrian infrastructure and drainage. Higher investment in this category would improve the attractiveness and walkability of residential streets, through adding sidewalks, crossing improvements, traffic calming and street trees.

    - Basic Investment Package: The City maintains current levels of spending and neighbourhood streets would not be updated unless they are immediately adjacent to major new developments. The annual budget for crosswalks, traffic calming, and the Safe Routes to School program would stay the same. This package would do little to improve the walkability of communities as they grow, which reflects a misalignment with the endorsed OCP Growth Scenario.

    - Medium Investment Package: The City create a new program which helps fund sidewalks, drainage, and street trees on neighbourhood streets. Funding for the Traffic Calming and Safe Routes to School programs is increased. This package would help improve the walkability of communities as they grow, supporting the endorsed OCP Growth Scenario.

    - Highest Investment Package: The City does everything in the Medium Package but provides more funding, enabling more streets to be improved. This package would do the most to create attractive and walkable neighbourhood streets in communities as they grow, in full alignment with the endorsed OCP Growth Scenario.

5. **Biking:** As more residents move to the Core Area, more future trips will be within biking distance. Providing a safe and well-connected network of bikeways would make it easier and more convenient for people of all ages and abilities to get around by bike.

    - Basic Investment Package: The City maintains current levels of spending, building a network of protected bikeways that provides basic coverage. People would be able to reach some destinations by bike, but many destinations in the Core Area would still require biking in mixed-traffic and/or high stress environments. This package provides some benefits but would fail to fully support the OCP endorsed Growth Scenario. (A link to a map will be provided in the online tool).

    - Medium Investment Package: The City invests in the Basic Package and builds some additional protected bikeways. This would result in a more extensive network of protected bikeways, as well as new, low-cost bike routes on neighborhood streets. People would be able to ride a bike to many of Kelowna's key destinations, but some higher cost connections would remain unbuilt. This package would help reduce future traffic congestion, improve safety and public health, and would support the OCP endorsed Growth Scenario. (A link to a map will be provided in the online tool).

    - Highest Investment Package: The City invests in the Basic and Medium Packages and builds some additional projects that result in the completion of the Pedestrian and Bicycle Master Plan. Residents would be able to reach the majority of destinations within the Core Area

safely and conveniently by bicycle. This package would maximize benefits by reducing future congestion, improving safety and public health, and fully supporting the OCP endorsed Growth Scenario. (A link to a map will be provided in the online tool).

6. **Transit:** This category includes investments focused on improving Kelowna's transit service, including options to increase service hours as well as infrastructure (eg. bus stops, exchanges, and dedicated transit lanes)[2].

    - Basic Investment Package: This package would maintain current levels of funding. The City would partner with senior governments to fund only critical, needed transit infrastructure such as a new operations facility and the Orchard Park exchange. Other enhancements would be limited only to 'business as usual' annual increases in transit service. This level of investment would not keep pace with the demands of a growing population and would be misaligned with the OCP endorsed Growth Scenario. (A link to a map will be provided in the online tool).

    - Medium Investment Package: The City would invest in the Basic Package and also increase the transit service frequency and hours of service along the existing Frequent Transit Network. Transit fare payment technologies would be enhanced and the budget for HandyDart service would increase. A feasibility study would be conducted to examine the potential for higher order transit linking Downtown Kelowna, Pandosy, and Capri-Landmark. This package would serve the transit needs of a growing population and provide fundamental synergies with the OCP endorsed Growth Scenario, providing transit service investment along the urban corridors targeted for growth. (A link to a map will be provided in the online tool).

    - Highest Investment Package: The City would invest in the Basic and Medium packages and would also invest in transit priority infrastructure to help separate buses from traffic (e.g. transit queue jump lanes or bus-only lanes). New routes and service would also be added. This package would result in faster and more reliable transit service between key destinations, fully supporting and aligning with the OCP endorsed Growth Scenario. (A link to a map will be provided in the online tool).

7. **Multimodal Urban Corridors:** Recognizing that the mix of transportation demand in our Urban Centres and Core Area will change as these areas grow, options in this category will help to rebalance our urban transportation corridors to better accommodate convenient travel by multiple modes, including people walking, biking, taking transit and driving.

    - Basic Investment Package: The City completes streetscaping in the Rutland and Pandosy Urban Centres, extends Sutherland to Spall as a complete street through Landmark, and develops a Complete Streets Policy. This level of investment would include some benefits but would not fully support the endorsed 2040 OCP Growth Scenario and would be insufficient to achieve Imagine Kelowna and the TMP Vision by 2040. (A link to a map will be provided in the online tool).

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[2] Note that the potential for higher order transit along Harvey Avenue (which is owned by the Ministry of Transportation and Infrastructure) is being discussed separately, as part of the Regional Transportation Plan.

- Medium Investment Package: This package would maintain current levels of City spending and includes all the improvements in the Basic Package, plus multi-modal retrofits to additional corridors (such as segments of Lakeshore, Burtch and Guisachan). This level of investment would help support the endorsed 2040 OCP Growth Scenario and take steps toward achieving Imagine Kelowna and the TMP Vision. (A link to a map will be provided in the online tool).

- Highest Investment Package: This package includes all the improvements in the Basic and Medium packages, plus additional projects, such as the urbanization of Richter Street south of Sutherland, as well as the completion of the Sutherland Avenue extension. This level of investment would fully support the endorsed 2040 OCP Growth Scenario and accelerate achievement of Imagine Kelowna and the TMP Vision. (A link to a map will be provided in the online tool).

8. **Road Improvements and Connections:** The long-term vision from Imagine Kelowna is to give people more options to get around besides driving. Still, cars and trucks will play a vital role in Kelowna for the foreseeable future. This category includes projects focused primarily on increasing the safety, efficiency, and capacity of Kelowna's road network for vehicles in the outlying parts of town.

    - Basic Investment Package: The City focuses on improving the safety and efficiency of Kelowna's existing road network, through measures like better coordination of signals, developing a safety plan, developing a goods movement strategy, and targeted intersection improvements (which are the main constraint for roadway capacity). This package would result in a cost-efficient approach to improving traffic flow and safety for the greatest number of Kelowna residents and is the most aligned with Imagine Kelowna and the TMP Vision. (A link to a map will be provided in the online tool).

    - Medium Investment Package: This package would maintain current levels of spending on road connections and includes all the investments in the Basic Package, plus additional widening projects and suburban roadway extensions. This package would result in traffic flow improvements in the vicinity of each project, but would benefit fewer Kelowna residents overall and is less aligned with achievement of Imagine Kelowna and the TMP Vision. (A link to a map will be provided in the online tool).

    - Highest Investment Package: This package would include the investments in the Basic and Medium packages, plus a significant increase in spending on new capacity for vehicles, primarily in the outlying parts of town. This package would result in traffic flow improvements in the vicinity of each project, but would risk over-building and is not aligned with the endorsed OCP Growth Scenario, Imagine Kelowna, or the TMP Vision. (A link to a map will be provided in the online tool).

9. **Clement / Highway 33 Extension:** This category refers to the project idea of extending Clement from Spall east to either Highway 33 or McCurdy[3].

    - Basic Investment Package: This package would maintain current levels of spending and would include acquiring and protecting land for the corridor from Spall to UBCO for future transportation use. This package would not build the connection, but would provide a high

[3] Project would be contingent upon grant support from senior levels of government.

degree of readiness for the future, in alignment with Imagine Kelowna and the TMP Vision. (A link to a map will be provided in the online tool).

- Medium Investment Package: This package includes the Basic package and would also construct an extension of Clement from Spall east to Highway 33 as an arterial road, with a traffic light at Dilworth. This package would add an additional east-west roadway. Further analysis is needed to determine overall impacts to traffic flow, nearby wetlands and surrounding communities. (A link to a map will be provided in the online tool).
- Highest Investment Package: This package includes the Basic package, but would also construct an extension of Clement from Spall east to Highway 33 or McCurdy Rd and turn the road into a freeway. This includes interchanges at Spall and Highway 33, along with a flyover at Dilworth. This package would provide some traffic flow improvements, but risks overbuilding, in misalignment with Imagine Kelowna and the TMP Vision. (A link to a map will be provided in the online tool).

Transportation Master Plan

Option Categories and Investment Packages

Nov 18, 2019

Transportation Master Plan

Phases and Milestones

Existing and Future Conditions

- Shift as many future trips as possible to more space-efficient, sustainable travel modes
- Develop a well-connected complete urban street network
- Seek "healthy" levels of congestion

Option Development

Transportation "Options"

- Projects (Traditionally Infrastructure)
  - E.g. Build a new road, cycle track, transit exchange, pedestrian crossing, etc.
- Programs
  - E.g. Safe Routes to School Program, Traffic Calming Program, Transit Pass Program, Bike Skills Training / Education Program, Bikeshare Program, etc.
- Policies
  - E.g. Safety Policy, Design Standards, Parking Regulations, Congestion Pricing, etc.

Option Development . . .

- From existing plans and policies
- From technical analysis
- From public and stakeholder input

from existing plans and policies . . .

- 10-Year Capital Plan (2019 – 2028)
- Capri-Landmark Urban Centre Plan (2019)
- Central Okanagan Transit Future Action Plan (2018)
- Community Climate Action Plan (2018)
- Pedestrian and Bicycle Master Plan (2016)
- Urban Centres Roadmap (2016)
- Kelowna's Community for All Plan (2016)
- Hospital Area Plan (2016)
- Central Okanagan Clean Air Strategy (2015)

from technical analysis . . .

- Review of Existing and Future Conditions
- 30 Opportunities/ Challenges to Keep Kelowna Moving

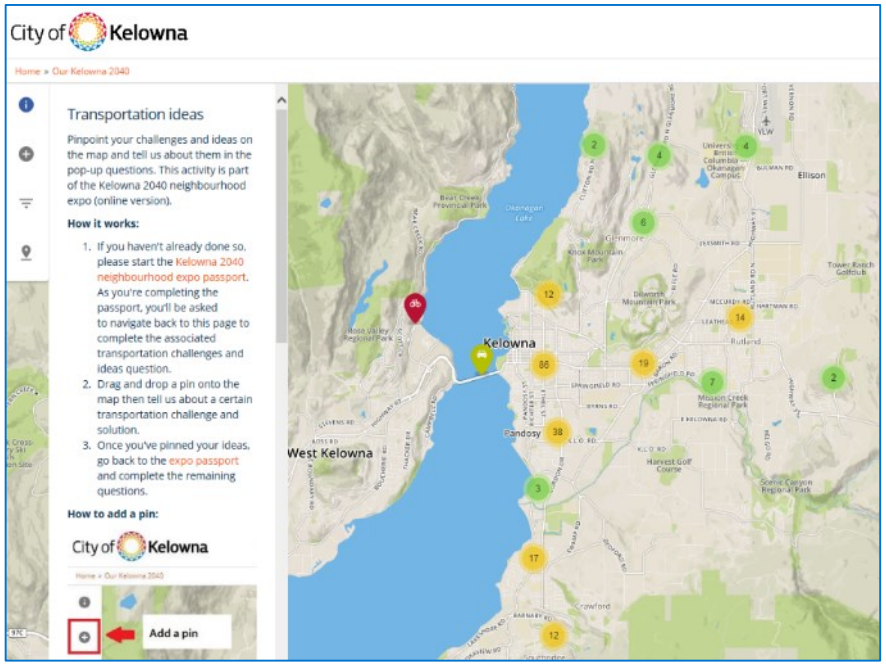
Walking	1	Design for Walkability in the Urban Centres
	2	Connect the Pedestrian Network in the Core Area
	3	Shift Short Trips to Walking
	4	Ensure People Walking Feel Safe
	5	Create Flexible and Adaptable Pedestrian Spaces
Biking	6	Shift Trips within the Core Area to Biking
	7	Increase Perception of Biking as a Safe Mode of Travel
	8	Make Biking Accessible to More People
	9	Integrate Bicycles with Transit
	10	Build-out a Complete Bicycle Network
Transit	11	Focus Growth near Frequent Transit and Ensure Multimodal Access
	12	Increase Transit Investment where Effective to Serve Growing Demand
	13	Speed Up Transit and Make it More Reliable
	14	Maximize Benefits of Technology Change on Transit
	15	Collect High Quality Data to Support Transit Planning
Driving	16	Growth in Downtown and South Pandosy
	17	Continued Growth in Suburban Hillsides
	18	Increasing Travel Demand through Midtown
	19	Employment Growth Along Highway 97
	20	Reduce the Frequency and Severity of Traffic Collisions
	21	Develop a Well-Connected, Complete Urban Street Network
Shared Mobility	22	Expand and Improve Bikeshare and other Emerging Options
	23	Attract One-way Carshare
	24	Prepare for the Arrival of Ride-Hailing
	25	Prepare for the Arrival of Autonomous Vehicles
Programs	26	Build Community Capacity
	27	Enhance Safe Routes to School
	28	Improve Transit Passes and Payment
	29	Manage the Curb
	30	Move Toward Parking On-Demand

from technical analysis . . .

- Review of TMP Vision
- Option ideas to achieve that vision by 2040

from public and stakeholder input . . .

- 2040 Neighborhood Expo Public Engagement
- Interactive Mapping Tool
  - 242 option ideas
  - 65 contributors
- Stakeholder Interviews
- OCP/TMP Stakeholder Workshop
- Phase 1 Vision and Goals



Option Screening and Sorting

Options "Long List"

- Over 700 options

Screening:

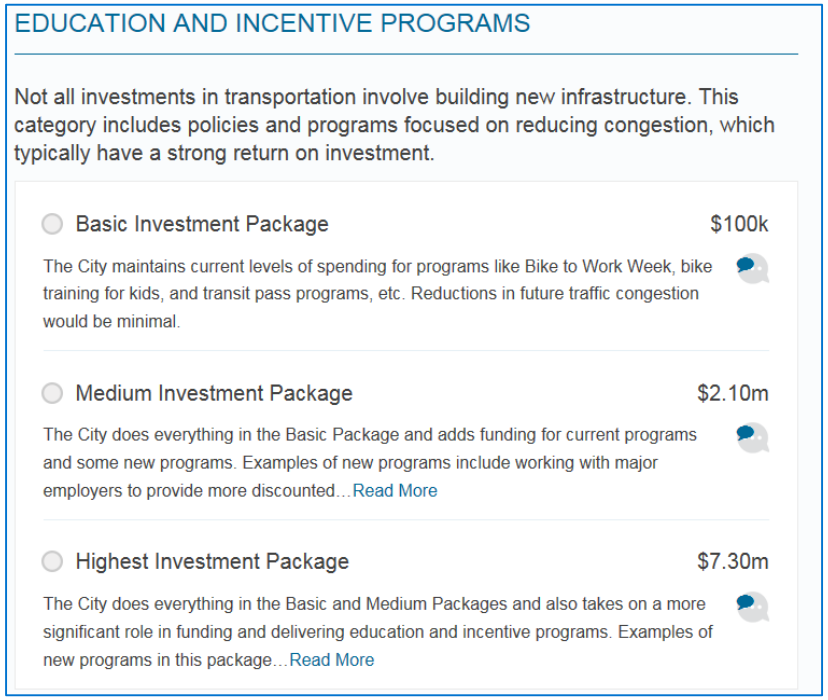
- Removed redundancies
- Removed fatal flaws
- "Bundled" for evaluation

= Over 400 options

Public Engagement

Build your own TMP

- Budget Allocator Tool
- 3 Investment Levels:
  - Basic Investment Package
  - Medium Investment Package
  - Highest Investment Package
- "Business As Usual" Transportation Budget out to 2040
- Mapping tool



Key Issues & Trade-offs

- Imagine Kelowna and TMP Vision achievement will likely require changes to how the City invests in transportation
- Budget Allocation
  - Ability to "dial up or down" within each category
- Total Budget:
  - Ability to increase or decrease spending
    - Property tax rates
    - Alignment with OCP endorsed Growth Scenario
    - Pace of Imagine Kelowna achievement

Maintenance and Renewal (all modes)

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Education and Incentive Programs

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Shared Mobility and New Technology

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Neighbourhood Streets

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Biking

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Transit

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Multimodal Urban Corridors

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Road Improvements and Connections

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Clement / Highway 33 Extension

- Basic Investment Package
- Medium Investment Package
- Highest Investment Package

Public Engagement

- Budget Allocator Tool & Questionnaire
- Stakeholder Workshop
- Student Focus Group at UBCO
- In-person events
- Marketing / Social Media Campaign

Next Steps

Technical Evaluation

The options will be evaluated using a Multiple Accounts Evaluation Framework & Regional Travel Model

- Policy Alignment
- Benefits
- Costs

Preliminary Recommendations

- Engagement Results + Technical Evaluation =
  - comprehensive suite of recommended transportation policies, programs and projects to service future growth
  - Informed by public values related to benefit and cost trade-offs

Next Steps

- Public Engagement (Nov 18th – Dec 6th)
- Technical Evaluation (Nov/Dec/Jan)
- Preliminary Recommendations (winter/spring 2020)
- Draft Plan (summer 2020)

Report to Council

Date: November 18, 2019

To: Council

From: City Manager

Subject: 2019 Community Trends Report: Impacts of the Sharing Economy

Department: Policy and Planning

Recommendation:
THAT Council receives, for information, the report from the Planner Specialist dated November 25, 2019, with respect to Kelowna's 2019 Community Trends Report.

Purpose:
To introduce "Sharing Economy Impacts: 2019 Community Trends Report".

Background:
The 2019 Community Trends Report (CTR) explores the rapid growth of the "sharing economy" and how it is disrupting our cities and putting pressure on local governments to respond. Globally, the value of the sharing economy is projected to rise to $700 billion by 2025 reflecting a larger societal shift in in values with people opting for access over ownership, changing the way our cities function. The sharing economy is largely being shaped by new business models that take advantage of the idle capacity or underutilization of different assets, allowing a car that usually sits parked 90% of the time to be shared by 10 different people over the course of the day. As cities across Canada begin to respond to the impacts of the sharing economy, the CTR (Attachment A) explores what tools are available to shape this transformation.

This year's trends topic reflects the many ways the sharing economy is affecting the day-to-day lives of Canadians. The CTR introduces three different approaches for how a local government might respond to new sharing economy businesses or organizations. The three approaches are intended to provide a foundation for the City as it begins to develop a policy framework and response for new sharing economy business models.

The CTR is one of several analytical reports produced by the Policy and Planning Department. While other reports focus on specific areas, such as housing or development statistics, the CTR explores major trends and examines possible local impacts. The CTR is not meant to establish formal City recommendations, but instead sets the stage for the City's future-forward work on other planning

projects or initiatives. Ultimately, the aim of the CTR is to research and to understand future trends that will impact the City and the broader community in the years ahead and to understand where attention and action may be needed.

Discussion:

The CTR focuses on three different parts of the sharing economy that are impacting our cities:

- *How We Live* – As more people move to cities, there is growing interest in housing forms that offer people a greater sense of belonging. The sharing economy is beginning to disrupt the housing market, introducing new housing forms with different shared spaces (kitchens, yard space, communal gathering/living spaces, amenity spaces). These new housing forms are being developed with a view to building 'community', catering to those looking for social interaction in the digital age.
- *How We Work* – The nature of work is changing rapidly as a result of shifts in technology and broader economic forces. A growing number of jobs no longer require the traditional bricks and mortar office space with more people growing their businesses online. The sharing economy is responding to these shifts with shared working spaces that challenge many of the conventional work and office environments.
- *How We Move* – The transportation landscape is shifting with new mobility options being rapidly introduced throughout North America. The sharing economy is supporting a shift in transportation where people share access to a range of different transport options (shared mobility). Moreover, smartphone applications and location-based technology are facilitating on-demand access to different transportation options in real-time.

The Role of the City of Kelowna in Regulating the Sharing Economy

Many cities are looking for ways to better manage the impacts of the sharing economy balancing economic innovation and community benefit. The trends report identifies how cities have several different tools and approaches available to shape the impact of the sharing economy. Where and when a city chooses to apply each of these approaches identified below will vary depending on the potential risks and benefits involved.

1. **Wait-and-see** – Where there is a low risk of negative impacts to the City or to the broader community, in these instances a more observational approach may be appropriate, allowing time to understand the situation before formulating a regulatory response.

2. **Partner or Pilot** – In cases where a sharing economy model aligns with City policies, but lacks the regulatory framework to operate, the City could partner with an organization to encourage local implementation and use the pilot phase to understand the public impacts and inform the development of future regulations.

3. **Early Regulation** – Where there is seen to be a significant risk to the City or to the community, early regulation may be warranted. This would pause or slow the impacts and would allow the City to develop a more involved and comprehensive approach.

Conclusion:

Cities across Canada are taking steps to respond to the sharing economy, recognizing new business models are disrupting how people live, work and move in our cities. Cities are faced with the choice of taking an active or passive role in both embracing and/or regulating the sharing economy. The traditional approach of taking a more passive role may be a less effective as the sharing economy has demonstrated it does not always wait for permission to begin operations. A more active role could allow cities to work proactively with sharing economy businesses and organizations to determine how these new platforms will be implemented with a deliberate focus to enhance public benefit. This approach will require cities to be more flexible as they test new ideas and refine policies and regulation to respond to the change. This year's CTR is intended to serve as a resource for council, staff and the public as the City embraces an uncertain future and develops new policies and regulations for the sharing economy in the years ahead. Ultimately, the sharing economy is here to stay, compelling cities to think creatively about how these new business models can provide long-term community benefit to how we grow, move, and work in untraditional ways.

Internal Circulation:

Policy and Planning
Communications
Integrated Transportation
Business and Entrepreneurial Development
Development Planning
Real Estate Services
City Clerk

Communications Comments:

The 2019 Community Trends report can be found on kelowna.ca/about. The report will also be highlighted through corporate communications channels and distributed to key stakeholders and the broader community.

Submitted by: Submitted by R. Soward, Planner Specialist

Approved for inclusion: J. Moore. Long Range Policy Planning Manager

Attachments: 2019 Community Trends Report – Impacts of the Sharing Economy

Impacts of the Sharing Economy

2019 Community Trends Report

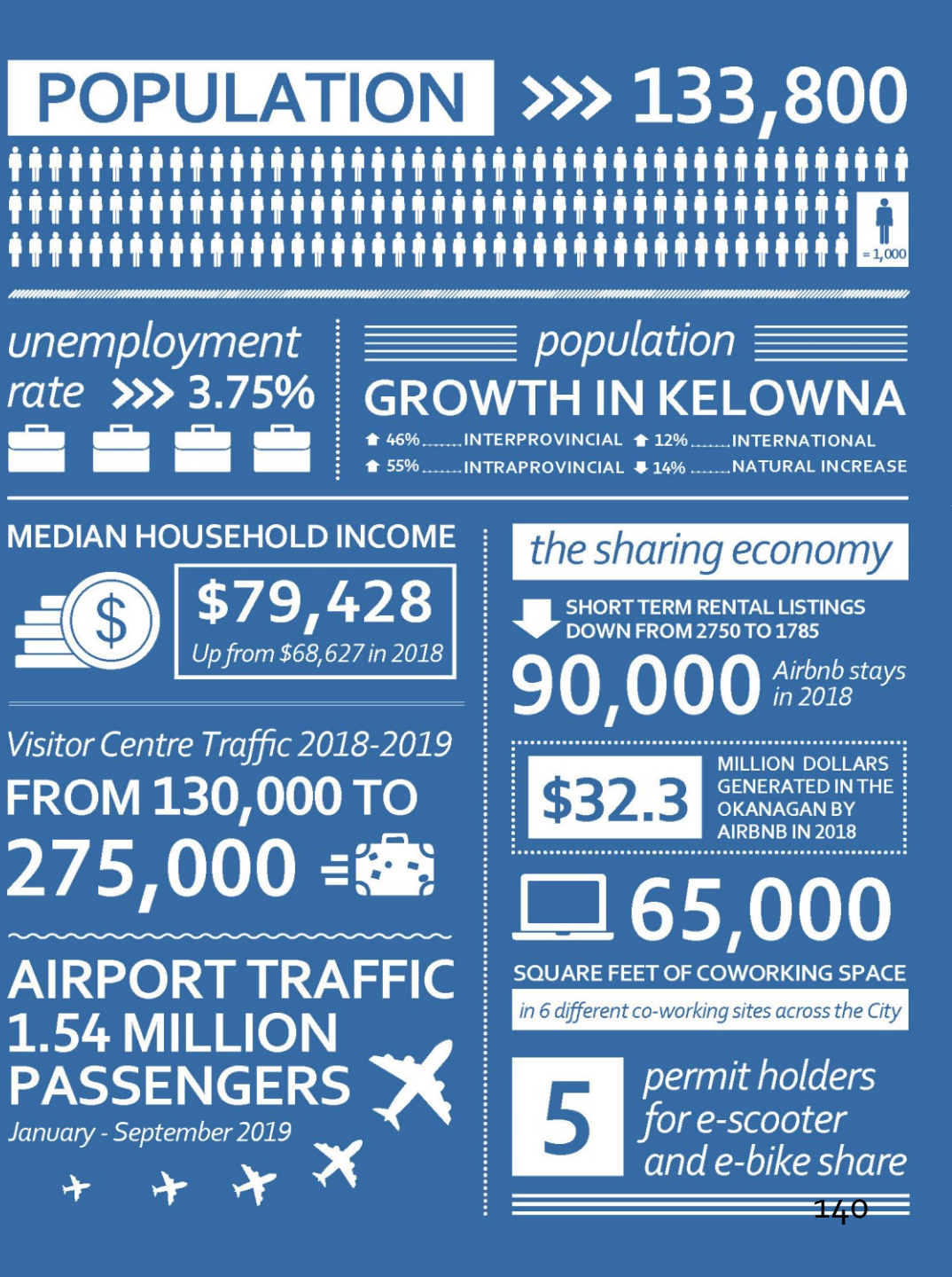
November 18, 2019

Role of Community Trends Report

- Forward looking document
- Infographic & major theme
- Understand local impact of global trends
- Prepare City & community for future trends

Infographic Dashboard

- Downtown development
- Rental housing rebound
- Co-working on the rise
- Car Share growth
- AirBnB by the numbers



Sharing Economy

- Disrupting traditional business models
- Sharing of underutilized assets
- Enabled by technology / online connectivity
- Shared access over private ownership
- Access to community assets & unique experiences

Modo Car Share has over 20,000 members in BC

Why Focus on Sharing Economy?

City of Kelowna

- Rapid growth globally & locally
- Disrupting various sectors
- Changing the way our cities function
- Limited policy framework

*From 2017 World Economic Forum Sharing Economy Report

Different Roles for Cities to respond to the Sharing Economy

City of Kelowna

Wait-and-see

- Low risk of negative impacts ,allow time to understand the situation before action

Partner or Pilot

- Alignment with City policies, pilot to understand community impacts & inform future regulations

Early Regulation

- Significant risk to community, regulation is needed to mitigate negative impacts

How We Live

City of Kelowna

HOUSING

Zero vacan
Inside Can
crisis

Years of lacklustre con
vacancies and soaring
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ECONOM

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RACHELLE

"TENANTS ARE LOOKING FOR MODELS THAT OFFER BOTH AFFORDABILITY & THE COMMUNITY LIFESTYLE THEY DESIRE. SIMULTANEOUSLY, MULTI-FAMILY OPERATORS HAVE SOUGHT MODELS THAT CAN OPTIMIZE PER SQUARE FOOT RENT"

Survey of the Coliving Landscape 2019
Cushman & Wakefield

M
P
U

Survey Says: Canadians are Lonely and Socially Isolated

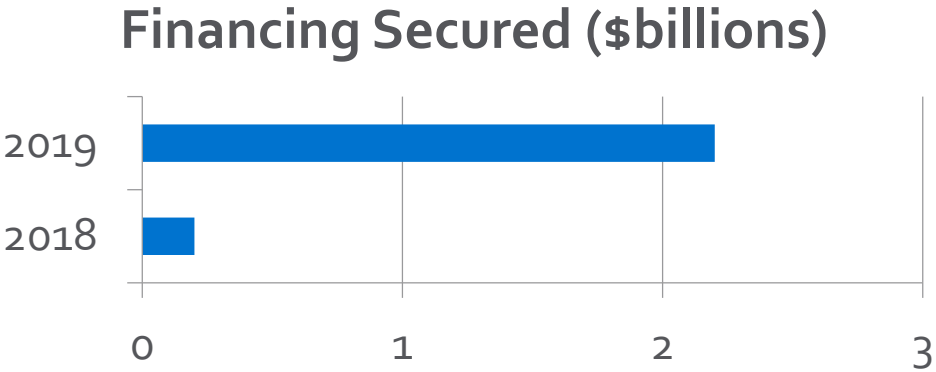
Posted on July 29, 2019
By Glenda Cooper

Sharing Economy Response

City of Kelowna

Co-living

- Smaller rental units with shared amenities
- Catering to young professionals
- Several Canadian projects slated for 2020
- Does not always provide "housing basics"



Source: Cushman & Wakefield. 2019. Survey of the Coliving Landscape.

Sharing Economy Response

City of Kelowna

Co-housing

- Grassroots with residents active in planning & design
- Shared ownership
- Caters to range of ages & life stages
- Fully equipped private units with shared spaces

Ericka Stephens-Rennie views cohousing as a great way to achieve livability in cities, explaining how her family lives in an 850 sq. ft unit, largely because of their easy access to shared spaces.

How We Live: Policy Response

City of Kelowna

Co-housing – Pilot or Partner

- Aligns with Healthy Housing Strategy
- Explore local partnerships
- Better understand benefits delivered to community

Co-living – Wait-and-See

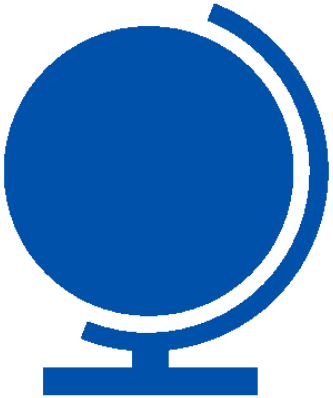
- Monitor first wave of Canadian projects
- Better understand benefits / risks to community

Source: Photo Contributed by 33rd Ave Cohousing Community

Forces Changing How We Work

Technological Change

Flexible Work Hours



Globalization

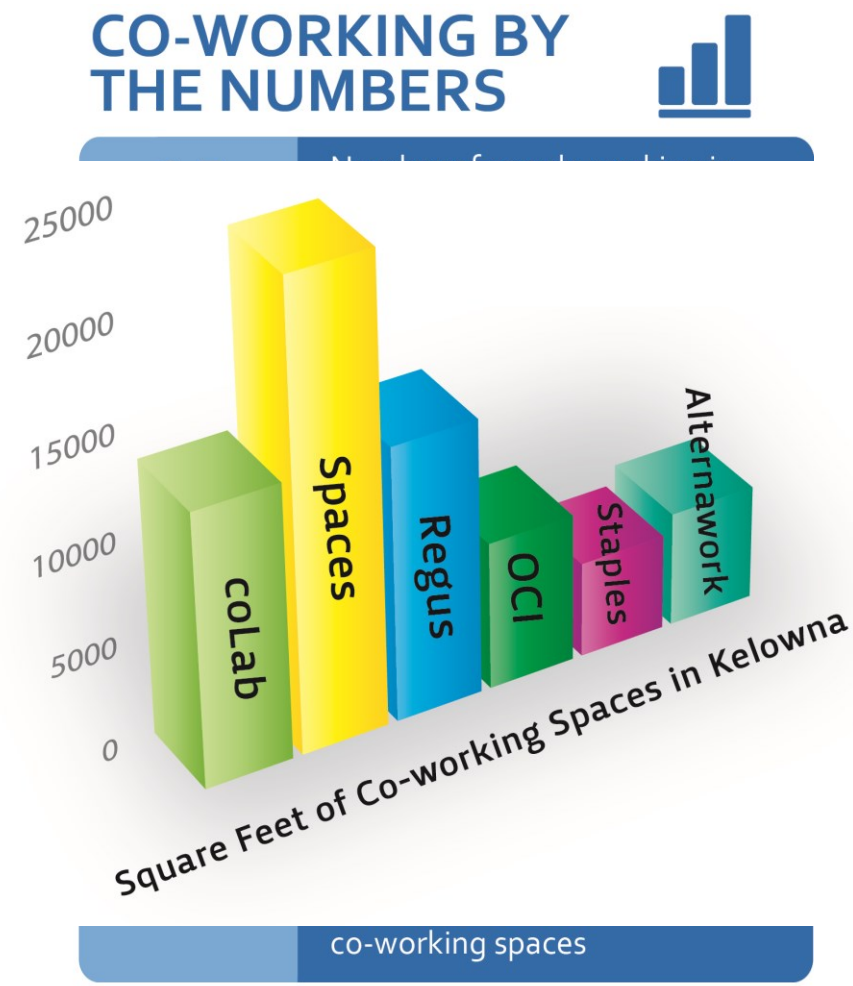
Gig economy)

Sharing Economy Response

City of Kelowna

Co-working

- Membership-based model for office space
- Work spaces often integrate amenities
- Focus on networking and community
- Flexibility for small business and freelancers



Local Sharing Economy Spotlight

City of Kelowna

Co-working in Kelowna

- Growing sector with range of spaces
- Alternative to brick & motor office space
- Larger companies offer access to network of work spaces
- Grassroots spaces emphasize mentorship, networking & services

"A lot of what we create is actually built on the space as a platform as opposed to the space as a business model."

Shane Austin, Owner of coLab

How We Work: Policy Response

City of Kelowna

Co-working – Wait-and- See

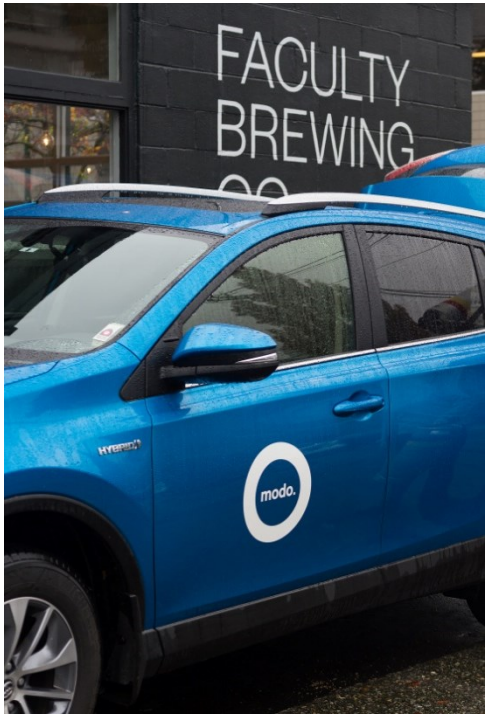
- Already part of local community
- Supporting local tech and startup sector
- Delivering benefit to community
- Low risk to community, monitor impacts on future office needs & zoning bylaw

Source: Photo Contributed by co-Lab (Daniel Jones)

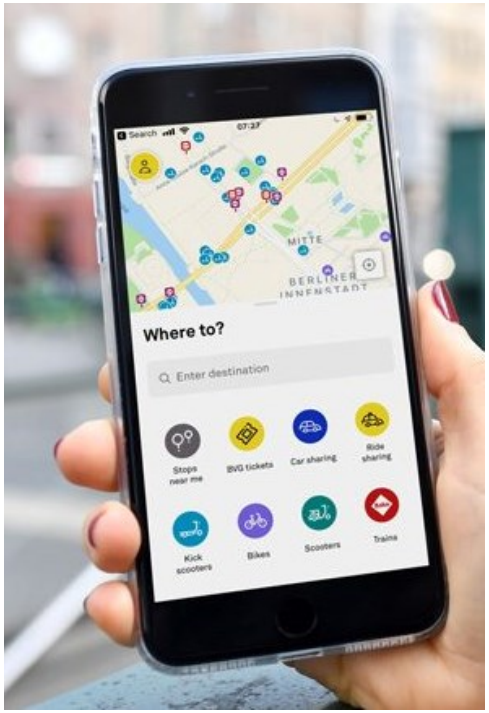
How We Move

City of Kelowna

More short trips



Shared access



Mobility as a service

Global platforms

Sharing Economy Response

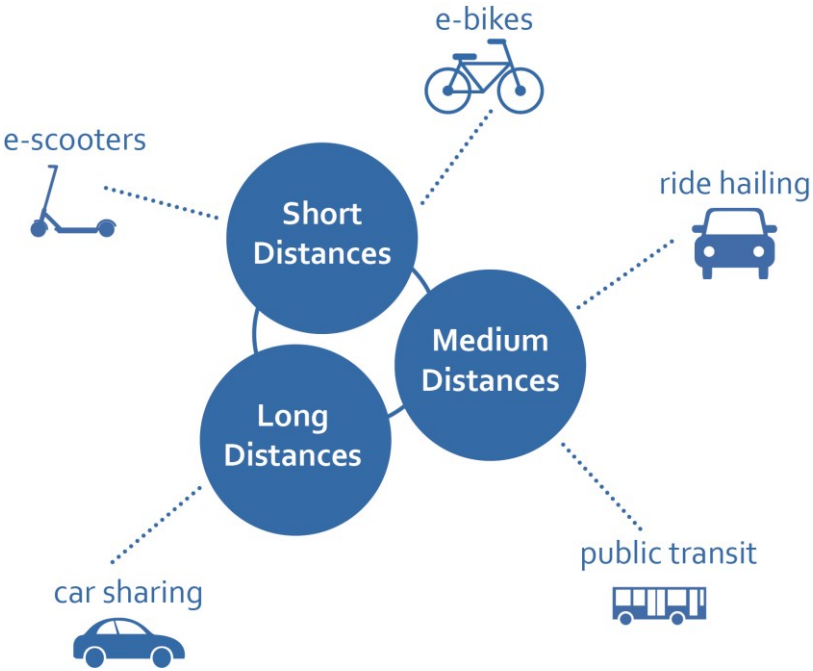
City of Kelowna

Car sharing

- Shared fleet
- More efficient utilization of vehicles
- Car sharing supports "car-lite" lifestyle

Ride-hailing

- On-demand access via app
- Allows for people to share rides
- Impact on transport behaviour requires further research



Sharing Economy Response

City of Kelowna

Micromobility

- Growing demand for short trips
- A range of technologies are emerging
- Different options based on the nature of the trip
- Can result in conflict with pedestrians in already constrained areas

Kelowna posted Jul 29, 2019 @ 12:30pm by Dylan McCullough

50 e-bikes and 150 more e-scooters approved for Kelowna

Kelowna residents have likely seen people zipping around on e-scooters over the past month.

On Monday, the City of Kelowna announced that two more permits have been approved for a fleet of 50 electric bicycles and 200 more electric scooters.

How We Move- Policy Response

City of Kelowna

Car sharing – Pilot & Partner

- Aligns with City's transportation goals, explore options new partnerships to promote community use

Ride-hailing – Wait & See

- Provincial legislation sets high bar for operators, still unclear which operators will enter local market,
- Could address local transportation gaps
- Need to monitor impacts given potential for negative risks

Micromobility – Pilot & Partner

- Potential to support sustainable transport goals
- Need to better understand risks and benefits
- Monitor health and safety impacts for community

Conclusion

- Pace of change a challenge for all cities
- Not possible to opt out of sharing economy
- Active role gives cities more agency
- Cities need to be resilient to remain competitive
- Cities need to be nimble as they adapt and refine policies to respond to sharing economy

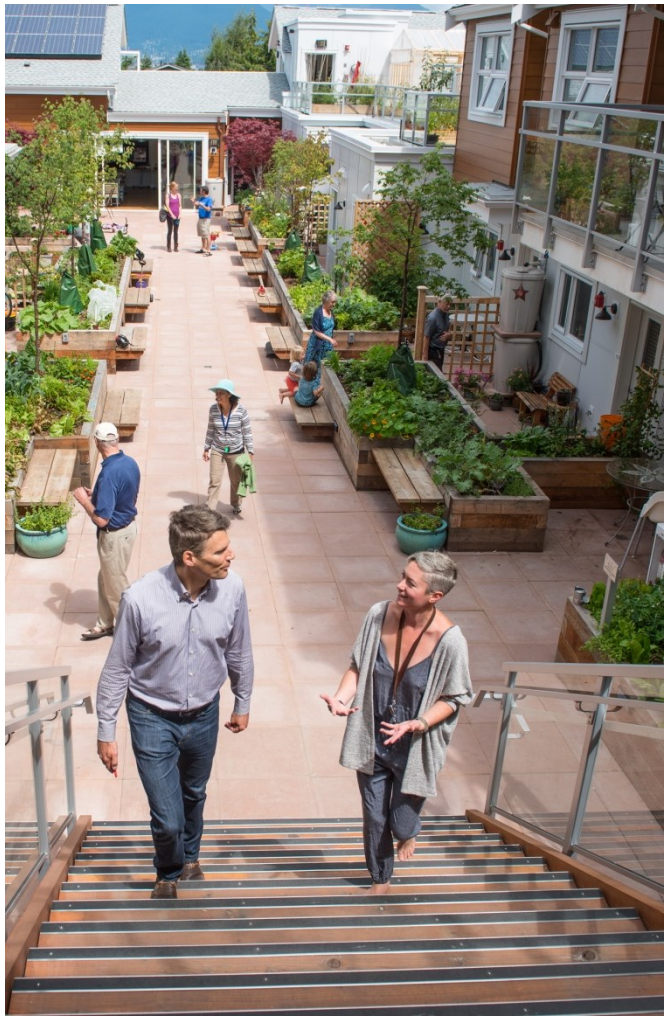


Photo Contributed by 33rd Ave Cohousing Community

City of Kelowna

Community Trends 2019

IMPACTS OF THE SHARING ECONOMY

INTRODUCTION

For the purposes of this year's Community Trends Report, the sharing economy will include the range of technology enabled business models and platforms that employ sharing to disrupt traditional business models and are changing the way our cities function.

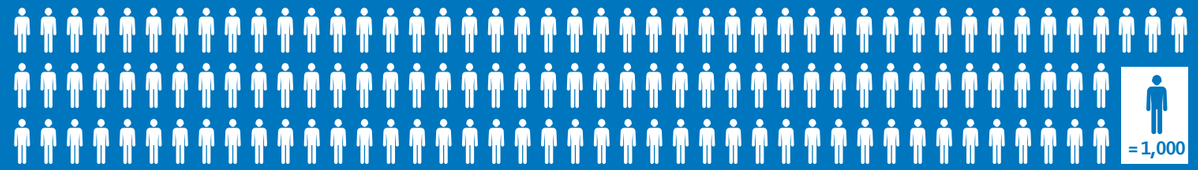
The sharing economy is evolving at a rapid pace, disrupting various sectors of our cities and putting pressure on local governments to respond. The sharing economy is part of a shift in values with people favouring access over ownership, promoting a less consumptive lifestyle and changing the way our cities function. These new business models allow for cities to take advantage of the idle capacity or underutilization of assets resulting in a car that usually sits parked 90% of the time to be shared by 10 different people over the course of the day.[i] Cities are beginning to respond, looking to develop policies and regulations that promote innovative ideas, but ensure there is a public beneift to the local community.

The Community Trends Report is prepared annually to explore how major changes in the future might impact the long-term management of cities. The Community Trends Report is a researched-based document that sets the stage for future action by identifying the local implications of broader national trends. The report allows the corporation to explore complex topics in a less formal manner that is more accessible to the broader community. For this reason, the Community Trends Report is intended to serve as a resource for the corporation and to inform residents, businesses and local community organizations about future shifts in society and the anticipated local impacts.

The Community Trends Report is comprised of two key parts each year. First, the Trends Infographic highlights current key data to gain an understanding of today's community landscape in Kelowna. The other part of the Community Trends Report is a broader review of a larger theme that warrants further research to better understand how the City of Kelowna might shift business practices, policies or service delivery to adapt to challenges that we're seeing other cities face nationally. This year's Sharing Economy theme was selected to support the City as it considers the policy response to this emering part of our economy.

The Trends Infographic, represents key statistical information related to Kelowna's economy, demography, housing market, and other relevant information to paint a picture of Kelowna's sharing economoy in 2018-19. Statistical information from 2018 and 2019 are used to show year-over-year changes on key metrics. The data reinforces that Kelowna continues to see strong growth with considerable population increase alongside healthy development activity. Also, the airport and tourism centre reported strong levels of activity in 2019. The sharing economy data confirms that Kelowna is experiencing a similar level of disruption as many other cities across Canada within the transportation, housing, and emloyment sectors of our economy.

POPULATION >>> 133,800



unemployment rate >>> 3.75%

population

GROWTH IN KELOWNA

- ⬆ 46% ..... INTERPROVINCIAL
- ⬆ 55% ..... INTRAPROVINCIAL
- ⬆ 12% ..... INTERNATIONAL
- ⬇ 14% ..... NATURAL INCREASE

Kelowna self-employment rate

15.7%

Based on 2016 Census

Visitor Centre Traffic 2018-2019

FROM 130,000 TO 275,000

AIRPORT TRAFFIC 1.54 MILLION PASSENGERS

January - September 2019

the sharing economy

⬇ SHORT TERM RENTAL LISTINGS DOWN FROM 2750 TO 1785

90,000 *Airbnb stays in 2018*

$32.3 MILLION DOLLARS GENERATED IN THE OKANAGAN BY AIRBNB IN 2018

65,000

SQUARE FEET OF COWORKING SPACE

in 6 different co-working sites across the City

5 *permit holders for e-scooter and e-bike share*

Demographic Stats Retrieved from Environics and Stats Canada

1,500 UNITS

under review in downtown Kelowna

BUILDING

permit values

$760 MILLION

1823 PERMITS ISSUED

FOR NEW HOUSING UNITS THROUGH AUGUST 2019

FOR SALE

38%

SALES OF HOMES OVER $1 MILLION

down from 178 to 115

580 AFFORDABLE RENTAL UNITS

under construction or built in the last 5 years

MEDIAN HOUSING COSTS

SINGLE HOMES ⬆ 0.5%

$685,000

TOWNHOUSES ⬆ 0.2%

$465,000

CONDOS ⬆ 8%

$352,750

2018 Average Rental Rates

SECONDARY RENTAL MARKET: $1,755

PRIMARY RENTAL MARKET: $1,222

LONG-TERM RENTAL HOUSING

- *63% of units built before 1980*
- *6,700 market rental units*
- *75% of units located in the core area*

CAR SHARE

24% INCREASE

IN MEMBERSHIP SINCE 2018

7.5 rides per day per e-scooter in the summer months

GROWTH OF SHARING ECONOMY

WHY FOCUS ON THE SHARING ECONOMY?

The concept of a sharing economy, one in which individuals share assets or services, has been around for decades, and can be seen in services like libraries or even public transit. Over the last 10 years, however, with advancements in technology that facilitates sharing transactions, cities have seen the growth across various sectors of new "sharing economy" business models that utilize online platforms.

The five largest areas of growth relate to finance, accommodation, transportation, household services and professional services with significant impacts to cities across Canada.[ii] Partly due to the rapid growth and change of the sharing economy, many organizations have operated with limited government regulation, such as AirBnB. The majority of these sharing economy organizations barely existed 10 years ago, but now they impact the day-to-day lives of many Canadians.

In Kelowna alone, AirBnB accounted for roughly $32 million in revenue in 2018. [iii] In BC, the provincial government estimates roughly $400 million in AirBnB bookings from October 2018 to the end of September 2019.[iv] The growth of the sharing economy is quickly reshaping how people live, work and move in cities, reflecting a shift in values toward optimizing the use of underutilized assets in cities and a shift from private ownership to shared ownership. People are using new product service models where ownership is no longer the norm. For example, the emergence of shared mobility is changing the way people make short trips in larger cities. Already, car sharing is a fixture in Kelowna with MODO's recent expansions, and continued growth in Vancouver and Victoria boosting their membership over 22,000.[v] The rise in collaborative consumption has dovetailed with rising cost of living in larger centres and new housing forms are emerging. Meanwhile, new organizations are upending the traditional approach to work and office space, resulting in the rapid growth of co-working spaces throughout North America.

Although many cities are struggling to manage the impacts of the sharing economy, there is potential to leverage these models and platforms to build trust and social capital and to encourage greater sharing of public and community assets (Mclaren,Pg. 5).[vi]

Cities have a range of different tools and approaches available to shape the impact of the sharing economy. Where and when a city chooses to apply each approach will vary depending on the potential risks and benefits involved.

SHARING ECONOMY GLOBAL VALUE OF SECTOR TO RISE TO

$700 BILLION BY 2025

*From 2017 World Economic Forum Sharing Economy Report

Wait-and-see – where there is a low risk of negative impacts to the City or to the broader community, in these instances a more observational approach may be appropriate, allowing time to understand the situation before formulating a response.

Partner or Pilot – In cases where a sharing economy model aligns with City policies, but lacks the regulatory framework to operate, the City could partner with an organization to encourage local implementation and use the pilot phase to understand the public impacts and inform the development of future regulations.

Early Regulation – where there is seen to be a significant risk to the community, early regulation may be warranted. This would pause or slow the impacts and would allow the City to develop a more comprehensive approach.

Moving forward the sharing economy presents big questions around how to assess the benefit of these large sharing platforms and the level of regulation required. Due to the rapid growth and change of the sharing economy, cities will need to be nimble as they adapt and refine regulations to ensure the sharing economy is providing a public benefit. This year's trends report examines the sharing economy, asking how it will impact our community in the coming years and what types of research, monitoring or regulation might be required in this burgeoning sector.

HOW WE LIVE

HOUSING FORMS ARE BEING DEVELOPED WITH A VIEW TO BUILDING COMMUNITY.

With rising housing costs, the traditional vision of a detached home in the suburbs is now out of reach for many Canadians, and ground-oriented housing known as the "missing middle" is becoming more popular. As more people move to cities, there is growing interest in new compact housing forms that offer people a greater sense of belonging. The sharing economy is responding to these trends and is disrupting the housing market, introducing housing forms with different shared spaces (kitchens, amenity spaces). These new housing forms are being developed with a view to building community, catering to those looking for social interaction in the digital age.

CO-LIVING

Co-living is a new housing type appearing in larger cities, delivering micro-unit (200-300 sq ft) developments that emphasize shared amenity spaces.[vii] Co-living caters mostly to young professionals in larger cities where the cost of housing is out of reach for many people.[viii] Co-living provides residents with a micro-unit and access to shared spaces such as a large kitchen, gym, or entertainment room. The shared spaces are marketed as a way to build community, allowing residents to get to know one another through events and activities (movie nights, cooking classes etc) that are part of monthly rent and coordinated by an on-site community curator. Also, co-living developments are often situated in central locations with good access to transit and downtown amenities. With growing interest in urban living, co-living offers a new option for single young professionals looking for a new type of rental housing.

The co-living model could also be seen as part of a decline in housing conditions in large cities.[ix] These developments often don't provide many of the basic elements of a home, like a full kitchen or a minimum amount of private space for living or relaxing. Instead, co-living focuses on shared spaces and amenities, allowing developers to increase their units per floor, and maximizing revenue. As a result, co-living projects are now big business with large developers planning co-living projects throughout the North America. Although co-living provides a short-term housing option for young singles, its small units and limited private space make it unsuitable for many groups, including couples and those with children. Co-living demonstrates how the sharing economy is disrupting sectors by creating a new business model that emphasizes sharing.

CASE STUDY: OTTAWA CO-LIVING PROJECT

One of the first major co-living developments in Canada is slated to begin construction next year in Ottawa.[x] The 252 unit project being developed by Dream Unlimited Canada will have a mix of co-living units and traditional rental apartments with the shared co-living suites targeting rents of $1,250 per month per resident.[x] The co-living suites will be shared between 4-6 residents with residents sharing bathrooms and kitchen spaces with furnishings and cleaning services included. This model could be described as dorm housing for young professionals, targeting "rents that are cheaper than living alone but more profitable than purpose built rental for the developers".[x] The development will also offer shared lounges, shared kitchens, community rooms, in-unit laundry and a gym that will be managed by large American co-living company Common.[xi]

CITY OF KELOWNA'S ROLE: WAIT & SEE

Although, co-living is squarely within the authority of local governments to regulate, it may be best for the City of Kelowna to take a wait and see approach as the initial co-living projects move forward over the next couple years. This part of the sharing economy does not pose any immediate risks to the community or housing market, allowing Kelowna to take a more cautious approach. Kelowna may want to observe how other local governments are regulating co-housing and learn from their experience. More specifically, what areas of the City are most appropriate for co-living? Should there be minimum unit size or requirements for shared spaces in co-living developments? Overall, co-living has the potential to support the city's broader housing goals, but it may be prudent to adapt zoning bylaw regulations once this model is better understood

CO-HOUSING

The notion of housing with shared spaces is not new, with co-housing originating in the post-war era in Europe. Co-housing shares some similarities with co-living, but with very different underlying values. Co-housing emphasizes long-term ownership in a housing form that works for a range of age groups and life cycles (including families, couples & seniors).[xii] Also, in co-housing residents typically lead the planning and design of the housing and communal spaces as opposed to a developer marketing a building with some common amenity space. Co-housing requires the community members to work together to determine what shared spaces best reflect their unique needs and values.

Within co-housing projects, each resident has their own full private unit, but their unit is complemented by common areas such as a large communal kitchen or living room that is shared among all the residents. Most co-housing communities share weekly meals or work on shared projects, fostering deeper friendships among the residents. Co-housing also emphasizes shared decision making for the stewardship of the property. Often co-housing projects integrate further sharing opportunities (car sharing, outdoor spaces, garden areas) to reduce the environmental footprint of the development.

In BC, there are a number of co-housing projects that have been developed or are under construction as more people look for creative ways to get access to housing that reflects their needs.[xii] Overall, co-housing shows how the sharing economy can be a bottom-up solution to create innovative housing forms that respond to a host of challenges facing our cities.

CASE STUDY: VANCOUVER CO-HOUSING PROJECT

This 31-unit project includes a range of unit types from studios to four-bedroom units – all equipped with kitchens, living and dining rooms. The co-housing residents also share access to a common house that includes a community kitchen, dining room, and lounge; activity rooms; office areas and guest rooms.[xiii] Because all the residents share access to the common house and outdoor spaces, they can reduce the size of each of their private dwellings. Co-founder Ericka Stephens-Rennie views cohousing as a great way to achieve livability in cities, explaining how her family of four lives compactly in an 850 sq. ft unit, largely because of their easy access to the shared amenities.[xiv] The shared spaces act as a platform to build community among the residents by supporting shared weekly meals, activities, and holiday gathering that have made the co-housing project an informal neighbourhood hub.[xiv]

CITY OF KELOWNA'S ROLE: PILOT OR PARTNER

Based on the alignment of co-housing with the City's Healthy Housing Strategy it may be beneficial for the City to consider piloting or partnering with a co-housing group to determine what policy support or regulation might be beneficial to enable more co-housing projects. The City could also partner with a co-housing group to assist them in navigating the planning process as opposed to viewing them as a conventional developer. By partnering with a local co-housing group there may be an opportunity to better understand what community benefits these projects deliver to the city.

Photo credit: 33rd Ave Cohousing Community

HOW WE WORK

THE FUTURE OF WORK IS CHANGING AND THE SHARING ECONOMY IS RESPONDING WITH NEW WORKING SPACES.

The nature of work is changing as a result of shifts in technology and broader economic forces. A growing number of jobs no longer require the traditional bricks and mortar office space with more people growing their businesses online. Also, many businesses are casting a wider net, catering to national or international markets, resulting in more travel for work and less emphasis on a centralized office space. The proliferation of smartphones and online connectivity has made it easier than ever for employees to work remotely and define their own schedules. Meanwhile, the expansion of digital communication has increased expectations around 24-7 access to staff, blurring the boundaries between work and leisure. As a result of these forces, the future of work is changing, and the sharing economy is responding with new working spaces.

The sharing economy is changing the way people work, disrupting how work spaces and offices are designed and operated. Co-working offers a membership-based model that looks to increase the number of people who can use a space by taking advantage of the spare capacity associated with many offices.[xv] A handful of co-working companies like We Work, Impact Hub, and Regus now control millions of square feet of office space, offering members access to a global network of co-working locations throughout North America and Europe.[xvi] These larger co-working companies are looking to generate greater profit by challenging the traditional approach of single-purpose office spaces which often sit vacant up to 60% of the time, providing members more flexibility with work hours via 24-7 access.[xvii] Co-working also allows smaller firms to avoid signing expensive leases, reducing overhead costs and offering potential for collaboration and mentorship within the co-working community.

Co-working spaces provide all the conventional office supports (e.g. internet, printers, private rooms and conference rooms) as well as unique shared amenities such as gyms, kitchens, and child care. This allows for freelancers and entrepreneurs to have some of the benefits of a brick and mortar office at a fraction of the cost.

Many co-working spaces offer amenities, events, or services that members encourage more social interactions among members outside of traditional work hours. The spaces are also designed with a greater focus on creating a desirable space to spend time outside of work hours. Some co-working organizations, including local organization coLab (featured in case study on pg.9) or Wing (based in USA), create spaces that provide people with a deeper social connection and help them network with the local business community. These co-working organizations function as local hubs for entrepreneurs or startups to get professional advice and mentorship to build their companies and to meet like-minded co-working members. These community-focused co-working spaces prioritize grassroots entrepreneurs or other freelancers promoting their spaces as platforms for business development and collaboration. Some co-working spaces such as Wing are positioning themselves as the service clubs of the 21st century offering office space, professional networking and social connections all at the same time.[xviii]

Although co-working was initially viewed as a niche market catering to freelancers and tech professionals, this model has seen growing interest with many large employers now leaving behind their corporate offices in favour of co-working memberships for their employees.

CO-WORKING BY THE NUMBERS

1.7 Million	Number of people working in co-working spaces globally at end of 2018
19,000	Number of co-working spaces globally at the end of 2018
5.3 Million	Total square feet of office space at We Work's 300 Manhattan co-working sites
82	Average number of members per co-working space in 2018 global survey
65,000	Total square footage associated with Kelowna's six co-working spaces

*Global stats via deskmag.com State of Coworking Spaces 2018 Report

Photo Credit: Daniel Jones via coLab

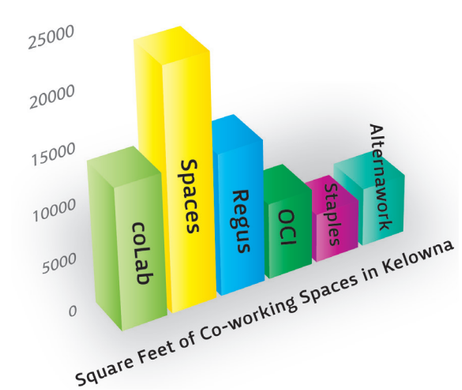
In Kelowna, there are several co-working spaces (coLab, OKGN Works) catering to the local tech and social innovation sector. Other larger co-working companies such as Regus and Spaces have entered the Kelowna market more recently, offering members access to a broader network of offices. For example, Spaces and Regus have co-working locations all over the world and members can gain access to all locations.

CASE STUDY: KELOWNA CO-WORKING - COLAB

Opened in 2011 with 6 members and 1,500 sq ft, CoLab offers a space for a range of different groups such as entrepreneurs, startups, and creative professionals in Kelowna to work in a new way – through co-working.[xix] CoLab has grown steadily to where it is today with 13,000 square feet and a growing membership that ranges from 150-300 members, helping people grow their businesses. Co-lab has become a magnet for the local startup community offering close to 20 monthly events, as well as professional services and a range of networking opportunities that have cemented them as the leading co-working space in Kelowna. Shane Austin founder of CoLab reflects on how their focus on culture has been the key to their long-term success "a lot of our value is in the network and relationships and the community surrounding us and we only exist because we were built through that community. It's harder but its more sustainable in the long-term. A lot of what we create is actually built on the space as a platform as opposed to the space as a business model."[xix]

ROLE OF CITY OF KELOWNA: WAIT & SEE

Co-working is already part of the local sharing economy of Kelowna and so far has operated effectively without significant government intervention. Moving forward, Kelowna is likely best served to continue to take a wait and see approach given the very limited risk associated with this emerging business model. Furthermore, co-working aligns well with Kelowna given the high number of local residents who are self-employed and the growing tech sector in the region. Looking ahead, the City may want to investigate the impact of co-working on future office projections and how this use functions differently from a transportation perspective than traditional office space. Also, there may be prudent to consider updating the zoning bylaw as co-working becomes a more common use in the City over time.



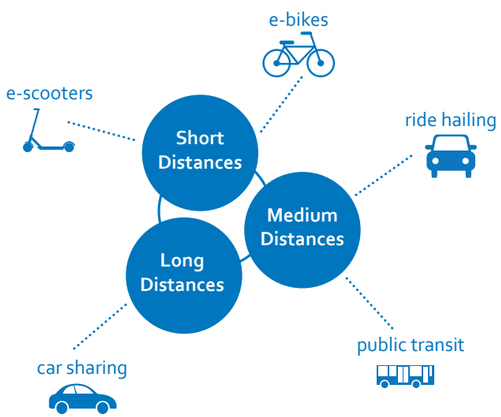
HOW WE MOVE

SMARTPHONE APPS ARE FACILITATING ACCESS TO ON-DEMAND TRANSPORTATION OPTIONS.

For the first time in decades, the transportation landscape is shifting with new mobility options being introduced throughout North America. The sharing economy is supporting a shift in transportation where people share access to a range of different transport options (shared mobility), resulting in a more efficient transportation system where cars or bikes rarely sit idle. Moreover, smartphone applications and location-based technology are facilitating on-demand access to different transportation options in real-time.

With rising numbers of people living and working in cities, urban centres will only become busier and more congested. Looking ahead, instead of owning and driving a car for various trips, a citizen might pick from a menu of transportation options (bike-share, transit, car share, ride share, and e-scooter) over the course of the day based on the demands of each trip. Cities may identify situations where shared mobility could be used as an alternative service delivery option in cases where transit is no longer cost-effective. Kelowna has already seen several shared mobility operators enter Kelowna, highlighting how the sharing economy is reshaping the transportation sector.

In the near future, shared mobility could make it possible for people in cities to no longer own a personal vehicle. The expansion of car sharing and ride-hailing platforms are making on-demand access to a vehicle a viable option.



CAR SHARING

Car sharing offers members on-demand access to a fleet of vehicles via an app, making it easy for a member to book a vehicle for a short errand across town. Members are only charged for the time they use the vehicle with the car share organization responsible for insurance, gas and fleet maintenance. Car sharing allows for much greater utilization of the cars, considering most personal automobiles sit parked 90% of the time.[xx] As car sharing grows, people may opt for memberships with several car-sharing organizations giving them access to a shared vehicle anywhere in the city without the financial costs and environmental impacts of ownership.

Classic car sharing is where the car-share vehicle is returned to the same location at the end of thrip, requiring a permanent parking spot. One-way or floating car-share allows for the vehicle to be left at a different location than where the trip started

There is strong evidence from Metro Vancouver that car sharing can support more of a "car-lite" lifestyle with Modo members in Vancouver owning on average 0.51 vehicles per household compared to the general population which on average owns 1.07- 1.17.[xxi] Car share members are also more likely to report higher rates of transit, walking and cycling, demonstrating a multi-modal approach where individuals select the most efficient option for each trip.[xxii] Already, Kelowna has seen considerable growth in car sharing with a 25% increase in Modo membership since summer August 2018.[xxiii]

CITY OF KELOWNA'S ROLE: PILOT & PARTNER

Car sharing has grown gradually thanks in part to its agreement with the City to provide fleet services. To encourage more multi-modal options the City may need to explore new partnerships to grow car sharing from a community perspective. This could take the form of new incentives for developers to provide car memberships in residential developments. The City also needs to monitor the regulatory environment to ensure car sharing can become a viable transportation option for the community. Another opportunity could be to look at how car share vehicles could support transit (e.g. vanpooling) in areas where transit is not cost-effective. Overall, by embracing a spirit of experimentation the City can supports the growth of car sharing at the community scale.

RIDE-HAILING

Ride-hailing platforms such as Uber and Lyft are also changing the calculus for car ownership. Over the last five years these online platforms have seen rapid growth, offering on-demand ride-hailing and presenting new benefits and challenges in the process. Hypothetically, ride-hailing could allow someone who walks, cycles or takes transit for most of their trips to live car-lite using ride-hailing for the handful of trips where they need a vehicle each week. In addition, ride-hailing platforms allow users to share or carpool with another user in real-time through the Uber Pool and Lyme Line service. Data is still limited, but Lyft has targeted 50% of all trips as shared rides by 2020.[xxiv] For these reasons, Uber and Lyft claim their platforms can be tools to ease congestion and promote sustainable options that reduce greenhouse gas emissions and free up road space.

In contrast, however, recent studies point to correlations between the entrance of Uber and increased congestion in some cities. [xxv] For example, as Uber entered smaller cities in the USA with less extensive transit service there is evidence that the entrance of Uber has coincided with a reduction in transit use. [xxvi] It is challenging to state definitively that these platforms are easing congestion or contributing to an increase in vehicle trips in congested areas. Ride hailing provides a convenient shared mobility option for people living in dense urban areas; however, the long-term transportation impacts require further research.

CITY OF KELOWNA'S ROLE: WAIT & SEE

The legislation passed by the province in the summer of 2019 establishes the rules for companies to apply for ride-hailing licensees in British Columbia. The made in BC regulatory approach for ride-hailing prioritizes safety, setting more stringent requirements for ride-hailing drivers than other parts of North America. As the provincial application process is underway it is still unknown how many operators will enter the Kelowna market.

The City sees potential for ride-hailing to fill gaps in the local transportation market but recognize the importance of monitoring data to understand the impacts of ride-hailing on transit use and broader transportation behaviours in Kelowna. From a regulation perspective, the City is considering removing the requirement for all drivers to have their own businesses license, allowing for drivers to operate under the company ride-hailing licensee. The City is also investigating how to manage curb-side areas to support ride-hailing. As ride-hailing enters Kelowna in the next year, there is potential for significant impacts, highlighting the importance for the City to monitor this sector to identify any potential community risks.

MICROMOBILITY

As our urban centres become the hubs for jobs and housing, there will be a significant increase in the number of short trips. As cities densify and congestion increases, the automobile will become a less practical option for these short urban trips. Shared mobility options such as bike share, e-scooters, and e-bikes (micromobility) are emerging to address the growing demand for short trips within dense urban centres where parking is limited. For example, over the last 3 years dockless e-scooters have entered many cities targeting trips of 1-3 kms. E-scooters offer people an efficient way to make short city trips, improving the first and last mile connection to and from transit in larger cities.[xxvii]

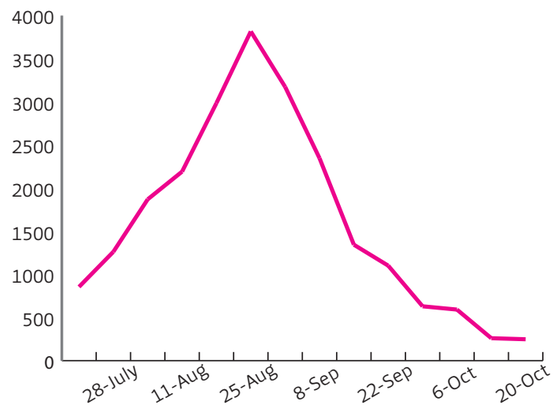
However, this mobility disruption has resulted in e-scooters competing for space with pedestrians on already congested sidewalks and multi-use pathways. Also, some cities have seen issues in terms of e-scooters cluttering up public spaces where there is no management plan in place with the operator. Kelowna was one of the first cities in western Canada to pilot e-scooters (see chart to right), but based on the small geographic area where the e-scooters were allowed it had a limited transportation impact. Moving forward, new provincial legislation allows cities to pilot e-scooters on a wider range of streets which could enhance the transportation impact. Micromobility can provide new options for short trips in our busy urban areas, but cities need to consider the safety impacts.

CASE STUDY: MOBILITY AS A SERVICE (MAAS)

Berlin is one of the first cities to create a publicly available online smartphone app that offers a full menu of transportation options from the city's metro system to e-scooter operators. In total there are nine different transportation modes available through the app. The approach of bringing all shared mobility options under one online platform is known as "mobility as a service" or MaaS. Berlin's MaaS app was piloted in June 2019 and is now available to all 3.6 million residents making it one of the largest MaaS systems in the world providing access to roughly 13,000 vehicles /options available for users.[xxviii] So far app users are booking on average of 2.3 rides per week after the launch in September 2019.[xxviii]

Several companies have also introduced e-bike share systems to expand the potential market of conventional pedal bike share systems. E-bikes are targeting longer trips (3-5km) and because they require less physical exertion, they are accessible to more people than pedal bikes. Also, they make it possible for people to use bikes for longer and more hilly trips. In Minnesota, bike share systems are offering both e-bike and pedal bike share systems so people have different options based on the nature of their trip. Other systems are mixing station-based and dockless bike share systems to provide more flexibility on where someone can begin and end their trip.[xxix] Kelowna recently accepted an application from an e-bike operator out of Toronto that is expected to provide e-bike service in Kelowna later this fall and build on the success of last year's DropBike system. The evolution of bike sharing demonstrates the importance of catering to the different needs of users to encourage a multi-modal future.

KELOWNA E-SCOOTER TRIPS PER WEEK (2019)[xxx]



CITY OF KELOWNA'S ROLE: PILOT OR PARTNER

Micro-mobility options have entered the Kelowna market in the last two years, demonstrating the potential for shared mobility to provide more sustainable options for short trips in our urban centres supporting the Community Climate Action Plan. However, because these technologies are so new, the City could take the approach of partnering with operators through the City's permit application process to understand the impacts of these new mobility options before introducing major regulatory changes. By piloting micro-mobility technologies, the City can assess impacts to public space and review impacts to community health and safety. The pilots can also assist staff in understanding if the designs of popular shared pathways will need to be reviewed to account for increased traffic associated with micro-mobility. The City could also pursue partnership opportunities with shared mobility operators to create a mobility as a service (MaaS) platform in Kelowna.

CONCLUSION

NO ONE-SIZE-FITS-ALL APPROACH FOR LOCAL GOVERNMENTS AS THEY REGULATE THE SHARING ECONOMY

The Sharing Economy is evolving at a tremendous rate, challenging the status quo and forcing us to re-think how we live, work and move around our cities. In the face of this rapid transformation, cities are tasked with the choice of taking an active or passive role in regulating the sharing economy. Historically, cities have opted for a more passive approach, emphasizing regulatory consistency for businesses and landowners. However, the "wait and see" approach poses unique challenges when applied to the sharing economy where rapid changes can have significant impacts before local governments are able to act.

Alternatively, cities have the option of taking a more active role in this transformation by working with sharing economy actors to determine how and where these new platforms are rolled out. Cities have different tools available to regulate the sharing economy. In some cases, piloting new approaches, such as Kelowna's work with bike share and e-scooters, may be the most effective way to gather data that will help local governments better understand benefits and challenges. An active role will require cities to be more nimble, monitoring and refining policies and bylaws to ensure long-term community benefit in the face of this new economy.

The Sharing Economy is nearly as diverse as the traditional economy, and there is no "one size fits all" approach for local governments. In cases where there is potential for significant negative impacts, early regulatory intervention may be warranted. In other instances, a more hands-off approach may make sense. Ultimately, we may not have a choice about whether the sharing economy will affect Kelowna, but we can play a part in shaping how it does.

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Report to Council

Date: November 18th, 2019

To: Council

From: City Manager

Subject: Okanagan Rail Trail Status Update

Department: Development Services

Recommendation:

THAT Council receives, for information, the report from the Manager, Development Engineering dated November 18, 2019, with respect to the Okanagan Rail Trail – Status Update;

AND THAT Council endorses the project principles that will guide the development of the Okanagan Rail Trail Master Plan.

Purpose:

To provide the elected leaders of the participating jurisdictions of the Okanagan Rail Trail Committee (ORTC) with a report on the status of the Okanagan Rail Trail and the activities of the Committee.

Background:

The Okanagan Rail Trail Committee (ORTC) was established in July 2018 by the Councils and Board of the participating jurisdictions (i.e. Okanagan Indian Band, City of Kelowna, District of Lake Country, Regional District of North Okanagan, District of Coldstream, City of Vernon). ORTC is responsible for the joint coordination of activities related to the planning, development, maintenance and use of the corridor. The establishment of this committee is the function of the natural progression from the Interjurisdictional Acquisition Team (IAT) through to the Interjurisdictional Development Team (IDT).

The efforts undertaken by the Okanagan Rail Trail Committee in collaboration with community partners will further assist in achieving the vision of the collective Councils and Board as established in the Okanagan Rail Trail Committee Terms of Reference:

- Develop and manage a world class rail trail that links the communities of the valley.
- Provide a range of recreational, transportation, tourism, and event opportunities in the communities through which it passes.
- Optimize the enjoyment of the rail trail experience for all.
- Manage uses to minimize conflicts between users.

- Integrate community and other regional level trails with the rail trail.
- Provide for the enjoyment, convenience, and safety of users by developing and managing and well-designed facilities along the route.
- Coordinate development, management, maintenance, and governance of the rail trail between owner jurisdictions
- Protect the long-term opportunity to develop a multi-modal transportation corridor linking the communities along the corridor.
- Ensure owner jurisdictions retain decision-making authority for all matters within their legislated mandate, bylaws and adopted official policies.
- Ensure that future planning efforts recognize that the role of the corridor may change from recreational amenity to transportation network in the long term.

Discussion:

Staff are currently pursuing the creation of a master plan. The master plan intends to ensure a balanced approach is taken for recreational and commuting opportunities in the communities through which it passes and to optimize the enjoyment of the Okanagan Rail Trail experience.

1) <u>Okanagan Rail Trail Master Plan</u>

The primary objective of the ORT Master Plan is to create a comprehensive guiding document that will assist in the long-term management and phased development of the corridor.

As part of the Master Plan development, the following are proposed principles to guide in the development of the Plan:

a) Protection and enhancement of the corridor's unique natural ecosystems and wildlife.

b) Identification of the existing and proposed future uses, features, furniture, paths, and trail enhancements.

c) Identification of recreational, cultural and historical values,

d) Respect of registered archaeological sites or sites of heritage interest including any traditional use by First Nations.

e) Balancing recreational opportunities with environmental protection.

f) Identification of areas for ecological restoration.

g) Opportunities and benefits analysis to determine use or need for existing structures, new structures, and amenities.

h) Educational values of the site, including past, present and future research activities and opportunities as well as opportunities for ongoing partnerships.

i) Creation of a long-term Development and Implementation Plan

Internal Circulation:

Policy & Planning
Development Planning
Infrastructure Operations
Parks & Buildings

Considerations applicable to this report:

As the Committee achieves key milestones and deliverables, it will bring updates back to Council.

Submitted by:

J.Kay, P.Eng
Manager, Development Engineering & Co-Rep ORTC

Approved for inclusion: M. Bayat, Director, Development Services

cc:

A.Newcombe, Divisional Director, Infrastructure
D.Edstrom, Divisional Director, Partnership & Investment
R.Smith, Divisional Director, Planning & Development
M. Kam, Sustainability Coordinator & Co-Rep ORTC

CITY OF KELOWNA

BYLAW NO. 11954

Amendment No. 35 to Airport Fees Bylaw No. 7982

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Airport Fees Bylaw No. 7982 be amended as follows:

1. THAT all references to the words "sq. metre" be deleted and replaced with "square metre".

2 AND THAT **Schedule A, 1**. **AIRCRAFT LANDING FEES** be amended by adding the following in its appropriate location:

"1.1(e) Effective January 1, 2020: Landing fees for Jet and Turbine aircraft will be $6.63 per 1,000 kgs or fraction thereof, of maximum take-off weight."

3. AND THAT **Schedule A, 2**. **GENERAL TERMINAL FEES** be amended by deleting the following:

		Effective Apr. 1/15	**Effective Jan. 1/17**	**Effective Jan. 1/18**	**Effective Jan. 1/19**
Number of Passenger Seats in Aircraft		Cost per Aircraft per use	Cost per Aircraft per use	Cost per Aircraft per use	Cost per Aircraft per use
01 – 09		$12.58	$12.96	$13.15	$13.35
10 – 15		$29.98	$30.88	$31.34	$31.81
16 – 25		$46.33	$47.72	$48.44	$49.16
26 – 45		$81.31	$83.75	$85.01	$86.28
46 – 60		$115.78	$119.25	$121.04	$122.86
61 – 89		$184.75	$190.29	$193.15	$196.04
90 - 125		$254.40	$262.03	$265.96	$269.95
126 - 150		$300.81	$309.83	$314.48	$319.20
151 - 200		$415.82	$428.29	$434.72	$441.24
201 - 250		$542.05	$558.31	$566.69	$575.19
251 - 300		$668.16	$688.20	$698.53	$709.01
301 - 400		$798.62	$822.58	$834.92	$847.44
Over 400		$984.44	$1,013.97	$1,029.18	$1,044.62

And replacing it with:

		Effective Jan. 1/17	**Effective Jan. 1/18**	**Effective Jan. 1/19**	**Effective Jan 1/20**
Number of Passenger Seats in Aircraft		Cost per Aircraft per use	Cost per Aircraft per use	Cost per Aircraft per use	Cost per Aircraft per use
01 – 09		$12.96	$13.15	$13.35	13.62
10 – 15		$30.88	$31.34	$31.81	32.45
16 – 25		$47.72	$48.44	$49.16	50.14
26 – 45		$83.75	$85.01	$86.28	88.01
46 – 60		$119.25	$121.04	$122.86	125.32
61 – 89		$190.29	$193.15	$196.04	199.96
90 - 125		$262.03	$265.96	$269.95	275.35
126 - 150		$309.83	$314.48	$319.20	325.58
151 - 200		$428.29	$434.72	$441.24	450.06
201 - 250		$558.31	$566.69	$575.19	586.69
251 - 300		$688.20	$698.53	$709.01	723.19
301 - 400		$822.58	$834.92	$847.44	864.39
Over 400		$1,013.97	$1,029.18	$1,044.62	1065.51

4. AND THAT **Schedule A, 12**. **LAND RENTAL RATES** be amended by deleting the following:

"Based on an independent study conducted in December, 2011 the land rental rates effective January 1, 2015 are:

±.50 acres	=	$0.9092 per square foot
1.0 – 5.0 acres	=	$0.6062 per square foot
6.0 – 10.0 acres	=	$0.5196 per square foot
11.0 – 20.0 acres	=	$0.4330 per square foot"

5. AND THAT **Schedule A, 14. AUTOMOBILE PARKING FEES** be amended by:

(a) Deleting the following:

"14. **AUTOMOBILE PARKING FEES"**

(b) Deleting the following:

"(b) Long Term Lot $1.75 per hour or part thereof "

And replacing it with:

"(b) Long Term Lot $1.75 for the 1st hour or part thereof "

6. AND THAT **Schedule A, 21. <u>AIRPORT FUEL TRUCK LICENCE FEE</u>** be amended by deleting the following:

"Effective February 15, 2014 a charge of $0.040 per kilogram based on the Gross Vehicle Weight of the vehicle."

And replacing it with:

"Effective December 1, 2019, a charge in accordance with *Airport Traffic Regulations* based on the Gross Vehicle Weight of the vehicle."

7. AND THAT **Schedule A, 22. <u>AIRPORT IMPROVEMENT FEES</u>**, 22.1 FOR SIGNATORY AIR CARRIERS TO THE AIR TRANSPORT ASSOCIATION (ATAC) MEMORANDUM OF AGREEMENT, 22.2 FOR NON-SIGNATORY AIR CARRIERS and 22.3 WAIVER OF AIRPORT IMPROVEMENT FEE be amended by deleting the following:

"22.1 FOR SIGNATORY AIR CARRIERS TO THE AIRPORT IMPROVEMENT FEE (AIF) MEMORANDUM OF AGREEMENT

A fee per departing passenger, less the handling fee provided for in the ATAC Memorandum of Agreement is as follows:

For travel	Fee per departing passenger
Up To December 31, 2010	$10.00
January 1, 2011 to December 31, 2012	$12.00
January 1, 2013 to March 31, 2019	$15.00
April 1, 2019 on	$20.00

22.2 FOR NON-SIGNATORY AIR CARRIERS

For travel	Fee per departing passenger
Up to December 31, 2010	$10.00
January 1, 2011 to December 31, 2012	$12.00
January 1, 2013 to March 31, 2019	$15.00
April 1, 2019 on	$20.00

22.3 WAIVER OF AIRPORT IMPROVEMENT FEE

The Airport Director may at his discretion waive the Airport Improvement Fee where an airline has donated the departing passenger's flight for a charitable clause."

And replacing it with:

"22.1 FOR SIGNATORY AIR CARRIERS TO THE AIRPORT IMPROVEMENT FEE (AIF) MEMORANDUM OF AGREEMENT OR AIF AGREEMENT

A fee per departing passenger, less the handling fee provided for in the AIF Memorandum of Agreement or AIF Agreement is as follows:

For travel	Fee per departing passenger
Up To December 31, 2010	$10.00
January 1, 2011 to December 31, 2012	$12.00
January 1, 2013 to March 31, 2019	$15.00
April 1, 2019 to February 29,2020	$20.00
March 1, 2020 on	$25.00

22.2 FOR NON-SIGNATORY AIR CARRIERS

For travel	Fee per departing passenger
Up to December 31, 2010	$10.00
January 1, 2011 to December 31, 2012	$12.00
January 1, 2013 to March 31, 2019	$15.00
April 1, 2019 to February 29,2020	$20.00
March 1, 2020 on	$25.00

22.3 WAIVER OF AIRPORT IMPROVEMENT FEE

The Airport Director may at their discretion waive the Airport Improvement Fee where an airline has donated the departing passenger's flight for a charitable clause."

8. AND THAT **Schedule A, 26**. **ELECTRIC VEHICLE CHARGING STATIONS** be added as follows:

"**26**. **ELECTRIC VEHICLE CHARGING STATIONS**

26.1 Effective December 1, 2019, a charge of $1.50 per hour."

9. This bylaw may be cited for all purposes as "Bylaw No. 11954, being Amendment No. 35 to Airport Fees Bylaw No. 7982."

10. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this 4th day of November, 2019.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk