

City of Kelowna

Regular Council Meeting

AGENDA



Monday, August 12, 2019
1:30 pm
Council Chamber
City Hall, 1435 Water Street

Pages

1. Call to Order

I would like to acknowledge that we are gathered today on the traditional, ancestral, unceded territory of the syilx/Okanagan people.

This meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

2. Confirmation of Minutes

5 - 11

Regular PM Meeting - July 29, 2019

3. Development Application Reports & Related Bylaws

3.1 Hartman Rd 280 and Maygard Rd 940, Z18-0001 - JE Holdings Inc.

12 - 13

To extend the deadline for adoption of Rezoning Bylaw No. 11597 to May 1, 2020.

3.2 Holland Road 3010, Z18-0010 - Scott R. and Chandra S. Payer, John W. and Alexandra T. Woodfield

14 - 18

To extend the deadline for adoption of Rezoning Bylaw No. 11620 to June 12, 2020.

3.3 Hwy 97 N 2339-2397, Z19-0018 Dilworth Shopping Centre Ltd, Inc. No. 319846

19 - 23

To rezone the subject property from the C4 - Urban Centre Commercial and C4rls - Urban Centre Commercial (Retail Liquor Sales) zones to C4rcs - Urban Centre Commercial (Retail Cannabis Sales) and C4rls/rcs - Urban Centre Commercial (Retail Liquor Sales/ Retail Cannabis Sales) zones to facilitate the development of a retail cannabis sales establishment.

3.4 Hwy 97 N 2339-2397, BL11919 (Z19-0018) - Dilworth Shopping Centre Ltd, Inc. No. 319846

24 - 24

To give Bylaw No. 11919 first reading in order to rezone the subject property from the C4 and C4rls zones to the C4rcs and C4rls/rcs zones.

3.5	Findlay Rd 1308, Z18-0107 - R 547 Enterprises Ltd	25 - 32
	To rezone the subject property from the RU1 – Large Lot Housing zone to the I2 – General Industrial zone to facilitate the use of the property for an industrial development.	
3.6	Findlay Rd 1308, BL11920 (Z18-0107) - R 547 Enterprises Ltd	33 - 33
	To give Bylaw No. 11920 first reading in order to rezone the subject property from the RU1 – Large Lot Housing zone to the I2 – General Industrial zone.	
3.7	Greene Road 604, Z19-0073 - James A. Baumgartner and Elanda C. Baumgartner	34 - 38
	To rezone the subject property from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone to allow the conversion of an existing accessory building to a carriage house.	
3.8	Greene Road 604, BL11921 (Z19-0073) - James A. Baumgartner and Elanda C. Baumgartner	39 - 39
	To give Bylaw No. 11921 first reading in order to rezone the subject property from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone.	
3.9	Lakeshore Rd 110-3140, Z19-0059 - D and R Properties Ltd, Inc. No BCo562497	40 - 45
	To rezone the subject property from the C4lp - Urban Centre Commercial (Liquor Primary) zone to the C4lp/rcs - Urban Centre Commercial (Liquor Primary/Retail Cannabis Sales) zone to facilitate the development of a retail cannabis sales establishment.	
3.10	Lakeshore Rd 110-3140, BL11922 (Z19-0059) - D and R Properties Ltd, Inc. No BCo562497	46 - 46
	To give Bylaw No. 11922 first reading in order to rezone the subject property from the C4lp - Urban Centre Commercial (Liquor Primary) zone to the C4lp/rcs - Urban Centre Commercial (Liquor Primary/Retail Cannabis Sales) zone.	
3.11	McIntosh 280, DP19-0059 - Provincial Rental Housing Corporation, Inc. No BC0052129	47 - 70
	To consider a Development Permit for the form and character of a proposed 4-storey supportive housing project on the subject property.	

4. Bylaws for Adoption (Development Related)

- 4.1 Airport Way 5505-5507, BL11828 (Z19-0034) - Midwest Ventures Ltd., Inc. No. BC0046021** 71 - 72

To adopt Bylaw No. 11828 in order to rezone the subject property from the Cg - Tourist Commercial zone and from the CD15 - Airport Business Park to the Cgrcs - Tourist Commercial (Retail Cannabis Sales) zone.

- 4.2 Commerce Ave 1675-1677, BL11855 (Z19-0004) - Braemar Properties Ltd, Inc. No. BC1023360** 73 - 73

To adopt Bylaw No. 11855 in order to rezone the subject property from the C10lp/rls – Service Commercial (Liquor Primary/Retail Liquor Sales) zone to the C10lp/rls/rcs - Service Commercial (Liquor Primary/Retail Liquor Sales/Retail Cannabis Sales) zone.

5. Non-Development Reports & Related Bylaws

- 5.1 Transportation Master Plan Existing and Future Conditions** 74 - 117

To provide Council with an overview of existing and projected future transportation conditions, along with a summary of future challenges and opportunities to be addressed in the upcoming Transportation Master Plan.

- 5.2 2040 Infrastructure Impact Analysis** 118 - 152

To provide Council with a high-level review of the infrastructure impacts associated with the 2040 Official Community Plan (2040 OCP) Growth Scenario.

- 5.3 OCP 2040: Draft Land Use Map and Public Engagement Process** 153 - 195

To provide Council with a draft Future Land Use map that translates the ideas and objectives of the Official Community Plan Update process to date into a draft land use vision for the purposes of public engagement in Fall 2019.

- 5.4 Healthy Housing Strategy Implementation - Rental Housing Incentives Update** 196 - 214

To update the Council Policy 335: Rental Housing Grants Policy, the Housing Opportunities Reserve Fund Bylaw 8593 and the Revitalization Tax Exemption Bylaw 9561 to align with the recommendations of the Healthy Housing Strategy.

- 5.5 BL11911 - Amendment No. 6 to the Housing Opportunities Reserve Fund Bylaw No. 8593** 215 - 216

To give Bylaw No. 11911 first, second and third reading.

- 5.6 BL11912 - Amendment No. 7 to Revitalization Tax Exemption Bylaw No. 9561** 217 - 220

To give Bylaw No. 11912 first, second and third reading.

5.7	Subdivision, Development and Servicing Bylaw 7900 - Schedule 4 and 5 Update - Stormwater	221 - 238
	To amend the Subdivision, Development and Servicing Bylaw 7900 Design so it aligns with industry best practice and ensure construction of quality and long-lasting infrastructure.	
5.8	Bylaw No.11913 - Amendment No.20 to the Subdivision, Development and Servicing Bylaw No.7900	239 - 269
	To give Bylaw No.11913 first, second and third reading.	
5.9	2019 Financial Plan Amendment - Kelowna International Airport	270 - 272
	To obtain Council's approval to amend the 2019 Financial Plan.	
6.	Bylaws for Adoption (Non-Development Related)	
6.1	BL11851 - Amendment No. 34 to Traffic Bylaw No. 8120	273 - 284
	To adopt Bylaw No. 11851 in order to amend the Traffic Bylaw No. 8120	
6.2	BL11852 - Amendment No. 25 to Bylaw Notice Enforcement Bylaw No. 10475	285 - 286
	To adopt Bylaw No. 11852 in order to amend the Bylaw Notice Enforcement Bylaw No. 10475.	
6.3	BL11853 - Amendment No. 2 to Payment in Lieu of Parking Bylaw No. 8125	287 - 289
	To adopt Bylaw No. 11853 in order to amend Payment in Lieu of Parking Bylaw No. 8125.	
7.	Mayor and Councillor Items	
8.	Termination	



City of Kelowna

Regular Council Meeting

Minutes

Date: Monday, July 29, 2019
 Time: 1:30 pm
 Location: Council Chamber
 City Hall, 1435 Water Street

Members Present Mayor Colin Basran, Councillors Maxine DeHart, Ryan Donn, Gail Given, Brad Sieben, Mohini Singh, Luke Stack and Loyal Wooldridge

Members Absent Councillor Charlie Hodge

Staff Present City Manager, Doug Gilchrist; City Clerk, Stephen Fleming; Divisional Director, Planning & Development Services, Ryan Smith*; Development Planning Department Manager, Terry Barton*; Urban Planning & Development Policy Manager, Laura Bentley*; Cultural Services Manager, Christine McWillis*; Integrated Transportation Department Manager, Rafael Villarreal Pacheco*, Transportation Engineer, Chad Williams*; Parking Services Manager, Dave Duncan*; Legislative Coordinator (Confidential) Clint McKenzie

Guests Present Jamie Vann Struth, Vann Struth Consulting*

(* Denotes partial attendance)

1. Call to Order

Mayor Basran called the meeting to order at 1:32 p.m.

I would like to acknowledge that we are gathered today on the traditional, ancestral, unceded territory of the syilx/Okanagan people.

Mayor Basran advised that the meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

2. Confirmation of Minutes

Moved By Councillor Loyal Wooldridge/Seconded By Councillor Donn

R723/19/07/29 THAT the Minutes of the Regular Meetings of July 15 and July 17th be confirmed as circulated.

Carried

3. Development Application Reports & Related Bylaws

3.1 Glenwood Ave 455, Z19-0038 (BL11914) - Mark Procknow

Staff:

- Displayed a PowerPoint Presentation summarizing the application.

Moved By Councillor Stack/Seconded By Councillor Loyal Wooldridge

R724/19/07/29 THAT Rezoning Application No. Z19-0038 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot C, District Lot 14, ODYD, Plan 5742, located at 455 Glenwood Avenue, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration.

Carried

3.2 Glenwood Ave 455, BL11914 (Z19-0038) - Mark Procknow

Moved By Councillor Loyal Wooldridge/Seconded By Councillor Given

R725/19/07/29 THAT Bylaw No. 11914 be read a first time.

Carried

3.3 Fitzpatrick Rd 249, Z19-0064 (BL11915) - Patrick and Hendrika Hayden

Staff:

- Displayed a PowerPoint Presentation summarizing the application

Moved By Councillor Donn/Seconded By Councillor Loyal Wooldridge

R726/19/07/29 THAT Rezoning Application No. Z19-0064 to amend the City of Kelowna Zoning bylaw No. 8000 by changing the zoning classification of Lot 1, Section 35, Township 26, Osoyoos Division Yale District Plan KAP55899, located at 249 Fitzpatrick Rd, Kelowna, BC from the RU1-Large Lot Housing zone to the RU1c- Large Lot Housing with Carriage House zone, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration.

Carried

3.4 Fitzpatrick Rd 249, BL11915 (Z19-0064) - Patrick and Hendrika Hayden

Moved By Councillor Given/Seconded By Councillor Loyal Wooldridge

R727/19/07/29 THAT Bylaw No. 11915 be read a first time.

Carried

3.5 Gordon Drive 1450 and Lawson Avenue 1094, Z19-0058 (BL11916) - 1094 Lawson Avenue Ltd. Inc. No. BC1176688

Staff:

- Displayed a PowerPoint Presentation summarizing the application and responded to questions from Council.

Moved By Councillor Donn/Seconded By Councillor Singh

R728/19/07/29 THAT Rezoning Application No. Z19-0058 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 1 District Lot 138 ODYD Plan 3809, located at 1450 Gordon Dr., Kelowna, BC from the RU6 – Two Dwelling Housing zone to the RM5 – Medium Density Multiple Housing zone;

AND THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 2 District Lot 138 ODYD Plan 3809, located at 1094 Lawson Ave., Kelowna, BC from the RM4 – Transitional Low Density Housing zone to the RM5 – Medium Density Multiple Housing zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Development Planning Department dated July 29, 2019;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the consolidation of Lots 1 & 2 District Lot 138 ODYD Plan 3809;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered in conjunction with Council's consideration of a Development Permit for the subject property.

Carried

3.6 Gordon Drive 1450 and Lawson Avenue 1094, BL11916 (Z19-0058) - 1094 Lawson Avenue Ltd. Inc. No. BC1176688

Moved By Councillor Loyal Wooldridge/Seconded By Councillor Given

R729/19/07/29 THAT Bylaw No. 11916 be read a first time.

3.7 Kneller Rd 210, BL11738 (Z18-0099) - Gurdev and Kamalpreet Gill

Moved By Councillor Given/Seconded By Councillor Loyal Wooldridge

R730/19/07/29 THAT Bylaw No. 11738 be adopted.

Carried

3.8 Kneller Rd 210, DP18-0183 - Gurdev and Kamalpreet Gill

Staff:

- Displayed a PowerPoint Presentation summarizing the application.

Moved By Councillor Given/Seconded By Councillor Loyal Wooldridge

R731/19/07/29 THAT final adoption of Rezoning Bylaw No. 11738 be considered by Council;

AND THAT Council authorizes the issuance of Development Permit No. DP18-0183 for Lot B Section 27 ODYD Plan 17467, located at 210 Kneller Road, Kelowna BC, subject to the following:

1. The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";
2. The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";
3. Landscaping to be provided on the land be in accordance with Schedule "C";
4. The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect;

AND THAT the applicant be required to complete the above noted conditions of Council's approval of the Development Permit Application in order for the permit to be issued;
AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

Carried

4. Bylaws for Adoption (Development Related)

4.1 Lawrence Ave 1100, BL11846 (LUC19-0001) - M.R.D. Holdings Inc. No. BCo681200

Moved By Councillor Stack/Seconded By Councillor DeHart

R732/19/07/29 THAT Bylaw No. 11846 be adopted.

Carried

4.2 Lawrence Ave 1100, BL 11847 (Z19-0031) - M.R.D. Holdings Inc. No. BCo681200

Moved By Councillor DeHart/Seconded By Councillor Stack

R733/19/07/29 THAT Bylaw No. 11847 be adopted.

Carried

5. Non-Development Reports & Related Bylaws

5.1 Creative Sector Economic Impact Assessment

Staff introduced, Jamie Vann Struth of Vann Struth Consulting, who displayed a PowerPoint Presentation summarizing the results of the assessment and responded to questions from Council .

Moved By Councillor Singh/Seconded By Councillor Donn

R734/19/07/29 THAT Council receives for information the report from the Cultural Services Department dated July 29, 2019 regarding the results of the Creative Sector Economic Impact Assessment.

Carried

5.2 2019 Active Transportation Capital Projects Budget Amendments

Staff:

- Summarized the capital budget amendments.

Moved By Councillor Given/Seconded By Councillor Singh

R735/19/07/29 THAT Council receives, for information, the report from the Transportation Planning Engineer dated July 29, 2019 regarding the 2019 Active Transportation Capital Projects Budget Amendments;

AND THAT the 2019 Financial Plan be amended to reflect the \$237,000 grant funding received from Bike BC to support the Ethel 4 DCC (Sutherland – Springfield) Active Transportation (ATC) project;

AND THAT the 2019 Financial Plan be amended to include the new Ethel 3C DCC (Rose-Raymer) Active Transportation Corridor project, funded from a transfer of \$130,000 from the Ethel 4 DCC, ATC project;

AND THAT the 2019 Financial Plan be amended to include the new Casorso 4 DCC (Raymer Ave-KLO Rd) Active Transportation Corridor project, funded from a transfer of \$15,000 from the Ethel 4 DCC, ATC project;

AND THAT the 2019 Financial Plan be amended to include the new Okanagan Rail Trail Paving, Gordon to West of Ellis project, funded from a transfer of \$100,000 from the Ethel 4 DCC, ATC project;

AND FURTHER THAT the 2019 Financial Plan be amended to include the new Clement and Gordon Intersection Improvements project, funded from a transfer of \$50,000 from the Ethel 4 DCC, ATC project and \$80,000 from the Rails with Trails (Commerce Ave to Hwy 97 Overpass) project.

Carried

5.3 Downtown Area Parking Plan - Bylaw Updates

Staff:

- Displayed a PowerPoint presentation summarizing the bylaw amendments and responded to questions from Council.

Moved By Councillor Loyal Wooldridge/Seconded By Councillor Donn

R736/19/07/29 THAT Council receives, for information, the Report from Parking Services, dated July 29, 2019, with respect to the Downtown Area Parking Plan – Bylaw Amendments;

AND THAT Bylaw No. BL11851, being Amendment No. 34 to Traffic Bylaw No. 8120, be forwarded for reading consideration;

AND THAT Bylaw No. BL11852, being Amendment No. 25 to Bylaw Notice Enforcement Bylaw No. 10475, be forwarded for reading consideration;

AND FURTHER THAT Bylaw No. BL11853, being Amendment No. 2 to Payment in Lieu of Parking Bylaw No. 8125, be forwarded for reading consideration.

Carried

5.4 BL11851 - Amendment No. 34 to Traffic Bylaw No. 8120

Moved By Councillor Stack/Seconded By Councillor DeHart

R737/19/07/29 THAT Bylaw No. 11851 be read a first, second and third time.

Carried

5.5 BL11852 - Amendment No. 25 to Bylaw Notice Enforcement Bylaw No. 10475

Moved By Councillor DeHart/Seconded By Councillor Sieben

R738/19/07/29 THAT Bylaw No. 11852 be read a first, second and third time.

Carried

5.6 BL11853 - Amendment No. 2 to Payment in Lieu of Parking Bylaw No. 8125

Moved By Councillor Donn/Seconded By Councillor Sieben

R739/19/07/29 THAT Bylaw No. 11853 be read a first, second and third time.

Carried

6. Bylaws for Adoption (Non-Development Related)

6.1 BL11647 - Amendment No. 1 to the Soil Removal and Deposit Regulation Bylaw No. 9612

Moved By Councillor Donn/Seconded By Councillor Sieben

R740/19/07/29 THAT BYLAW NO. 11647 be adopted.

Carried

7. Mayor and Councillor Items

Councillor Wooldridge:

- Attended the Bridge Youth and Families 6th anniversary celebrations.
- Noted the importance of the wrap around programs in the community from the organization.

Councillor DeHart:

- Attended a new City employee orientation on behalf of the Mayor.
- Attended the Okanagan Mission Seniors Centre 50th anniversary along with several other councillors.
- Noted the success of the East Kelowna Market held at the Mission Community Hall.
- Attended Rick Weber's retirement celebration.

Councillor Stack:

- Attended Rick Weber's retirement celebrations and reflected on how the event was a fun opportunity to reflect back on how the news media has evolved over the years.
- Attended the Kelowna Chamber luncheon last week with BC Housing CEO Shayne Ramsay as guest speaker. It was great to have them speak directly and have an open discussion with questions. It showed BC Housing is paying attention and want to make a positive impact in Kelowna.

Mayor Basran congratulated Rick Weber on his retirement.

8. Termination

This meeting was declared terminated at 2:33 p.m.

Mayor Basran

sf/cm



City Clerk

DRAFT

Report to Council



Date: August 12 2019
To: Council
From: City Manager
Subject: Z18-0001 Extension Report for Bylaw 11597
Department: Development Planning

Recommendation:

THAT in accordance with Development Application Procedures Bylaw No. 10540, the deadline for the adoption of Rezoning Amending Bylaw No. 11597, Z18, 0001, for Lots 1 & 2 Section 26 Township 26 Osoyoos Division Yale District Plan 22455, 280 Hartman Road and 940 Maygard Road, be extended from May 1, 2019 to May 1, 2020;

AND THAT Council directs Staff to not accept any further extension requests.

Purpose:

To extend the deadline for adoption of Rezoning Bylaw No. 11597 to May 1, 2020.

Development Planning:

Final adoption of the Zone Amending Bylaw is subject to the applicant meeting the Development Engineering requirements for off-site work. The applicant has made progress on the rezoning application and is requesting additional time to meet all the engineering conditions.

Staff are recommending that Council supports extending the deadline for adoption for the Rezoning Bylaw No. 11597 by one year to May 1, 2020.

Subject Property Map:



Application Chronology

Date of Application Received: December 20, 2017

Date of Extension Received: July 27, 2019

Report prepared by: Heather Benmore, Development Planning Department

Approved for inclusion:



Terry Barton, Development Planning Manager

cc: T. Atwood

REPORT TO COUNCIL



Date: August 12, 2019
To: Council
From: City Manager
Department: Development Planning

Application: Z18-0010
Owner: Scott Richard Payer and Chandra Sarasotee Payer; John William Woodfield and Alexandra Tiffany Woodfield

Address: 3010 Holland Road
Applicant: Urban Options Planning & Permits

Subject: Rezoning Extension Application

Existing OCP Designation: S2RES – Single / Two Unit Residential

Existing Zone: RU1 – Large Lot Housing

Proposed Zone: RU6 – Two Dwelling Housing

1.0 Recommendation

THAT in accordance with Development Application Procedures Bylaw No. 10540, the deadline for the adoption of Rezoning Bylaw No. 11620, be extended from June 12, 2019 to June 12, 2020.

AND THAT Council directs Staff to not accept any further extension requests.

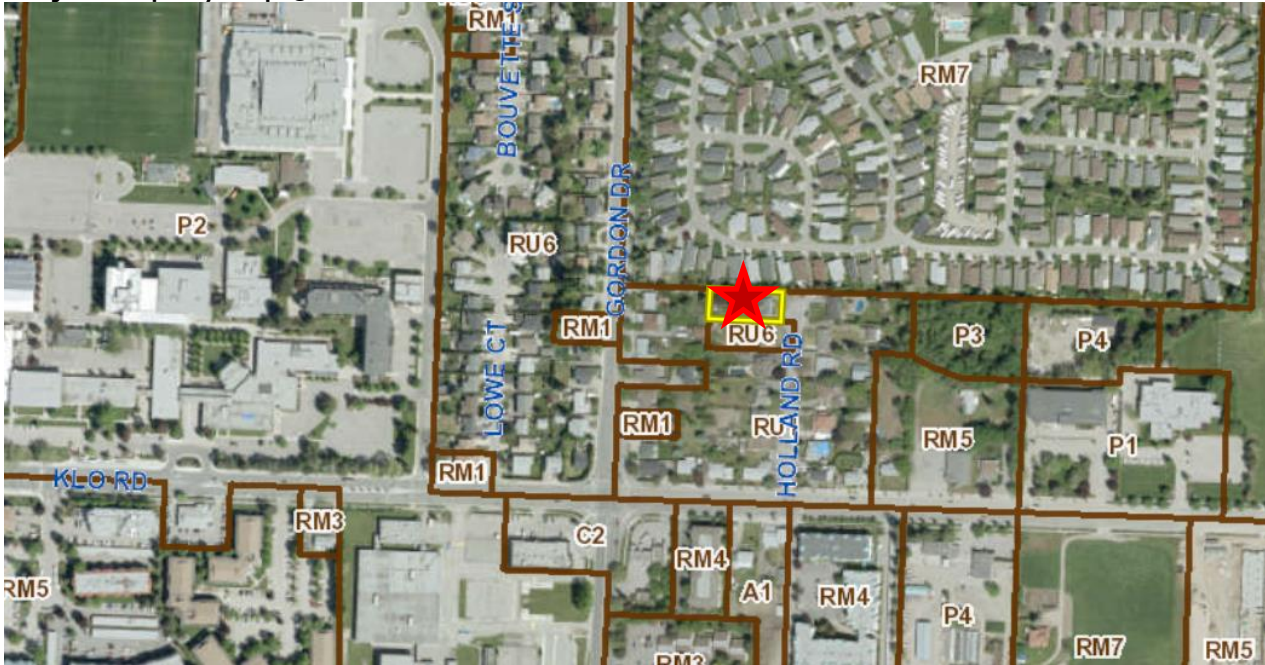
2.0 Purpose

To extend the deadline for adoption of Rezoning Bylaw No. 11620 to June 12, 2020.

3.0 Development Planning

Rezoning Bylaw No. 11620 received second and third readings at a Regular meeting of Council held on Tuesday, June 12, 2018. Final adoption of the zone amendment bylaw is subject to the applicant meeting the requirements of Schedule A, Development Engineering Memorandum. The applicant made progress on the rezoning application; however, has not yet met all of the engineering requirements. Staff are recommending that Council supports extending the deadline for adoption for the Rezoning Bylaw No. 11620 by one year to June 12, 2020.

Subject Property Map: 3010 Holland Road



4.0 Application Chronology

Date of Application Received: January 3, 2018
Date of Second and Third Readings: June 12, 2018
Date of Extension Request Received: July 23, 2019

Report prepared by: Barbara B. Crawford, Planner II

Reviewed by: Laura Bentley, Urban Planning & Development Policy Manager

Approved for Inclusion: Terry Barton, Development Planning Department Manager

Attachments:

Schedule A - Development Engineering Memorandum
Attachment A – Rationale Letter

CITY OF KELOWNA

MEMORANDUM

Date: February 01, 2018
File No.: Z18-0010
To: Community Planning (BC)
From: Development Engineering Manager(JK)
Subject: 3010 Holland Road RU1 to RU6

The Development Engineering Department has the following comments and requirements associated with this rezoning application. The road and utility upgrading requirements outlined in this report will be a requirement of this development. The Development Engineering Technologist for this project is Jason Angus

1. Domestic Water and Fire Protection

This property is currently serviced with a 19mm-diameter water service. The disconnection of the existing small diameter water services and the tie-in of a larger new 50mm service c/w two curb stops can be provided by City forces at the developer's expense. The applicant will be required to sign a Third Party Work Order for the cost of the water service upgrades. For estimate inquiry's please contact Jason Angus, by email jangus@kelowna.ca or phone, 250-469-8783.

2. Sanitary Sewer

Our records indicate that this property is currently serviced with a 100mm-diameter sanitary sewer service. An inspection chamber (IC) complete with brooks box must be installed on the service at the owner's cost. Service upgrades can be provided by the City at the applicant's cost. The applicant will be required to sign a Third Party Work Order for the cost of the service upgrade. For estimate inquiry's please contact Jason Angus, by email jangus@kelowna.ca or phone, 250-469-8783

3. Road Improvements

Holland Road must be upgraded to an urban arterial standard along the full frontage of this proposed development, including sidewalk, pavement removal and replacement, boulevard landscaping, street lighting and re-location or adjustment of utility appurtenances if required to accommodate the upgrading construction. A one-time cash payment in lieu of construction must be collected from the applicant for future construction by the City. The cash-in-lieu amount is determined to be **\$30,602.56** not including utility service cost.

4. Development Permit and Site Related Issues

Direct the roof drains into on-site rock pits or splash pads.

One access to the property will be permitted. A maximum 6m wide driveway will be permitted.

SCHEDULE		A
This forms part of application # Z18-0010		
Planner Initials	BC	 City of Kelowna <small>DEVELOPMENT PLANNING</small>

5. Electric Power and Telecommunication Services

The electrical and telecommunication services to this building must be installed in an underground duct system, and the building must be connected by an underground service. It is the developer's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for these services, which would be at the applicant's cost.

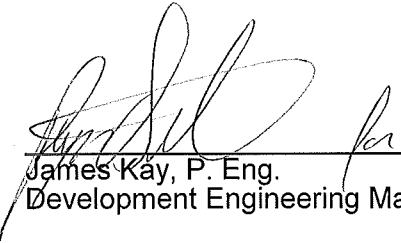
7. Bonding and Levy Summary

(a) Levies

1. Holland Road. frontage improvements	\$30,602.56
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(b) Bonding

1. Service upgrades	To be determined
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James Kay, P. Eng.
Development Engineering Manager

JA



ATTACHMENT		A
This forms part of application		
# Z18-0010		
Planner Initials	BC	The logo for the City of Kelowna Development Planning, featuring a colorful circular design with the text "City of Kelowna" and "DEVELOPMENT PLANNING" below it.

July 15, 2019

City of Kelowna
Urban Planning Department
Attn. Barbara Crawford
1435 Water Street
Kelowna, BC

RE: Extension of Rezoning application Z18-0010, at 3010 Holland Road

Dear Planning Staff:

We would like to extend the adoption date for application Z18-0010, the application to rezone the property at 3010 Holland Road to the RU6 – Two Dwelling Housing zone.

The owners are seeking extension of this application, which was considered at a Public Hearing on June 12, 2019, where the application received 2nd and 3rd readings. The owners have been attempting to engage the services of an engineering consultant to provide the engineering work that was outlined by the Council report. However, owing to current market conditions and the small size of the required work, they have been unable to find a consultant willing to take on the work.

We are seeking this extension in order to provide additional time to find a consultant to take on this project.

Regards,

A handwritten signature in blue ink, appearing to read "Birte Decloux".

Birte Decloux

REPORT TO COUNCIL



Date: August 12, 2019

To: Council

From: City Manager

Department: Development Planning

Application: Z19-0018

Owner: Dilworth Shopping Centre Ltd.,
Inc. No. 319846

Address: 2339-2397 Hwy 97 N

Applicant: BC Liquor Distribution Branch

Subject: Rezoning Application

Existing Zone: C4 – Urban Centre Commercial
C4rls – Urban Centre Commercial (Retail Liquor Sales)

Proposed Zone: C4rcs - Urban Centre Commercial (Retail Cannabis Sales)
C4rls/rcs – Urban Centre Commercial (Retail Liquor Sales/ Retail Cannabis Sales)

1.0 Recommendation

THAT Rezoning Application No. Z19-0018 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of portions of Lot A, District Lots 126 and 532 Osoyoos Division Yale District Plan 40108, located at 2339-2397 Hwy 97 N, Kelowna, BC, from the C4 - Urban Centre Commercial and C4rls - Urban Centre Commercial (Retail Liquor Sales) zones to C4rcs - Urban Centre Commercial (Retail Cannabis Sales) and C4rls/rcs - Urban Centre Commercial (Retail Liquor Sales/ Retail Cannabis Sales) zones as shown on Map "A" attached to the report from the Development Planning Department dated August 12, 2019, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the approval of the Ministry of Transportation and Infrastructure;

AND FURTHER THAT if the Rezoning Bylaw is adopted, Council direct Staff to send a recommendation to the British Columbia Liquor and Cannabis Regulation Branch that they support issuance of a non-medical cannabis retail store license for this legal lot with the following comments:

- The proposed location meets local government bylaw requirements and as such, no negative impact is anticipated;
- The views of the residents were captured during a public hearing process for the rezoning of the property and Council meeting minutes summarizing those views are attached; and

- Local government recommends that the application be approved because of compliance with local regulations and policies.

2.0 Purpose

To rezone the subject property from C₄ and C₄rls to C₄rcs and C₄rls/rcs to facilitate the development of a retail cannabis sales establishment.

3.0 Development Planning

Development Planning Staff recommend support for the rezoning application to allow for a retail cannabis sales establishment on the subject property. This application was received prior to the close of the first intake of retail cannabis sales establishment applications on November 30, 2018. As such, it was evaluated in accordance with a Council-endorsed process and was selected, through a lottery process, to move forward with the City's standard rezoning process, established in the Development Application Procedures Bylaw. This application received a score of 63.57, and the overall average overall score of all applications evaluated was 69.53.

Should Council support the proposed Rezoning Bylaw, the property would be rezoned to a retail cannabis sales subzone, and Staff would send a recommendation to the British Columbia Liquor and Cannabis Regulation Branch indicating support for issuance of a non-medical cannabis retail store for this property.

The application meets Zoning Bylaw No. 8000 and no variances are being requested. Further, the applicant has confirmed the completion of public notification in accordance with Council Policy No. 367.

4.0 Proposal

4.1 Background

A retail cannabis sales establishment is proposed to be located at the north corner of the Dilworth Shopping Centre in an existing commercial unit.

4.2 Site Context

Sensitive uses identified within a 150m radius include a Retail Liquor Sales Establishment and a Liquor Primary Establishment.

Specifically, adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	C9P- Tourist Commercial	Hotel
East	C10- Service Commercial	Retail stores, service commercial
South	RM5- Medium Density Multiple Housing	Multiple dwelling housing
West	C4- Urban Centre Commercial/ C10- Service Commercial	Retail stores, service commercial

Subject Property Map: 2339-2397 Hwy 97 N



5.0 Technical Comments

5.1 Development Engineering Department

This application does not compromise any City of Kelowna municipal infrastructure.

6.0 Application Chronology

Date of Committee Evaluation of Application: January 10, 2019

Date of Application Accepted: February 8, 2019

Date Public Consultation Completed: March 14, 2019

Report prepared by: Jocelyn Black, Planner Specialist

Reviewed by: Laura Bentley, Urban Planning & Development Policy Manager

Approved for Inclusion: Terry Barton, Development Planning Department Manager

Attachments:

Attachment A: Site Plan

Attachment B: Map "A"

A

#Z19-0018



Bj



City of
Kelowna
DEVELOPMENT PLANNING

SITE PLAN

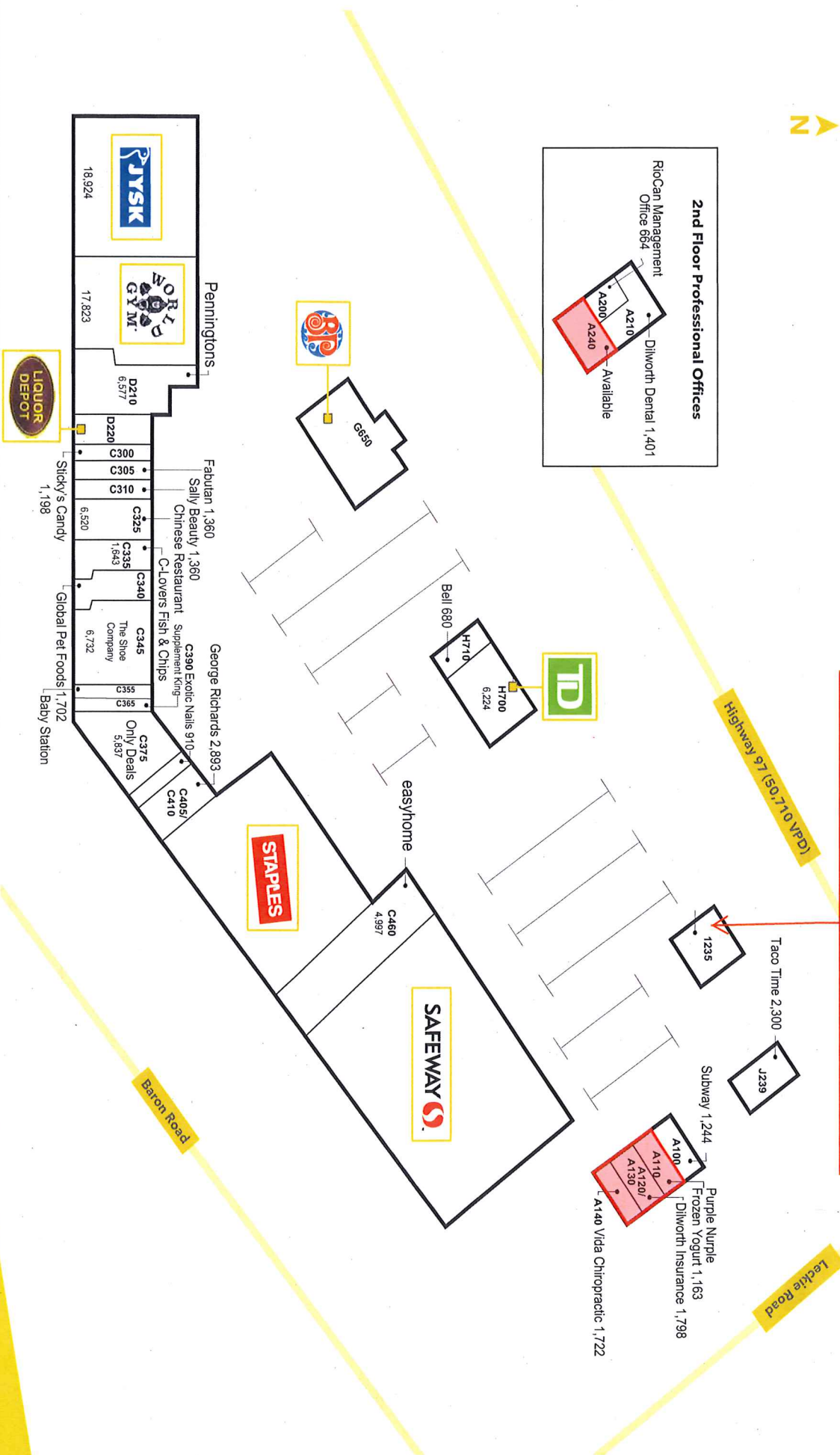
PROPOSED RETAIL CANNABIS STORE:

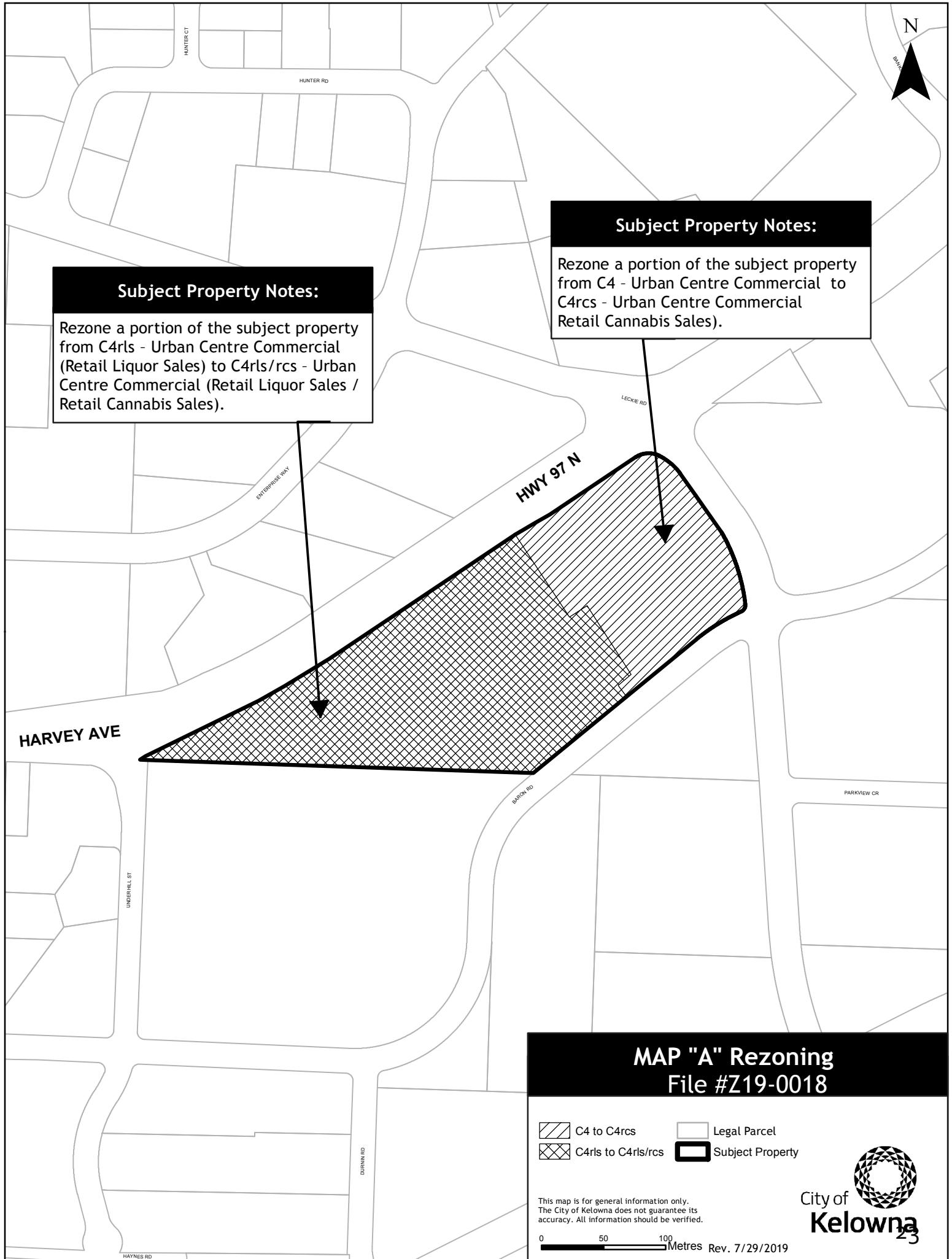
-C4 permitted zoning

-500m away from other cannabis retail store

- 500 m away from all schools

2
DILWORTH SHOPPING CENTRE





CITY OF KELOWNA
BYLAW NO. 11919
Z19-0018 – 2339-2397 Highway 97 N

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of portions of Lot A, District Lots 126 and 532 Osoyoos Division Yale District Plan 40108, located on Hwy 97 N, Kelowna, BC, from the C₄ - Urban Centre Commercial and C₄rls - Urban Centre Commercial (Retail Liquor Sales) zones to C₄r_{cs} - Urban Centre Commercial (Retail Cannabis Sales) and C₄rls/r_{cs} - Urban Centre Commercial (Retail Liquor Sales/Retail Cannabis Sales) zones as per Map "A" attached to and forming part of this bylaw.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Approved under the Transportation Act this

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: August 12, 2019

RIM No. 1250-30

To: City Manager

From: Community Planning Department (SS)

Application: Z18-0107

Owner: R 547 Enterprises Ltd.

Address: 1308 Findlay Rd

Applicant: Urban Options Planning & Permits

Subject: Rezoning Application

Existing OCP Designation: Industrial/ Park

Existing Zone: RU1 – Large Lot Housing

Proposed Zone: I2 – General Industrial

1.0 Recommendation

THAT Rezoning Application No. Z18-0107 to amend the City of Kelowna Zoning Bylaw No.8000 by changing the zoning classification of Lot B, Section 34, Township 26, ODYD Plan 14612 located at 1308 Findlay Rd, Kelowna, BC from the RU1 – Large Lot Housing zone to the I2 – General Industrial zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Development Planning Department dated August 12th, 2019;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered subsequent to a technical subdivision for the dedication of the linear park and riparian management area along Francis Brook.

2.0 Purpose

To rezone the subject property from RU1 – Large Lot Housing to I2 – General Industrial to facilitate the use of the property for an industrial development.

3.0 Development Planning

Staff are supportive of the proposed rezoning of the subject property to I2 – General Industrial zone as it is consistent with the Official Community Plan (OCP) Future Land Use Designation. The property is located within the Permanent Growth Boundary and is located adjacent to and near other industrially zoned properties. The I2 – General Industrial zone will allow several industrial uses on the property similar to a number of properties in the immediate area.

4.0 Proposal

4.1 Project Description

The subject property currently has as a residential dwelling on the south east corner and a stream, Francis Brook flowing parallel to the north property boundary line. The applicant would like to use the property for industrial purposes. A preliminary site plan drawing is attached to the rezoning application and Staff are not tracking any variances associated with the proposal. While it is not part of this application, there are future plans to construct an industrial building on the site. This application does trigger two Development Permits (DP), a Comprehensive DP and a Natural Environmental DP, should Council support the Rezoning bylaw, the Natural Environmental DP will be required, and a Comprehensive DP required prior to development of the property.

4.2 Site Context

The subject property is located in the Rutland Sector of the city. Adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	A1 - Agriculture	Residential
East	RM3 – Low Density Multiple Housing	Multi-family Residential
South	I1 – Business Industrial	Concrete business
West	I2 – General Industrial/ A1 - Agriculture	Industrial / Residential

Subject Property Map: 1308 Findlay Rd



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Industrial Land Use Policies

Objective 5.28.¹ Focus industrial development to areas suitable for industrial use.

6.0 Technical Comments

6.1 Development Engineering Department

- See Attachment A.

6.2 Parks & Buildings Planning

- Per OCP Policy 5.14.2, a 5-meter-wide linear park must be dedicated at the north end of the property, outside the riparian management area requirements per OCP Chapter 12 (15m width per Table 12.1 in the OCP). The OCP outlines that this dedication is to occur at rezoning, so the definition of the riparian area and adjacent linear park must occur at rezoning and not wait for the Environmental Development Permit.

7.0 Application Chronology

Date of Application Received: September 14th, 2018

Date Public Consultation Completed: December 17th, 2018

Report prepared by:

Sergio Sartori, Development Technician

Reviewed by:

Dean Strachan, Suburban and Rural Planning Manager

Approved for Inclusion:

Terry Barton, Development Planning Department Manager

Attachments:

Schedule A: Development Engineering Memorandum

Attachment A: Site Plan

¹ City of Kelowna Official Community Plan, Objective 5.28, Chapter 10 (Development Process Chapter).

CITY OF KELOWNA

MEMORANDUM

Date: October 9, 2018
File No.: Z18-0107
To: Community Planning (AK)
From: Development Engineering Manager(JK)
Subject: 1308 Findlay Rd RU1 to I2

Development Engineering has the following requirements associated with this application. The road and utility upgrading requirements outlined in this report will be a requirement of this development.

The Development Engineering Technologist for this project is Aaron Sangster

1) General

- a) These are Development Engineering comments/requirements and are subject to the review and requirements from the Ministry of Transportation (MOTI) Infrastructure Branch.

2) Domestic Water and Fire Protection

- a) This development is within the service area of the Black Mountain Irrigation District (BMID). The developer is required to make satisfactory arrangements with the BMID for these items. All charges for service connection and upgrading costs, as well as any costs to decommission existing services are to be paid directly to BMID.
- b) Provide an adequately sized domestic water and fire protection system complete with individual lot connections. The water system must be capable of supplying domestic and fire flow demands of the project in accordance with the Subdivision, Development & Servicing Bylaw. Only one service will be permitted for this development.

3) Sanitary Sewer

- a) The development site is presently serviced with a 100mm-diameter sanitary sewer service. Only one service will be permitted for this development. The developer's consulting civil engineer will determine sanitary sizing for this development. The applicant, at his cost, will arrange for the removal of the existing service and the installation of a new larger service if required.
- b) 1308 Findlay Rd is currently within Sanitary Sewer Specified Area # 20. The developer will be responsible to cash commute, pay in full, the specified area charges for this development. The charge is currently set at \$1,786.52 per Single Family Equivalent (SFE). The calculation of this fee is as follows: 5.368 EDU X \$1,786.52/EDU = \$9,590.00

Z18-0107 - 1308 Findlay Rd. RU1 to I2 AS.doc

4) Storm Drainage

The developer must engage a consulting civil engineer to provide a storm water management plan for these sites which meets the requirements of the City Subdivision Development and Servicing Bylaw 7900. The storm water management plan must also include provision of lot grading plans, minimum basement elevations (MBE), if applicable, and provision of a storm drainage service and recommendations for onsite drainage containment and disposal systems.

5) Road Improvements

- a) Findlay Rd. fronting this development must be upgraded to a 2 lane arterial urban standard to including barrier curb & gutter, concrete sidewalk, landscaped boulevard, LED street lights, drainage system including catch basins, and relocation or adjustment of existing utility appurtenances if required to accommodate the upgrading construction.
- b) The developer will build the frontage of 1292 Findlay rd to the same cross section as 1308 Findlay rd. Cash-in-lieu in the amount of \$22,539.28 (\$22,300 + \$239.28, original + interest) for 1292 Findlay frontage has already been collected and will be used to assist in paying for this construction.

6) Road Dedication and Subdivision Requirements

By registered plan to provide the following:

- a) The ultimate width of Findlay Road is established as a 2-lane arterial roadway, complete with a 30.0m right of way (R.O.W). The R.O.W. in the front of the subject property is deficient by approximately 5.5m. Provide 5.5m of road dedicated along the frontage of this development.
- b) Grant statutory rights-of-way if required for utility services

7) Electric Power and Telecommunication Services

- a) All proposed distribution and service connections are to be installed underground. Existing distribution and service connections, on that portion of a road immediately adjacent to the site, are to be relocated and installed underground as this site is located within the South Pandosy urban town centre.
- b) Re-locate existing poles and utilities, where necessary. Remove aerial trespass (es).

8) Other Engineering

If any road dedication affects lands encumbered by a Utility right-of-way (such as Terasen, etc.) please obtain the approval of the utility prior to application for final subdivision approval. Any works required by the utility as a consequence of the road dedication must be incorporated in the construction drawings submitted to the City's Development Manager.

9) Design and Construction

- a) Design, construction supervision and inspection of all off-site civil works and site servicing must be performed by a Consulting Civil Engineer and all such work is subject to the approval of the City Engineer. Drawings must conform to City standards and requirements.

Z18-0107 - 1308 Findlay Rd. RU1 to I2 AS.doc

- b) Engineering drawing submissions are to be in accordance with the City's "Engineering Drawing Submission Requirements" Policy. Please note the number of sets and drawings required for submissions.
- c) Quality Control and Assurance Plans must be provided in accordance with the Subdivision, Development & Servicing Bylaw No. 7900 (refer to Part 5 and Schedule 3).
- d) A "Consulting Engineering Confirmation Letter" (City document 'C') must be completed prior to submission of any designs.
- e) Before any construction related to the requirements of this subdivision application commences, design drawings prepared by a professional engineer must be submitted to the City's Works & Utilities Department. The design drawings must first be "Issued for Construction" by the City Engineer. On examination of design drawings, it may be determined that rights-of-way are required for current or future needs.

10) Servicing Agreements for Works and Services

- a) A Servicing Agreement is required for all works and services on City lands in accordance with the Subdivision, Development & Servicing Bylaw No. 7900. The applicant's Engineer, prior to preparation of Servicing Agreements, must provide adequate drawings and estimates for the required works. The Servicing Agreement must be in the form as described in Schedule 2 of the bylaw.
- b) Part 3, "Security for Works and Services", of the Bylaw, describes the Bonding and Insurance requirements of the Owner. The liability limit is not to be less than \$5,000,000 and the City is to be named on the insurance policy as an additional insured.

11) Survey Monuments and Iron Pins

If any legal survey monuments or property iron pins are removed or disturbed during construction, the developer will be invoiced a flat sum of \$1,200.00 per incident to cover the cost of replacement and legal registration. Security bonding will not be released until restitution is made.

12) Charges and Fees

- a) Development Cost Charges (DCC's) are payable
- b) Fees per the "Development Application Fees Bylaw" include:
 - i. Street/Traffic Sign Fees: at cost if required (to be determined after design).
 - ii. Survey Monument, Replacement Fee: \$1,200.00 (GST exempt) – only if disturbed.
- c) Engineering and Inspection Fee: 3.5% of construction value (plus GST)

14) Development Permit and Site Related Issues

Access and Manoeuvrability

- a) Access to 1308 Findlay rd. must follow bylaw 7900 standards.

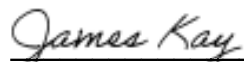
SCHEDULE		A
This forms part of application # Z18-0107		
Planner Initials	SS	 City of Kelowna DEVELOPMENT PLANNING

15. Geotechnical Report

- a) Provide a geotechnical report prepared by a Professional Engineer competent in the field of geotechnical engineering to address the items below: NOTE: The City is relying on the Geotechnical Engineer's report to prevent any damage to property and/or injury to persons from occurring as a result of problems with soil slippage or soil instability related to this proposed development.

The Geotechnical report must be submitted prior to submission of Engineering drawings or application for subdivision approval.

- i. Area ground water characteristics, including any springs and overland surface drainage courses traversing the property. Identify any monitoring required.
- ii. Site suitability for development.
- iii. Site soil characteristics (i.e. fill areas, sulphate content, unsuitable soils such as organic material, etc.).
- iv. Any special requirements for construction of roads, utilities and building structures.
- v. Recommendations for items that should be included in a Restrictive Covenant.
- vi. Recommendations for roof drains, perimeter drains and septic tank effluent on the site.
- vii. Any items required in other sections of this document.
- viii. Additional geotechnical survey may be necessary for building foundations, etc.



James Kay, P.Eng.
Development Engineering Manager

AS

CITY OF KELOWNA
BYLAW NO. 11920
Z18-0107 – 1308 Findlay Road

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of portions of Lot B, Section 34, Township 26, ODYD Plan 14612 located on Findlay Rd, Kelowna, BC from RU1 – Large Lot Housing zone to the I2 – General Industrial zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Approved under the Transportation Act this

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: August 12, 2019

RIM No. 1250-30

To: Council

From: City Manager

Application: Z19-0073

Owner: James A. Baumgartner & Elanda C. Baumgartner

Address: 604 Greene Road

Applicant: Gord Turner Renovations Ltd.

Subject: Rezoning Application

Existing OCP Designation: Single / Two Unit Residential (S2RES)

Existing Zone: RU1 – Large Lot Housing

Proposed Zone: RU1c – Large Lot Housing with Carriage House

1.0 Recommendation

THAT Rezoning Application No. Z19-0073 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 2 District Lot 358 ODYD Plan 9564, located at 604 Greene Road, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration.

2.0 Purpose

To consider a rezoning from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to allow the conversion of an existing accessory building to a carriage house.

3.0 Development Planning

Development Planning supports the proposed rezoning to RU1c – Large Lot Housing with Carriage House to facilitate the conversion of an existing accessory building to a carriage house.

The subject property has a Future Land Use Designation of Single / Two Unit Residential (S2RES), which supports the proposed RU1c zone. The addition of residential units in already built-up areas advances the OCP's goals of developing a compact urban form. As the conversion of an existing building, the proposed carriage house is sensitive to the context of the neighbourhood, and thus is consistent with the OCP's Policy of Sensitive Infill.

4.0 Proposal

4.1 Project Description

The subject property currently contains a single family home, a pool, and an accessory building that is being used for a home-based business. The applicant proposes to convert the existing accessory building into a carriage house. Interior renovations are planned, but the exterior of the building will remain as is. The accessory building would keep its existing access off Willms Rd. No variances to the Zoning Bylaw have been identified.

4.2 Site Context

The subject property is on Greene Ave, two lots to the east of Lakeshore Rd in the North Okanagan Mission neighbourhood. The property is surrounded by single family homes; however, several properties have been rezoned to RU1c in the immediate area, and there are also numerous secondary suites in the vicinity.

Specifically, adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	RU1 – Large Lot Housing	Residential
East	RU1 – Large Lot Housing	Residential
South	RU1 – Large Lot Housing	Residential
West	RU1 – Large Lot Housing	Residential

Subject Property Map: 604 Greene Rd.



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Chapter 5: Development Process

Objective 5.3 Focus development to designated growth areas.

Policy .2 **Compact Urban Form**. Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Objective 5.22 Ensure context sensitive housing development.

Policy .6 **Sensitive Infill**. Encourage new development or redevelopment in existing residential areas to be sensitive to or reflect the character of the neighbourhood with respect to building design, height and siting.

6.0 Application Chronology

Date of Application Received: June 7, 2019

Date Public Consultation Completed: July 3, 2019

Report prepared by: Aaron Thibeault, Planner II

Reviewed by: Laura Bentley, Urban Planning & Development Policy Manager

Approved for Inclusion: Terry Barton, Development Planning Department Manager

Attachments:

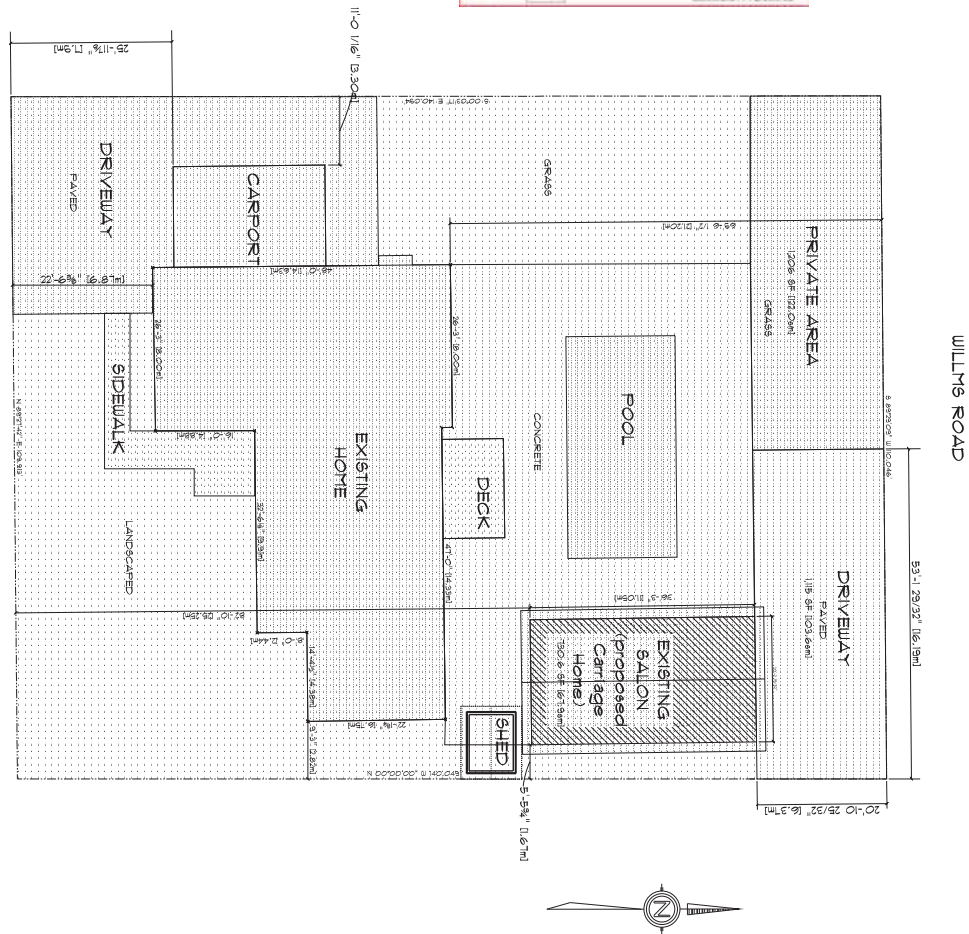
Attachment A: Site Plan

Attachment B: Applicant Rationale

SITE PLAN

SCALE: 1" = 20'-0"

PP



ZONING: RUI PROPOSED: RUC
SETBACKS (CARRIAGE HOME):
 FRONT: 6.0m
 SIDE: 2.0m
 REAR: 1.5m
SETBACKS (EXIST. HOUSE):
 FRONT: 6.0m
 SIDE: 1.5m
 REAR: 0.9m
SITE COVERAGE:
 LOT AREA: 15,404 SF
 PROPOSED AREA: 130.6 SF
 COVERAGE: 14%
 PROPOSED AREA: 6,193 SF
 (HOME, CARRIAGE HOME, GARAGE & PARKING)
 COVERAGE: 50%

Gord Turner Renovations



#1-3304 Appaloosa Road
 Kelowna BC
 V1V 2W5
 PHONE: 250-469-9379
 tom@gordturner.com



Jim & Elanda Baimgartner
 604 Greene Road
 Kelowna BC
 V1W 1B8
 PHONE: 250-868-6063
 FAX:
 jim@gobaumgartner.com

DRAWN BY: TM
 SCALE: 1" = 20'-0"
 DATE: May-21-19

PAGE: 1/4
 SITE PLAN

Letter of Rationale

Carriage house rezone

604 Greene Road



To whom it may concern,

Our clients would like to convert their existing accessory building, which currently houses a small home based business, into a carriage house. We are applying for a rezone to add the 'c' designation to this RU1 lot, to allow for the carriage house conversion.

We will not be changing the existing footprint of any building on site; the carriage house would be completely within the existing footprint of the accessory building. We also would not be seeking to vary any of the setbacks on the property, as the existing accessory building already sits within the appropriate setbacks, as defined by the City Zoning Bylaw, and explained in the Carriage Houses Bulletin received from the Building and Development Department.

As the accessory building is already being used for a small home based business, there is also already services, lighted pathways, and proper access, as will be required. We have noted the outdoor private space on the site plan, and all other elements of the landscape on the property will be remaining as they are currently. The invasiveness of this change would be extremely minimal, and outwardly there would be almost no change to how the property looks and functions.

Please see on the following pages a few pictures of the property in question, to aid in visualizing the plan.

We appreciate any further questions or discussion on the matter.

Thank you.

Kyle Turner

Operations Manager

Gord Turner Renovations Ltd.

CITY OF KELOWNA
BYLAW NO. 11921
Z19-0073 – 604 Greene Road

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 2 District Lot 358 ODYD Plan 9564, located on Greene Road, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: August 12, 2019

RIM No. 1250-30

To: Council

From: City Manager

Application: Z19-0059

Owner: D & R Properties Ltd, Inc. No
BC0562497

Address: 3140 Lakeshore Rd

Applicant: Flora Recreational Cannabis

Subject: Rezoning Application

Existing OCP Designation: MXR- Mixed-Use (Residential/ Commercial)

Existing Zone: C4lp - Urban Centre Commercial (Liquor Primary)

Proposed Zone: C4lp/rcs - Urban Centre Commercial (Liquor Primary/Retail Cannabis Sales)

1.0 Recommendation

THAT Rezoning Application No. Z19-0059 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot A, District Lot 14 Osoyoos Division Yale District Plan 35059, located at 3140 Lakeshore Rd, Kelowna, BC, from the C4lp - Urban Centre Commercial (Liquor Primary) zone to the C4lp/rcs - Urban Centre Commercial (Liquor Primary/Retail Cannabis Sales) zone, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND FURTHER THAT if the Rezoning Bylaw is adopted, Council direct Staff to send a recommendation to the British Columbia Liquor and Cannabis Regulation Branch that they support issuance of a non-medical cannabis retail store license for this legal lot with the following comments:

- The proposed location meets local government bylaw requirements and as such, no negative impact is anticipated;
- The views of the residents were captured during a public hearing process for the rezoning of the property and Council meeting minutes summarizing those views are attached; and
- Local government recommends that the application be approved because of compliance with local regulations and policies.

2.0 Purpose

To rezone the subject property to facilitate the development of a retail cannabis sales establishment.

3.0 Development Planning

Development Planning Staff recommend support for the rezoning application to allow for a retail cannabis sales establishment on the subject property. The additional use is consistent with the Future Land Use designation for the property and the proposal complies with Zoning Bylaw regulations regarding the retail cannabis sales use.

The application is the first to be processed following the initial intake of retail cannabis sales rezoning applications. As such, it was evaluated with the City's standard rezoning process, established in the Development Application Procedures Bylaw.

Should Council support the proposed Rezoning Bylaw, the property would be rezoned to a liquor primary and retail cannabis sales subzone, and Staff would send a recommendation to the British Columbia Liquor and Cannabis Regulation Branch indicating support for issuance of a non-medical cannabis retail store license for this property.

There are no variances requested. The applicant has confirmed the completion of public notification in accordance with Council Policy No. 367.

4.0 Proposal

4.1 Project Description

A retail cannabis sales establishment is proposed to be located in an existing ground-floor commercial unit in a building that has several offices and health services, along with a fast-food restaurant on the property.

4.2 Site Context

The property is within the South Pandosy Urban Centre and has a Future Land Use designation of MXR – Mixed-Use (Residential / Commercial). The surrounding area has a mix of retail, services, offices, and residential uses and densities. Sensitive uses identified within a 150m radius include a Liquor Primary Establishment and a Liquor Primary Establishment.

Specifically, adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	C4L- Urban Centre Commercial Liquor Primary	Liquor primary establishment
East	C4/ C4L/	Retail stores, general
South	C9- Tourist Commercial	Hotel
West	RU1- Large Lot Housing	Single dwelling housing

Subject Property Map: 3140 Lakeshore Rd



5.0 Technical Comments

5.1 Development Engineering Department

The application does not compromise any municipal infrastructure.

6.0 Application Chronology

Date of Application Received: April 10, 2019

Date Public Consultation Completed: June 13, 2019

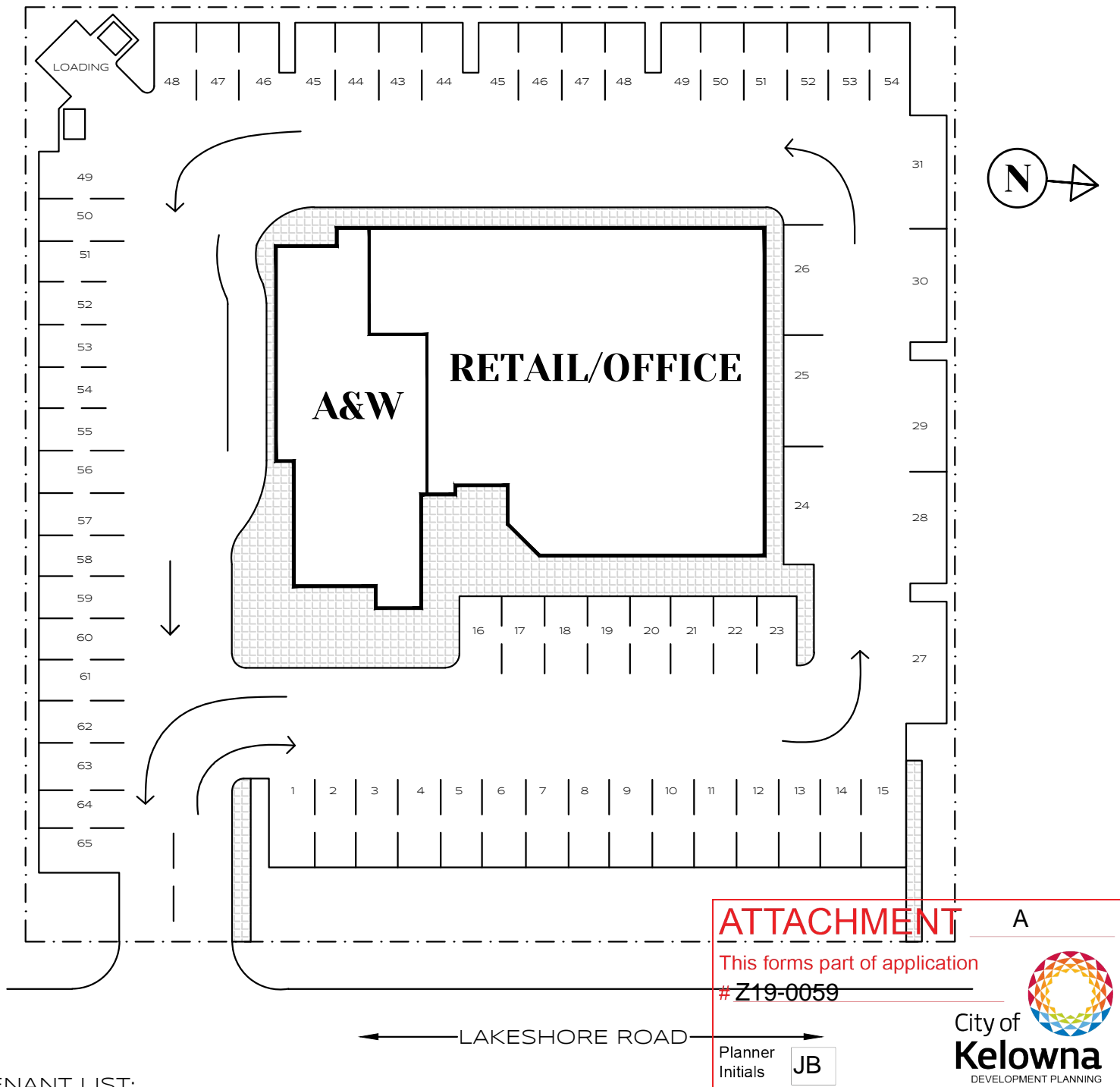
Report prepared by: Jocelyn Black, Planner Specialist

Reviewed by: Laura Bentley, Urban Planning & Development Policy Manager

Approved for Inclusion: Terry Barton, Development Planning Department Manager

Attachments:

Attachment A: Site Plan



TENANT LIST:

- UNIT 100 - A&W RESTAURANT
- UNIT 101 - HARMONY CHIROPRACTIC CLINIC
- UNIT 102 - DENTURE CLINIC
- UNIT 103 - TYMKIW PERIODONTICS
- UNIT 105 - HOLLISWEALTH FINANCIAL
- UNIT 110 - PROPOSED FLORA RECREATIONAL CANNABIS
- UNIT 200 - ADAMS, HEYMAN, OWEN ACCOUNTANTS
- UNIT 203 - MUSLEH COUNSELING
- UNIT 204 - DR. MICHAEL BOBYN INC. AND DR. PAUL HART

Leigh Blackburn

REGISTERED INTERIOR DESIGNER

LBLACKBURNCREATIVE@GMAIL.COM
250 575 8833

Flora Recreational Cannabis

#110 - 3140 LAKESHORE RD.
KELOWNA, BC
PLAN KAP35059 LOT A DISTRICT
LOT 14
PID: 002-905-485

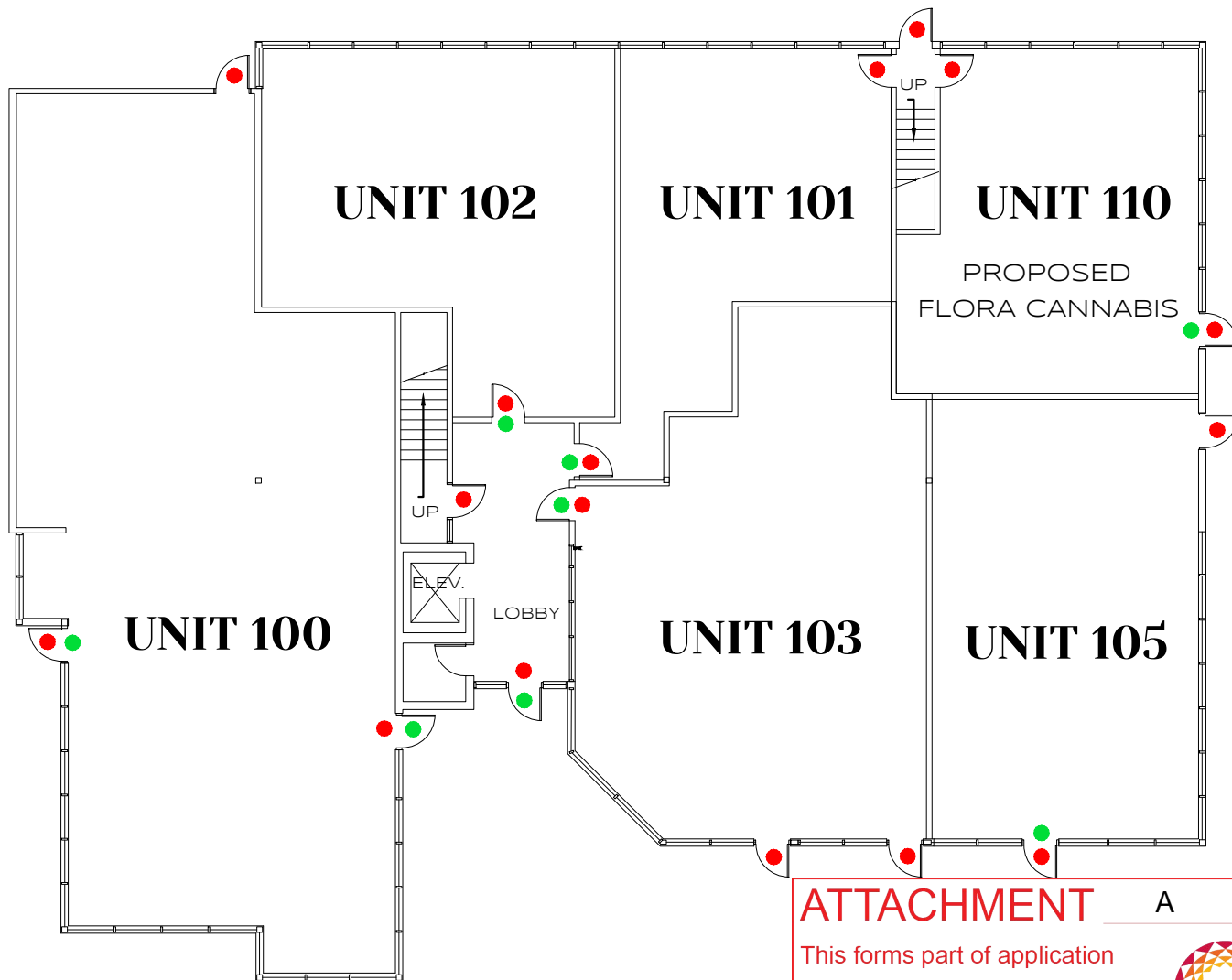
/ DATE: FEB 27/19
/ SCALE: 1/32" = 1'-0"
/ DRAWN BY: LB

ISSUED FOR:

1 CITY APPLICATION MAR. 4/19
2 REV. APPLICATION APR. 10/19
3 REV. APPLICATION MAY 15/19
4

SITE PLAN

1 of 3



ATTACHMENT

A

This forms part of application

Z19-0059

Planner
Initials

JB

City of
Kelowna
DEVELOPMENT PLANNING



TENANT LIST:

- UNIT 100 - A&W RESTAURANT
- UNIT 101 - HARMONY CHIROPRACTIC CLINIC
- UNIT 102 - DENTURE CLINIC
- UNIT 103 - TYMKIW PERIODONTICS
- UNIT 105 - HOLLISWEALTH FINANCIAL
- UNIT 110 - PROPOSED FLORA RECREATIONAL CANNABIS

NOTE:

ALL PARTITIONS THAT SEPARATE
TENANTS ARE FULL HEIGHT, FLOOR TO
CEILING, DEMISING WALLS

LEGEND

● EXIT DOORS

● ENTRANCE DOORS



Leigh Blackburn

REGISTERED INTERIOR DESIGNER

LBLACKBURNCREATIVE@GMAIL.COM
250 575 8833

**Flora Recreational
Cannabis**

#110 - 3140 LAKESHORE RD.
KELOWNA, BC
PLAN KAP35059 LOT A DISTRICT
LOT 14
PID: 002-905-485

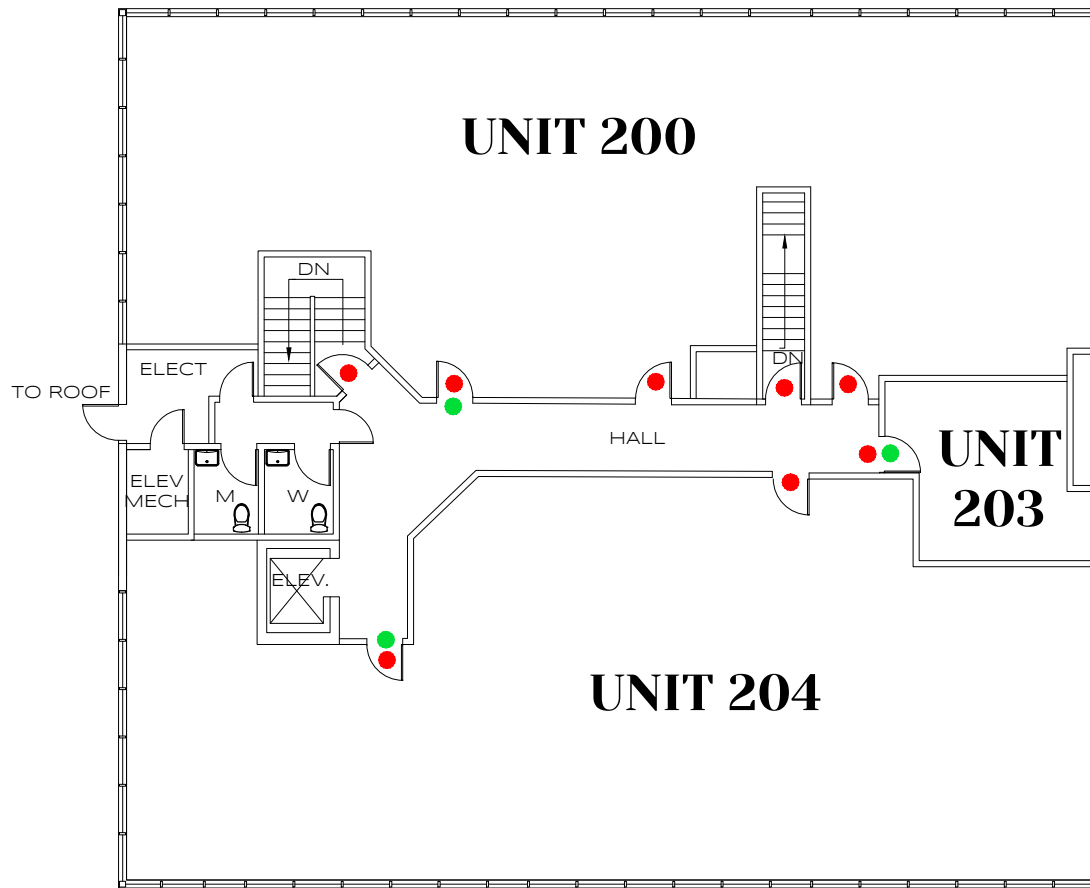
/ DATE: FEB 27/19
/ SCALE: 1/16" - 1'-0"
/ DRAWN BY: LB

ISSUED FOR:

1 CITY APPLICATION	MAR. 4/19
2 REV. APPLICATION	APR. 10/19
3 REV. APPLICATION	MAY 15/19
4	

MAIN FLOOR
SITE PLAN

2 of 3



TENANT LIST:

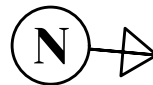
- UNIT 200 - ADAMS, HEYMAN, OWEN ACCOUNTANTS
- UNIT 203 - MUSLEH COUNSELING
- UNIT 204 - DR. MICHAEL BOBYN INC. AND DR. PAUL HART

LEGEND

- EXIT DOORS
- ENTRANCE DOORS

NOTE:

ALL PARTITIONS THAT SEPARATE TENANTS ARE FULL HEIGHT, FLOOR TO CEILING, DEMISING WALLS

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4	

2ND FLOOR
SITE PLAN**3 of 3**

CITY OF KELOWNA
BYLAW NO. 11922
Z19-0059 – 3140 Lakeshore Road

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot A, District Lot 14 Osoyoos Division Yale District Plan 35059, located on Lakeshore Rd, Kelowna, BC, from the C4lp - Urban Centre Commercial (Liquor Primary) zone to the C4lp/rcs - Urban Centre Commercial (Liquor Primary/Retail Cannabis Sales) zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: August 12, 2019

RIM No. 0940-00

To: Council

From: City Manager

Application: DP19-0059

Owner: Provincial Rental Housing
Corporation, Inc. No.
BC0052129

Address: 280 McIntosh Road

Applicant: S2 Architecture

Subject: Development Permit

Existing OCP Designation: MRM – Multiple Unit Residential (Medium Density)

Existing Zone: RM5 – Medium Density Multiple Housing

1.0 Recommendation

THAT Council authorizes the issuance of Development Permit No. DP19-0059 for Lot 1 Section 26 Township 26 ODYD Plan 6518, located at 280 McIntosh Road, Kelowna, BC subject to the following:

1. The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A,"
2. The exterior design and finish of the building to be constructed on the land, be in accordance with Schedule "B";
3. Landscaping to be provided on the land be in accordance with Schedule "C";
4. The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect;

AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

2.0 Purpose

To consider the form and character of a proposed 4-storey supportive housing project on the subject property.

3.0 Community Planning

Community Planning Staff are supportive of the Development Permit as it meets many of the objectives and supporting policies of the Official Community Plan (OCP), as well as the applicable urban design guidelines. These include:

- Promote interesting, pedestrian friendly streetscape design and pedestrian linkages;
- Provide for a scale and massing of buildings that promotes a safe, enjoyable living, pedestrian and working experience;
- Incorporate architectural features and detailing of buildings and landscapes that define an area's character;
- Provide outdoor spaces, including rooftops, balconies, patios and courtyards;
- Mitigate the actual and perceived bulk of buildings by utilizing appropriate massing, including:
 - Architectural elements (e.g. balconies, bay windows, cantilevered floors, cupolas, dormers);
 - Step back upper floors to reduce visual impact;
 - Detailing that creates a rhythm and visual interest along the line of the building;
 - Wall projections and indentations, windows and siding treatments as well as varied material textures should be utilized to create visual interest and to articulate building facades.

The property is located in the Rutland Urban Centre at the corner of McIntosh Road and Asher Road. The property has a Walk Score of 74 (Very Walkable- most errands can be accomplished on foot) and a Transit Score of 40 (Some Transit - a few nearby public transportation options). The proximity to the Hwy 33 commercial shopping area, provides nearby amenities including restaurants, shops and recreational opportunities in the immediate area.

The Development Permit application meets all of the RM5 Zoning Development Regulations and does not require any variances. Due to this, no Public Consultation is required. BC Housing chose to host a Public Information Session on January 29, 2019 from 5:30 pm – 7:00 pm at the Rutland Centennial Hall at 215 Shepherd Road. The session was to inform the neighbours that a supportive housing project is proposed for Kelowna residents who are either experiencing or are at-risk of homelessness. The development is funded by BC Housing and will be operated by the John Howard Society of Central and South Okanagan (JHSCSO)

4.0 Proposal

4.1 Background

In 2007, an application to rezone the subject property from RU6 – Two Dwelling Housing to RM5 – Medium Density Multiple Housing was approved by Council. The Development Permit was approved the same year to allow the construction of a 4-storey 52-unit apartment building. The application failed to move forward to the Building Permit stage and was finally abandoned in 2010. The parcel has remained vacant since then.

4.2 Project Description

BC Housing has made an application for a 4-storey, 50-unit supportive housing apartment building. The development will be operated by the John Howard Society of Central and South Okanagan (JHSCSO) to provide safe and secure housing and support services to people who are experiencing homelessness. Support services will include outreach workers, life skills training, employment assistance and referrals to community services and support groups.

Surface parking is provided on the north east side of the site with access from the north lane. The proposed development provides a visually interesting façade along the streetscape through the articulated façade, flat-roof design and a mixture of building finish colours and textures. The building is finished with fibre cement siding which includes plank siding (dark grey) and fibre cement panels (dark brown). Brick is used along the base of the building facing McIntosh. The brick wraps both ends of the building.

The project includes a large outdoor fenced amenity area with a resident's garden and a dog run. A separate secure bike storage building is provided along with a large grass amenity area on the north portion of the site. This aids in addressing CPTED requirements by providing clear visibility from the building to the street. Trees are provided along the street frontage, and a shrub hedge extends along the shared property line to provide screening of headlights to the adjacent parcel from the resident parking area. The building will have security protocols in place for both residents and guests to check in with the front desk reception area.

4.3 Site Context

The building location fall within the Rutland Urban Centre and the Permanent Growth Boundary and is within walking distance to many amenities including: Rutland Centennial Park, Rutland Bluff Linear Park and the Rutland Road Commercial shopping node.

Specifically, adjacent land uses are as follows:

Orientation	Zoning	Land Use
North	RU1 – Large Lot Housing	Single Dwelling Housing
East	RM3 – Low Density Multiple Housing	Row Housing
South	RM3 – Low Density Multiple Housing RM5 – Medium Density Multiple Housing	Three Dwelling Housing Apartment Building
West	RU1 – Large Lot Housing RU6 – Two Dwelling Housing	Single Dwelling Housing Two Dwelling Housing

Subject Property Map: 280 McIntosh Road



4.4 Zoning Analysis Table

Zoning Analysis Table		
CRITERIA	RM5 ZONE REQUIREMENTS	PROPOSAL
Existing Lot/Subdivision Regulations		
Lot Area	1400 m ²	4066 m ²
Lot Width	30 m	52.14 m
Lot Depth	35 m	78 m
Development Regulations		
Floor Area Ratio	1.1	0.48
Site Coverage	40%	22%
Site Coverage Building, driveways & parking	65%	46%
Height (lesser of)	18.0 m or 4.5 storeys	15.7 m & 4 storeys
Front Yard	6.0 m	6.0 m
Side Yard (east)	7.0 m	7.0 m
Side Yard (west)	6.0 m	6.0 m
Rear Yard	7.0 m	+40m
Other Regulations		
Minimum Parking Requirements	17 stalls	17 stalls provided
Bicycle Parking	Class I - 25 spaces Class II – 5 spaces	Class I - 25 spaces Class II – 5 spaces
Private Open Space	375 m ²	497 m ²

5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Chapter 14: Urban Design DP Guidelines

Comprehensive Development Permit Area Objectives

- Convey a strong sense of authenticity through urban design that is distinctive for Kelowna;
- Promote a high urban design standard and quality of construction for future development that is coordinated with existing structures;
- Integrate new development with existing site conditions and preserve the character amenities of the surrounding area;
- Promote interesting, pedestrian friendly streetscape design and pedestrian linkages;
- Provide for a scale and massing of commercial buildings that promotes a safe, enjoyable living, pedestrian, working, shopping and service experience;
- Incorporate architectural features and detailing of buildings and landscapes that define an area's character;
- Promote alternative transportation with enhanced streetscapes and multimodal linkages;
- Highlight the significance of community institutional and heritage buildings; and
- Protect and restore the urban ecology (i.e. architectural and site consideration with respect to the ecological impact on urban design).
- Moderate urban water demand in the City so that adequate water supply is reserved for agriculture and for natural ecosystem processes.
- Reduce outdoor water use in new or renovated landscape areas in the City by a target of 30%, when compared to 2007.

Revitalization Development Permit Area Objectives

- Use appropriate architectural features and detailing of buildings and landscapes to define area character;
- Convey a strong sense of authenticity through high quality urban design that is distinctive of Kelowna;
- Enhance the urban centre's main street character in a manner consistent with the area's character;
- Provide for a scale and massing of buildings that promotes an enjoyable living, pedestrian, working, shopping and service experience;
- Encourage an appropriate mix of uses and housing types and sizes;
- Design and facilitate beautiful public open spaces that encourage year-round enjoyment;
- Create open, architecturally-pleasing and accessible building facades to the street; and
- Improve existing streets and sidewalks to promote alternative transportation.

6.o Application Chronology

Date of Application Received: March 1, 2019

Date of Public Information Session: January 29, 2019

Report Prepared by: Lydia Korolchuk, Planner II

Reviewed by: Laura Bentley, Planning Manager

Approved for Inclusion: Terry Barton, Development Planning Department Manager

Attachments:

Attachment A: Draft Development Permit DP19-0059 & Schedules (A, B, C)

Attachment B: OCP Comprehensive & Revitalization Design Guidelines

Development Permit DP19-0059

ATTACHMENT

A

This forms part of application

DP19-0059

Planner
Initials

LK

City of
Kelowna
DEVELOPMENT PLANNING



This permit relates to land in the City of Kelowna municipally known as

280 McIntosh Road

and legally known as

Lot 1 Section 26 Township 26 ODYD Plan 6518

and permits the land to be used for the following development:

Supportive Housing

USE as per Zoning Bylaw

Apartment Housing

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

Date of Council Decision July 15, 2019

Decision By: COUNCIL

Development Permit Area: Revitalization and Comprehensive

This permit will not be valid if development has not commenced by July 15, 2021.

Existing Zone: RM5 Future Land Use Designation: MRM

This is NOT a Building Permit.

In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

NOTICE

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner: Provincial Rental Housing Corporation, Inc. No. BC0052129

Applicant: S2 Architecture

Terry Barton
Development Planning Department Manager
Planning & Development Services

Date

1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

2. CONDITIONS OF APPROVAL

- a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";
- b) The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";
- c) Landscaping to be provided on the land be in accordance with Schedule "C"; and
- d) The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect.

This Development Permit is valid for two (2) years **from the date of Council approval**, with no opportunity to extend.

3. PERFORMANCE SECURITY

As a condition of the issuance of this Permit, Council is holding the security set out below to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Developer and be paid to the Developer or his or her designate if the security is returned. The condition of the posting of the security is that should the Developer fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the Municipality may use enter into an agreement with the property owner of the day to have the work carried out, and any surplus shall be paid over to the property own of the day. Should the Developer carry out the development permitted by this Permit within the time set out above, the security shall be returned to the Developer or his or her designate. There is filed accordingly:

- a) An Irrevocable Letter of Credit in the amount of **\$185,355.00**
- b) A certified cheque in the amount of **\$185,355.00**
- c) Before any bond or security required under this Permit is reduced or released, the Developer will provide the City with a statutory declaration certifying that all labour, material, workers' compensation and other taxes and costs have been paid.

4. INDEMNIFICATION

Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:

- a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

**The PERMIT HOLDER is the CURRENT LAND OWNER.
Security shall ONLY be returned to the signatory of the
Landscape Agreement or their designates.**

BC HOUSING SUPPORTIVE HOUSING

280 MCINTOSH ROAD
KELOWNA, BC

ISSUED FOR REVISED DP SUBMISSION
JUNE 07, 2019



HORIZON NORTH
CONTACT INFORMATION
COURTNEY FORSYTH
T 250.882.8789
F 250.882.8789
CFORSYTH@HORIZONNORTH.CA
PROJECT MANAGER

S2
CONTACT INFORMATION
CHAD ZYLA
T 403.870.7059 • 1051
F 403.870.7051
CZYLA@S2ARCHITECTURE.COM
ARCHITECTURE

msp
CONTACT INFORMATION
ROBERT FERSHAU
T 250.489.1714
F 250.482.4499
ROBERT.FERSHAU@MSP.COM
LANDSCAPE ARCHITECTURE

LAWSON
CONTACT INFORMATION
BLAKE LAWSON
T 250.382.3220
F 250.382.3220
BLAKE@LAWSONDEVELOPMENTS.COM
CIVIL ENGINEERING

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	2	19	06	07	ISSUED REVISED DP	MEH	CZ
	1	19	07	24	ISSUED FOR DP	MR	CZ





BC HOUSING	BC HOUSING	BC HOUSING
280 MCINTOSH ROAD, KELOWNA, BC	280 MCINTOSH ROAD, KELOWNA, BC	280 MCINTOSH ROAD, KELOWNA, BC
UNIT 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100	BC HOUSING	BC HOUSING
COVER SHEET	COVER SHEET	COVER SHEET

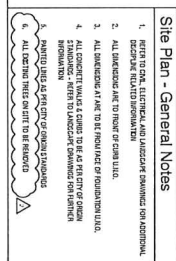
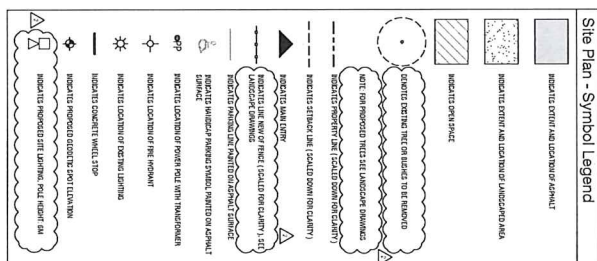
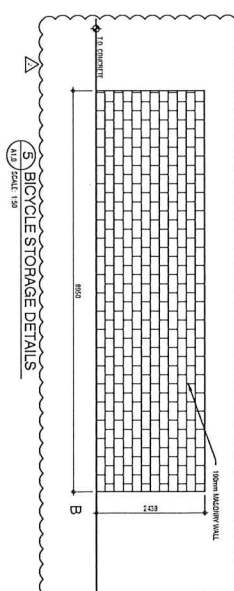
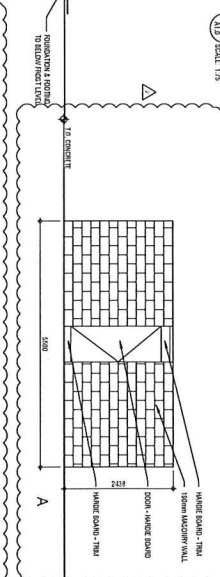
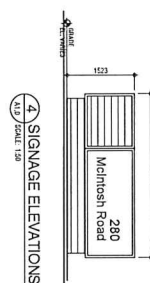
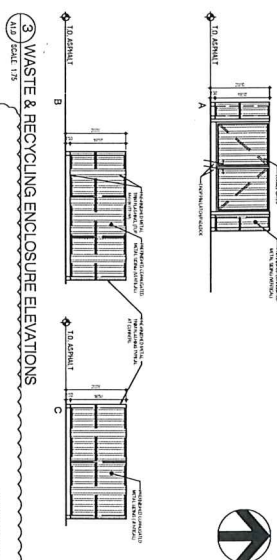
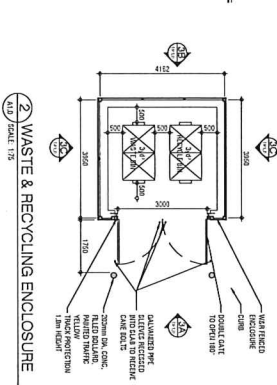
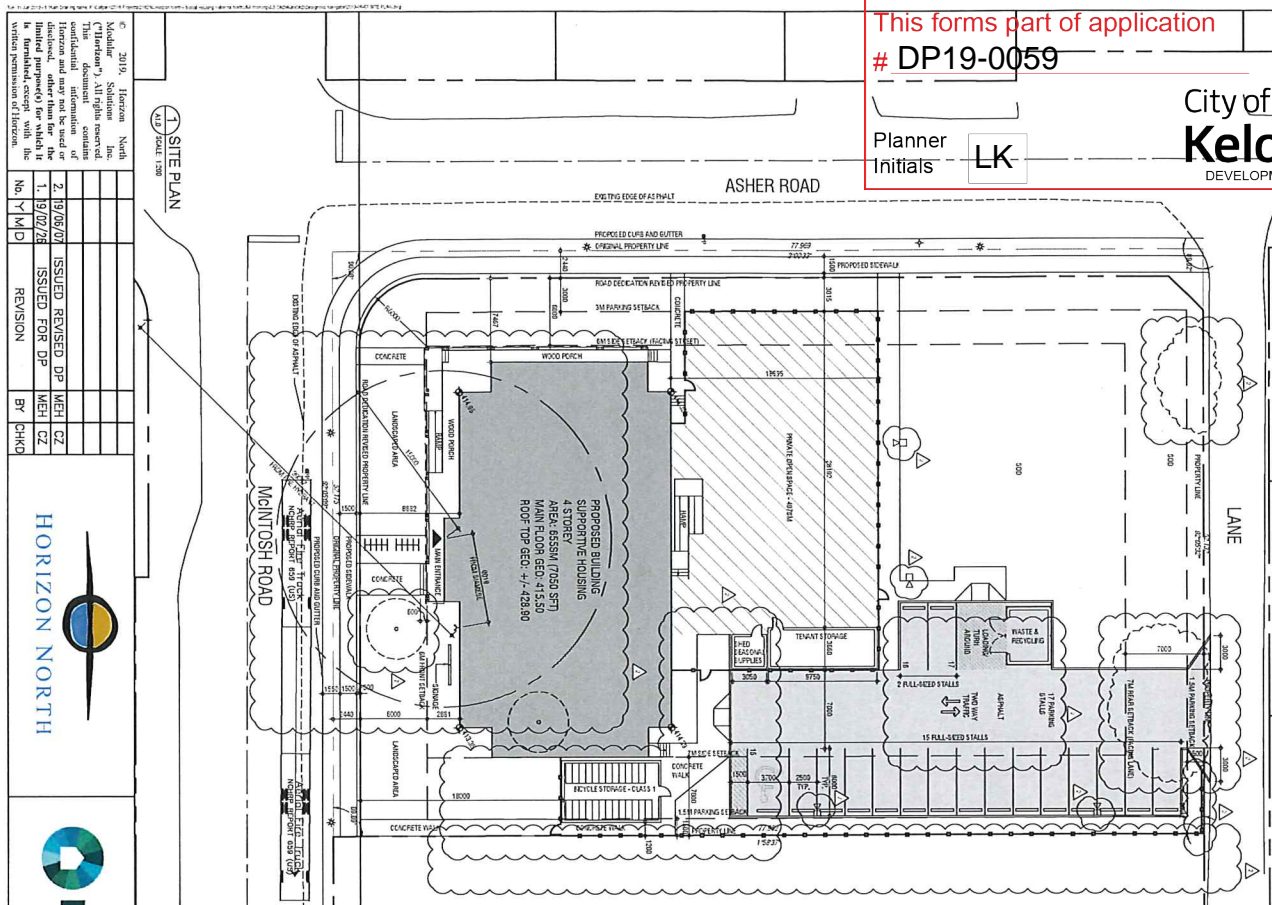





City of
Kelowna
DEVELOPMENT PLANNING

This forms part of application

DP19-0059

Planner Initials LK

BC HOUSING BCH MCINTOSH KELOWNA, BC SITE PLAN & SITE DETAILS	DATE: A1.0
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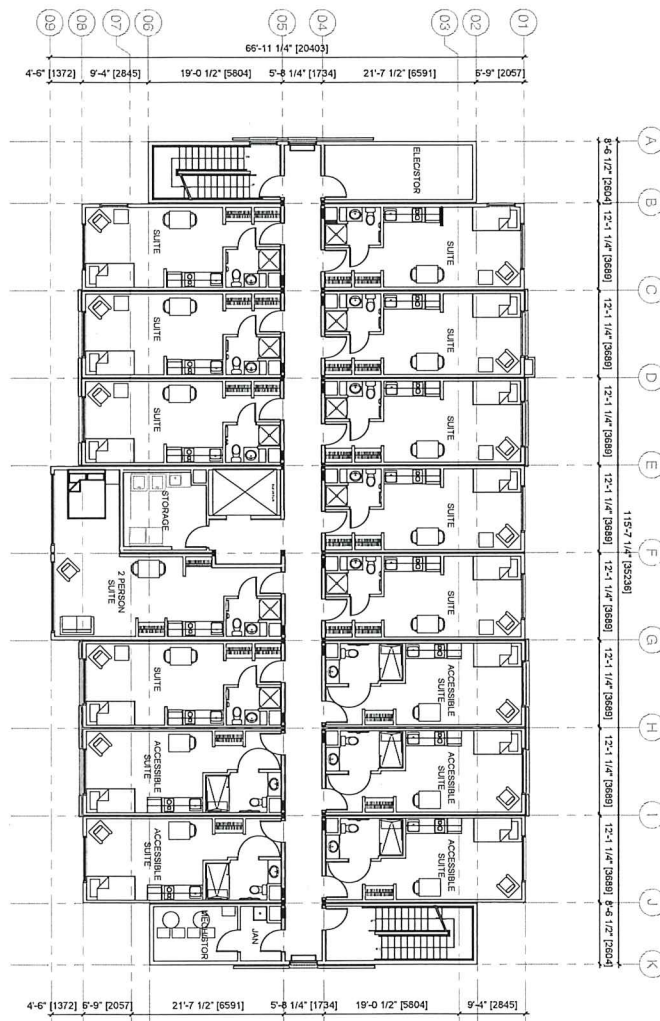


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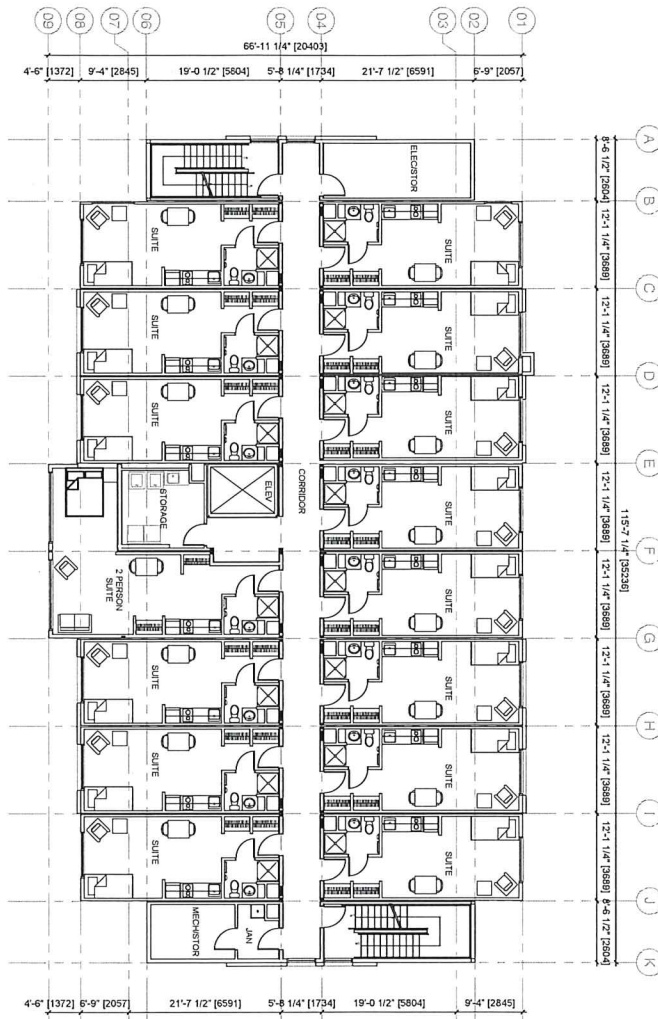


1 SECOND FLOOR PLAN
SCALE: 1/8" = 1'-0"



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1 THIRD FLOOR PLAN
A2.3 SCALE: 1/8" = 1'-0"

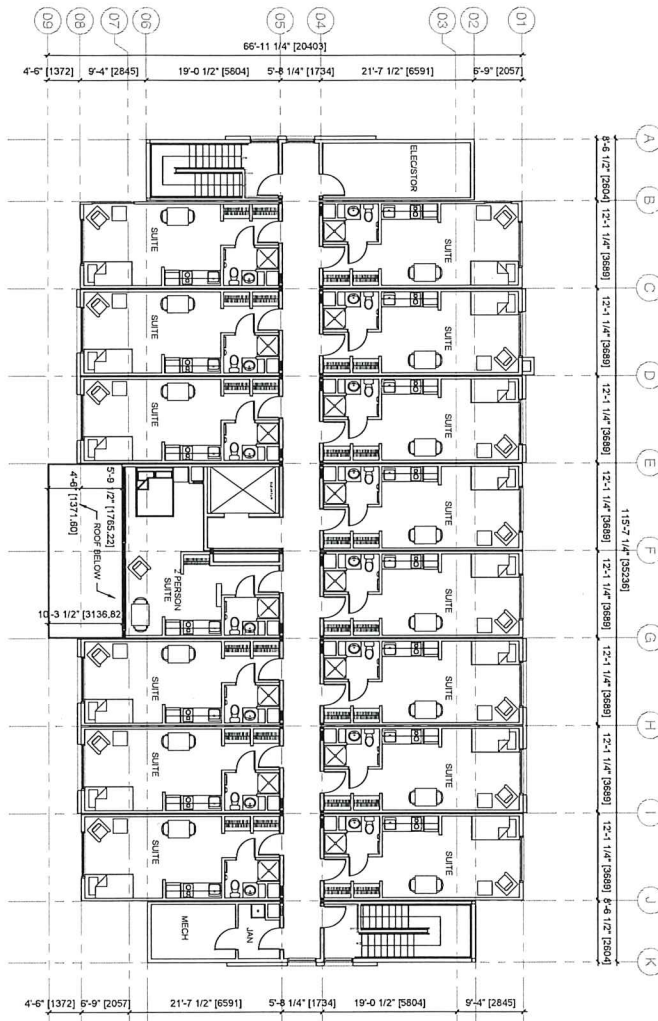


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1 FOURTH FLOOR PLAN
A2.3 SCALE: 1/8" = 1'-0"



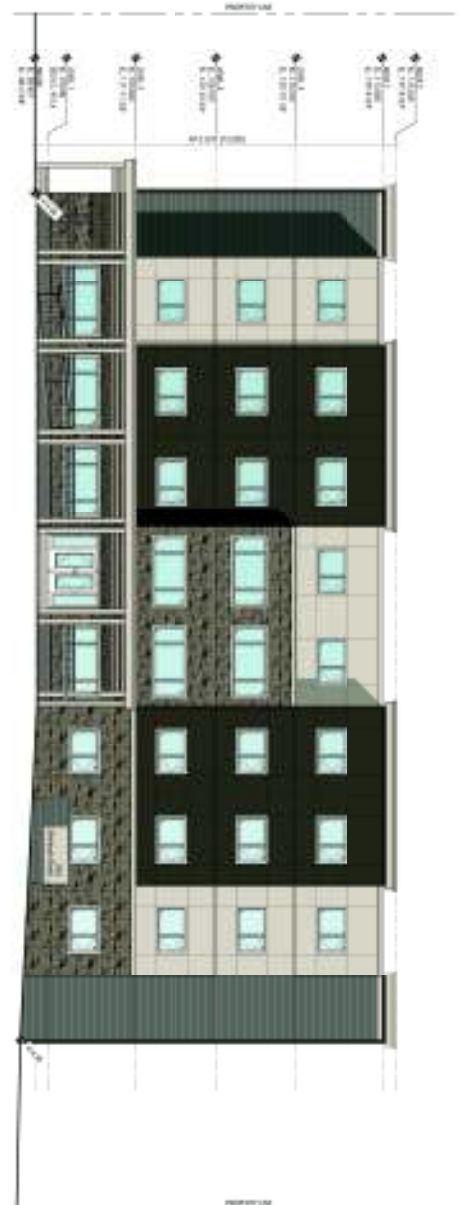
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DP19-0059



City of Kelowna
DEVELOPMENT PLANNING

LK

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role	

BC HOUSING BCH MCINTOSH KELOWNA, BC S & E COLOUR ELEVATIONS	MS 10 A5.0
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B

DP19-0059



LK



SCHEDULE B

This forms part of application

DP19-0059

Planner
Initials

LK

City of
Kelowna
DEVELOPMENT PLANNING



FIBRE CEMENT PLANK
- AGED PEWTER



FIBRE CEMENT PANEL
- RICH ESPRESSO



FIBRE CEMENT PANEL
- COBBLESTONE



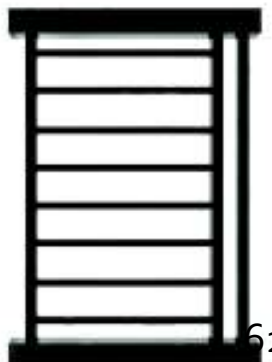
BRICK
- CASTLEWOOD



PVC WINDOW
- WHITE



METAL RAILING
- BLACK



*FLAT ROOF NOT VISIBLE
- TPO WHITE ROOFING

MATERIAL BOARD
SUPPORTIVE HOUSING, 280 MCINTOSH ROAD, KELOWNA, BC



BC HOUSING





DEVELOPMENT PERMIT GUIDELINES

Comprehensive Development Permit Area

Consideration has been given to the following guidelines as identified in Section 14.A. of the City of Kelowna Official Community Plan relating to Comprehensive Development Permit Areas:

COMPREHENSIVE DEVELOPMENT PERMIT AREA	YES	NO	N/A
Authenticity and Regional Expression			
Do landscaping and building form convey a character that is distinct to Kelowna and the Central Okanagan?	✓		
Are materials in keeping with the character of the region?	✓		
Are colours used common in the region's natural landscape?	✓		
Does the design provide for a transition between the indoors and outdoors?	✓		
Context			
Does the proposal maintain the established or envisioned architectural character of the neighbourhood?	✓		
Does interim development consider neighbouring properties designated for more intensive development?			✓
Are façade treatments facing residential areas attractive and context sensitive?	✓		
Are architectural elements aligned from one building to the next?			✓
For exterior changes, is the original character of the building respected and enhanced?			✓
Is the design unique without visually dominating neighbouring buildings?	✓		
For developments with multiple buildings, is there a sense of architectural unity and cohesiveness?			✓
Relationship to the Street			
Do buildings create the desired streetscape rhythm?	✓		
Are parkade entrances located at grade?			✓
For buildings with multiple street frontages, is equal emphasis given to each frontage?		✓	
Massing and Height			
Does the design mitigate the actual and perceived mass of buildings?	✓		
Does the height consider shading and view impacts for neighbouring properties and transition to less intensive areas?	✓		
Human Scale			
Are architectural elements scaled for pedestrians?	✓		
Are façades articulated with indentations and projections?	✓		

COMPREHENSIVE DEVELOPMENT PERMIT AREA	YES	NO	N/A
Are top, middle and bottom building elements distinguished?	✓		
Do proposed buildings have an identifiable base, middle and top?	✓		
Are building facades designed with a balance of vertical and horizontal proportions?	✓		
Are horizontal glazed areas divided into vertically proportioned windows separated by mullions or building structures?	✓		
Does the design incorporate roof overhangs and the use of awnings, louvers, canopies and other window screening techniques?	✓		
Is the visual impact of enclosed elevator shafts reduced through architectural treatments?		✓	
Exterior Elevations and Materials			
Are buildings finished with materials that are natural, local, durable and appropriate to the character of the development?	✓		
Are entrances visually prominent, accessible and recognizable?	✓		
Are higher quality materials continued around building corners or edges that are visible to the public?		✓	
Are a variety of materials used to create contrast, enhance the pedestrian environment and reduce the apparent mass of a building?	✓		
Are elements other than colour used as the dominant feature of a building?	✓		
Public and Private Open Space			
Does public open space promote interaction and movement through the site?	✓		
Are public and private open spaces oriented to take advantage of and protect from the elements?	✓		
Is there an appropriate transition between public and private open spaces?	✓		
Are amenities such as benches, garbage receptacles, bicycle stands and community notice boards included on site?	✓		
Site Access			
Is the safe and convenient movement of pedestrians prioritized?	✓		
Are alternative and active modes of transportation supported through the site design?	✓		
Are identifiable and well-lit pathways provided to front entrances?	✓		
Do paved surfaces provide visual interest?	✓		
Is parking located behind or inside buildings, or below grade?		✓	
Are large expanses of parking separated by landscaping or buildings?	✓		
Are vehicle and service accesses from lower order roads or lanes?	✓		
Do vehicle and service accesses have minimal impact on the streetscape and public views?	✓		

COMPREHENSIVE DEVELOPMENT PERMIT AREA	YES	NO	N/A
Is visible and secure bicycle parking provided in new parking structures and parking lots?	✓		
Environmental Design and Green Building			
Does the proposal consider solar gain and exposure?	✓		
Are green walls or shade trees incorporated in the design?	✓		
Does the site layout minimize stormwater runoff?	✓		
Are sustainable construction methods and materials used in the project?	✓		
Are green building strategies incorporated into the design?		✓	
Decks, Balconies, Rooftops and Common Outdoor Amenity Space			
Are decks, balconies or common outdoor amenity spaces provided?	✓		
Does hard and soft landscaping enhance the usability of decks, balconies and outdoor amenity spaces?	✓		
Are large flat expanses of roof enhanced with texture, colour or landscaping where they are visible from above or adjacent properties?	✓		
Amenities, Ancillary Services and Utilities			
Are loading, garage, storage, utility and other ancillary services located away from public view?	✓		
Are vents, mechanical rooms / equipment and elevator penthouses integrated with the roof or screened with finishes compatible with the building's design?	✓		
Landscape Development and Irrigation Water Conservation			
Does landscaping:	-	-	-
• Compliment and soften the building's architectural features and mitigate undesirable elements?	✓		
• Maintain the dominant pattern of landscaping along the street and surrounding properties?	✓		
• Enhance the pedestrian environment and the sense of personal safety?	✓		
• Screen parking areas, mechanical functions, and garbage and recycling areas?	✓		
• Respect required sightlines from roadways and enhance public views?	✓		
• Retain existing healthy mature trees and vegetation?			✓
• Use native plants that are drought tolerant?	✓		
• Define distinct private outdoor space for all ground-level dwellings?			✓
Do any fences and retaining walls create visual interest and enhance the pedestrian environment?			✓
Do parking lots have one shade tree per four parking stalls?	✓		

COMPREHENSIVE DEVELOPMENT PERMIT AREA	YES	NO	N/A
Does the Landscape Architect's Landscape Water Conservation Report:	-	-	-
<ul style="list-style-type: none"> Meet the requirements for Landscape Water Budget calculations for the landscaped area? 	✓		
<ul style="list-style-type: none"> Indicate how the development complies with or varies from the Landscape Water Conservation Guidelines? 	✓		
<i>Landscape Water Conservation Guidelines</i>			
Are plants grouped into "hydro-zones" of high, medium and low or unirrigated / unwatered areas?	✓		
Does at least 25% of the total landscaped area require no irrigation / watering?	✓		
Does at least 25% of the total landscaped area require low water use?	✓		
Does at most 50% of the total landscaped area require medium or high water use?	✓		
Is mulch cover provided for shrubs and groundcover to reduce soil evaporation?	✓		
Do water features such as pools and fountains use recirculated water systems?			✓
Do landscape installation standards meet the requirements of the BC Landscape Standard and / or the Master Municipal Construction Document?	✓		
Are the required written declarations signed by a qualified Landscape Architect?	✓		
<i>Irrigation System Guidelines</i>			
Is the Irrigation Plan prepared by a Qualified Professional?	✓		
Are irrigation circuits grouped into "hydro-zones" of high, medium and low or unirrigated / unwatered areas consistent with the landscaping plan?	✓		
Is drip or low volume irrigation used?	✓		
Are the required written declarations signed by a qualified Certified Irrigation Designer?	✓		
Crime prevention			
Are CPTED practices as related to landscaping, siting, form and exterior design included in the design?	✓		
Are building materials vandalism resistant?	✓		
Universal Accessible Design			
Is access for persons with disabilities integrated into the overall site plan and clearly visible from the principal entrance?	✓		
Are the site layout, services and amenities easy to understand and navigate?	✓		
Lakeside Development			
Are lakeside open spaces provided or enhanced?			✓
Are lake views protected?			✓
Does lakeside development act as a transition between the lake and inland development?			✓

COMPREHENSIVE DEVELOPMENT PERMIT AREA	YES	NO	N/A
Signs			
Do signs contribute to the overall quality and character of the development?	✓		
Is signage design consistent with the appearance and scale of the building?	✓		
Are signs located and scaled to be easily read by pedestrians?	✓		
For culturally significant buildings, is the signage inspired by historical influences?			✓
Lighting			
Does lighting enhance public safety?	✓		
Is "light trespass" onto adjacent residential areas minimized?	✓		
Does lighting consider the effect on the façade, neighbouring buildings and open spaces?	✓		
Is suitably scaled pedestrian lighting provided?	✓		
Does exterior street lighting follow the International Dark Sky Model to limit light pollution?	✓		



City of
Kelowna
DEVELOPMENT PLANNING

Planner
Initials
LK

Revitalization Development Permit Area

Consideration has been given to the following guidelines as identified in Section 14.B. of the City of Kelowna Official Community Plan relating to Revitalization Development Permit Areas.

REVITALIZATION DEVELOPMENT PERMIT AREA	YES	NO	N/A
Relationship to the Neighbourhood and Street			
Does the proposal maintain the established or envisioned architectural character of the neighbourhood?	✓		
Do developments adjacent to non-revitalization areas create an appropriate transition?			✓
Are spaces for pedestrian friendly amenities, such as street furniture, included on site?	✓		
Is the ratio of streetwall height to street width less than 0.75:1?	✓		
Does the building frontage occupy the entire length of the street, without drive aisles or other dead zones?	✓		
Building Design			
Are architectural elements aligned from one building to the next?			✓
Are the effects of shadowing on public areas mitigated?	✓		
Are doors or windows incorporated into at least 75% of street frontage?	✓		
Do proposed buildings have an identifiable base, middle and top?	✓		
Are windows, entrances, balconies and other building elements oriented towards surrounding points of interest and activity?	✓		
Are architectural elements such as atriums, grand entries and large ground-level windows used to reveal active interior spaces?		✓	
Are buildings designed with individual entrances leading to streets and pathways rather than with mall style entrances and internal connections?			✓
For multiple unit residential projects, is ground level access for first storey units provided?		✓	
Are buildings finished with materials that are natural, local, durable and appropriate to the character of the development?	✓		
Are prohibited materials such as vinyl siding, reflective or non-vision glass, plastic, unpainted or unstained wood, and concrete block not used in the design?	✓		
Are stucco and stucco-like finishes omitted as a principal exterior wall material?	✓		
Are vents, mechanical rooms/equipment and elevator penthouses integrated with the roof or screened with finishes compatible with the building's design?	✓		

View Corridors			
Are existing views preserved and enhanced?	✓		
Vehicular Access and Parking			
Are at-grade and above-grade parking levels concealed with façade treatments?			✓
Are garage doors integrated into the overall building design?			✓
Are pedestrian entrances more prominent features than garage doors and vehicle entrances?	✓		
Is surface parking located to the rear of the building or interior of the block?	✓		
Are truck loading zones and waste storage areas screened from public view?			✓
Do parking lots have one shade tree per four parking stalls?		✓	
Are pedestrian connections provided within and between parking lots?			✓
Are driving, parking, pedestrian and cycling areas distinguished through changes in colour or pattern of paving materials?			✓
Signage			
Is signage design consistent with the appearance and scale of the building?	✓		
Are corporate logos on signs complimentary to the overall building character?			✓
Is signage lighting minimized?			✓
Public Art			
Is public art incorporated into the project?		✓	

CITY OF KELOWNA
BYLAW NO. 11828
Z19-0034 – 5505 – 5507 Airport Way

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of portions of Lot A Section 14 Township 23 Osoyoos Division Yale District Plan EPP23036, located on Airport Way, Kelowna, BC from the Cg – Tourist Commercial zone and the CD15 – Airport Business Park zone to the Cgrcs – Tourist Commercial (Retail Cannabis Sales) zone as per Map "A" attached to and forming part of this bylaw.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 6th of May, 2019.

Considered at a Public Hearing on the 21st day of May, 2019 .

Read a second and third time by the Municipal Council this 21st day of May, 2019 .

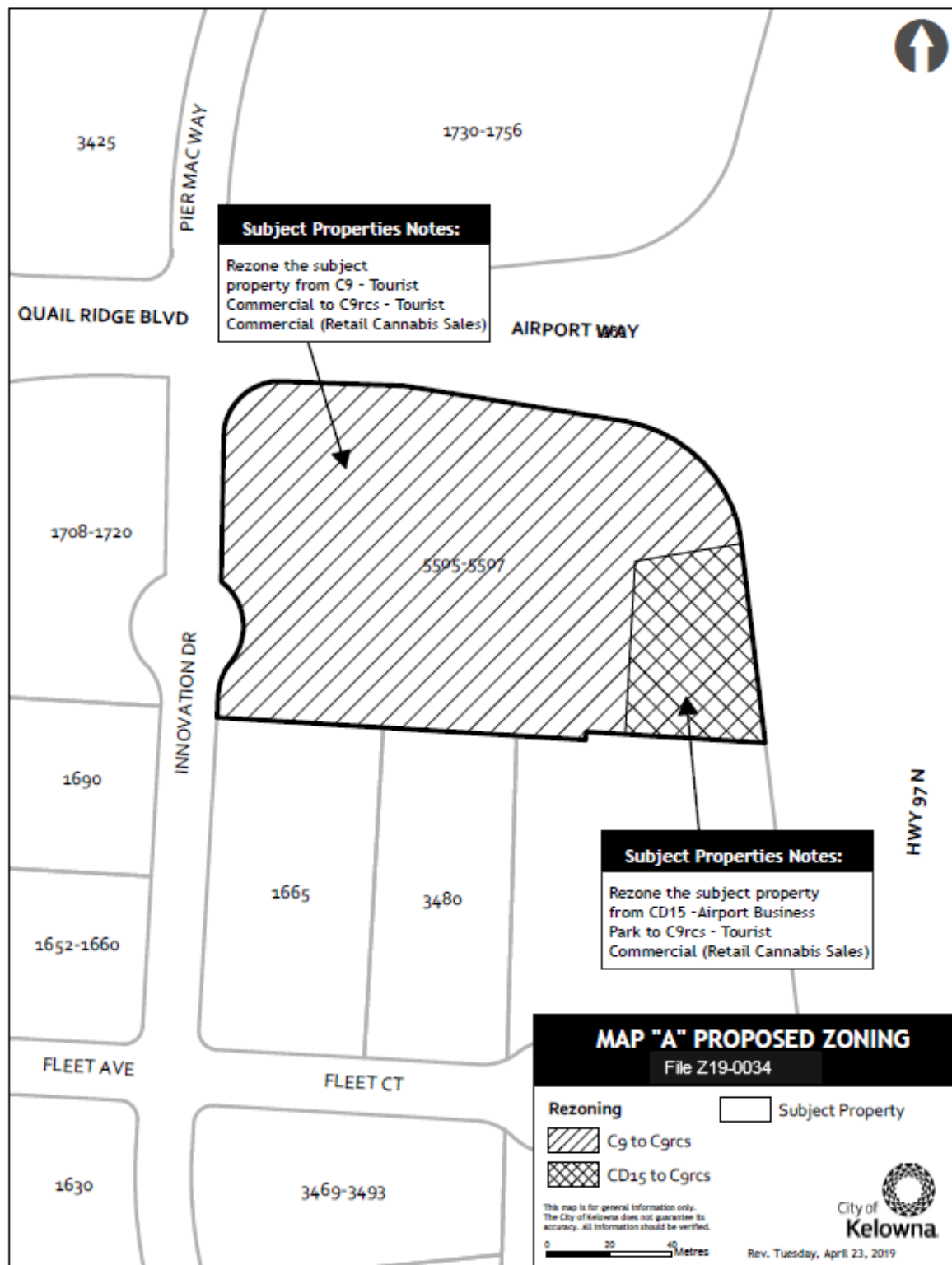
Approved under the Transportation Act this 6th day of August, 2019

Audrie Henry
(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk



CITY OF KELOWNA
BYLAW NO. 11855
Z19-0004
1675-1677 Commerce Avenue

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot A, District Lot 125, Osoyoos Division Yale District Plan KAP90858, except part in Plan EPP52444, located on Commerce Avenue, Kelowna, BC from the C10lp/rls – Service Commercial (Liquor Primary/Retail Liquor Sales) zone to the C10lp/rls/rcs - Service Commercial (Liquor Primary/Retail Liquor Sales/Retail Cannabis Sales) zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 17th day of June, 2019.

Considered at a Public Hearing on the 16th day of July, 2019.

Read a second and third time by the Municipal Council this 16th day of July, 2019.

Approved under the Transportation Act this 31st day of July, 2019.

Audrie Henry

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

Report to Council



Date: August 12, 2019

To: Council

From: City Manager

Subject: Transportation Master Plan: Problem Identification and Existing and Future Conditions Technical Report

Department: Integrated Transportation

Recommendation:

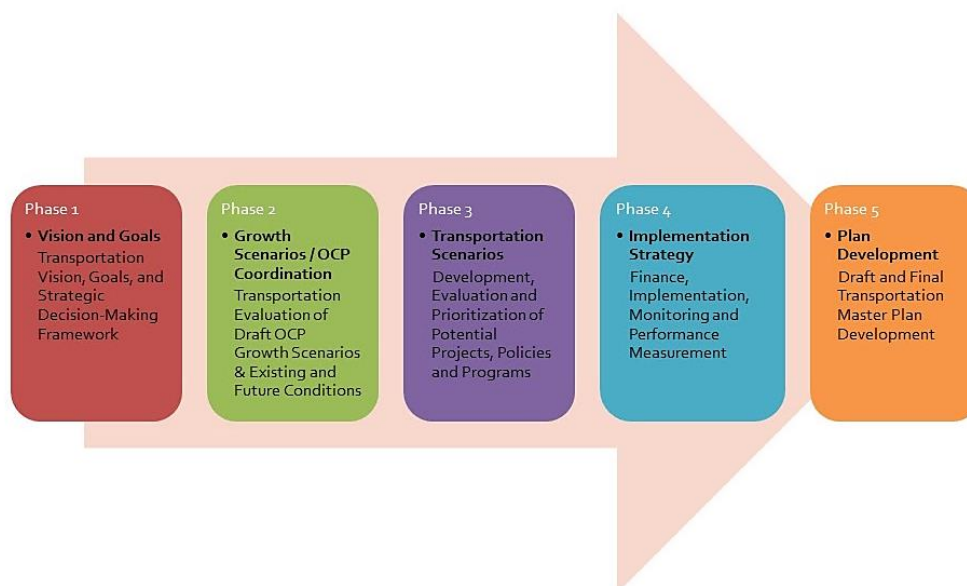
THAT Council receives, the report from the Integrated Transportation Department dated August 12th, 2019, with respect to the Existing and Future Conditions Technical Report for the Transportation Master Plan.

Purpose:

To provide Council with an overview of existing and projected future transportation conditions, along with a summary of future challenges and opportunities to be addressed in the upcoming Transportation Master Plan.

Background:

Development of the Transportation Master Plan – our Kelowna as we Move was launched in 2018 and is being developed in five phases.



Phase 1 began by developing a vision and goals for the Transportation Master Plan (TMP), derived from Imagine Kelowna and presented to the public during spring 2018. The final vision and associated set of goals that will guide the TMP are below:

TMP Vision:

"Kelowna will be a city with vibrant urban centres where people and places are conveniently connected by diverse transportation options that help us transition from our car-centric culture."

TMP Goals:



Phase 1 also included the development of four background “Facts in Focus” topic papers for Transportation. The papers were developed to address questions and themes heard during the spring 2018 TMP public engagement. The paper topics are listed below and are available on the TMP project website.

- [Transportation and Land Use](#)
- [How Transit can keep Kelowna Moving](#)
- [The Congestion Paradox](#)
- [Transportation, Technology and our Changing Future](#)

Phase 2 of the TMP involved detailed coordination with the 2040 Official Community Plan (OCP). The TMP team provided support during the development and evaluation of the four different growth scenarios presented as part of the Pick Your Path to 2040 engagement. For each scenario, an assessment to understand the broad impacts of land use choices on the transportation system was developed, including the amount of vehicle travel, mode share, and planning-level cost ranges. On March 3rd, 2019 Council endorsed the preferred Growth Scenario, which serves as the foundation for the Transportation Master Plan moving forward.

Phase 2 also includes the development of an Existing and Future Conditions Technical Report for the Transportation Master Plan. The technical report helps to provide a comprehensive understanding of Kelowna’s existing transportation system performance, challenges and opportunities, for all modes, as well as the projected future transportation system performance in 2040 under the endorsed Growth Scenario. The technical report helps bring the issues and challenges that the Transportation Master Plan will need to address into better focus and lays the foundation for the identification of potential

projects, policies and programs that will be needed to achieve Kelowna's vision and goals for transportation in 2040.

Purpose of the [Existing and Future Conditions Technical Report](#):

This Council Report provides a summary of the more detailed Existing and Future Conditions Technical Report. The Existing and Future Conditions Technical Report is intended to be a reference document that can be used as a resource for Council, staff and the public for looking up data related to various aspects of the current and future performance of Kelowna's transportation network. The [Existing and Future Conditions Technical Report](#) can be viewed online at: kelowna.ca/transportation2040

Summary of Future Changes:

By 2040, Kelowna is projected to grow by 50,000 residents, from a current population of approximately 130,000 to a future population of approximately 180,000. In accordance with the Imagine Kelowna Vision and the 2040 OCP endorsed Growth Scenario, 67 per cent of residential and 75 per cent of employment growth will be focused in Kelowna's Core Area and five Urban Centres. These are areas of the City where travel options like transit, walking and biking are increasingly becoming viable alternatives to driving. The remaining growth (33 per cent of residential and 25 per cent of employment) will occur in outlying suburban areas, including hillside neighborhoods that are mostly car-dependent. In addition, changes to transportation technology, demographic shifts, and changing weather patterns mean that transportation in 2040 will likely be very different than it is today.

Central Okanagan Regional Travel Model:

To prepare a Transportation Master Plan it is necessary to develop a baseline scenario for the future against which potential investments can be evaluated. To prepare this baseline, staff used the Central Okanagan Regional Travel Model, which is a traditional transportation planning tool that uses assumptions about population growth, land use and the transportation network to estimate future vehicle traffic volumes. In addition, the transportation model also considers human factors, including resident travel behaviors and travel mode choices when generating projections, making it more complex and less flexible than other infrastructure/utility forecasting tools. In regions where most trips are made by cars, travel models are less accurate at projecting future pedestrian, biking and transit trips and impacts. This is because traditional travel models do not account well for potential changes in traffic flow or travel behavior that may result from significant improvements in transportation technology or improvements to the bicycle, pedestrian, transit or shared mobility options available. For emerging transportation technologies, projections are even more limited. Travel model results should be interpreted keeping these limitations in mind.

To create the 2040 TMP Baseline Scenario, the 2040 OCP endorsed Growth Scenario was used in combination with the existing transportation network. A limited number of road improvement projects currently within the 10-Year Capital Plan were also included. These have an approximate value of \$43 million and include projects that are currently underway and/or very likely to be constructed by 2040, such as South Perimeter Road. This approach of including some, but not all, of the improvements in the 10-year Capital Plan represents a balance between a no future improvement scenario (which would have resulted in an overly pessimistic projection of the future), and a scenario that included all currently planned projects (which would have left the TMP without much flexibility to address emerging issues). Projects not included in the 2040 TMP Baseline Scenario will be considered as part of the TMP evaluation process.

2040 TMP Baseline Scenario:

Overall, the 2040 TMP Baseline Scenario reflects a future in which Kelowna grows in accordance with the 2040 OCP Growth Scenario, but does so in the absence of a Transportation Master Plan to guide future investment in infrastructure, policies and programs. Travel behaviors are assumed to remain the same as today and are estimated primarily as a function of travel time and cost. Traffic volume estimates are for the weekday afternoon peak, which typically represents the most congested period on Kelowna's transportation network. The purpose of creating this scenario is not to predict the future, but rather to create a baseline for the identification and evaluation of potential investments during Phase 3 of the Transportation Master Plan.

Citywide Results: Under the Baseline Scenario it is projected that 58 per cent of the intersections in this study would be at or over capacity in 2040 (compared to 10 per cent today). Additionally, the total amount of vehicle kilometres travelled (VKT) in the City would grow by approximately 40 per cent, total vehicle hours (time spent driving) would grow by approximately 70 per cent, and average travel speeds would fall by approximately 15 per cent. The greater increase in vehicle hours travelled compared to vehicle kilometres travelled, as well as the reduction in average travel speeds, indicates greater levels of traffic congestion under this future scenario.

To put this in context, Kelowna's population is projected to grow by 39 per cent over the same period. While total VKT is projected to increase in pace with population growth, VKT per capita is projected to fall by 5 per cent. This decrease reflects the endorsed Growth Scenario's focus on targeting future growth in Kelowna's Core Area and Urban Centres. As travel distances shorten, people living and/or working in the Core Area and Urban Centres will be able to walk, bike or take transit more easily, and when they do drive, they will not have to drive as far as residents living in car-dependent hillside neighbourhoods on the edge of town. While the total amount of VKT and congestion levels are still projected to increase citywide, the increase will be much less than it would have been under a more dispersed growth scenario.

Subarea Results: The 2040 TMP Baseline Scenario shows that future travel demand and traffic patterns will vary in different parts of the City. Some future trips will be inherently car-dependent, while others will be easier to accommodate using more space-efficient and sustainable travel modes. Overall, traffic is projected to become busier and more complex within the Core Area, where residents commuting in and out of car-dependent hillside neighbourhoods will compete for limited roadway space with Core Area residents who will have options to get around using a variety of travel modes. More focused projections of future travel patterns in different subareas of the City are provided in the Driving Chapter of the Existing and Future Conditions Technical Report.¹

What is Traffic Congestion?

With traffic levels projected to increase under the 2040 TMP Baseline Scenario, it is important to understand what traffic congestion is and potential options for managing it effectively. In economic terms, traffic congestion happens when the demand for roadway space exceeds the supply. Due to the way society is organized, this typically occurs during the morning and afternoon peaks when most people need to travel to work and/or school at the same time. This means that increasing congestion levels are often a sign of a growing, vibrant and economically productive city. In fact, traffic levels often

¹ See the Driving Chapter section titled "Future Traffic Conditions – the 2040 Baseline Scenario"

become heaviest when the economy is booming and notably decline during a recession². As such, one way to view traffic congestion is as a sign of prosperity and economic success, rather than a wholly negative phenomenon.

However, nobody likes being stuck in traffic. Inching along congested roads in a vehicle capable of going over 100 kilometres an hour is an inherently frustrating experience. Often the response by communities to growing traffic congestion is to increase roadway capacity by building new roads and widening existing ones. However, as discussed in [The Congestion Paradox Facts in Focus](#) discussion paper, this approach can be expensive and ineffective over the long-term, with negative community impacts.

In Kelowna, the construction of new roads is constrained by steep hillsides, Okanagan Lake and protected agricultural lands. In the Core Area, there is little room to widen roads without buying land, tearing down homes, or disrupting local businesses. This would be expensive and physically divide existing, established neighbourhoods, making Kelowna a less attractive and healthy place to live. On average, the cost to widen a major road in the Core Area is estimated at \$26 million per kilometer (but could be much higher where impacts to adjacent properties are significant). This means that substantial tax increases or new sources of revenue would be needed to try and build our way out of congestion.

Even if the space and funds were available, expanding roadways often reduces congestion to a smaller degree, and for less time, than initially expected. This is because when a new road opens, or an existing road is expanded, people typically respond by shifting routes, traveling at different times, traveling more often, or even relocating where they live or work. These effects are more prominent where new roads significantly reduce travel times between locations. This rebound effect, called “induced demand” by economists, can reduce the long-term congestion mitigation effects of roadway expansion projects³, often eroding the benefits they originally sought to achieve.

While free-flow automobile travel during the morning or afternoon peaks may not be achievable in a rapidly-growing, economically successful city⁴ like Kelowna, there are still a number of strategies that can be implemented to help reduce the rate at which traffic congestion intensifies. Strategies to help manage and minimize the growth of traffic congestion will be explored as part of the Transportation Master Plan.

Keeping Kelowna Moving:

One of the most effective long-term congestion mitigation strategies is to reduce auto-dependence by providing more convenient and realistic alternatives for getting around, especially during the morning and afternoon peaks. This requires a coordinated approach to land use and transportation that shortens trip distances and creates complete, connected and safe bicycle, pedestrian and transit networks between residential and commuting destinations.

Mode Shift: To help keep Kelowna moving, it will be necessary to shift as many future trips as possible to more sustainable transportation modes that can move more people in the same amount of space.

² Downs, A. (2014). Still Stuck in Traffic: Coping with Peak Hour Traffic Congestion

³ Ewing, R. & Proffitt, D. (2016). Improving Decision Making for Transportation Capacity Expansion: Qualitative Analysis of Best Practices for Regional Planning. Transportation Research Record, 2568, p.1

⁴ Downs, A. (2014). Still Stuck in Traffic: Coping with Peak Hour Traffic Congestion

The best opportunities for mode shift will be within the Urban Centres and the Core Area, where the terrain is relatively flat, and some supporting infrastructure for walking, biking and transit is already available. Increased densification will result in shorter trip distances, thus removing the primary barrier to walking and biking for nearby residents. If the City takes consistent and complementary action to ensure the transportation network provides safe, attractive and convenient infrastructure for walking, biking and transit in these densifying areas, the number of trips that are shifted to these modes can be maximized. This will help prioritize road space for trips that must be made by driving, while giving Kelowna residents more choices for getting around.

A Well-Connected, Complete Urban Street Network: Developing a permeable, well-connected and complete urban street network will also be important to keep Kelowna moving. Within the Core Area, where streets will be the busiest, streets with high traffic volumes, long blocks and limited crossings will make it challenging to accommodate growing numbers of people walking, cycling and riding transit. Additionally, streets with high vehicle speeds will require greater space and separation for people to walk and bike safely. To maximize the people-moving capacity in the Core Area and within our Urban Centres, it will be necessary to re-think our existing streets and roadway network. Developing a well-connected grid network of streets that are designed to accommodate growing numbers of people walking, biking, taking transit and driving in the future will be important.

A Progressive Approach to Congestion: As discussed in Appendix A of the Existing and Future Conditions Technical Report, it will be important to seek out *healthy levels of congestion* (congestion levels that are neither impractically low nor too excessive) to keep Kelowna moving while also achieving the City's vision and goals for transportation. This approach will ensure that the unintended negative consequences of building too much road capacity is minimized, while ensuring that investments in effective infrastructure are maximized.

Future Challenges / Opportunities:

To keep Kelowna moving, staff have identified 30 future challenges and opportunities around the themes of mode shift, developing a well-connected urban street network, and identifying a progressive approach to congestion management. These have been developed based on the review of existing and future conditions and are intended to work together to guide the development of potential projects, policies and programs to meet Kelowna's vision and goals for transportation. Each future challenge is also envisioned as an opportunity; that is – they are two different sides of the same coin. The future challenges and opportunities are listed below. Further details can be found in Chapter 4 of the Existing and Future Conditions Technical Report.

Report Chapter	Future Challenge / Opportunity	
4a) Walking	1	Design for Walkability in the Urban Centres
	2	Connect the Pedestrian Network in the Core Area
	3	Shift Short Trips to Walking
	4	Ensure People Walking Feel Safe
	5	Create Flexible and Adaptable Pedestrian Spaces
4b) Biking	6	Shift Trips within the Core Area to Biking
	7	Increase Perception of Biking as a Safe Mode of Travel
	8	Make Biking Accessible to More People
	9	Integrate Bicycles with Transit

	10	Build-out a Complete Bicycle Network
4c) Transit	11	Focus Growth near Frequent Transit and Ensure Multimodal Access
	12	Increase Transit Investment where Effective to Serve Growing Demand
	13	Speed Up Transit and Make it More Reliable
	14	Maximize Benefits of Technology Change on Transit
	15	Collect High Quality Data to Support Transit Planning
4d) Driving	16	Growth in Downtown and South Pandosy
	17	Continued Growth in Suburban Hillside
	18	Increasing Travel Demand through Midtown
	19	Employment Growth Along Highway 97
	20	Reduce the Frequency and Severity of Traffic Collisions
	21	Develop a Well-Connected, Complete Urban Street Network
4e) Shared Mobility	22	Expand and Improve Bikeshare and other Emerging Options
	23	Attract One-way Carshare
	24	Prepare for the Arrival of Ride-Hailing
	25	Prepare for the Arrival of Autonomous Vehicles
4g) Programs	26	Build Community Capacity
	27	Enhance Safe Routes to School
	28	Improve Transit Passes and Payment
	29	Manage the Curb
	30	Move Toward Parking On-Demand

Coordination with Other Plans:

Development of the TMP is occurring in coordination with development of the 2040 OCP and the 2040 Servicing Plan and Financing Strategy. These plans are being developed in parallel using Imagine Kelowna as a foundation, and will work together to support our growing City, while minimizing future challenges. While population growth will necessitate substantial future investment to maintain Kelowna's quality of life, the 2040 OCP endorsed Growth Scenario will help mitigate future costs by focusing transportation and infrastructure investments in locations that benefit a high number of people and yield strong returns on investment.

By focusing growth in the Core Area and Urban Centres and targeting investments to maximize mode shift, the endorsed Growth Scenario and TMP will work together to help address several global and local future challenges as well. In addition to mitigating the rise of traffic congestion, this combined approach can help strategically address other issues such as climate change, housing affordability, and an aging population:

- **Climate change:** Transportation accounts for the largest share of Kelowna's greenhouse gas emissions (55 per cent as of 2012). Working to reduce VKT and shift modes through combined land use and transportation planning is an effective long-term strategy for reducing transportation-related greenhouse gas emissions and is aligned with Kelowna's Community Climate Action Plan.
- **Housing Affordability:** While housing costs are typically viewed as the main culprit of an expensive city, transportation-related costs typically reflect the next highest share of a households' budget. Growing and investing in a way that enables households to reduce the cost of owning and

maintaining a vehicle can dramatically reduce a household's combined housing & transportation financial burden.

- Aging Population: Kelowna is projected to have more people in all age categories in 2040, with the greatest increases occurring in the segment of the population over 65 years old. Providing more housing close to services combined with travel options will help Kelowna's older citizens maintain mobility once they can no longer drive.

Public Engagement:

In coordination with the 2040 OCP and 2040 Servicing Plan and Financing Strategy, the TMP is preparing for public engagement in the Fall of 2019. Both online and in-person opportunities will be available for the public to review the TMP Existing and Future Conditions Technical Report, provide input on key issue areas, and share ideas for projects, policies and/or programs they would like considered in the TMP planning process.

Next Steps:

Moving forward, the 30 future challenges and opportunities described in the Existing and Future Conditions Technical Report will be used along with input from the public to identify potential transportation projects, programs and policies for evaluation. These potential options will be shared with Council and the public for input and evaluated using a strategic decision-making framework. Ultimately, the projects, programs, and policies that are projected to do the best job of helping the City reach its vision and goals for transportation will be brought forward as recommendations for consideration by Council. Final endorsement of the Transportation Master Plan is anticipated in summer 2020.

Internal Circulation:

Divisional Director, Infrastructure
Divisional Director, Planning & Development Services
Integrated Transportation Department Manager
Infrastructure Delivery Department Manager
Infrastructure Engineering Manager
Infrastructure Operations Department Manager
Utility Services Manager
Utility Planning Manager
Public Works Manager
Traffic Operations & Technical Support Supervisor
Policy & Planning Department Manager
Development Planning Department Manager
Development Engineering Manager
Parks & Buildings Planning Manager
Parking Services Manager
Community Communications Manager
Transportation Engineering Manager
Transit and Programs Manager
Long Range Policy Planning Manager
OCP Project Planner
Transportation Planner
Transportation Engineer Planning & Development

Active Transportation Coordinator
Sustainability Coordinator
Planner Specialist
Communications Advisor

Considerations not applicable to this report:

Legal/Statutory Authority
Legal/Statutory Procedural Requirements
Financial/Budgetary Considerations
External Agency/Public Comments
Communications Comments

Submitted by:

M. VanZerr, Strategic Transportation Planning Manager

Approved for inclusion:



Alan Newcombe, Divisional Directors, Infrastructure

Attachment 1 – TMP Existing and Future Conditions Technical Report Presentation



Transportation Master Plan
Existing and Future Conditions Technical Report
Council Workshop

August 12, 2019

Workshop Objectives

- ▶ Educational and gather Council's perspectives:
 - ▶ Overview of existing and future conditions
 - ▶ Discussion of the relationship between land use and transportation system performance
 - ▶ Deeper understanding of transportation and traffic congestion
 - ▶ **Introduce future challenges and opportunities to keep Kelowna competitive in a changing world**

TMP Vision

"Kelowna will be a city with vibrant urban centres where people and places are conveniently connected by diverse transportation options that help us transition from our car-centric culture"



TMP Goals

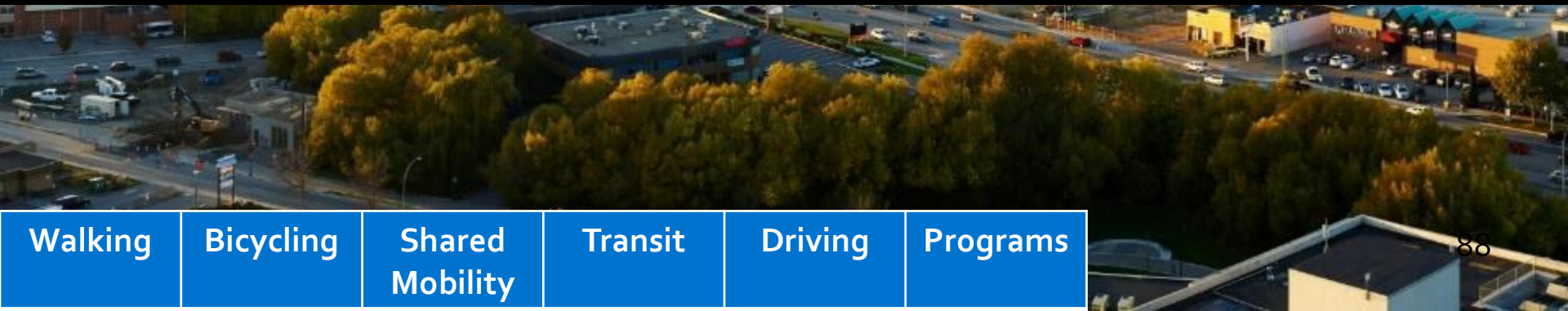




- ▶ Coordinated TMP / OCP Engagement
- ▶ Four different growth scenarios for Kelowna in 2040
 - ▶ Transportation impacts assessed at high level for each scenario
- ▶ On March 3rd, 2019 Council endorsed the preferred Growth Scenario
 - ▶ Foundation for development of the TMP



Existing Conditions – Summary by Mode



Walking

Bicycling

Shared
Mobility

Transit

Driving

Programs

Walking

Key Factors

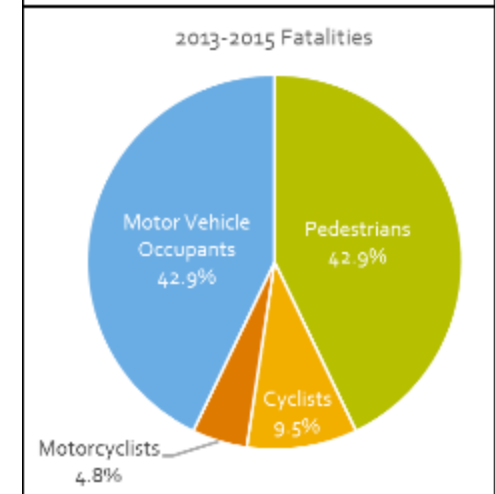
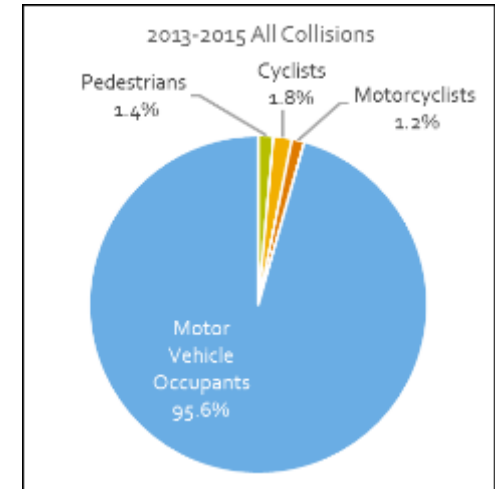
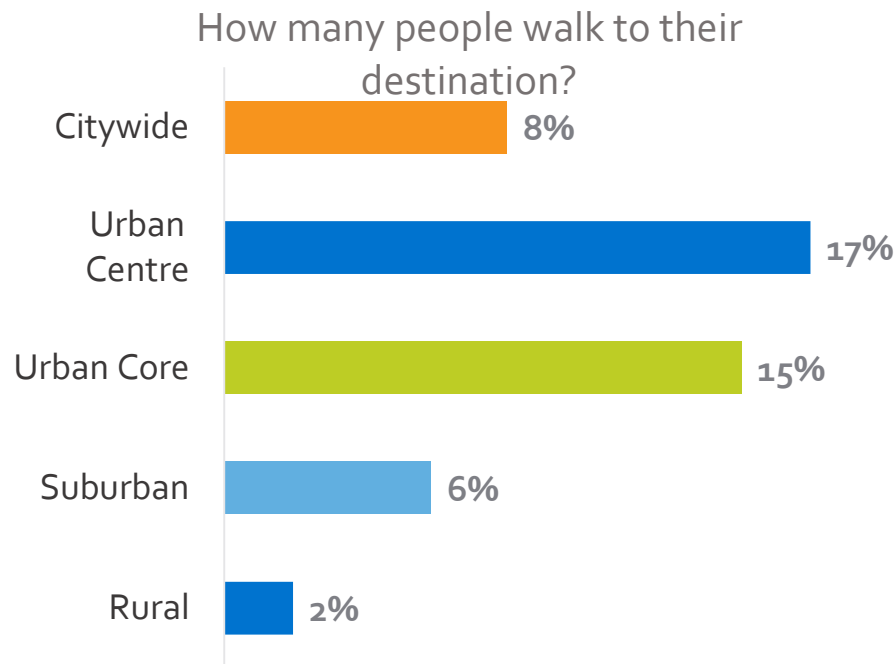
- ▶ Shorter Trips
- ▶ Direct Routes
- ▶ Human-Scale Urban Design
- ▶ Pedestrian Safety
- ▶ Grades

Walking



► Walking trips are short

62% of walking trips are less than 1 km



Bicycling

Key Factors

- ▶ Short – Med Length Trips
- ▶ Direct Routes
- ▶ Low-stress Bicycle Infrastructure
- ▶ Grades

Walking

Bicycling

Shared
Mobility

Transit

Driving

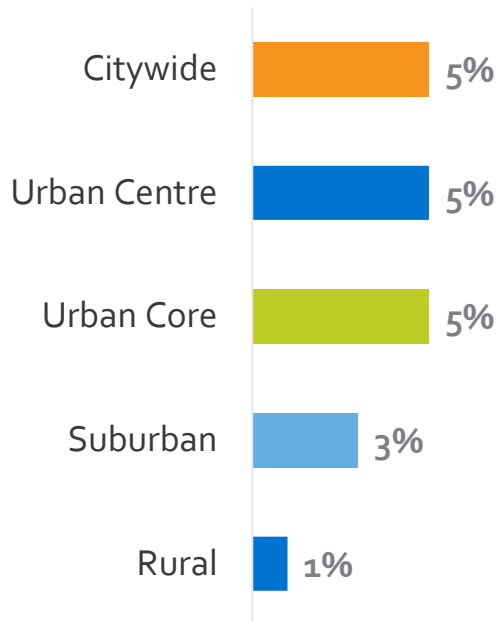
Programs

Bicycling

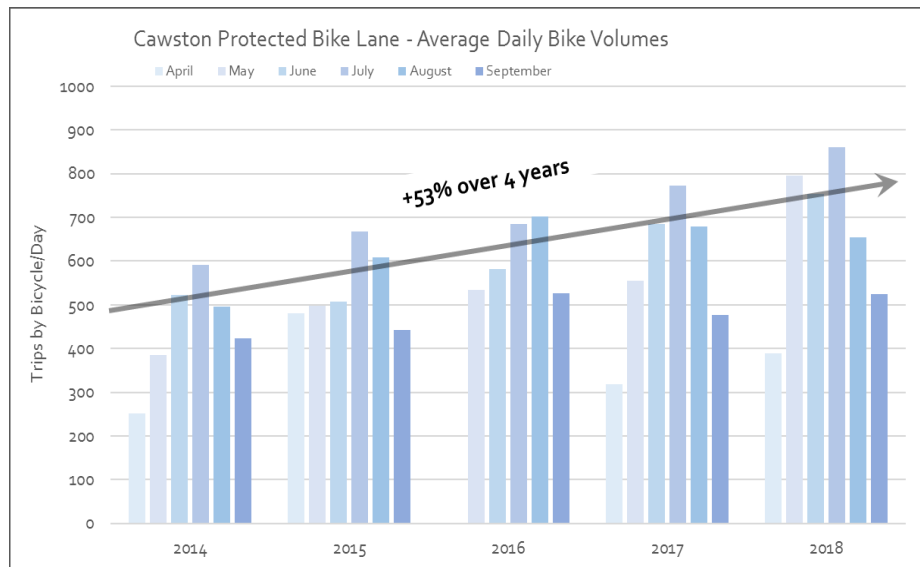


- Biking trips are short – mid length
88% of biking trips are less than 5 km

How many people bike to their destination?



Bike Trips are Increasing:



Shared Mobility



An Evolving Suite of Options

- ▶ Bikeshare
- ▶ Micro-mobility (e-scooters, e-bikes, other)
- ▶ Carshare (e.g. Modo)
- ▶ Ride-hailing (e.g. Uber, Lyft)



Transit

Key Factors

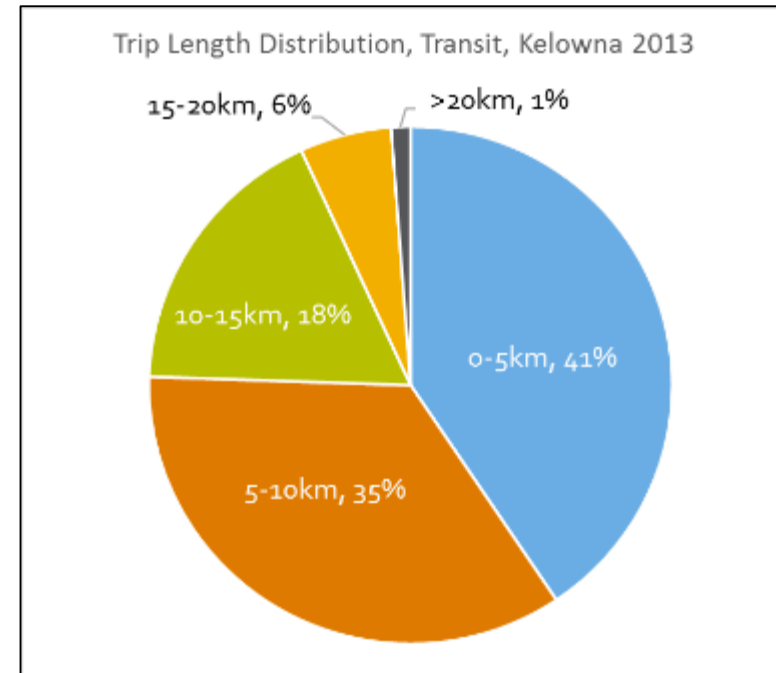
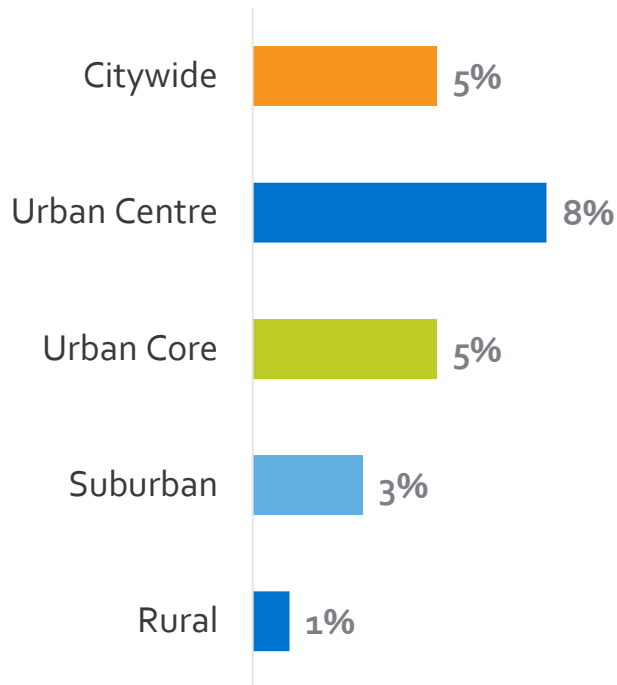
- ▶ Competitive Travel Times
- ▶ Schedule Frequency
- ▶ Travel Time Reliability
- ▶ Proximity to Transit Stop
- ▶ Perception of Safety/Comfort

Transit



- ▶ Transit trips are medium to long in length (average distance of 7 km)

How many people get around on transit?





Driving

Key Factors

- ▶ Long distances / topography
- ▶ Need to transport people or cargo
- ▶ Inclement weather
- ▶ Operating costs (e.g. fuel, insurance, etc.)
- ▶ Availability/cost of parking

Walking

Bicycling

Shared
Mobility

Transit

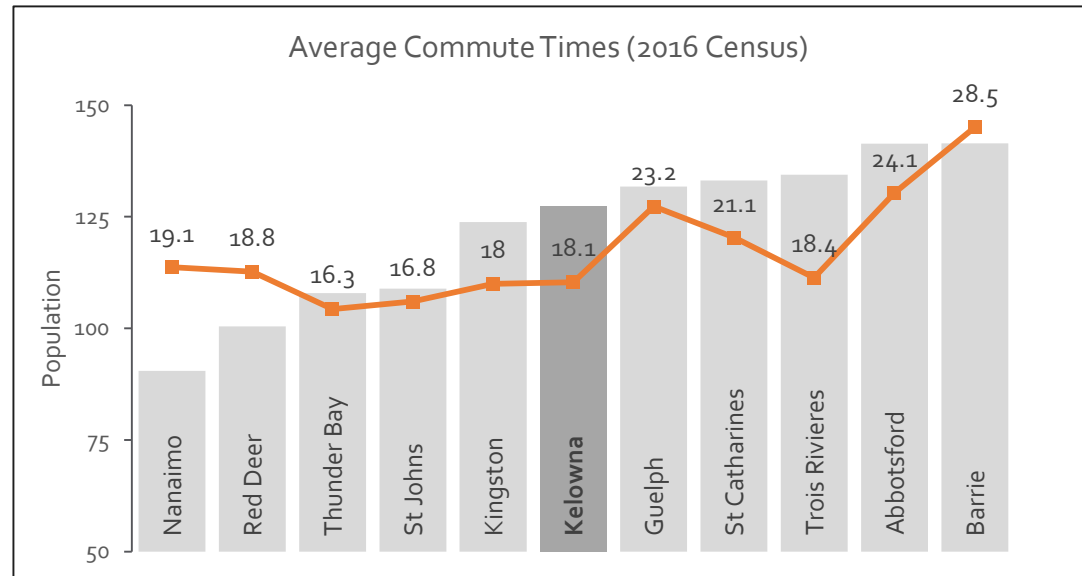
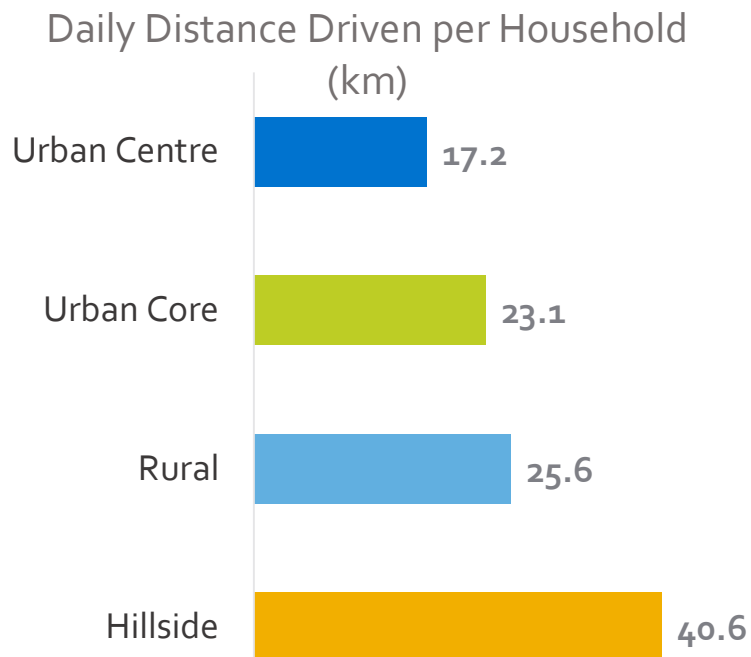
Driving

Programs

Driving



- ▶ Average commute time in 2016 was **18.1 minutes**



Walking

Bicycling

Shared
Mobility

Transit

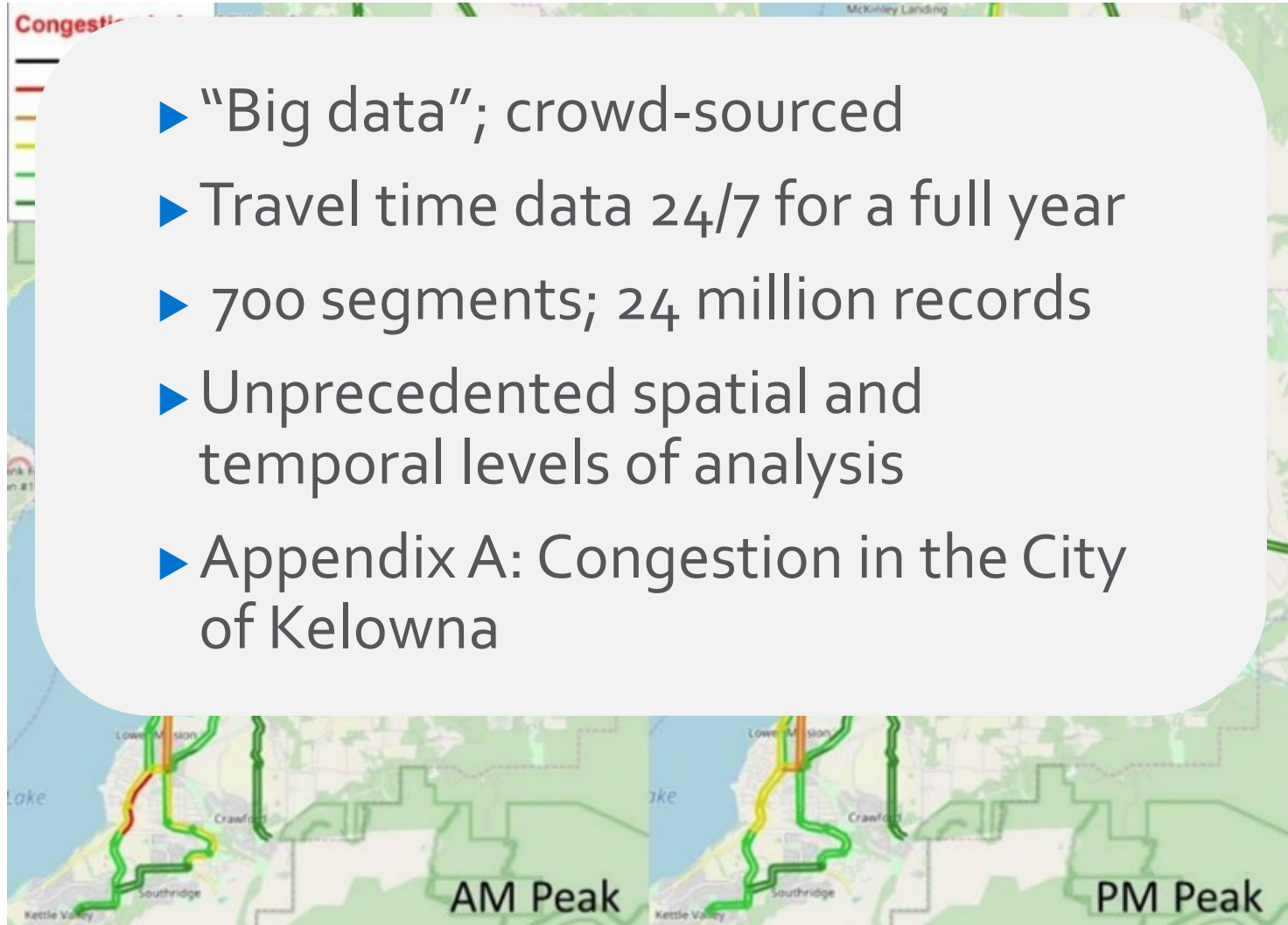
Driving

Programs

Congestion Data



- ▶ “Big data”; crowd-sourced
- ▶ Travel time data 24/7 for a full year
- ▶ 700 segments; 24 million records
- ▶ Unprecedented spatial and temporal levels of analysis
- ▶ Appendix A: Congestion in the City of Kelowna



Travel Time: Historic Trends

Comparison of the 2008 and 2017/18 travel times include the following observations:

- ▶ **Morning Peak:** Travel times increased an average of 1.3 minutes (6.4 per cent)
- ▶ **Midday:** Travel times mostly held steady, increasing by 0.6 per cent
- ▶ **Afternoon Peak:** Travel times increased an average of 1.2 minutes (4.7 per cent)

Programs



- ▶ Bike to Work Week
- ▶ Walk & Bike to School Week
- ▶ Bike Rodeos
- ▶ Clean Air and Safe Routes to School
- ▶ Transit Pass Programs
- ▶ Traffic Calming Program
- ▶ Parking Policies and Programs





FUTURE CONDITIONS



Predicting the Future . . .



- ▶ Central Okanagan Regional Travel Model
 - ▶ Not a crystal ball; limited

- ▶ 2040 TMP "Baseline" Scenario =

2040 OCP endorsed Growth Scenario

+

Current Transportation Network
(+ limited number of projects
in the current 10 year capital plan)



2040 TMP

“Baseline” Scenario

- Why a Baseline?
 - Helps identify and evaluate potential transportation investments against a base
- What the Baseline represents:
 - Land use changes with no corresponding transportation improvements
 - No behavior change
 - Weekday afternoon rush hour (highest congestion levels)



2040 TMP

“Baseline” Scenario

Citywide Metrics

Population growth	+ 39 %	↑
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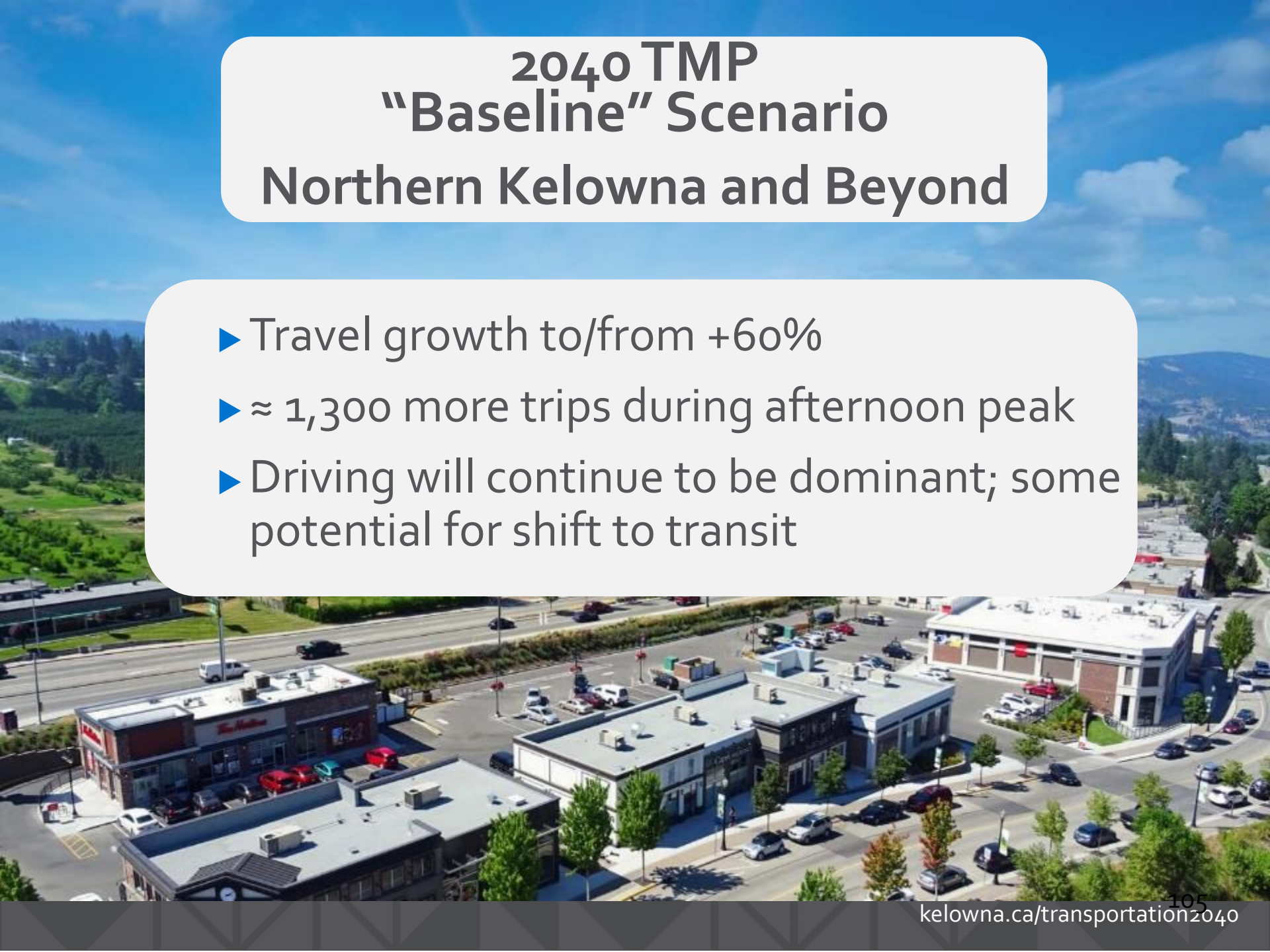
VKT per capita:	- 5 %	↓
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Total Vehicle Kilometres Travelled	+ 40 %	↑
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Total Vehicle Hours Travelled	+ 70 %	↑
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Average Travel Speeds	- 15 %	↓
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58 % of study intersections at or over capacity (up from 10 % currently)	↑
-----------------------------------------------------------------------------	---

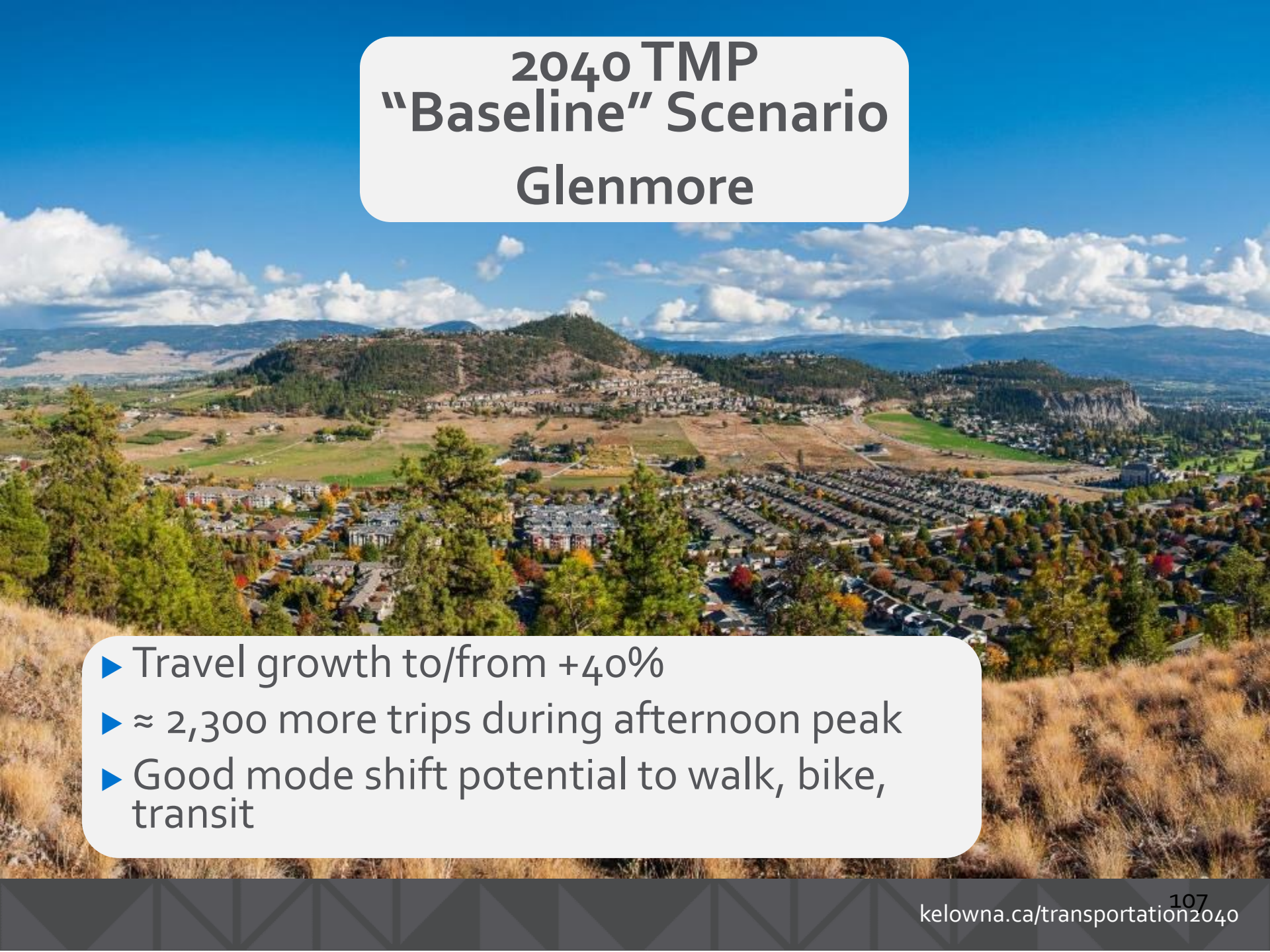
An aerial photograph of a city street scene, likely in Kelowna, British Columbia. The image shows a multi-lane road with several commercial buildings, including a large white building with a flat roof and a smaller building with a gabled roof. There are numerous cars parked in lots and on the street. In the background, there are green hills and a clear blue sky with some clouds. The overall scene depicts a typical urban environment.

2040 TMP “Baseline” Scenario Northern Kelowna and Beyond

- ▶ Travel growth to/from +60%
- ▶ $\approx 1,300$ more trips during afternoon peak
- ▶ Driving will continue to be dominant; some potential for shift to transit

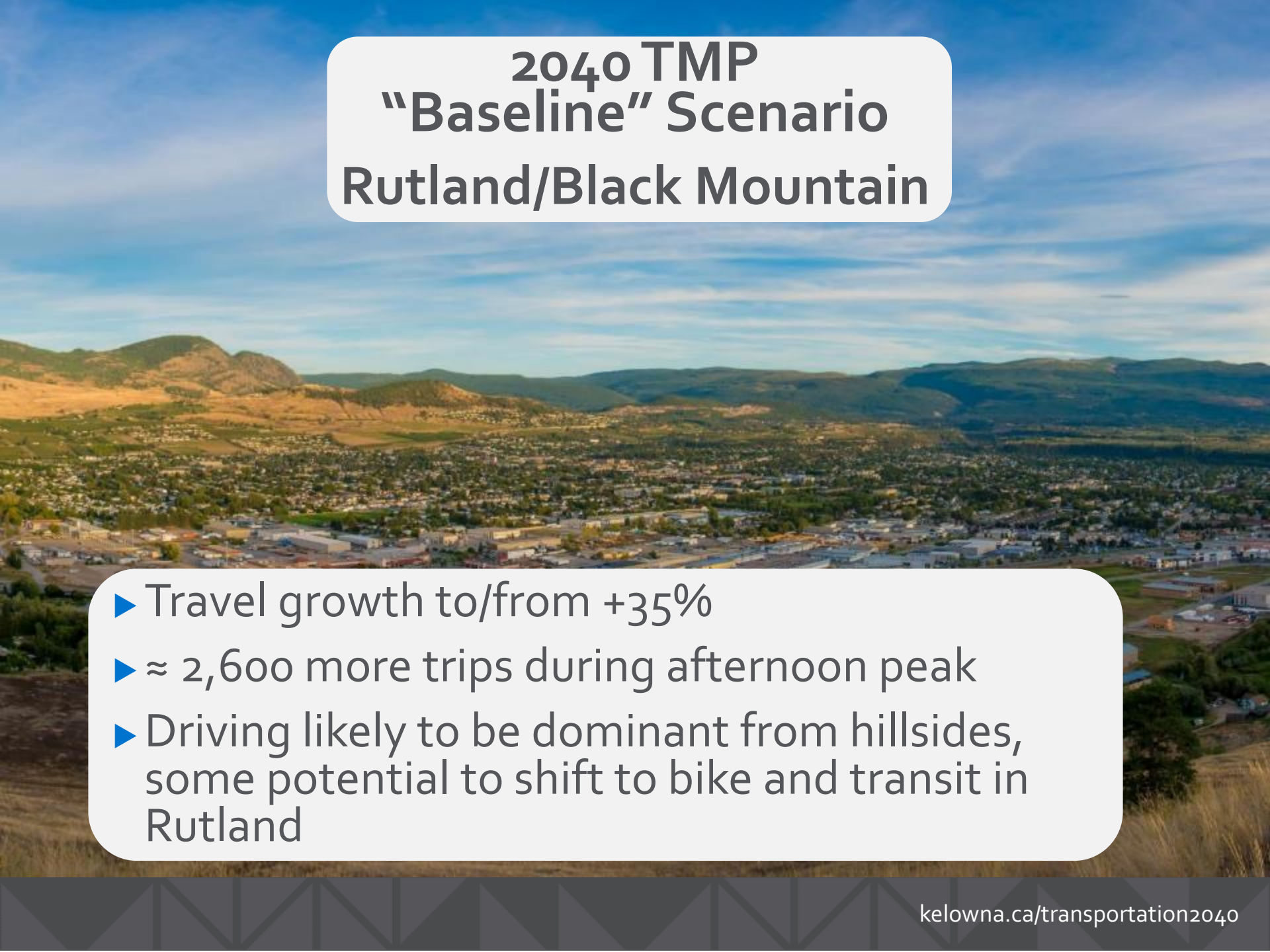
2040 TMP “Baseline” Scenario Okanagan Gateway

- 
- ▶ Travel growth to/from +65%
 - ▶ ≈ 3,400 more trips during afternoon peak
 - ▶ Driving, biking, and transit will serve different contexts within the area



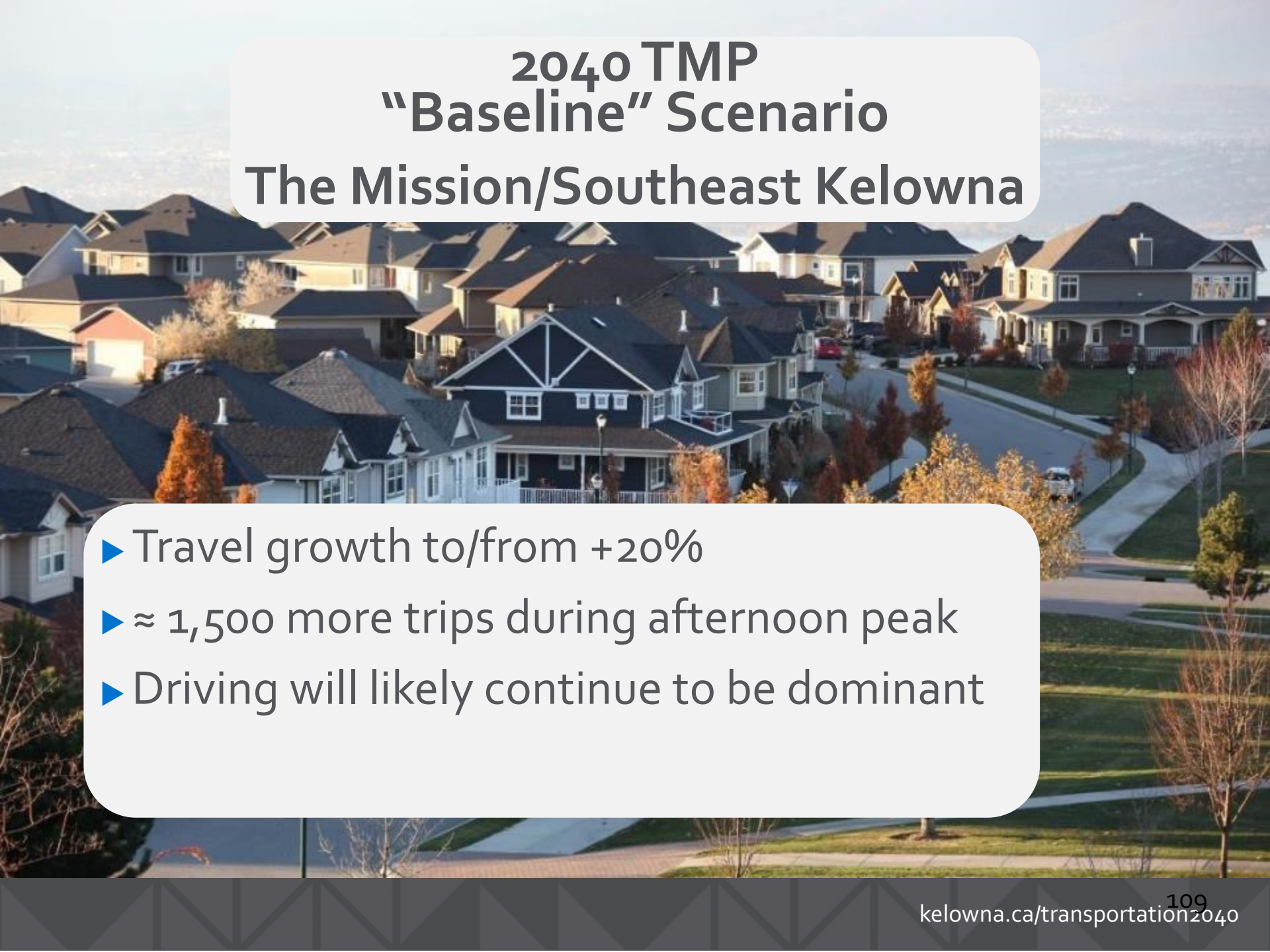
2040 TMP “Baseline” Scenario Glenmore

- ▶ Travel growth to/from +40%
- ▶ ≈ 2,300 more trips during afternoon peak
- ▶ Good mode shift potential to walk, bike, transit



2040 TMP “Baseline” Scenario Rutland/Black Mountain

- ▶ Travel growth to/from +35%
- ▶ ≈ 2,600 more trips during afternoon peak
- ▶ Driving likely to be dominant from hillsides, some potential to shift to bike and transit in Rutland



2040 TMP “Baseline” Scenario The Mission/Southeast Kelowna

- ▶ Travel growth to/from +20%
- ▶ $\approx 1,500$ more trips during afternoon peak
- ▶ Driving will likely continue to be dominant

2040 TMP “Baseline” Scenario City Centre/South Pandosy/Capri-Landmark



- ▶ Travel growth to/from +40%
- ▶ Internal trips will nearly double
- ▶ ≈ 7,500 more trips during afternoon peak
- ▶ High mode shift potential

A photograph of a shopping center entrance. On the left is a large stone pillar with a red apple logo and the text 'ORCHARD PARK'. Below the pillar are signs for 'Sears' and 'WALMART'. A wide road leads into the center, flanked by parking lots filled with cars. In the background, there are mountains under a cloudy sky. A 'SHOPPERS DRUG MART' sign is visible on the right side of the road.

2040 TMP “Baseline” Scenario Midtown

- ▶ Travel growth to/from +25%
- ▶ \approx 2,500 more trips during afternoon peak
- ▶ Potential to shift some trips to walk, bike, transit

What is Traffic Congestion?

- ▶ Congestion happens when the demand for road space exceeds the supply.
- ▶ Congestion is positively correlated with economic productivity. It becomes heaviest when the economy is booming and declines during recessions.
- ▶ Congestion-free travel during rush hour is not a realistic goal in a thriving urban economy.



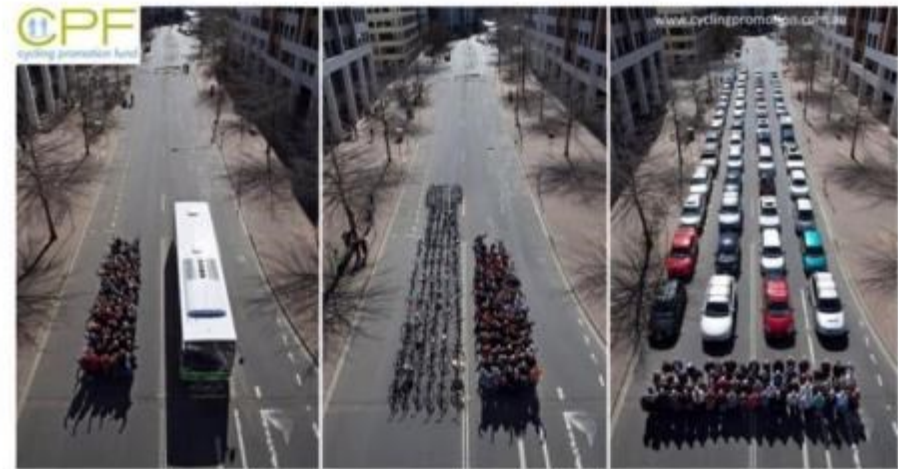
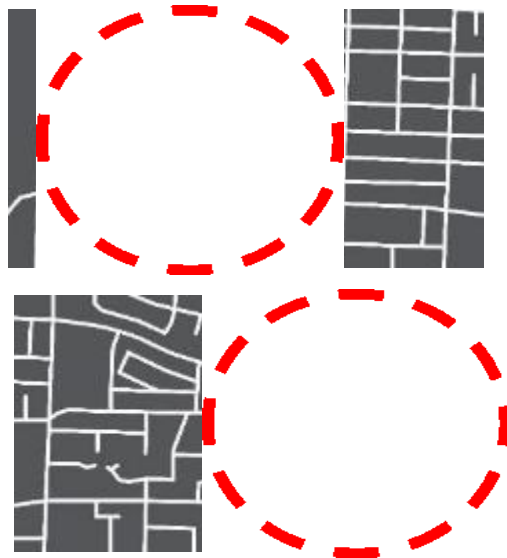
The Congestion Paradox

- ▶ Building and widening roads is expensive and ineffective as a long-term congestion relief strategy
 - ▶ Limited space
 - ▶ Expensive - \$26 million/km in the Core Area
 - ▶ “Induced demand” erodes benefits



Keeping Kelowna Moving

- ▶ Mode shift
- ▶ A well-connected, complete urban street network
- ▶ Progressive approach to congestion management



69 bicycles vs. 60 cars Photograph courtesy of Cycling Promotion Fund

Future Challenges / Opportunities

Walking	1	Design for Walkability in the Urban Centres
	2	Connect the Pedestrian Network in the Core Area
	3	Shift Short Trips to Walking
	4	Ensure People Walking Feel Safe
	5	Create Flexible and Adaptable Pedestrian Spaces
Biking	6	Shift Trips within the Core Area to Biking
	7	Increase Perception of Biking as a Safe Mode of Travel
	8	Make Bicycling Accessible to More People
	9	Integrate Bicycles with Transit
	10	Build-out a Complete Bicycle Network
Transit	11	Focus Growth near Frequent Transit
	12	Increase Transit Investment where Effective to Serve Growing Demand
	13	Speed Up Transit and Make it More Reliable
	14	Maximize Benefits of Technology Change on Transit
	15	Collect High Quality Data to Support Transit Planning

Future Challenges / Opportunities

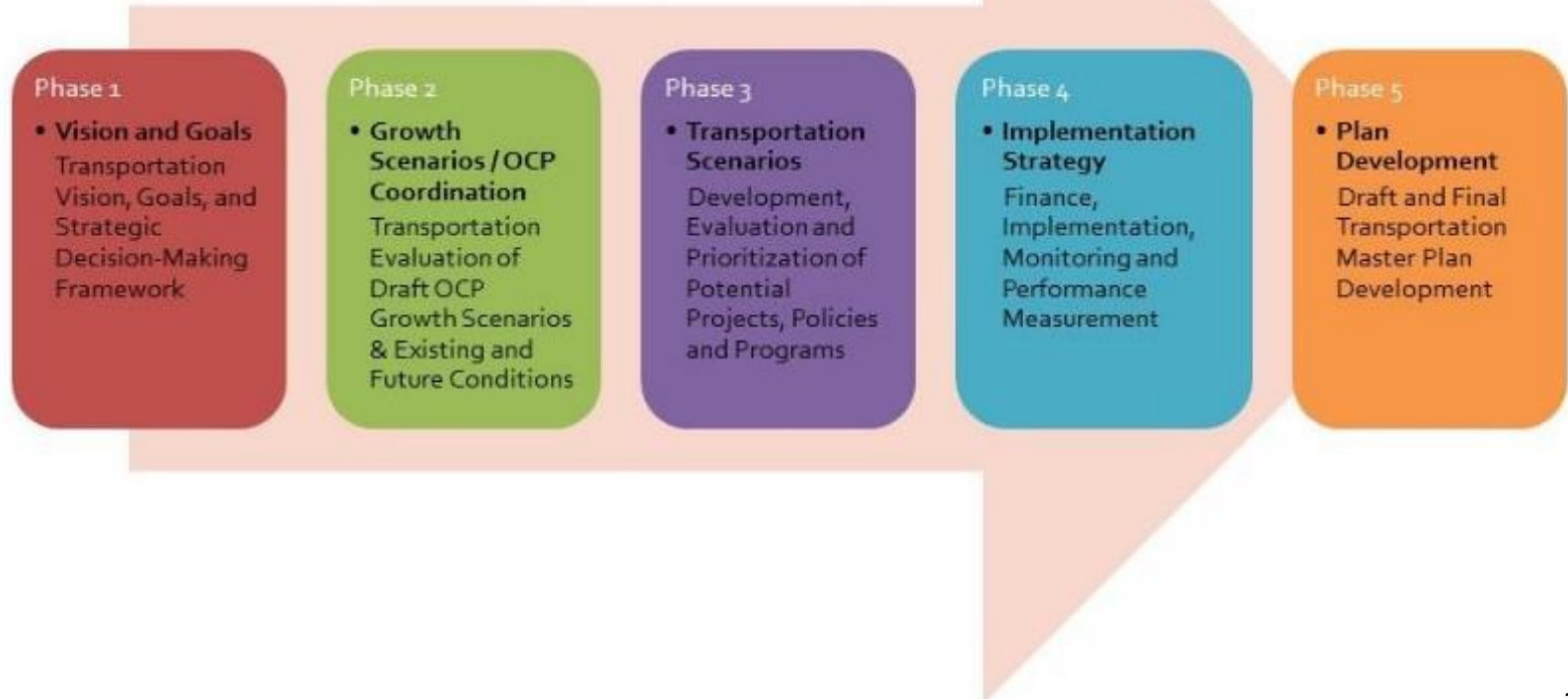


Driving	16	Growth in Downtown and South Pandosy
	17	Continued Growth in Suburban Hillside
	18	Increasing Travel Demand through Midtown
	19	Employment Growth Along Highway 97
	20	Reduce the Frequency and Severity of Traffic Collisions
	21	Develop a Well-Connected, Complete Urban Street Network
Shared Mobility	22	Expand and Improve Bikeshare and other Emerging Options
	23	Attract One-way Carshare
	24	Prepare for the Arrival of Ride-Hailing
	25	Prepare for the Arrival of Autonomous Vehicles
Programs	26	Build Community Capacity
	27	Enhance Safe Routes to School
	28	Improve Transit Passes and Payment
	29	Manage the Curb
	30	Move Toward Parking On-Demand

Next Steps

Phase 3: Transportation Scenarios for the Transportation Master Plan.

- Develop, Evaluate and Prioritize Potential Projects, Policies and Programs



Report to Council



Date: August 12, 2019
To: Council
From: City Manager
Subject: 2040 Infrastructure Impact Analysis
Division: Infrastructure

Recommendation:

THAT Council receives for information, the report from the Infrastructure Engineering Manager dated August 12, 2019, with respect to the 2040 Infrastructure Impact Analysis.

Purpose:

To provide Council with a high-level review of the infrastructure impacts associated with the 2040 Official Community Plan (2040 OCP) Growth Scenario.

Foreward:

Servicing and infrastructure must be in place in advance to allow our community to grow in a manner that meets citizen requirements and expectations. There is not a question of 'if we will grow', but more importantly, 'how are we prepared to respond to this anticipated growth'. Growth patterns have a direct impact on the cost of servicing and infrastructure. For example, public transit can be more effective at mitigating rising congestion levels in a dense urban environment. This is the deliberate product of more sustainable land use patterns and choices for moving around our community that yield fiscally, socially and environmentally responsible outcomes.

The City's goal is to optimize each infrastructure system to best service existing customers while consciously expanding systems to accommodate growth. This is accomplished through integrated master plans that anticipate infrastructure requirements guided by the 2040 OCP's growth strategy.

Background:

The 2040 OCP Update is currently in progress and Council has endorsed the preferred Growth Scenario (March 3rd, 2019). As part of the ongoing work to update the 2040 OCP and the associated infrastructure servicing plan, an Infrastructure Impact Analysis was completed to identify challenges and opportunities to service growth. This analysis will inform Council on any anticipated major servicing impacts and challenges that will influence the pending update to the 20 Year Servicing Plan and Financial Strategy (out to 2040). This is the first of a series of infrastructure servicing updates that will be presented to Council during the development of the 2040 OCP with the intent of keeping Council and the community informed.

There are four major, interdependent planning studies involved in this analysis (three are currently underway and one pending). These include:

- 2040 Official Community Plan (2040 OCP)
- 2040 Servicing Plan and Financial Strategy
- Transportation Master Plan (TMP)
- Parks Master Plan (pending Council approval for 2020)

These planning studies are being developed in parallel, with the project teams meeting regularly to coordinate activities and ensure alignment of the plans.

Service Areas Reviewed:

Major service areas reviewed as part of this analysis include Transportation, Parks, Water and Wastewater utilities. Additional commentary on Solid Waste and Storm Drainage is included as these infrastructure systems impact indirectly the 2040 Growth Scenario. Not included in this analysis are Building and Facility infrastructure required to satisfy recreational, cultural, safety and operational demands for growth.

Transportation

Kelowna's population is projected to grow by 50,000 people by 2040. If all those additional residents rely on driving to the degree we do today, the result will be a very congested Kelowna, with few transportation alternatives and a less attractive place to build a business or raise a family. The impacts will be most noticeable in the Core Area, where residents commuting in and out of car-dependent hillside neighbourhoods will be competing for limited roadway space with Core Area residents who will have options to get around using a variety of travel modes. Without a clear transportation policy, a change in behavior or improvements to the network, the project result will be higher traffic congestion.

One of the most effective long-term congestion mitigation strategies is to reduce auto-dependence by giving people more convenient and realistic alternatives for getting around, especially during the morning and afternoon rush hours. This requires a coordinated approach to land use and transportation planning that shortens trip distances and creates complete, connected and safe bicycle, pedestrian and transit networks between key destinations.

To help keep Kelowna moving, it will be necessary to shift as many future trips as possible to more sustainable transportation modes that can move more people in the same amount of space (i.e. biking, walking, transit and shared mobility options).

For the most part, the best opportunities for mode shift are within the Urban Centres and Core Area, where the terrain is relatively flat, and some supporting infrastructure for walking, biking and transit is already available. Increased densification will result in shorter trip distances, thus removing the primary barrier to walking and biking for nearby residents. If the City takes consistent and complementary action to ensure the transportation network provides safe, attractive and convenient infrastructure for walking, biking and transit in these densifying areas, the number of trips that are shifted to these modes can be maximized. This will help prioritize road space for trips that must be made by driving, while giving Kelowna residents more choices for getting around.

Investment will need to increase above existing funding levels for transportation to support the Transportation Master Plan and the 2040 OCP Growth Scenario. While population growth alone will necessitate substantial future investment in the transportation network to maintain Kelowna's quality of life, the endorsed Growth Scenario helps to manage future costs by focusing transportation and infrastructure investments in locations that benefit a high number of people and yield strong returns on investment. Details and cost estimates for needed future investment will be provided as part of the Transportation Master Plan moving forward.

Parks

Parks are integral to the establishment of new neighbourhoods and further development of existing communities. The acquisition and development of park space will be key to meet the needs of the 2040 OCP Growth Scenario. The Scenario assumes a 67% population increase in the Urban Core, and notably this geographic area envelopes three centres: Downtown, Rutland and South Padosy. In these urban centres, land is less available and real estate values are up to three times the City average. These centres are also currently less well served by local neighbourhood and community parks than other sectors in the City. The following challenges are anticipated:

- **Availability of suitable locations in high density areas:** The acquisition modelling studies concluded that larger areas are required for local neighbourhood and community parks. This is difficult to achieve in the three urban centres.
- **Rebalancing with the provision of Recreation and City-wide parks:** To balance any reduction in the current OCP's target of 1.0 Hectare (ha)/1000 standard for Neighbourhood and Community parks in the urban centres, Recreation and City-wide parks are increased elsewhere in the City to maintain the overall standard of 2.2 Ha/1000.
- **Availability of suitable locations for recreational use:** Recreation parks require large areas of flat land to meet the very specific geometries for sports fields, ball diamonds, tracks and courts. Due to the topography of our City, it is becoming increasingly challenging to identify suitable flat sites for such parks.
- **Acquisition cost and availability of waterfront property:** There is a strong demand by residents and visitors for increased access to the waterfront, and many water-based activities and sports groups request shared or dedicated access to waterfront properties. However, waterfront properties hold some of the highest land values, making them expensive to acquire for park use.

The City's current park strategy has financial challenges when combining past practices with the projected Growth Scenario. The 2040 OCP and subsequent Parks Master Plan¹ affords the City opportunities to maximize the use of existing municipal land assets within achievable financial targets. These may include:

- **Future land use:** The next phase of the OCP development will start to identify specific locations for future park sites. Low current land values and City ownership will be among the primary drivers in determining park location. In doing so, the proposed acquisition costs discussed above, currently based on average costs, may be reduced. Identifying specific sites for future park use is a valuable tool in ensuring the park acquisition and development program remains both achievable and affordable.

¹ The Parks Master Plan process is anticipated to commence in 2020 pending budget approval.

- **Linear parks:** Investing in more linear parks provides walkable green links between other parks, urban centres and other amenities. Mill Creek, Mission Creek and the Okanagan Rail Trail in particular, provide valuable pedestrian links through some of the densest urban sectors and improve walkability with limited interface with traffic.
- **Urban Parks:** Investment will be shifted to accommodate more compact urban neighbourhood parks, providing a greater density of amenities on a smaller footprint. This makes our urban parks more capable of adapting to existing lot sizes and reduces the need for consolidation.
- **Other opportunities under consideration include:** Shared streets, pedestrian or green streets, pockets parks or rooftop parks.

By developing strong neighbourhoods through attractive green spaces and generous park amenities, the City builds on the existing desirability of Kelowna as future home for new residents. Through the 2040 OCP, the City will seek opportunities to leverage benefits for the acquisition and development of our park system.

Water

There will be substantial changes in how the City supplies water to citizens to the year 2040. Integrating water service providers into the City's water utility has commenced in Southeast Kelowna with potentially more to follow to meet the goal of providing all areas of the City with high quality drinking water. The population increases anticipated from the 2040 OCP Growth Scenario will result in major water transmission projects that "shoulder" the supply between our largest lake supply at Poplar Point to our main reservoir on Dilworth mountain. System resiliency and upsizing will be key needs as existing large transmission mainlines are renewed and upgraded over the next 20 years. The key new projects include:

- Twinning and ultimate renewal of transmission main from Poplar Point to Knox Mountain Reservoir down Richter Street towards downtown.
- Upgrade to the transmission main between Knox Mountain Reservoir to Dilworth Reservoir, which services most of Clifton, Glenmore and commercial areas downtown.
- Adding a reservoir cell to Dilworth Reservoir to accommodate future capacity and fire flow needs.
- Adding pumps and capacity at Poplar Point.

Construction in these areas is complex and costly given the "congestion" of existing buried utilities, high groundwater levels and changing residential fire flow requirements. These new mainline projects will need to be coordinated with other actions in the Transportation and Parks sectors to improve efficiencies and cost effectiveness.

The City Utility's water quality, sourced from Okanagan Lake, meets and exceeds Canadian Drinking Water Quality Guidelines. To date, the City has had no need to filter the lake water prior to disinfection because of the high source water quality. Should the water quality deteriorate due to climate change or other factors, Interior Health may require the City to construct water filtration plants on each intake to meet mandated guidelines. This presents a financial risk to the City as the filtration plant costs would be well in excess of \$200 million. The City will continue to work in collaboration with provincial regulators to understand the need and timing of changes so that the City can plan in advance for the required infrastructure improvements. During this period, staff will identify potential sites for one of the filtration plants in the downtown industrial area.

Since 2018, the City has operated an irrigation water supply and transmission system to over 1000 properties in Southeast Kelowna. The converted works include several dams and storage, including McCulloch Reservoir, that impound water for irrigation and general farm use. The irrigation system also provides fire protection in Southeast Kelowna and will require upgrades in areas where future development occurs.

The 2040 Growth Scenario forecasts minimal development in Southeast Kelowna. However, the City, in allowing any growth in the agricultural sector, is cognizant that upland water supply is almost fully allocated. The City will be reviewing water consumption practices and water conservation opportunities.

Currently, the irrigation network requires a new balancing reservoir, pipe upgrades and upgrades to the dams and reservoirs upstream. The costs of maintaining these assets are yet to be determined. The portion of these projects attributed to growth will be addressed at a later date.

Wastewater

All wastewater collected in the City's sanitary sewer system routes to the Kelowna Wastewater Treatment Facility (WWTF). The facility has been in operation at this site since 1913 and utilizes a technology that biologically reduces and removes nutrients from the sewage stream and in turn discharges high quality effluent back into Lake Okanagan. While some minor future upgrades are necessary, the facility is expected to have adequate capacity to address future population growth until upgrades or a new treatment facility is constructed after 2050.

In the short-term, the City will need to address the growing quantities of bio-solids being generated from the WWTF as the City's Regional Compost Facility used for bio-solids treatment nears capacity. Significant infrastructure investment is required to expand the existing Compost Facility, or the City will need to implement a more process-oriented digestion and gas generation system.

Otherwise, the City is well positioned for wastewater capacity to 2040. The City will continue its programs to renew and upgrade its pumping facilities, large collector mains, and increasing capacity from the Rutland and Glenmore areas. The 2040 Growth Scenario will impact the order of which projects require construction, however funding levels are not likely to be impacted in the long-term. The major upgrades required include:

- Compost Facility expansion or Digester addition.
- Byrns/Barons 1050 mm transmission main routing existing and future flows from the Rutland and Glenmore areas.
- Transmission upgrades from Glenmore to Bernard, where growth from the north will overwhelm the crossing under Clement Drive towards the Ethel main.
- Renewal and upgrades to approximately 20 Lift Stations throughout the City.
- Upsizing, renewal and twinning of the crossing under Harvey (Hwy 97) at Water Street will be triggered by the Council-endorsed growth scenario. The project would likely extend down to the Water Street Lift Station.
- Should grant funding materialize, sanitary sewer for approximately 5,000 residents would be added, eliminating legacy septic systems across the City.

A planning process will be initiated over the next 20 years in preparation for the 2050 change-over to a larger, modern and higher capacity treatment complex. The City has purchased land north of the Munson

Pond area on KLO Road for the new future site. All new contemplated major works are to be constructed considering service to the future location.

Solid Waste

The Glenmore Landfill is not anticipated to reach its capacity for another 90 years. The facility is self-funded through tipping fees and related revenues.

There are many factors however, that can impact this timeline negatively, including excessive growth, wasteful practices, changes in regulatory needs and negative impacts from the recycling business. The City goes to great lengths to minimize landfill biogas and methane production: a greenhouse gas contributor. The City is also adding impervious lining to eliminate any risk of leachate intrusion into the surrounding area. This, combined with odor control efforts, will ensure that the landfill operates efficiently with minimal nuisance to residents and institutions.

The 2040 Growth Scenario is not expected to impact the current plan for the landfill, although care will be needed to assure that any form of development is planned while not impacting the location or operation of the facility. The City will continue to work with UBCO to research methods that will extend the life of the landfill, and further minimize impacts to the environment. Concepts such as bio-diesel, incineration and plastics management are all possible projects for the future.

Drainage and Flood Mitigation

The recent flood, fire, storm and landslide events have left residents and businesses concerned about safety and the City's capacity for recovery. Operationally, the City is committed to protecting public, environmental and economic health. To meet the development needs in the City's more populated areas means addressing flood management, where flood protection, flow measurement, indigenous nations consultation, riparian conditions and policy development will be required.

The 2040 OCP Growth Scenario identified areas of growth where flood mitigation is required. This is required as increased building density, a decreasing number of natural flow paths, and a changing climate are impacting how we look at flood management. Many grandfathered privileges allowed in the past must be removed in the future to re-establish flood capacity and protect areas from flooding throughout the City. To allow for future development in the key growth areas, two watercourses are recommended to take priority over the next 20 years; Mill Creek and Brandts Creek. Both creeks meander through industrial, commercial and populated residential areas of the City and have been identified as flood prone areas in recent studies.

The four flood mitigation projects identified as part of the future growth strategy include:

- Mill Creek re-alignment and flood conveyance through the airport property and Highway 97 to help achieve 2045 Airport Master Plan outcomes.
- Flood Capacity Improvements throughout the Mill Creek corridor to re-establish creek boundaries, enhance riparian habitat and flood capacity.
- Lower Mill Creek Diversion through Brandts Creek as a solution to further route flood flows around the more populated downtown flood zone.
- Upper Watershed Storage Attenuation – new operations or reservoir storage on Mill Creek and reduce flood impacts at the airport.

The City has secured federal grant funding of \$22M to subsidize the above four projects estimated to cost \$55M. Combined, these projects mitigate flooding in the inner core growth areas. The above projects are identified in the 10 year Capital Plan, and are anticipated to be completed over the next nine years.

Financial Considerations:

20 Year Servicing Plan & Finance Strategy

This impact analysis has outlined that in order to accommodate continuing growth in Kelowna, construction of new services or expansion of existing services will be required.

The purpose of the 20 Year Servicing Plan and Financing Strategy document is to provide a detailed analysis of the major infrastructure required to service growth as outlined in the 2040 OCP. The 20 Year Servicing Plan & Finance Strategy will reflect the impact of the new 2040 OCP on the City's municipal infrastructure through:

- Updating project costs to reflect more recent construction costs.
- Removing projects in the previous plan that have been completed.
- Removing, revising the scope and adding projects that respond to the locations and magnitude of projected growth in the 2040 OCP.
- Recalculating the Development Cost Charges (DCCs) based on the planned projects divided by the anticipated growth.
- Determining an equitable and fair allocation of costs between development and the existing tax base.

Infrastructure included in the 20 Year Servicing Plan & Finance Strategy

A municipality's ability to finance new infrastructure to accommodate growth is limited to powers granted by the *Local Government Act*. The Provincial Government through legislation, has empowered municipalities with the right to impose DCCs for major services such as arterial and collector roads, water systems, sanitary sewer systems, drainage systems, parkland acquisition and development. The City currently collects DCCs for all allowable services except:

- Park development (in progress)
- Drainage or flood mitigation (Mill and Brandts Creek areas)

Infrastructure not included the 20 Year Servicing Plan & Finance Strategy

Infrastructure that cannot be funded through DCCs, and is therefore excluded from the 20 Year Servicing Plan & Financing Strategy, includes natural open space acquisition, park development related to playing fields, recreational/cultural facilities, or operational facilities and equipment that will be required in order to satisfy operational, recreational, cultural and safety demands. All of these costs are funded through user fees and property taxes.

Next Steps for the 20 Year Servicing Plan & Finance Strategy

Integration of a servicing plan and financing strategy with the 2040 OCP growth plan is necessary to ensure that the plan is affordable in the form that Council and the community is being asked to support and adopt as a blueprint for future development. As the servicing areas are still in the process of developing and estimating costs of capital projects, multiple Council touch points will be necessary to achieve a holistic review of the proposed changes to the 20 Year Servicing Plan & Finance Strategy. These touch points will include updates on proposed projects, the investigation of new DCC's in the area of parks development and major storm drainage systems, and changes in DCC rates. Each phase of this

analysis will yield a more fine-grained analysis that will shift from high level impacts, to a capital list with corresponding infrastructure costs.

Existing Policy:

Imagine Kelowna called to create great public spaces, grow vibrant urban centres, preserve Okanagan Lake as a shared resource, and build healthy neighbourhoods for all.

Council Priorities 2019-2022 identified measures to transform this vision into action. Specifically, relevant to infrastructure in this report:

- Social and inclusive, through increasing inclusivity and diversity.
- Transportation and mobility, by increasing trips by transit, carpooling, cycling and walking.
- Vibrant neighbourhoods, by increasing the number of urban centre development plans.
- Vibrant neighbourhoods, by creating animated parks and public spaces.
- Vibrant neighbourhoods, through developing accessible and multi-purpose amenities.
- Vibrant neighbourhoods, by proactively planning key sites.
- Economic resiliency, through the reduction of the infrastructure deficit.
- Economic resiliency, attracting top talent to live in Kelowna.
- Environmental protection, through resiliency and adaptability to climate change.
- Environmental protection, by decreasing greenhouse gas emissions.

Internal Circulation:

OCP Project Planner
Policy and Planning Department Manager
Parks & Buildings Planning Manager
Integrated Transportation Manager
Utility Planning Manager
Communications Advisor
Deputy City Manager
Divisional Director, Active Living and Culture
Divisional Director, Corporate Strategic Services
Divisional Director, Infrastructure Division
Divisional Director, Partnerships & Investments

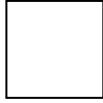
Considerations not applicable to this report:

Legal/Statutory Authority:
Legal/Statutory Procedural Requirements:
Personnel Implications:
External Agency/Public Comments:
Communications Comments:
Alternate Recommendation:

Submitted by:

J. Shaw, Infrastructure Engineering Manager

Approved for inclusion:



A. Newcombe, Infrastructure Divisional Director

Attachment 1 - 2040 Infrastructure Impact Analysis Presentation

cc: Airport Director

Deputy City Manager

Divisional Director, Active Living & Culture

Divisional Director, Corporate Strategic Services

Divisional Director, Financial Services

Divisional Director, Infrastructure

Divisional Director, Partnership & Investments



2040 Infrastructure Impact Analysis

August 12th , 2019



Presentation Outline

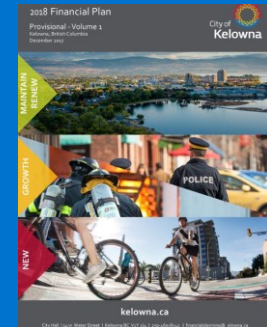
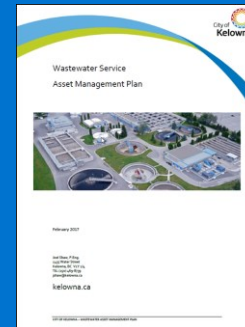
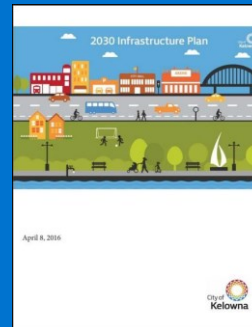
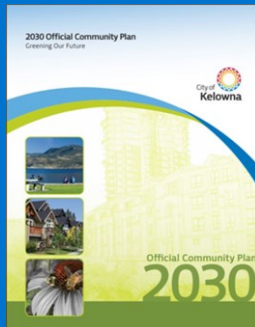
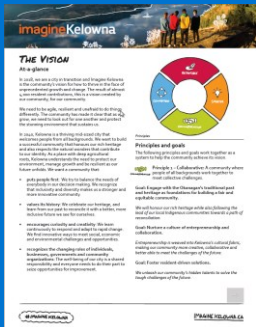
- ▶ Overview
- ▶ Service Areas
 - ▶ Transportation
 - ▶ Parks
 - ▶ Water
 - ▶ Wastewater
 - ▶ Other Services
 - ▶ Drainage
- ▶ Financial Summary
- ▶ Next Steps



Plan Alignment

Understanding
Community Service
Expectations

Delivering Infrastructure
that supports Service
Delivery



Imagine
Kelowna

OCP

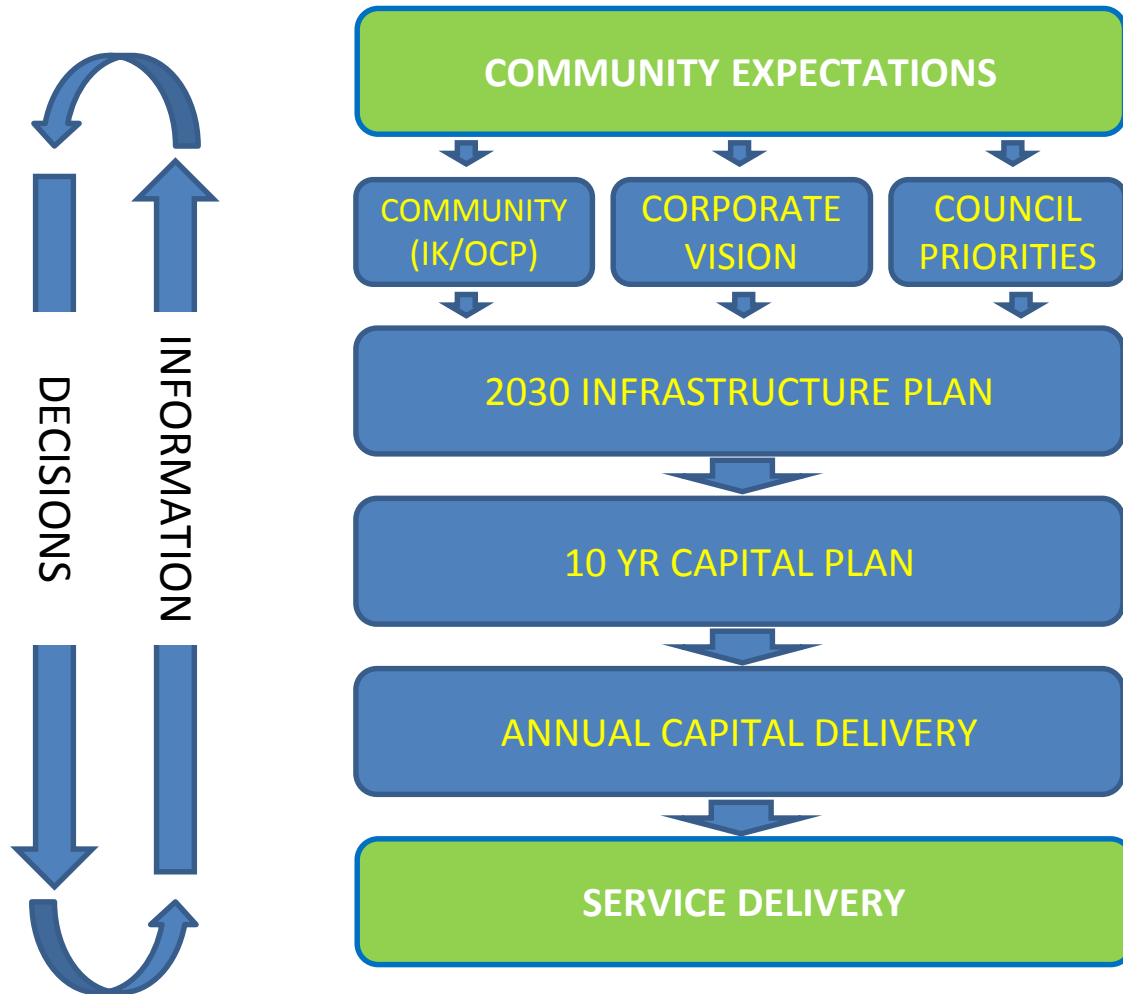
Council
Priorities

2030 Inf.
Plan

10-Yr
Capital Plan

Annual Capital
Program

INFRASTRUCTURE PLANNING MODEL



GROWTH

20-Yr Servicing Plan
(DCC)

NEW/GROWTH

Infrastructure
Master Plans

RENEW

Asset Management
Plans

2030 Infrastructure Plan



Operation &
Maintenance
Costs

Community
Engagement

Financial Strategy

Service Levels₁₃₁

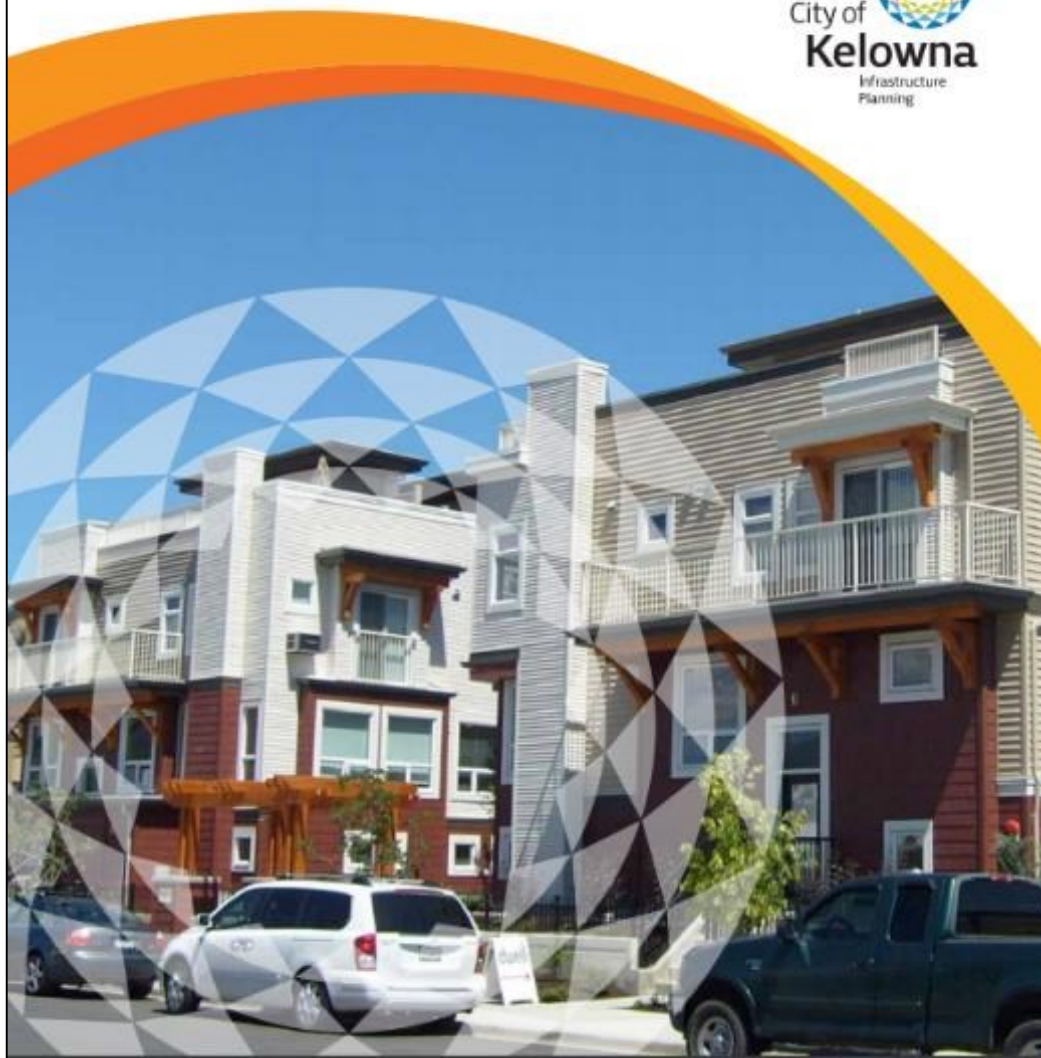
City of Kelowna



20-Year Servicing Plan & Financing Strategy

Development Cost Charge Rates

Effective June 13, 2011



Overview

20 Year Servicing Plan & Financing Strategy

- ▶ Eligible infrastructure is funded through a combination of Development Cost Charges (DCCs), taxation/user fees and government grants
- ▶ B.C. municipalities use DCCs because they are the best way to fairly distribute growth-related costs to development

Legislation- DCC Eligible Infrastructure

- ▶ Transportation (used by City)
- ▶ Parkland Acquisition (used by City) and Development (pending)
- ▶ Water (used by City)
- ▶ Wastewater (used by City)
- ▶ Drainage (not used by City)

Legislation – Ineligible Infrastructure

- ▶ Fire Halls, Police Buildings
- ▶ Recreation and Cultural Centers
- ▶ City Halls
- ▶ Works Yards
- ▶ Arenas
- ▶ Soft services – i.e. child care facilities, replacement housing

Addressing Growth

- ▶ 2040 OCP Growth Projections (~50,000 people)
 - ▶ Infrastructure must be in place
- ▶ 2040 OCP Growth Scenarios
 - ▶ Location of growth can impact sequencing and costs.



Transportation

- ▶ Guiding Master Plan – Transportation Master Plan (TMP)
 - ▶ In progress
- ▶ Project identification – In progress
 - ▶ Will include arterial roads, active transportation corridors and major bridge improvements
 - ▶ Does not include off-street parking and local roads
- ▶ Challenges – Shift in OCP growth pattern
 - ▶ Equitable split of developer and & taxation funding
 - ▶ Aligning funding levels with timing of requisite infrastructure investment over a 20-Year period

Parks

- ▶ Guiding Master Plan – Parks Master Plan (proposed 2020)
- ▶ Project identification – In progress
 - ▶ Includes park land acquisition and park development
 - ▶ Excludes natural park land and certain park development costs associated with specific user groups activities
 - ▶ Artificial turf field, sport courts, skate parks, activity centres, field house, etc.
- ▶ Challenges
 - ▶ Achieving required Park type hectares per 1,000 people with a high land cost per hectare in urban and waterfront areas
 - ▶ Aligning funding levels with timing of requisite infrastructure investment over a 20-Year period

Utilities – Water and Sanitary

- ▶ Guiding Master Plan – Water & Waste Water Asset Management Plans
- ▶ Project identification - Major projects are known
- ▶ Challenges
 - ▶ Major projects may require upfront borrowing which will be repaid through DCCs collected over time
 - ▶ Project timing with other major construction works
 - ▶ Connecting citizens to City infrastructure with assistance of senior government grant programs driven by regulatory requirements
 - ▶ Siting of future treatment facilities noting limited options.



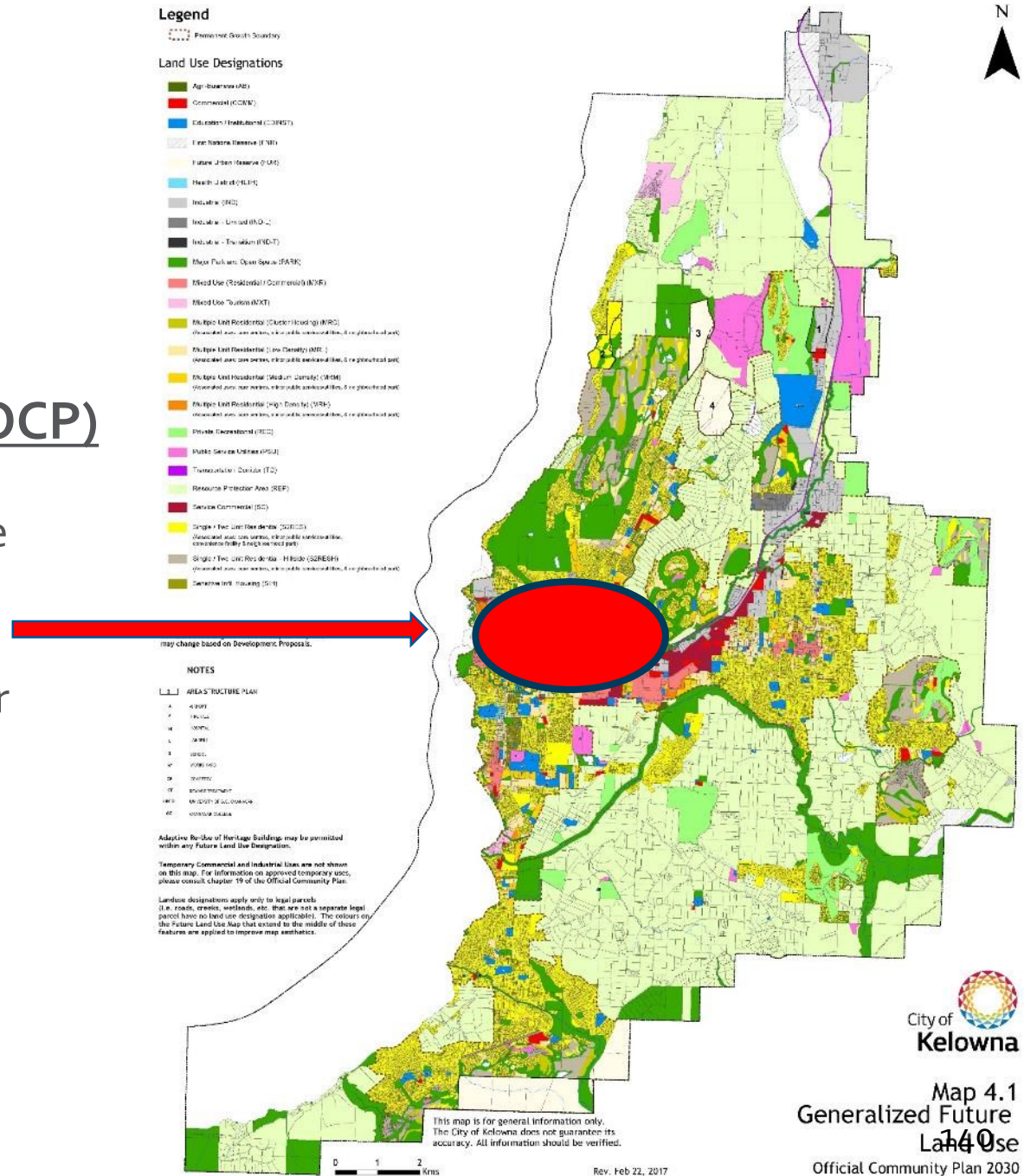
Water

Major Projects (2040 OCP)

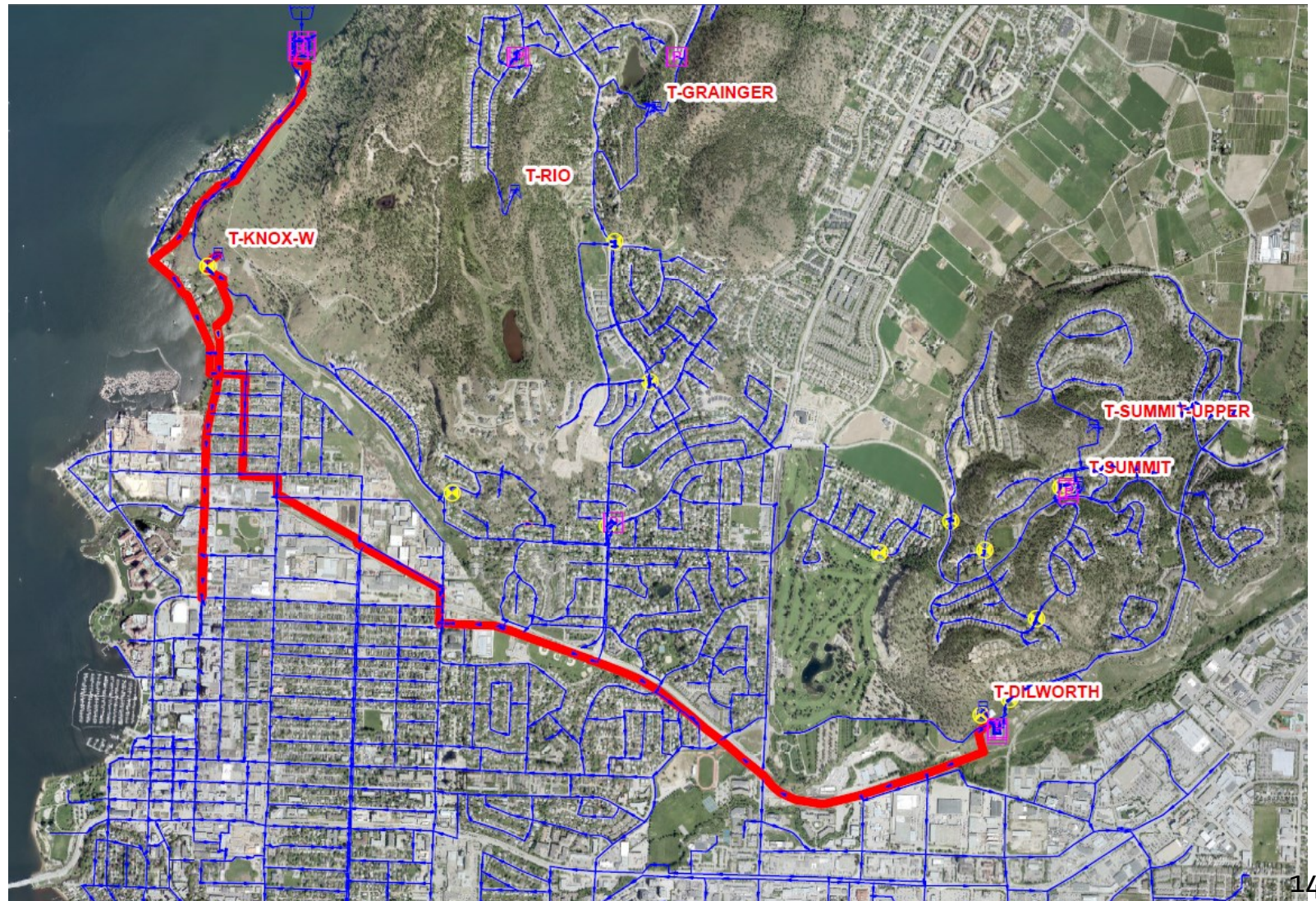
Dilworth Reservoir Upgrade

Transmission Mains

- Poplar to Knox Reservoir
- Poplar to downtown
- Poplar to Dilworth



Water

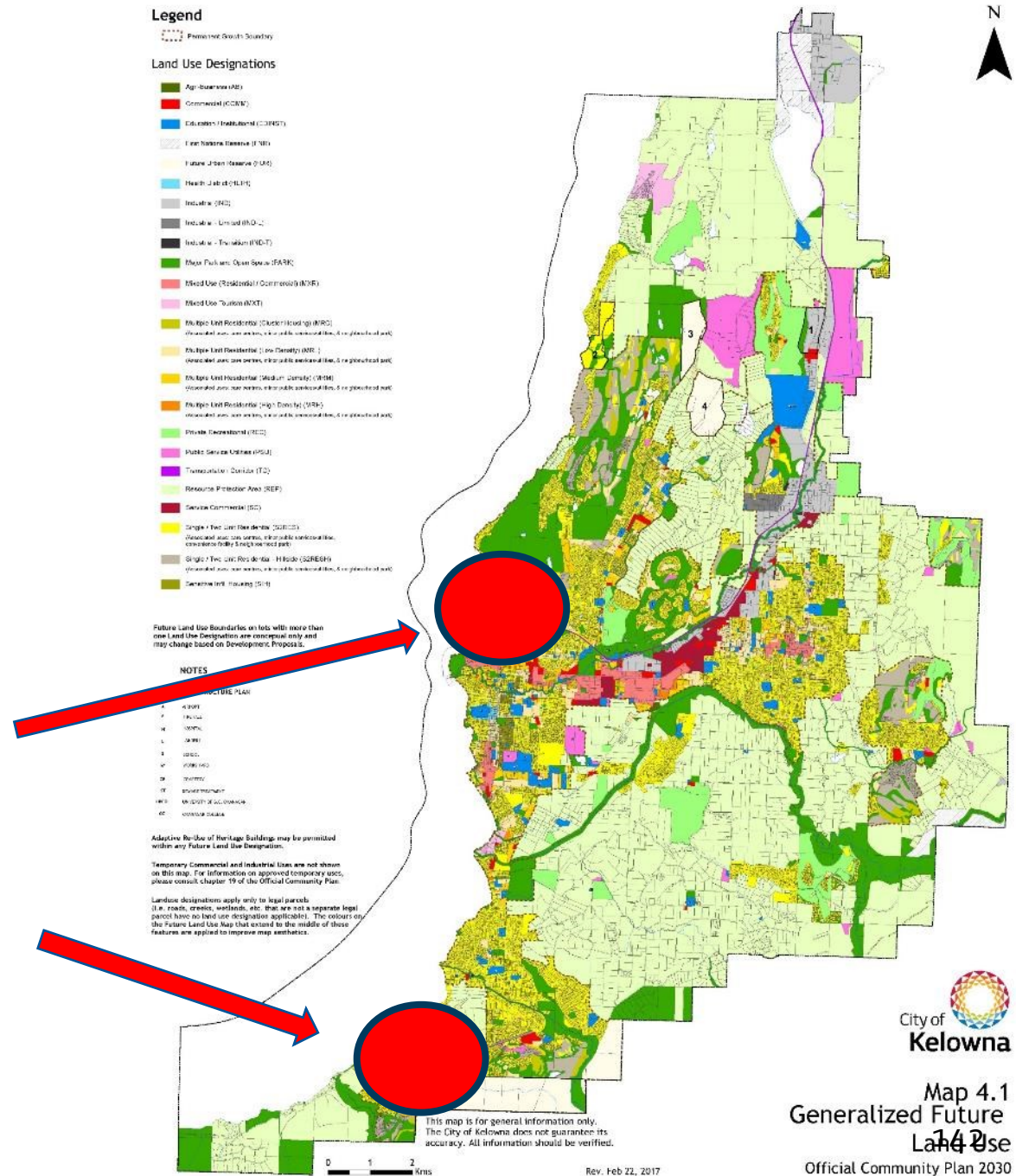


Water

Siting (Beyond 2040?)
Water Filtration Plants?

Poplar
@ Industrial Area

Cedar
@ KVR Reservoir



Sanitary

Existing Wastewater Treatment Plant Raymer Road

Legend

Future Land Use Boundary

Land Use Designations

- Agri-Business (AB)
- Commercial (COM)
- Education/Institutional (EDU)
- Future Urban Reserve (FUR)
- Future Urban Reserve (FUR)
- Health/Leisure (HL)
- Industrial (IND)
- Industrial - Limited (IND-L)
- Industrial - Town Centre (IND-T)
- Major Public Open Space (PAK)
- Mixed Use (Residential/Commercial) (MUR)
- Mixed Use (Urban) (MUR)
- Multiple Unit Residential (Dense) (MUR-D)
- Multiple Unit Residential (Low Density) (MUR-L)
- Multiple Unit Residential (Medium Density) (MUR-M)
- Multiple Unit Residential (High Density) (MUR-H)
- Private (Residential) (PRV)
- Public Service Urban (PSU)
- Transportation/Service (TSR)
- Resource Protection Area (RPA)
- Service Commercial (SC)
- Single-Family Low Density (SFLD)
- Single-Family Medium Density (SFLM)
- Single-Family High Density (SFLH)
- Service Industrial (SIL)

Future Land Use Boundaries on lots with more than one Land Use Designation are conceptual only and may change based on Development Proposals.

NOTES

AREA'S TO BE PLANNED

- 1. AB
- 2. COM
- 3. EDU
- 4. FUR
- 5. HL
- 6. IND
- 7. IND-L
- 8. IND-T
- 9. PAK
- 10. MUR
- 11. MUR-D
- 12. MUR-L
- 13. MUR-M
- 14. MUR-H
- 15. PRV
- 16. PSU
- 17. TSR
- 18. RPA
- 19. SC
- 20. SFLD
- 21. SFLM
- 22. SFLH
- 23. SIL

Adaptive Re-use of Heritage Buildings may be permitted within any Future Land Use Designation.

Temporary Commercial and Industrial uses are not shown on this map. For information on approved temporary uses, please consult chapter 19 of the Official Community Plan.

Land use designations apply only to legal parcels (i.e. roads, creeks, wetlands, etc. that are not a separate legal parcel have no land use designation applicable). The colours on the Future Land Use Map that extend to the middle of these features are applied to improve map aesthetics.

This map is for general information only. The City of Kelowna does not guarantee its accuracy. All information should be verified.

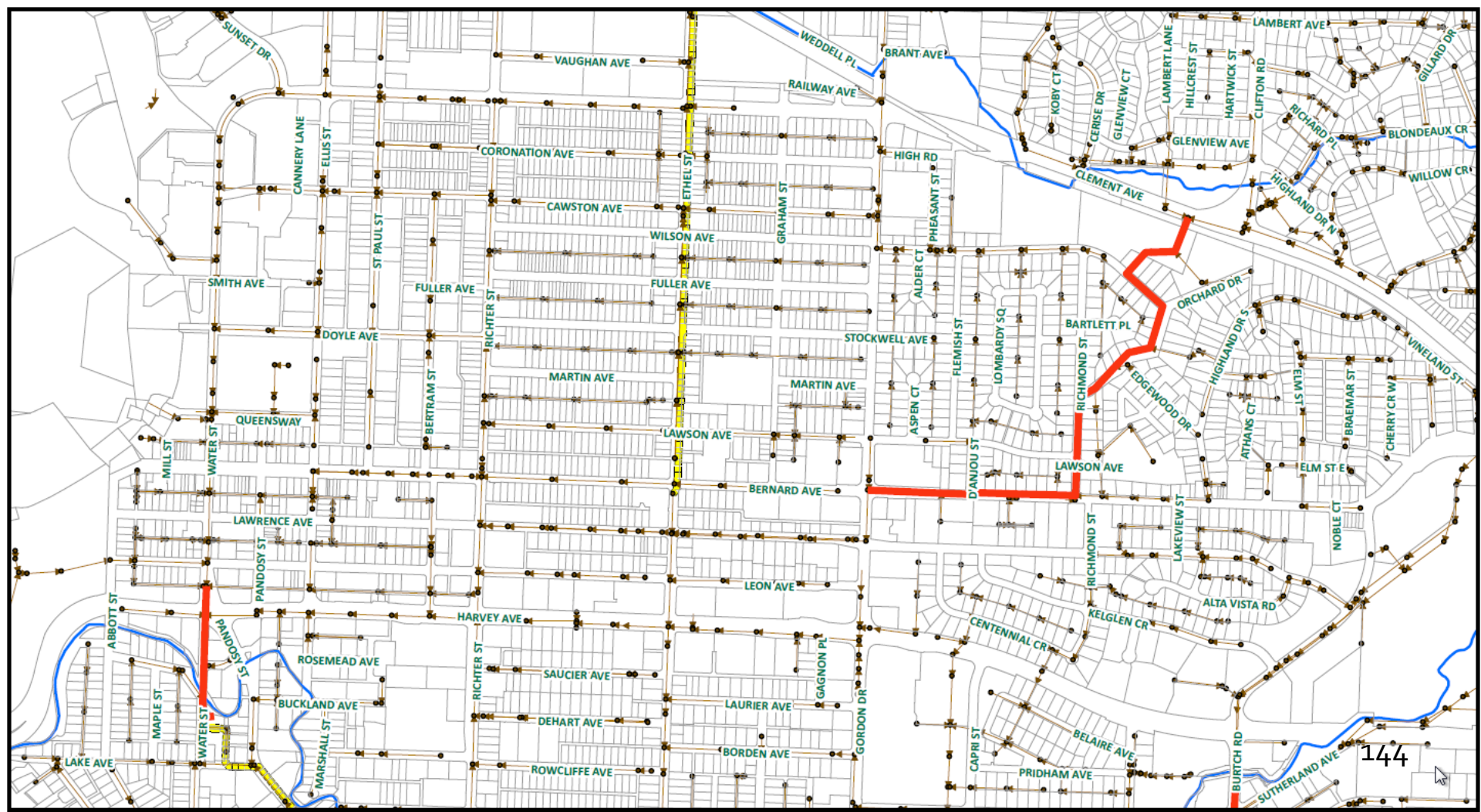
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Rev. Feb 22, 2017

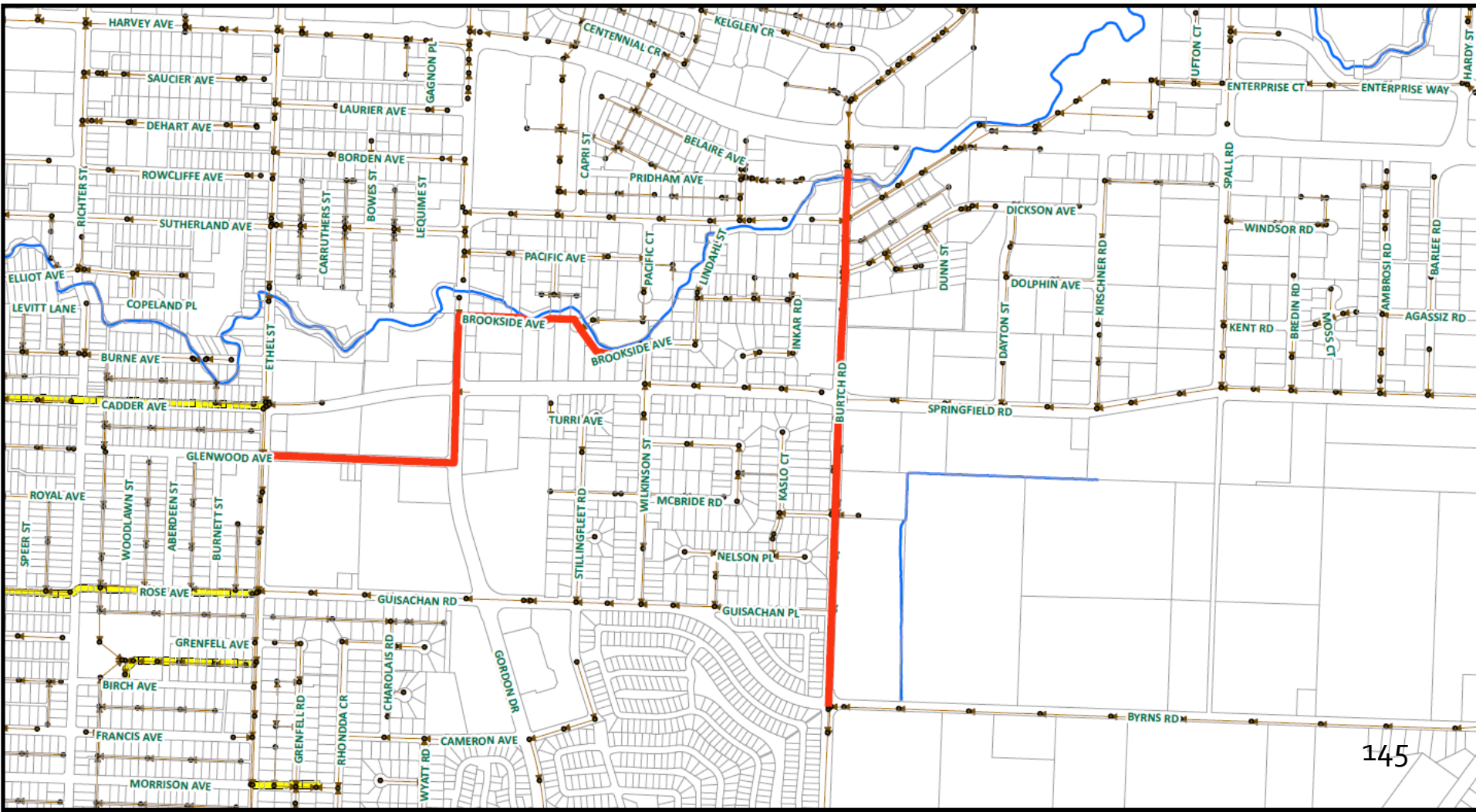


Map 4.1
Generalized Future
Land Use
Official Community Plan 2030

Sanitary – Major Projects



Sanitary – Major Projects

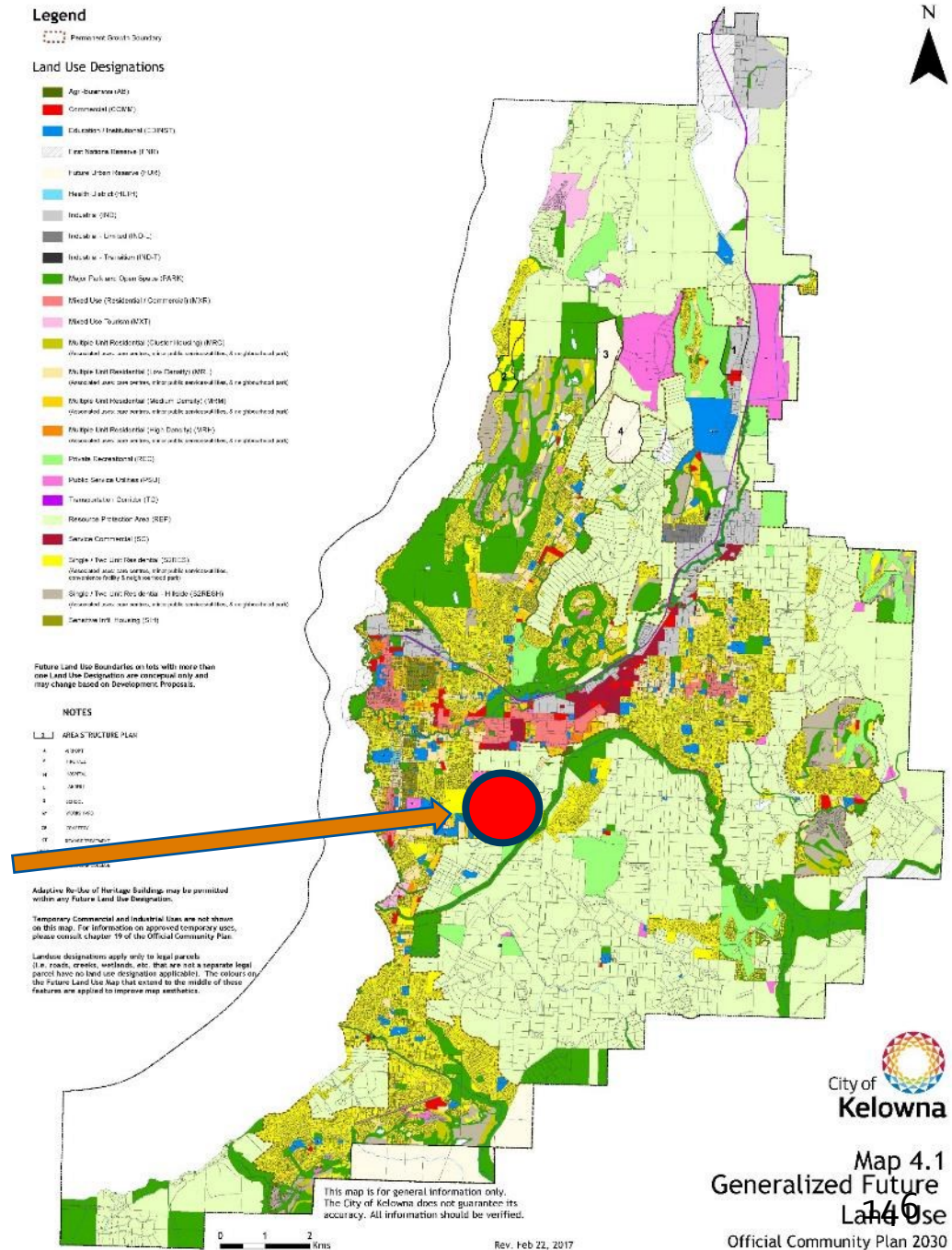


Sanitary

Future Siting

Wastewater Treatment Plant (Beyond 2050)

Munson Road Area



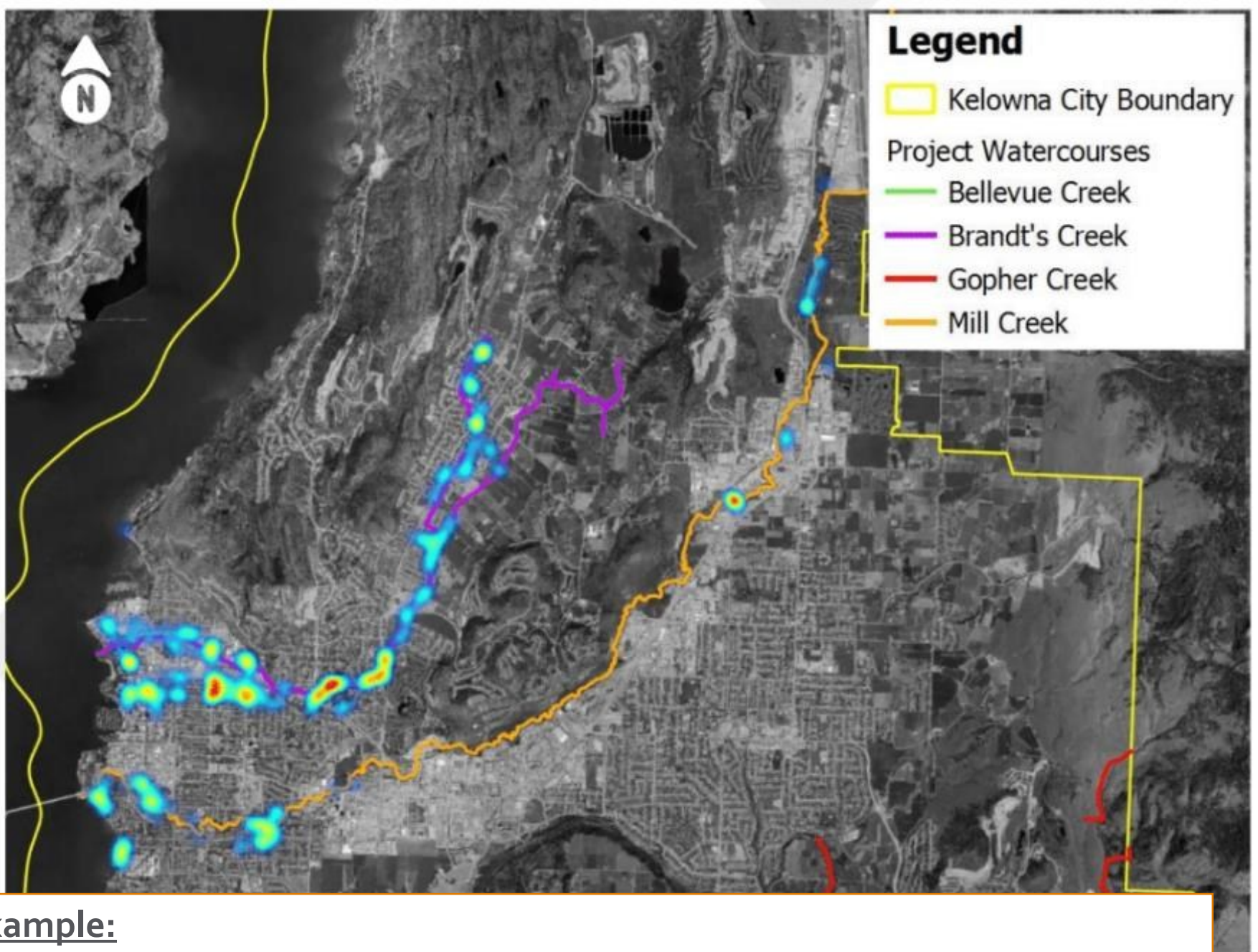
Drainage / Flood Mitigation

▶ Stormwater in Kelowna

- ▶ Recent food fire and storm events are community concerns
- ▶ Rain / Snowmelt must route safely to the lake.
 - ▶ Intensification must be well managed.
 - ▶ Minor events (< 5 year storm) are manageable.
 - ▶ Major events – Work needed.

▶ Growth Impacts

- ▶ Inner Core Growth areas – Flood mitigation measures needed
- ▶ Prioritize mitigation on Mill Creek and Brandts Creek
 - ▶ Funding sources needed to match Federal funding.
- ▶ Future policy adjustments - riparian and flood management.

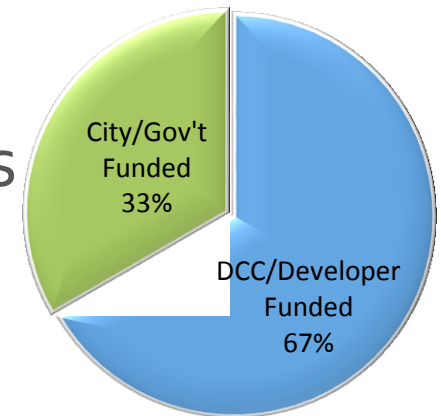


Example:

Relative concentration of Sanitary Sewer assets impacted by flooding

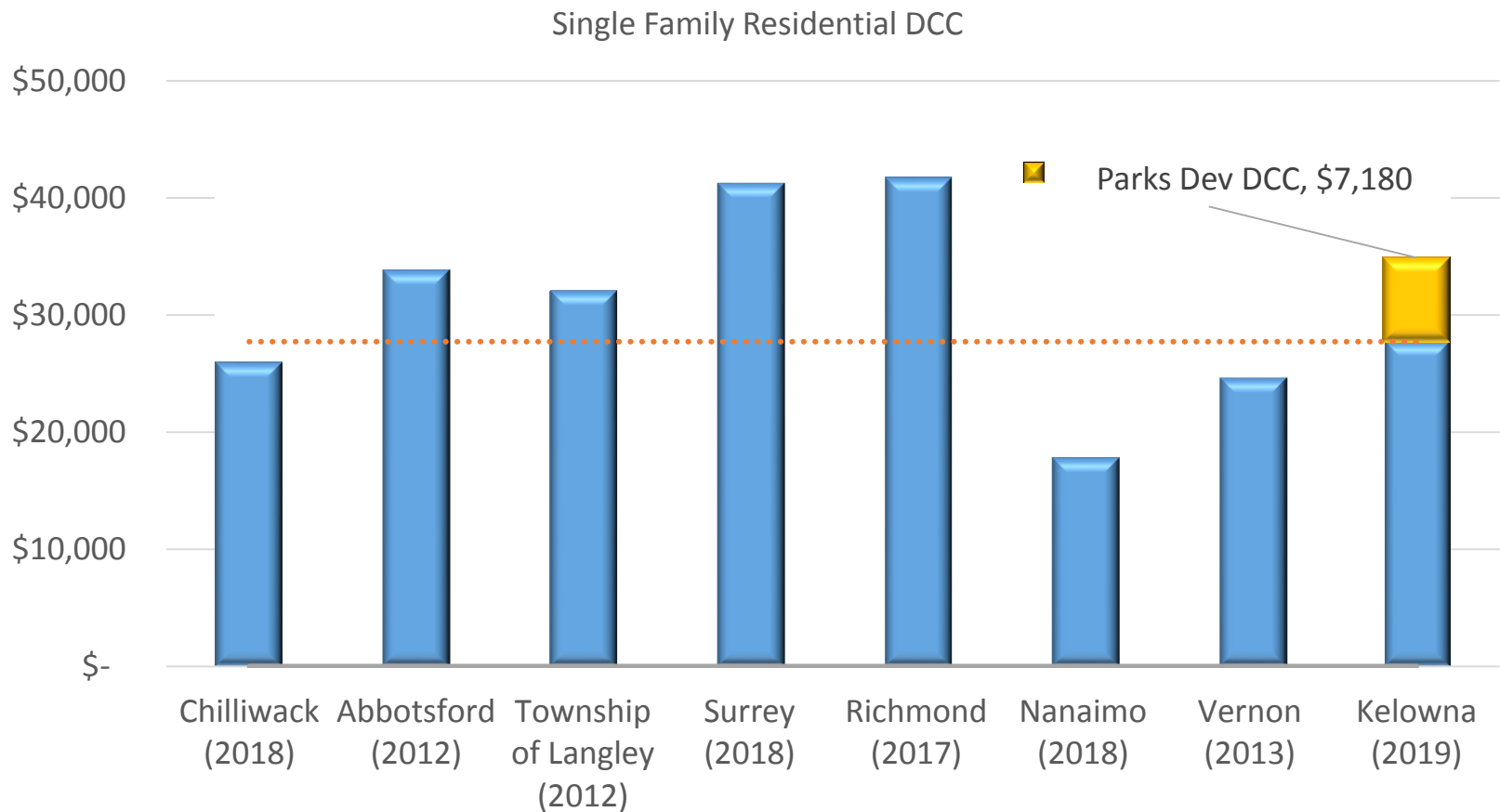
Source: Flood Risk Assessment - Draft (Associated Engineering, 2019)

Current DCC Program – Funding Sources



2030 Major Services - Funding Sources (\$ Millions) - 2018 Update							
Major Service	Gov't Funded	Developer Funded		City Funded		2030 OCP	
	Grant	Developer Construct	DCC's	Taxation	Utility User Rates	2018 Totals	% Total
Arterial Roads	39.7	75.1	234.7	168.4		517.9	59%
Water Distribution		6.4	26.3		31.3	64.0	7%
Wastewater Trunks		7.4	28.2		9.4	45.1	5%
Wastewater Treatment			66.1		21.9	88.0	10%
Parkland Acquisition	5.4		142.2	17.7		165.3	19%
2018 Totals	45.1	88.8	497.6	186.1	62.5	880.3	100%
% Total	5%	10%	57%	21%	7%	100%	

Comparable DCC Rates



Next Steps

- Project identification
- High level costing
- Impact on home ownership cost
- Review of DCC assumptions
- Stakeholder engagement



Questions?

For more information, visit kelowna.ca.

Report to Council



Date: August 12, 2019
File: 1200-31
To: Council
From: City Manager
Subject: OCP 2040: Draft Land Use Map and Public Engagement Process

Recommendation:

THAT Council receives the report from Policy and Planning Department, dated August 12, 2019, for information;

AND THAT Council direct staff to initiate the public engagement process, as outlined in this report, and include the initial draft Land Use Map in that process to gain community feedback.

Purpose:

To provide Council with a draft Future Land Use map that translates the ideas and objectives of the Official Community Plan Update process to date into a draft land use vision for the purposes of public engagement in Fall 2019.

Background:

The 2040 Official Community Plan (OCP) Update process is currently in Phase 3, of which the major deliverable is a draft Future Land Use Map for Council's endorsement. This Phase requires the translation of the ideas and 'Big Moves' into a refreshed Future Land Use Map and a public engagement process, all crafted concurrently with updates to the Transportation Master Plan (TMP) and 20 Year Servicing Plan projects in their respective phases.

At its meeting of May 27, 2019, Council received a report that outlined five major shifts, called "Big Moves" that characterized new directions of the 2040 OCP. The report also indicated that a draft Future Land Use map was being developed that reflected these Big Moves, the Growth Scenario endorsed by Council on March 4, 2019 and Imagine Kelowna, and that this land use plan would be provided for Council's review to be included as part of the public engagement process for Phase 3 of the OCP review, targeted for Fall 2019.

This report serves to provide Council with the introduction of the new future land use designations and the corresponding draft Future Land Use Plan that is intended to be rolled out for the next phase of public engagement.

A New Future Land Use Plan

Official Community Plans are intended to act as visionary documents, outlining at a high level, how and where a city intends to grow. The land use maps that form part of an OCP illustrate this vision, leaving the zoning bylaw to handle uses on a much more fine-grained, site-by-site basis. These visionary maps form one part of the overall growth strategy that will guide decisions on rezoning applications, investments in amenities and transportation, and prioritize the required utility and servicing improvements.

As outlined in the previous Council report, the draft Future Land Use Plan would take a refreshed approach to signaling land use in the city. Most notably, this approach would link many land use designations to five geographic Growth Strategy Districts: Urban Centres, Core Area, Suburban, Rural, and a newly proposed Gateway District (see Figure 1).

Each of these Growth Strategy Districts covers a geographic area within the city and signals what types of development would be supported and where prioritized investments in transportation, servicing and amenity infrastructure would be focused.

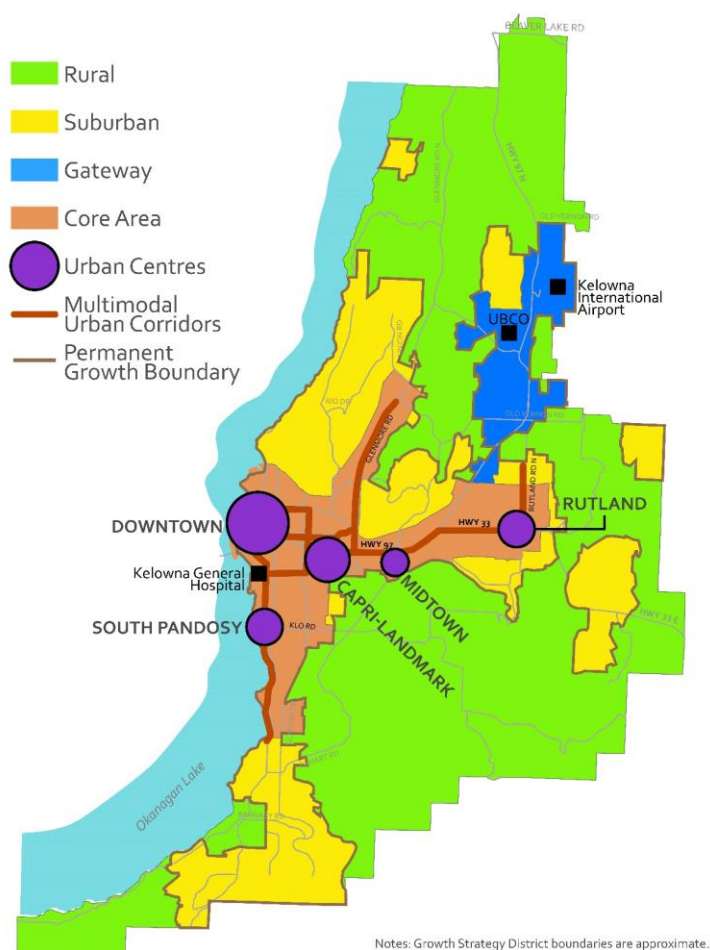
Urban Centres

The five Urban Centres (City Centre, South Pandosy, Capri Landmark, Midtown and Rutland) would continue to act as the hubs for employment, shopping, entertainment and high density residential development. This focused growth would provide a greater variety of housing and employment closer together to see more effective use of infrastructure investments, promote transportation choice and reduce pressure for development outside of the Permanent Growth Boundary. This approach would be coupled with investments in transportation that prioritize walking, cycling, transit, and other modes while de-prioritizing the car.

Core Area

The Core Area includes neighbourhoods that are adjacent to the Urban Centres within the central part of the city. The Core Area is currently characterized by lower density residential neighbourhoods, with somewhat more variety of multi-unit development. The Core Area also includes a significant amount of the city's big box retail, clustered along the Highway 97 Corridor, industrial lands, and several major

Figure 1: OCP 2040 Growth Strategy



employment destinations, including Kelowna General Hospital. Core Area neighbourhoods would provide a wide variety of housing forms, focusing primarily on ground-oriented housing, such as smaller homes, house-plexes and row housing. Low rise apartment development would be targeted along a series of corridors, following major transit routes between Urban Centres. Like the Urban Centres, investments in transit, active transportation and other modes that do not involve the vehicle would be prioritized with an emphasis on key network improvements to connect residents with the Urban Centre hubs.

Suburban

Suburban lands are located outside of the Core Area but within the Permanent Growth Boundary. They are characterized primarily by lower density residential neighbourhoods with some commercial uses. Residential growth would be primarily characterized by single and two-unit development, focused mainly in the new communities currently being built out (with some multi-unit development in strategic locations). Given the lower densities and road network patterns of these neighbourhoods, investments in transit and active transportation would be lower than the Urban Centres and Core Area.

Rural

These lands are located outside of the Permanent Growth Boundary and are mostly composed of agricultural lands or large tracts of resource lands. Some pockets of residential neighbourhoods are also included in these areas given their existing status, as are some industrial lands. Rural lands would not be supported for further urban development beyond areas already zoned and designated in the 2030 OCP. Transit and active transportation would not be prioritized in these areas.

Gateway

Following feedback from Council at its May 27, 2019 meeting, a new Growth Strategy District has been created: Gateway. The Gateway District includes the University of British Columbia – Okanagan campus (UBCO), Kelowna International Airport (YLW) and surrounding residential, commercial and industrial lands (see Map 1 and Attachment 1). The Gateway District was created to allow for policy development that specifically addresses the unique character of the area and the roles that UBCO and YLW perform in the city. These lands were formerly part of the Suburban District (which is now characterized almost exclusively by residential neighbourhoods), allowing policy development to focus more on guiding growth and development in this distinct area.

New Land Use Designations

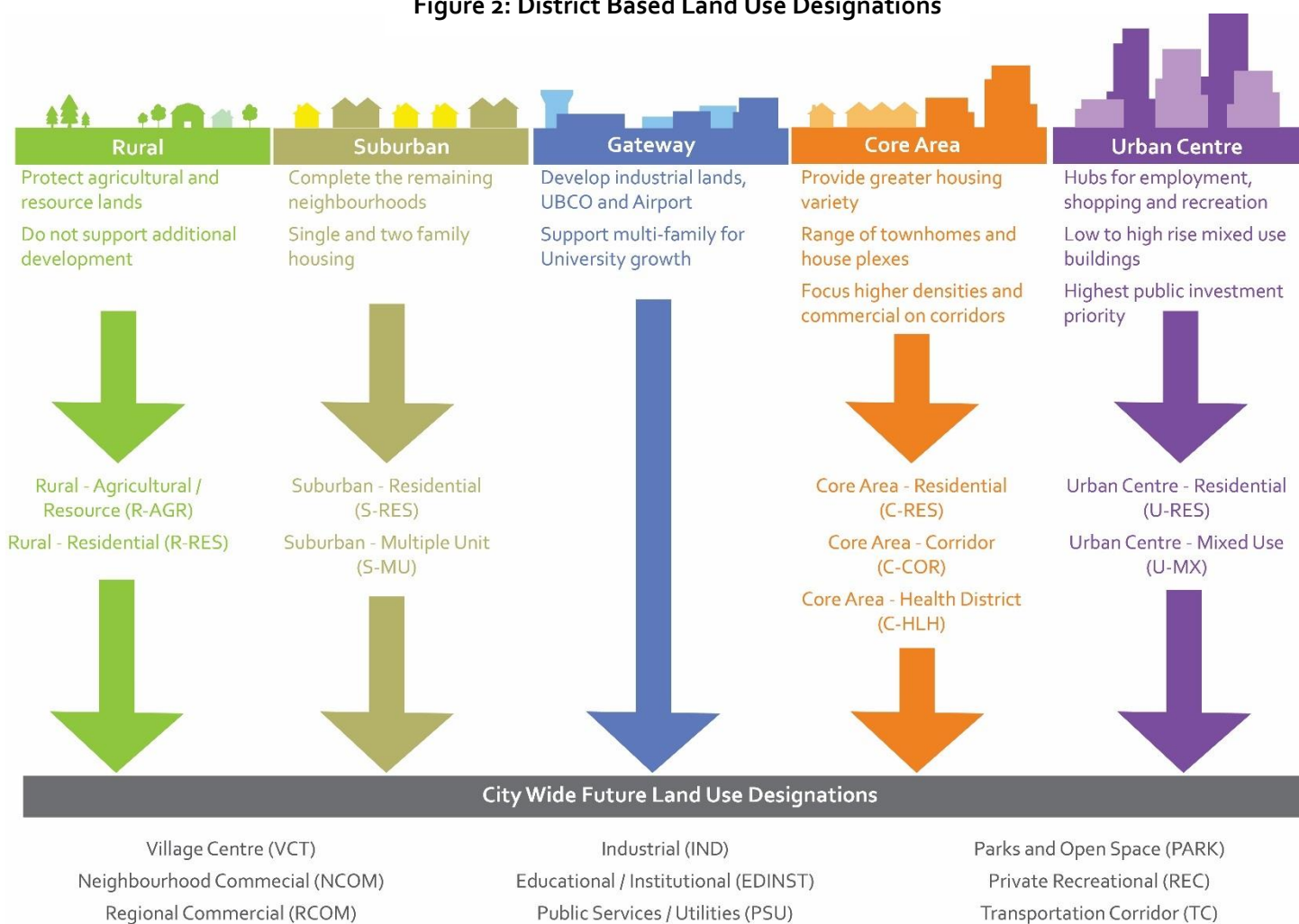
A new approach to the future land use designations is proposed as part of this draft land use map that reflects the overall direction for each of the five growth strategy districts (see Figure 2). For example, the Core Area would have land use designations that encourage the missing middle housing types that are exclusively targeted for that district, whereas the Urban Centres would have land use designations that encourage high density mixed-use development. However, select land use designations that have a universal role in each of the five districts, such as institutional or utilities uses, for example, will apply across the city.

Many future land use designations will accommodate a greater number of supported uses to illustrate the plan's vision for how the neighbourhood would develop and change over the next 20 years. These designations will not speak to supporting specific zones, but would include more in-depth policy to articulate this vision clearly. In some cases, additional policy maps such as density thresholds and building heights will be developed to provide further, more detailed guidance. This approach would allow for greater flexibility in land use decisions made by Council, in particular at the rezoning stage.

Updates to the Zoning Bylaw as part of OCP implementation would provide more detailed direction on a more property specific basis.

The draft Future Land Use Map and associated land use designations are included with this report as Attachments 2 and 3.

Figure 2: District Based Land Use Designations



Why a New Approach?

The deliberate coordination of transportation and long-range land use planning is fostering the principle of 're-urbanization' – a term used to describe a coordinated approach to the redevelopment of land within the existing urban fabric. By promoting efficiencies and optimizing land and infrastructure prior to promoting development at the fringes, re-urbanization helps to reduce demands on ecosystems and natural areas, and improve livability by:

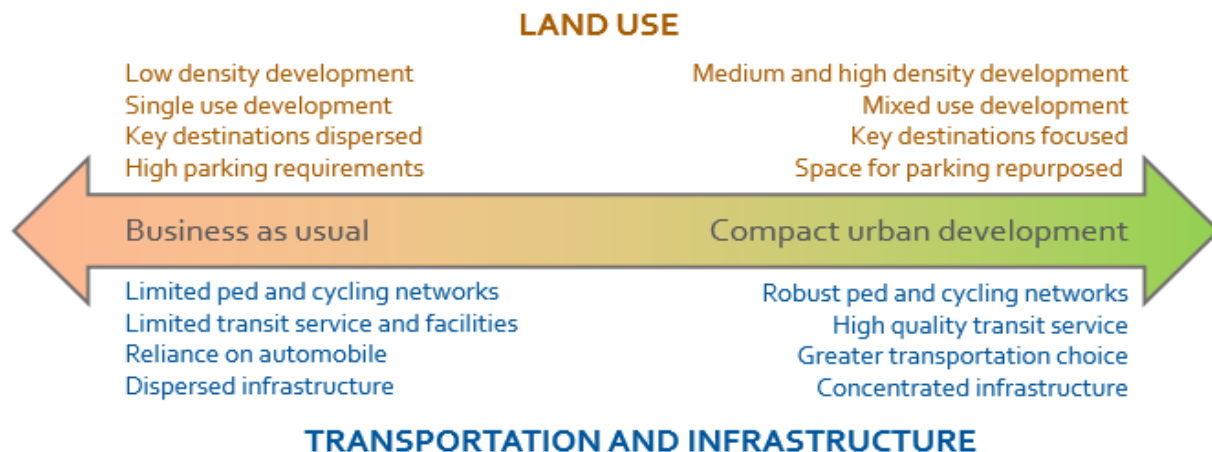
- Preserving high quality agricultural lands;
- Reducing reliance on the private automobile;
- Reducing GHG's in the face of climate change;

- Reducing consumption of non-renewable resources;
- Promoting financial responsibility by investing in infrastructure that will benefit the majority of the community population; and
- Promoting different forms and tenures of housing in targeted locations to meet residents' needs of all ages and life stages.

Land use designations are among the OCP's most important implementation tool for delivering the growth strategy to direct major growth to some parts of the City and deter it from others. Each land use designation establishes the general uses that are anticipated for each designation. In their whole, they tell a story where housing can be built, where retail stores, offices and industry can invest, and where mixed uses will be supported. The land uses in each designation are generalized, which would then default to the Zoning Bylaw to prescribe the development regulations and land use permissions that will reflect the various areas of our City.

As noted above, many of the new designations are crafted to encourage a broad range of commercial, residential, and industrial uses. Careful consideration has been applied to strategically signal where future redevelopment in these areas will best accommodate increases in population and jobs along transit corridors. Other employment hubs for a maturing City are the institutional areas where hospitals, university and college campuses, and key government centers can reinforce key linkages to ensure vibrant, economic centers. Together, this integrated approach aims to underscore mutually supportive transportation and land use policies that combine the mechanisms of mobility and proximity to maximize accessibility (see Figure 3).

Figure 3: The Land Use, Transportation and Infrastructure Relationship



From a financial perspective, the City benefits from proactively planning for growth and being positioned to anticipate and prepare for the additional strain that a growing population adds to existing infrastructure. The current 10-year Capital Plan establishes the City's infrastructure needs and currently forecasts \$1.25 billion in infrastructure investment required to renew existing infrastructure and to put in place the necessary infrastructure to accommodate growth and meet our community's evolving service expectations. The City's general fund is forecasted to provide funding of \$573 million, leaving a \$478 million dollar infrastructure deficit in the general fund. If we take this trajectory out to 2040, the rising cost of this infrastructure deficit will be even greater to correct. It is more cost effective and

efficient to renew infrastructure in our urbanized areas than to build new and expand this network with ongoing maintenance and renewal costs.

Translating the 'Big Moves' into a Future Land Use Plan

At its May 27, 2019 meeting, Council was provided with major changes that were anticipated in the draft land use plan based on the Big Moves, the OCP's Growth Scenario and Imagine Kelowna. These changes are outlined below.

More Housing Variety in the Core Area

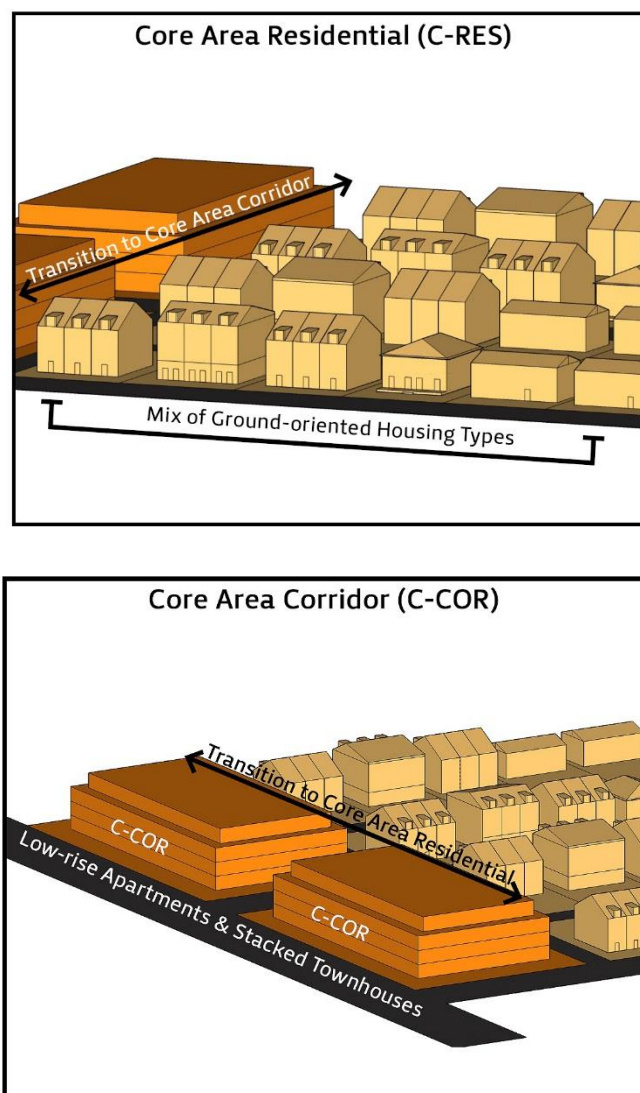
The May 27th report indicated that the Core Area (much of which is currently designated exclusively for single and two family housing), would be signaled to accommodate more housing types, allowing uses like fourplexes and row-housing to be considered in these areas. The draft Land Use Plan lays out this vision for future development in the Core Area with two land use designations: Core Area – Residential (C-RES) and Core Area - Corridor (C-COR). These two designations signal greater housing variety in the Core Area, with C-RES signaling lower density forms of housing, such as single detached homes, semi-detached homes and row-housing, and C-COR signaling medium density forms of housing, such as row-housing and low-rise apartments. Both land use designations signal small scale commercial and institutional uses to create more complete, walkable communities. Both of these land use designations signal different types of development for the Core Area, as outlined in Figure 4 and in Attachments 2 and 3.

Multimodal Urban Corridors

One of the other 'big moves' is for growth to be focused along transit-oriented corridors linking the 5 Urban Centres. This approach would further strengthen the integration of land use and transportation planning by locating medium density residential, and in some cases mixed-use development, along corridors that are targeted for transit prioritization. This approach would put more people, jobs and destinations in close proximity to transit infrastructure outside of the Urban Centres, making it a more viable and attractive option.

This approach would be reflected in the draft Future Land Use Plan by designating properties along these corridors as Core Area – Corridor (C-COR).

Figure 4: Core Area Land Use Designations



Suburban Neighbourhoods

The endorsed Growth Strategy signaled that approximately 33 per cent of future residential growth would be accommodated outside of the city's Urban Centres and Core Area. Most of this growth would take place in areas currently signaled in the 2030 OCP for future development. The draft Future Land Use plan largely reflects the existing vision of these neighbourhoods. However, the vision is reflected by new future land use designations exclusively for the Suburban Growth Strategy District: Suburban – Residential (S-RES) and Suburban – Multi Unit (S-MU) (see Attachments 2 and 3).

These two designations signal in Suburban neighbourhoods where various types of housing would be supported (often as a direct translation as identified in the corresponding Area Structure Plans- ASPs). S-RES would signal where single and two family development would be supported. S-MU would be used primarily where multi-unit development has already been identified in the 2030 OCP.

The Permanent Growth Boundary (PGB)

In keeping with the direction that no new suburban neighbourhoods would be signaled in the 2040 OCP, the Permanent Growth Boundary remains largely unchanged, with three notable exceptions:

- Modifications have been made to accommodate future airport expansion, as per the YLW *Airport Master Plan 2045*;
- The boundary area has been retracted from the Crawford rural residential neighborhood, as no further residential units are signaled for this area and infrastructure investments will not be prioritized to urbanize this neighborhood;
- The PGB now envelops the McKinley Beach neighborhood, to acknowledge and signal the existing and upgraded infrastructure services and number of residential units that will be built out in this node of the City.

Lands beyond the PGB are included in the Rural Growth Strategy District, with two land use designations that specifically guide land use direction: Rural – Agricultural and Resource (R-AGR) and Rural – Residential (R-RES). While R-AGR signals lands to be protected for agricultural uses and/or lands that are signaled to maintain their rural character, R-RES applies to properties where residential development has already taken place or would continue due to existing zoning.

Industrial Lands

A land capacity report has been completed by an independent third party consultant to inventory the anticipated demand for new commercial and industrial space in the city to 2040. This study has concluded that the current 2030 OCP has an adequate supply of industrial lands out to 2040. As such, no new lands would be signaled in the draft Land Use Plan. Industrial uses would now be signaled by a single Industrial Land Use Designation (IND), eliminating the current Industrial – Transitional (IND-T) and the Industrial – Limited (IND-L) designations.

Parks and Open Space

The draft Land Use Map has not identified new areas for parks at this stage. Through this process, Staff will evaluate the options to signal parks lands in a way that distinguishes natural areas from 'active' or programmed parks. This work will be ongoing throughout the OCP Update process and would be informed from the Fall 2019 public engagement process.

Other Changes

Retraction of Urban Centre Boundaries

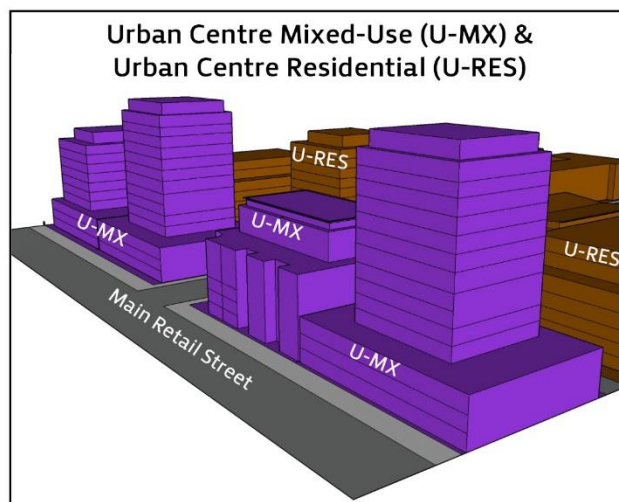
The five Urban Centres are envisioned as the primary hubs for employment, shopping, and other activities. They are also signaled to absorb nearly 50 per cent of future residential growth, creating more vibrant, diverse neighbourhoods. However, the boundaries for three Urban Centres: South Pandosy, Rutland and Midtown have been retracted.

The South Pandosy and Rutland Urban Centre boundaries were contracted to more accurately reflect where higher density mixed-use and residential development is anticipated, and to keep the development signaled in the Urban Centres relatively compact and close to amenities and services.

The Midtown Urban Centre boundary has been contracted significantly. This approach was taken to reflect that the Midtown is still considered at the very early stages of development, and as such, is characterized by a lack of parks and public spaces, an automobile dominated landscape, and poor pedestrian connectivity. Creating a vibrant Urban Centre in this environment will require significant investments in parkland purchases and development, new street connections and upgrades, and a major shift in private sector investment in the commercial landscape. Shrinking the boundary of Midtown, while maintaining the targets for residential growth outlined in the Growth Scenario would focus future residential growth into a smaller area, allowing investments in parks, public spaces, transportation infrastructure, and other amenities to benefit a much greater proportion of new residents.

Development in the Urban Centres would be guided primarily by two future land use designations: Urban Centre – Mixed Use (U-MX) and Urban Centre – Residential (U-RES). U-MX signals the development of hubs for activity through the mixing of commercial, employment, and residential uses in the five Urban Centres focused along identified retail streets, while U-RES signals primarily multifamily residential adjacent to these mixed-use hubs (see Attachments 2 and 3 and Figure 5). Both designations would signal the highest densities in the city; however, these densities would vary between different Urban Centres to reflect their specific context. For example, where Downtown would likely signal the highest densities supported in the city with more towers, Rutland would likely see somewhat lower densities achieved through more low or mid-rise development. Ongoing policy and urban design guideline development throughout the remainder of Phase 3 is expected to provide greater clarity and direction for differences in densities and heights across the Urban Centres.

Figure 5: Urban Centre Land Use Designations



New Village Centres Approach

The draft land use map takes a new approach to signaling Village Centres, which involves designating larger areas of those areas with a single Village Centre land use designation. The approach allows for greater flexibility for land use decisions within Village Centres, as guided by policy within the OCP.

A new Village Centre is signaled along Lakeshore Road between Mission Creek and Lexington/Bluebird Road to reflected continued growth of commercial and office uses mixed with residential development in that area.

New Regional Commercial Designation

While much of the OCP Update process has focused on the development of vibrant Urban Centres in compact, complete communities, Kelowna remains a regional destination for shopping, drawing in people from across the Southern Interior. Many of these destinations take the form of big box retail that require extensive parking and storage facilities, making them unsuitable for Urban Centres and many Core Area neighbourhoods. These uses would be directed to a new Regional Commercial (RCOM) land use designation (see Attachment 2).

The RCOM land use designation would be primarily applied to lands along Highway 97, Enterprise Way and Springfield Road that are currently designated Commercial and Service Commercial in the 2030 OCP. It would be characterized by large format development for the sale of goods and services and would include businesses that require extensive onsite storage as well as service commercial uses. Some residential uses would be permitted in conjunction with commercial uses, but only where they would be in close proximity and have easy pedestrian access to amenities like parks, high frequency transit and schools, for example.

Ongoing policy and design guideline development would see future development directed to make the highway corridor a more attractive urban environment, given the focus of rapid bus service on that corridor.

Next Steps

Fall Public Engagement

Engagement sessions in different areas of the City will provide an opportunity for stakeholders and residents to share feedback on key elements of the OCP in specific neighbourhood contexts. Residents will be directly invited to participate, in addition to smaller stakeholder meetings later in the process. The online engagement tool will reflect the same engagement, enabling residents to participate who cannot attend in person. Residents can share input via online surveys and/or discussion forum tools. A project topic will be created before the first in-person public engagement at getinvolved.kelowna.ca. In addition, Staff will be mobilizing engagement with identified stakeholder groups and developing process to engage with local First Nations and other indigenous groups.

Following the completion of this phase of the public engagement process, the following activities would be undertaken:

Further Land Use Plan Refinements & Detailed Servicing Impact and Costing Analysis

Using the feedback provided during the public engagement, Staff will further refine the draft land use map and provide an updated map for Council's consideration later in 2019. Should Council endorse that revised map, it would form the basis for more detailed infrastructure modelling for the Transportation Master Plan and the 20-Year Servicing Plan. This will yield the first preliminary costing analysis to support and implement the growth strategy.

Policy Development & Development Permit Guidelines

Policies for the following topic areas would be reviewed and updated to ensure they align with the growth strategy and 'big moves' direction:

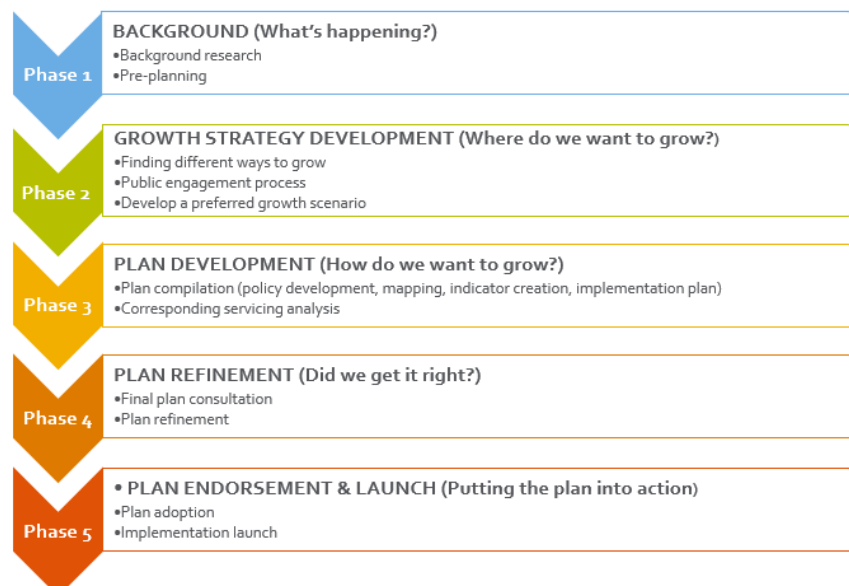
- Land use and housing
- Transportation
- Infrastructure
- Healthy, social and connected
- Arts, culture and heritage
- Climate change mitigation and adaptation
- Environment

As part of the OCP update process, the Development Permit guidelines need to be updated to reflect the corresponding policy. Notably, given the emphasis on neighborhood infill and densification in the Core Area, the importance of high caliber urban design guidelines will be imperative. Therefore, a significant amount of time and resources will be dedicated to updating the Urban Design Guidelines for various building typologies to ensure that the standard of design expectations will be clear and have corresponding explanatory graphics and illustrations. As such, the Guidelines will focus on high quality built environment, healthy community building, and ensuring a lasting contribution to the aesthetics of our community.

Draft Plan Development

The final deliverable for Phase 3 of the OCP update process is a to develop a complete draft Official Community Plan for the final phase of public engagement (see Figure 6). As part of this process, the new OCP will be revealed as an online, interactive document. In tandem with the TMP, this will be the first time the City showcases Master Plans through an online, interactive, web-friendly platform with the aim of heightening usability for all audiences. Should Council endorse the draft plan for further consultation and refinement, the project would move into Phase 4 (~2020) for final Plan refinement and consultation with the legislated agencies.

Figure 6: Official Community Plan Update Schedule



Summary

The progress of the 2040 OCP update is at an exciting time in its formation, and this stage of the Plan development represents the first draft of translating vision and ideas into an initial Future Land Use Map. This milestone represents an initial draft map for public feedback, and it is important to acknowledge that this draft Future Land Use Map will change and be refined as new information, feedback, and community considerations are factored in. This is part of an iterative and responsive process as the project works through the various stages prior to the final Plan endorsement.

This initial draft Future Land Use Map sets the stage for the corresponding content in the OCP, TMP and the Servicing Plan projects. Their collective goal is to ensure that as the City of Kelowna grows and evolves, our community will realize its full potential in areas such as transportation and transit investments, land use development and infrastructure and financing through long-term strategic plans and investments. At this critical point in the City's evolution, we have the opportunity to take the next step towards realizing the bold vision of Imagine Kelowna through Master Plans that demonstrate foresight and intention.

Legal/Statutory Authority:

Local Government Act, Part 14, Section 471

Legal/Statutory Procedural Requirements:

Local Government Act, Sections 472, 473, 474, 475, 476, 477, 478

Existing Policy:

2030 Official Community Plan
20 Year Servicing Plan
Council Policy No. 372: Engage Policy

Submitted by:
Robert Miles, OCP Project Planner

Approved for inclusion:  Danielle Noble-Brandt, Dept. Manager, Policy & Planning

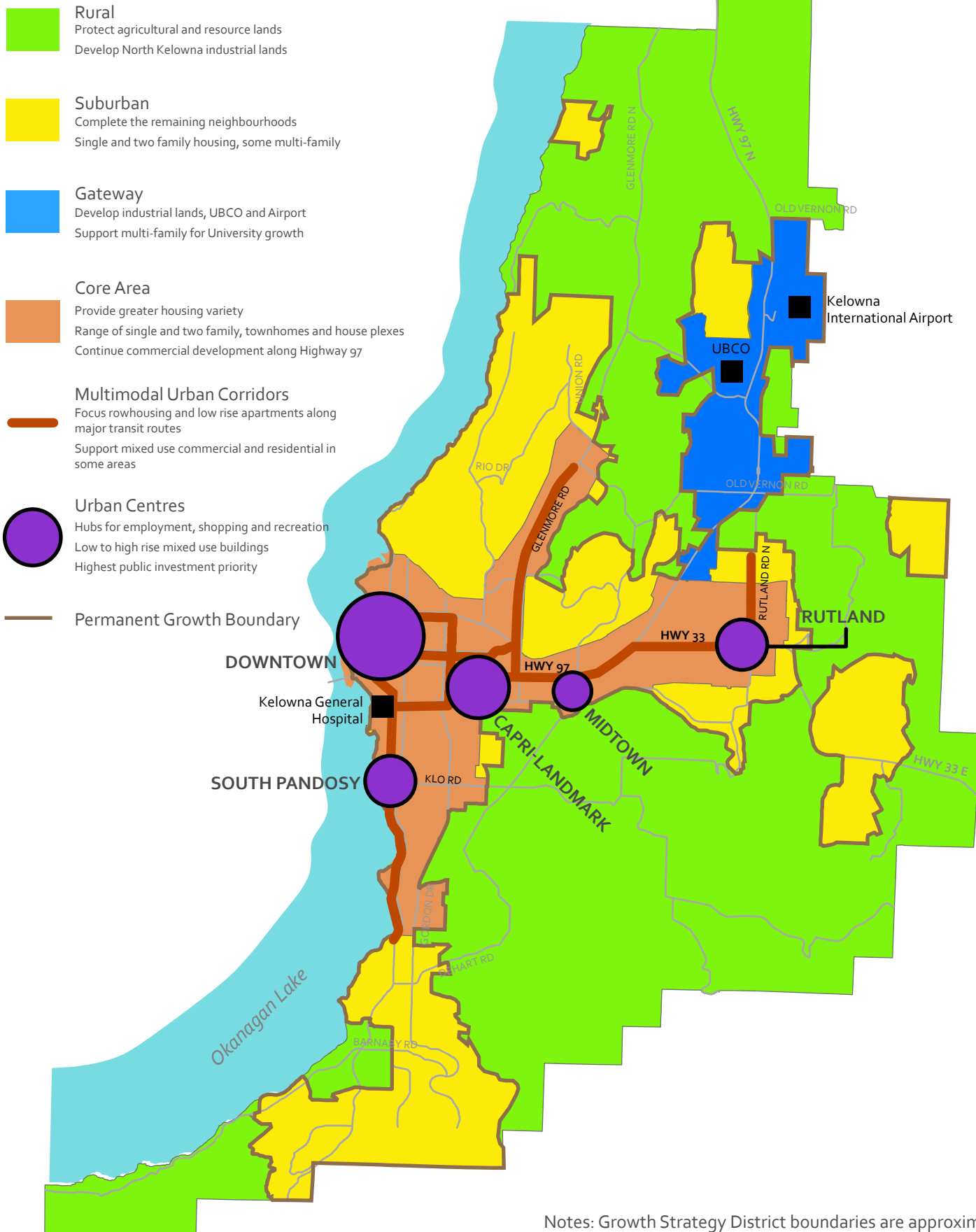
Attachments:

- 1) Attachment 1: Growth Strategy Map
- 2) Attachment 2: Draft Future Land Use Plan
- 3) Attachment 3: Draft Future Land Use Designations

cc:
Divisional Director, Planning and Development Services
Divisional Director, Infrastructure
Divisional Director, Financial Services
Divisional Director, Corporate Strategic Services
Divisional Director, Acting Living and Culture
Divisional Director, Strategic Investments

Department Manager, Integrated Transportation
Strategic Transportation Planning Manager
Infrastructure Engineering Manager
Community Planning Manager
Communication Advisor

Kelowna 2040: Growth Strategy



Notes: Growth Strategy District boundaries are approximate.

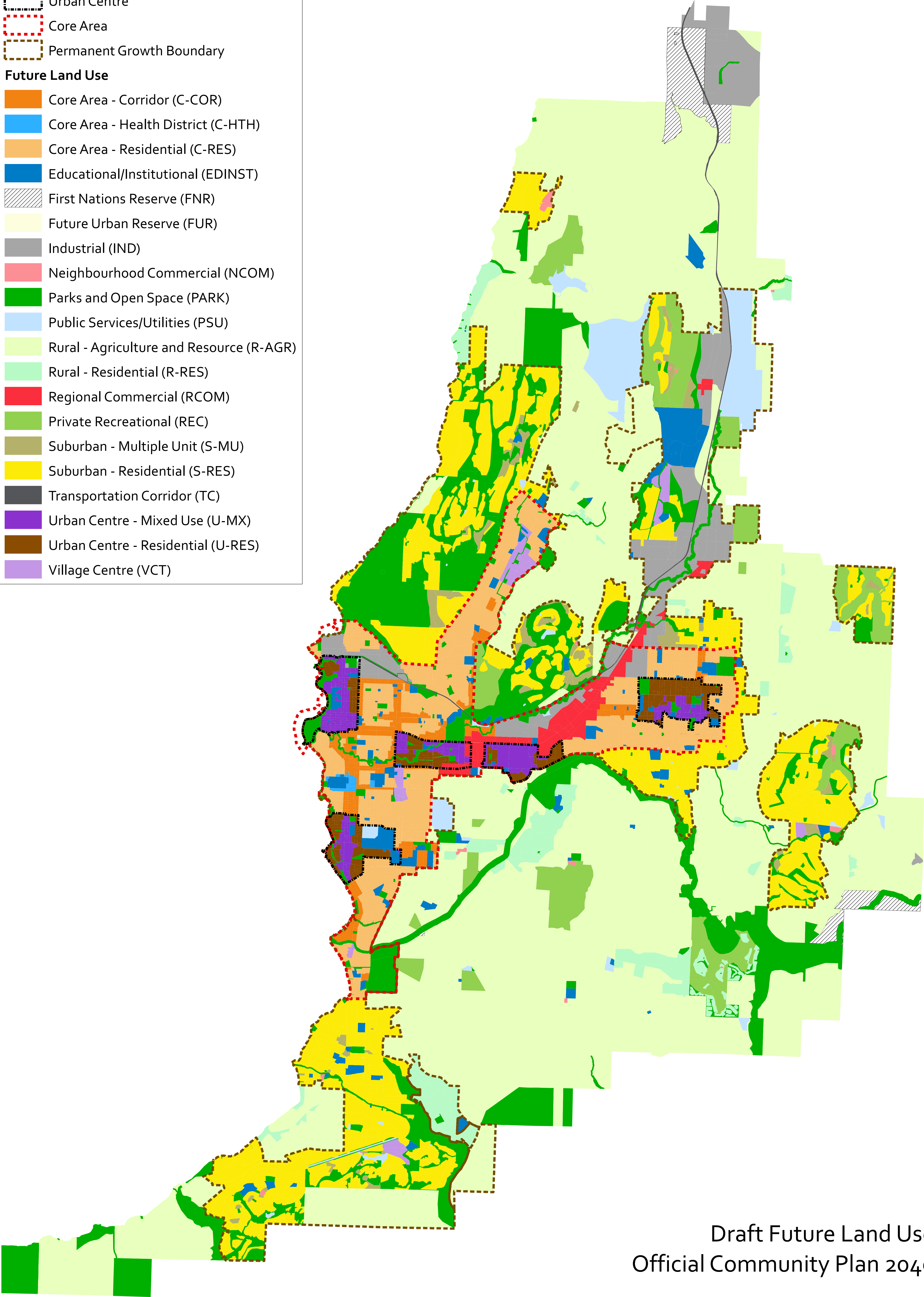


Legend

- Urban Centre
- Core Area
- Permanent Growth Boundary

Future Land Use

- Core Area - Corridor (C-COR)
- Core Area - Health District (C-HTH)
- Core Area - Residential (C-RES)
- Educational/Institutional (EDINST)
- First Nations Reserve (FNR)
- Future Urban Reserve (FUR)
- Industrial (IND)
- Neighbourhood Commercial (NCOM)
- Parks and Open Space (PARK)
- Public Services/Utilities (PSU)
- Rural - Agriculture and Resource (R-AGR)
- Rural - Residential (R-RES)
- Regional Commercial (RCOM)
- Private Recreational (REC)
- Suburban - Multiple Unit (S-MU)
- Suburban - Residential (S-RES)
- Transportation Corridor (TC)
- Urban Centre - Mixed Use (U-MX)
- Urban Centre - Residential (U-RES)
- Village Centre (VCT)



Draft Future Land Use
Official Community Plan 2040

Revised Aug. 2, 2019

OCP 2040 Future Land Use Designations: Quick Reference

Colour	Future Land Use Designation	Acronym	Purpose	Supported Uses
	Rural - Agricultural/Resource	R-AGR	Preserve agricultural and resource lands outside of the PGB, focus growth in the Core Area.	Agriculture, forestry, silviculture
	Rural - Residential	R-RES	Acknowledge low density housing outside of PGB, and discourage further intensification and servicing extension.	Single detached homes
	Suburban - Residential	S-RES	Signal where low density residential development in suburban neighbourhoods is to be located.	Primarily single and semi detached, semi detached, with secondary suites. Three-plex, four-plex and cottage housing may be supported in proximity to frequent transit, schools and other amenities.
	Suburban - Multiple Unit	S-MU	Identify where multiple unit housing in nodes in suburban neighbourhoods are appropriate.	Semi-detached, town housing, row housing, garden apartments, low rise apartments (as per ASP)
	Core Area - Residential	C-RES	Provide a variety of ground oriented housing choices in the Core Area close to Urban Centres, existing services and infrastructure.	Single detached, semi detached, carriage houses, suites, townhousing, rowhousing and complimentary uses.
	Core Area - Corridor	C-COR	Provide more opportunities for medium density residential development along key transit corridors. In some areas, include a commercial component at street level.	Townhousing, row housing, garden apartments, apartments, local commercial, small scale institutional and other complementary uses. Larger scale commercial uses at major intersections.
	Urban Centre - Residential	U-RES	Focus the highest density residential development within Urban Centres, adjacent to employment and shopping nodes.	Apartments, local commercial, complementary uses.

	Urban Centre - Mixed Use	U-MX	Build nodes of employment, entertainment, shopping mixed with residential in a high quality urban realm in areas best served by transit and active transportation.	Commercial development, office uses, high density residential development, institutional, other complementary uses.
	Village Centre	VCT	Provide smaller nodes of mixed residential and commercial services that serve their surroundings.	Commercial development, allowing for medium density development above or adjacent, institutional, complementary uses.
	Neighbourhood Commercial	NCOM	Small scale commercial nodes to provide basic services to the adjacent neighbourhood.	Low rise multiple unit mixed use residential and commercial, small scale commercial development
	Regional Commercial	RCOM	Signal where large format automobile oriented commercial services should be directed.	Commercial, auto-oriented commercial, some institutional. Limited residential close to existing amenities.
	Industrial	IND	Protect industrial lands from competing commercial and residential uses.	Light and heavy industrial uses, complementary uses that support the industrial activities.
	Educational / Institutional	EDINST	Signal existing and future institutional uses.	Schools, post-secondary, correctional facilities, hospitals, fire halls, cemeteries, government, cultural, recreational activities, religious assembly, seniors facilities.
	Core Area - Health District	C-HTH	Integrate the KGH campus with the neighbouring residential areas.	Hospital, small scale commercial, multi residential
	Public Services/Utilities	PSU	Signal lands required for basic public services.	Landfill, airport, utilities, sewer and water infrastructure
	Parks and Open Space	PARK	Identify where parks are needed to support a high quality of life for residents	Neighbourhood, Community, Recreation and City Wide Parks, golf courses.
	Private Recreational	REC	Identify areas for privately owned outdoor recreation activities.	Passive parks, natural areas, pathways, creeks
	Transportation Corridor	TC	Identify the Okanagan Rail Trail	Transportation uses



Our Kelowna as we Grow 2040



50,000 more
people

20 years





imagineKelowna

Principles & Goals

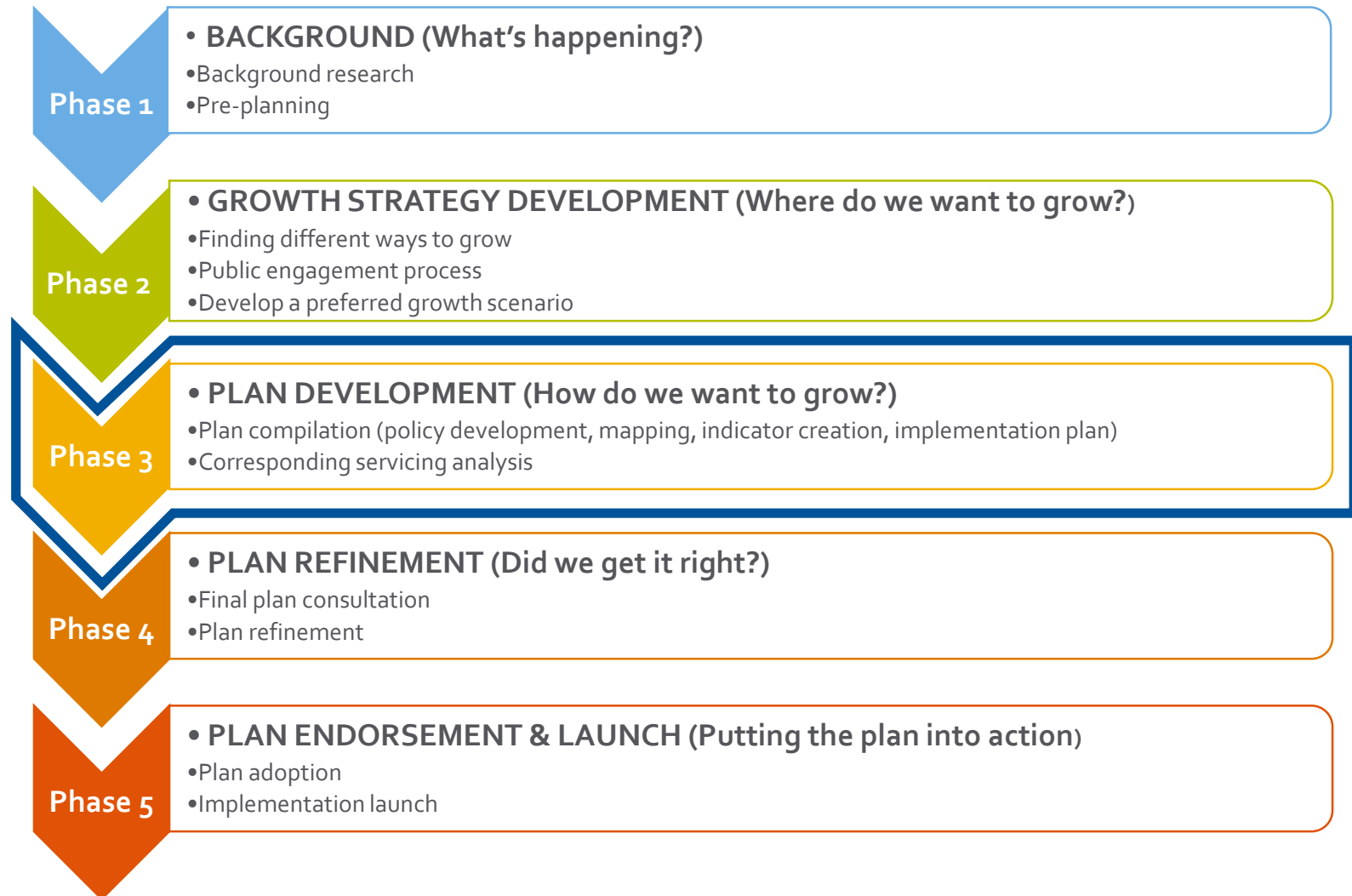
Official Community Plan

Transportation Master Plan

20 Year Servicing Plan



-
- 1 • Complete the Final Suburban Neighborhoods
 - 2 • Promote More Housing Diversity in the Core Area
 - 3 • Connect our Urban Centres with Transit Oriented Urban Corridors
 - 4 • Prioritize Parks & Public Spaces in the Core Area
 - 5 • Prepare a Resilient Community that is Adaptable to Change



Phase 3

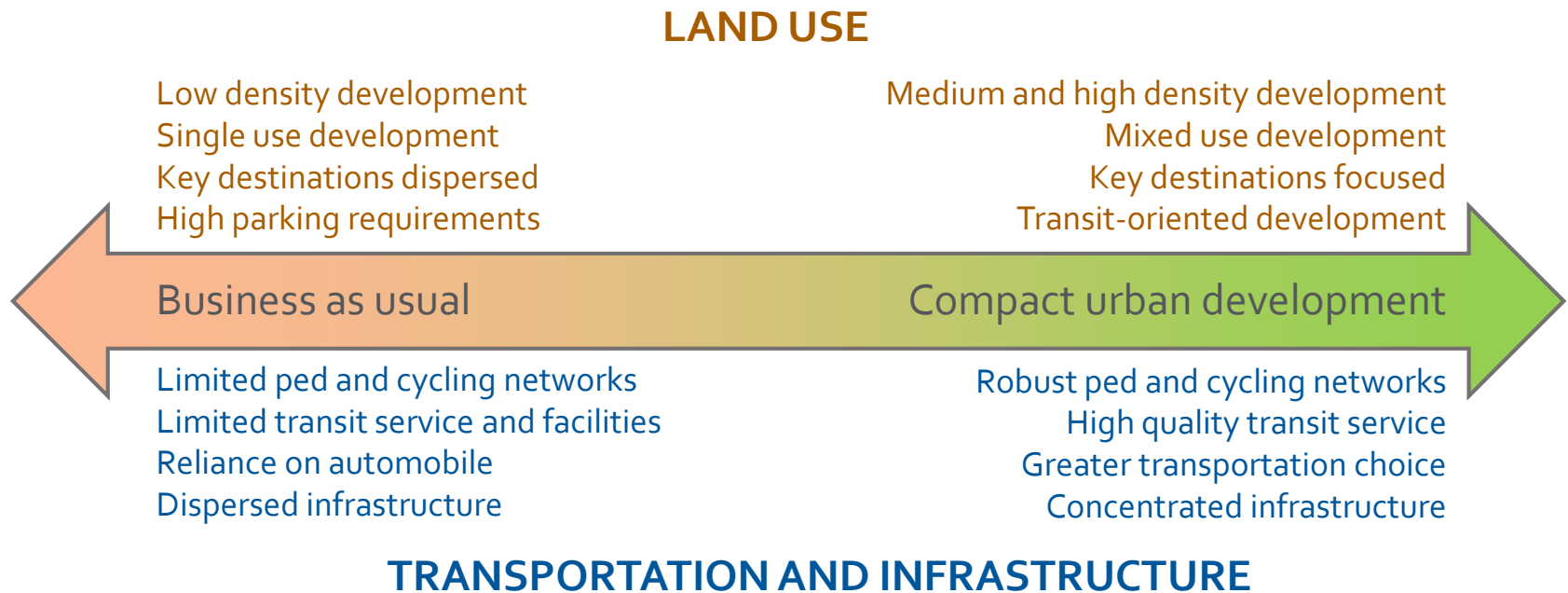
PLAN DEVELOPMENT (How do we want to grow?)

○ Focusing our Shared Vision

○ Translating the 'Big Moves'

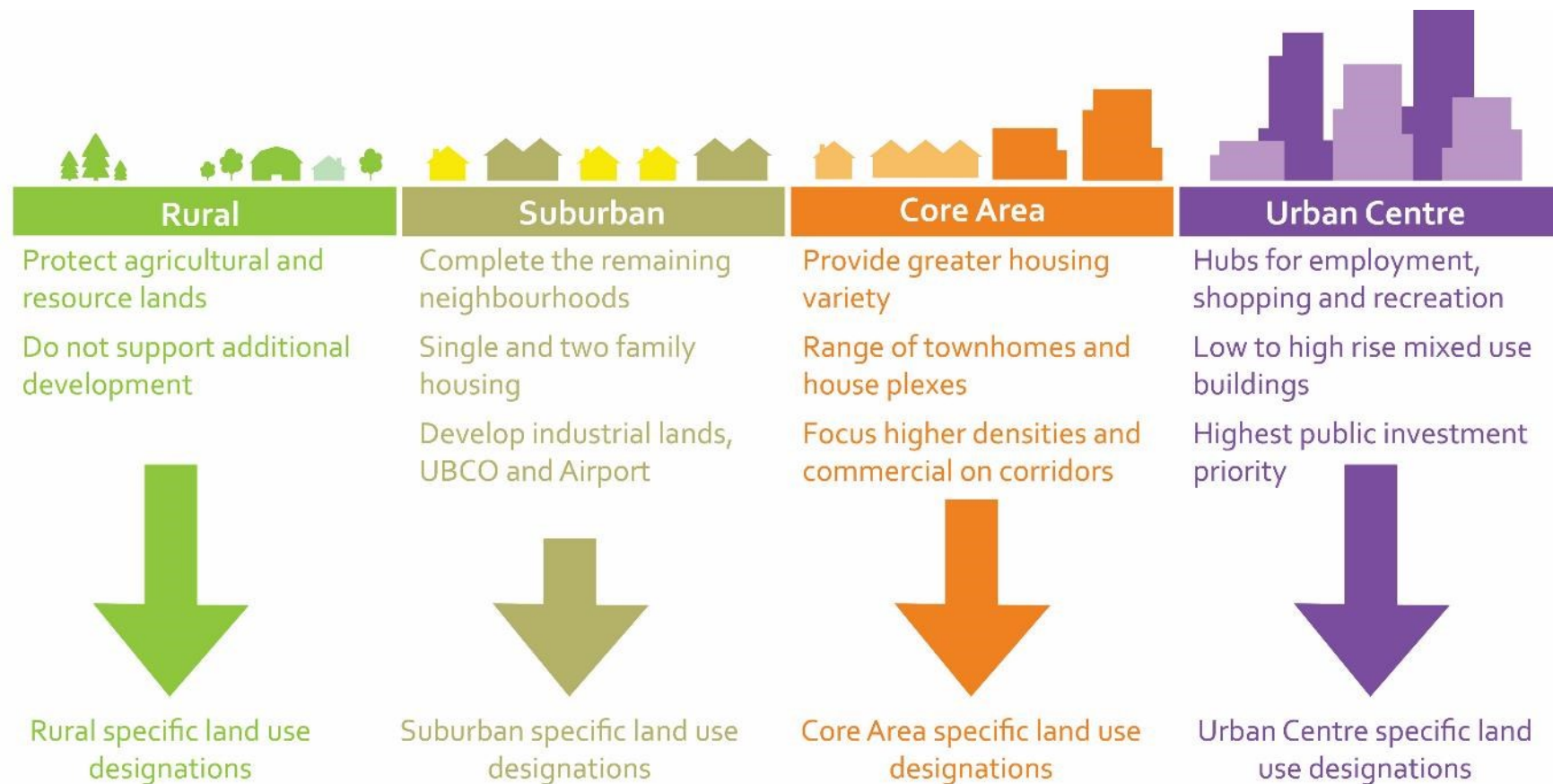
○ How do we make this happen?

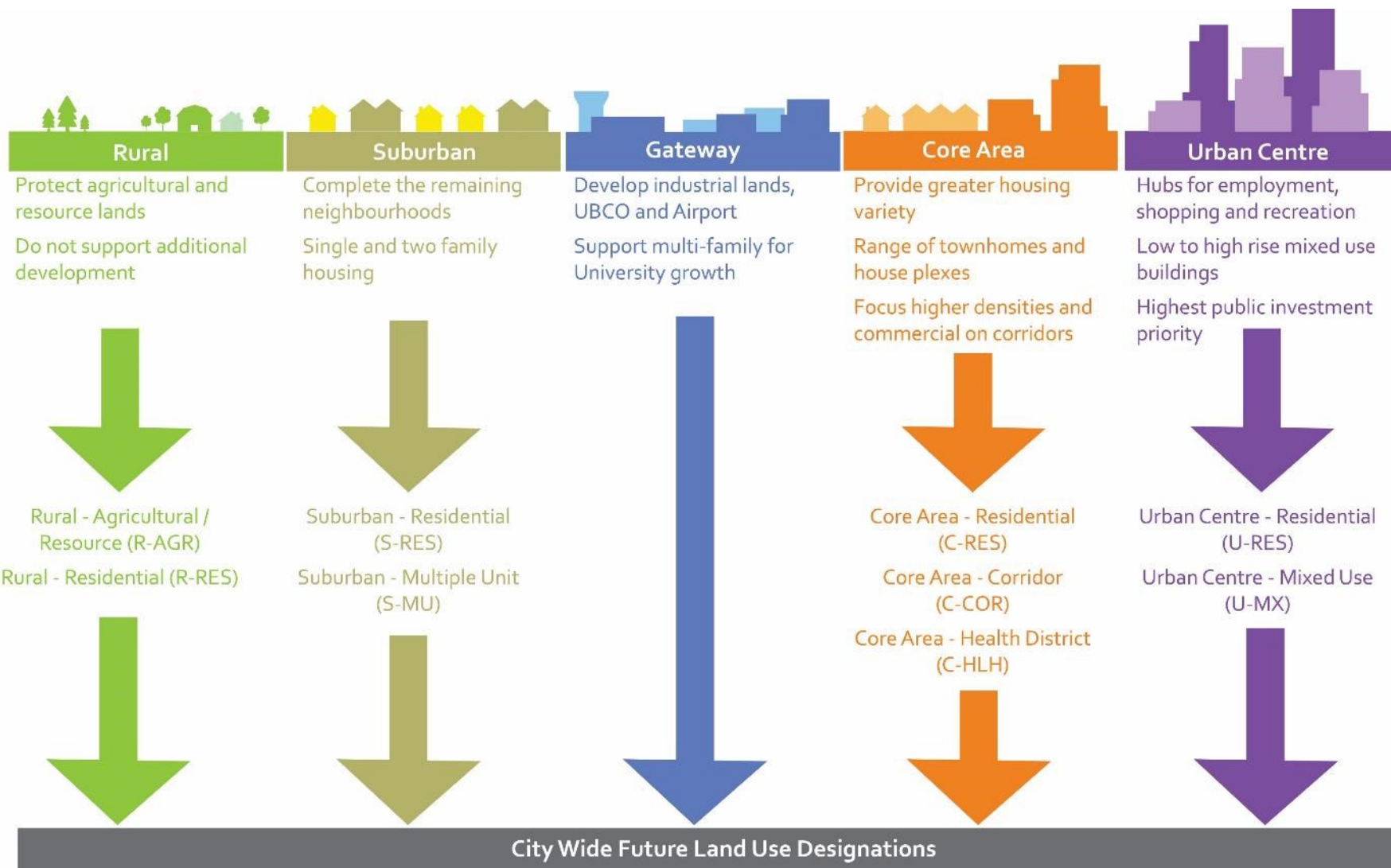
The Land Use, Transportation and Infrastructure Relationship





A New Approach





Village Centre (VCT)
Neighbourhood Commercial (NCOM)
Regional Commercial (RCOM)

Industrial (IND)
Educational / Institutional (EDINST)
Public Services / Utilities (PSU)

Parks and Open Space (PARK)
Private Recreational (REC)
Transportation Corridor (TC)

2040 Growth Strategy

More housing variety in the Core Area

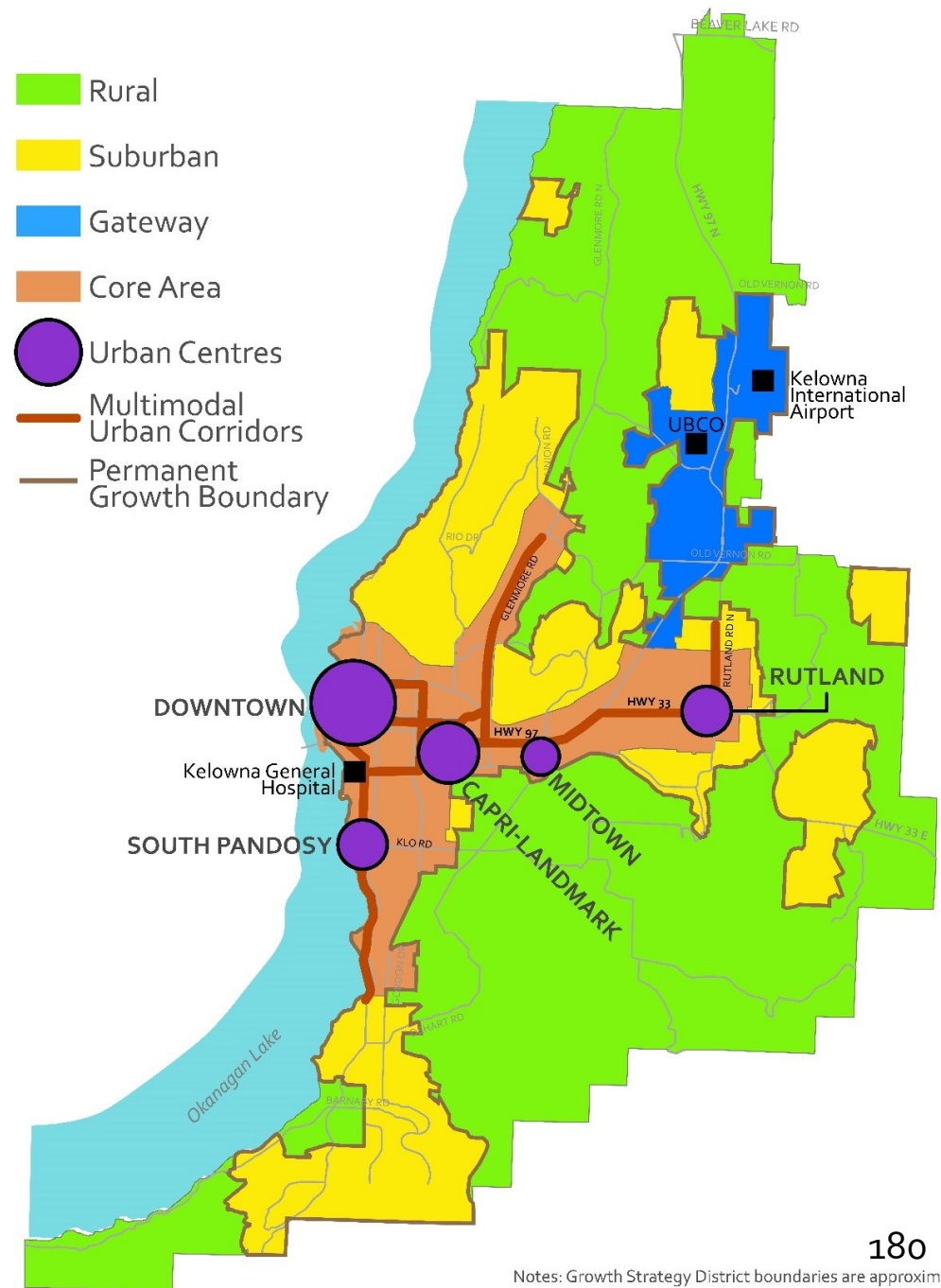
Transit oriented urban corridors

Maintaining the Permanent Growth Boundary

Complete the suburban neighbourhoods

Protect industrial lands

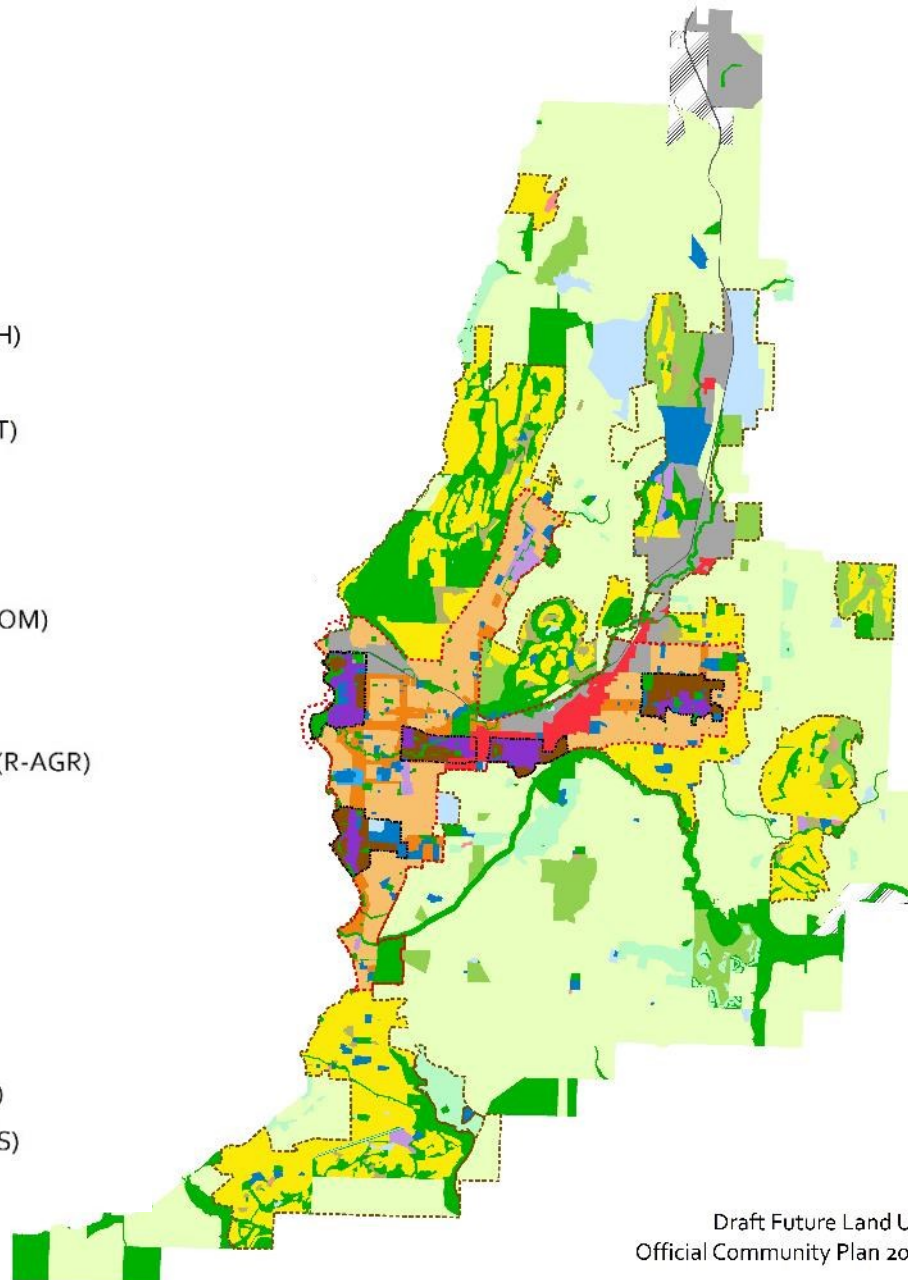
Priorities for parks and public space



-  Urban Centre
-  Core Area
-  Permanent Growth Boundary

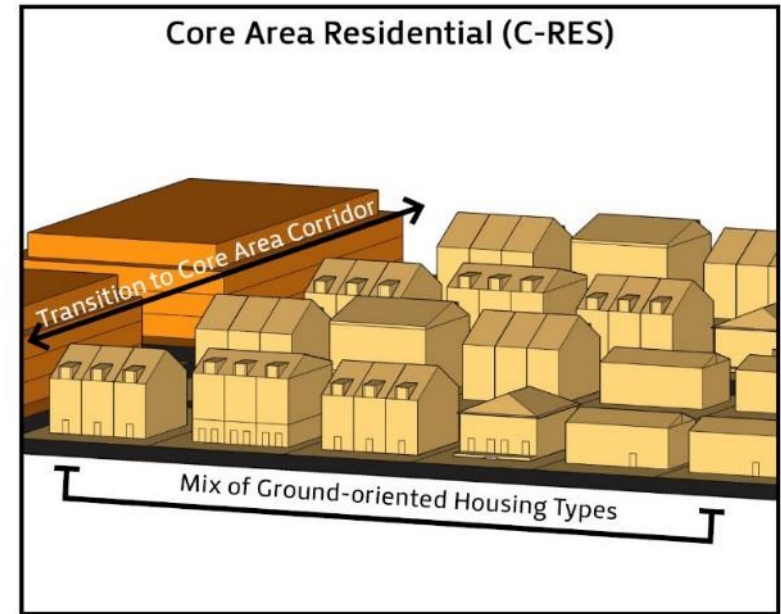
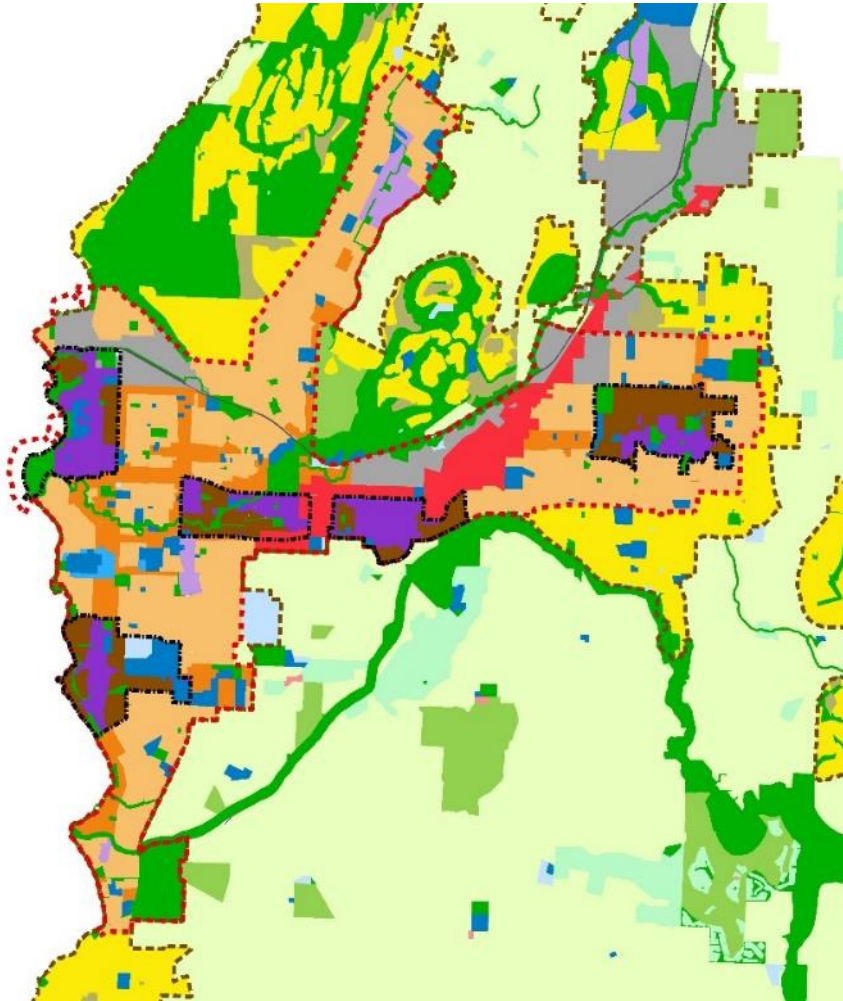
Future Land Use

-  Core Area - Corridor (C-COR)
-  Core Area - Health District (C-HTH)
-  Core Area - Residential (C-RES)
-  Educational/Institutional (EDINST)
-  First Nations Reserve (FNR)
-  Future Urban Reserve (FUR)
-  Industrial (IND)
-  Neighbourhood Commercial (NCOM)
-  Parks and Open Space (PARK)
-  Public Services/Utilities (PSU)
-  Rural - Agriculture and Resource (R-AGR)
-  Rural - Residential (R-RES)
-  Regional Commercial (RCOM)
-  Private Recreational (REC)
-  Suburban - Multiple Unit (S-MU)
-  Suburban - Residential (S-RES)
-  Transportation Corridor (TC)
-  Urban Centre - Mixed Use (U-MX)
-  Urban Centre - Residential (U-RES)
-  Village Centre (VCT)



Draft Future Land Use
Official Community Plan 2040

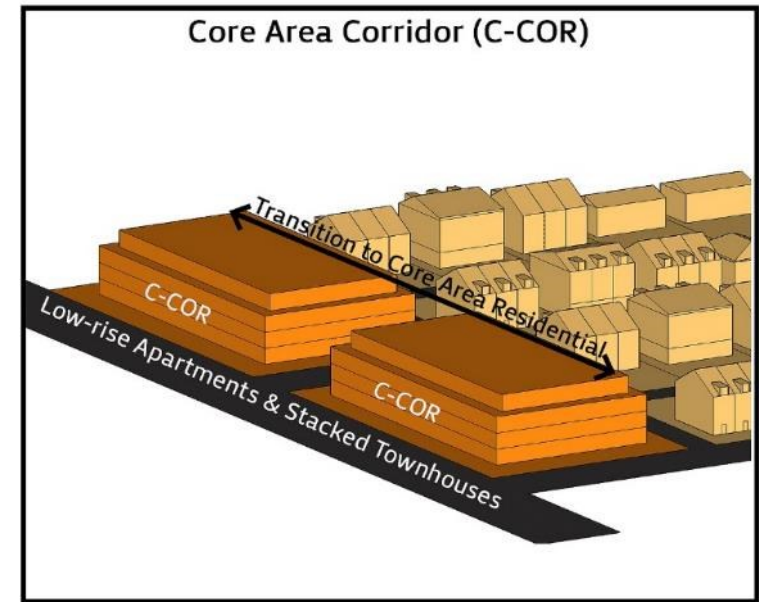
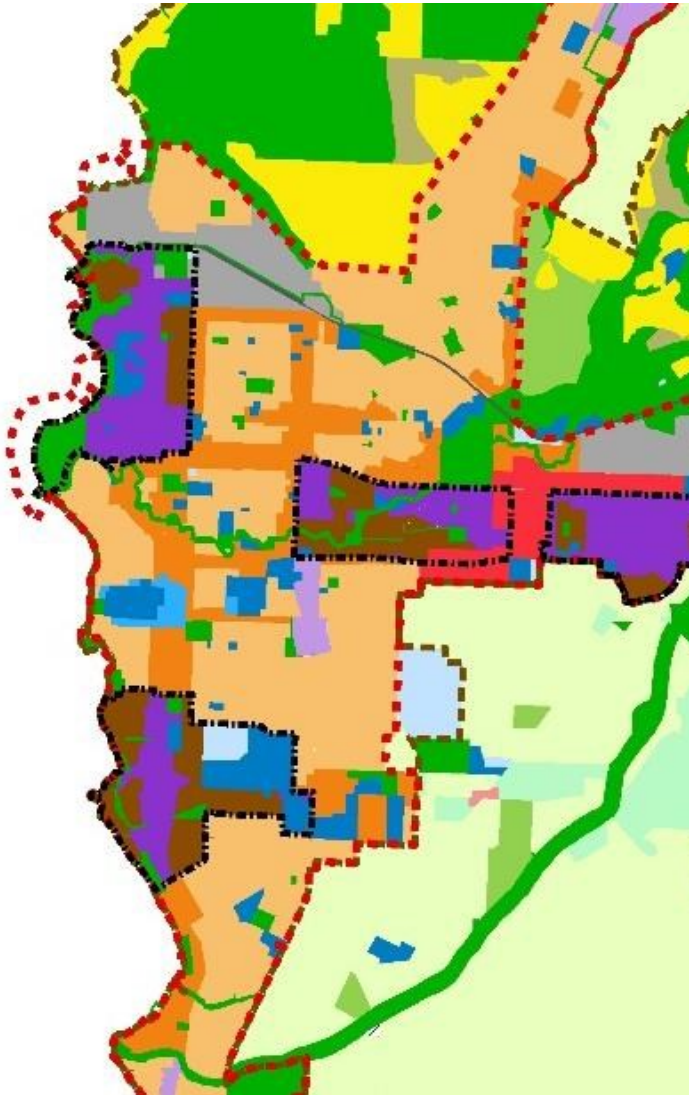
More Housing Variety in the Core Area



Core Area Residential

- Range of low density housing types
- Single detached, semi-detached, house plexes and row housing
- Local commercial, institutional and other neighbourhood supportive uses

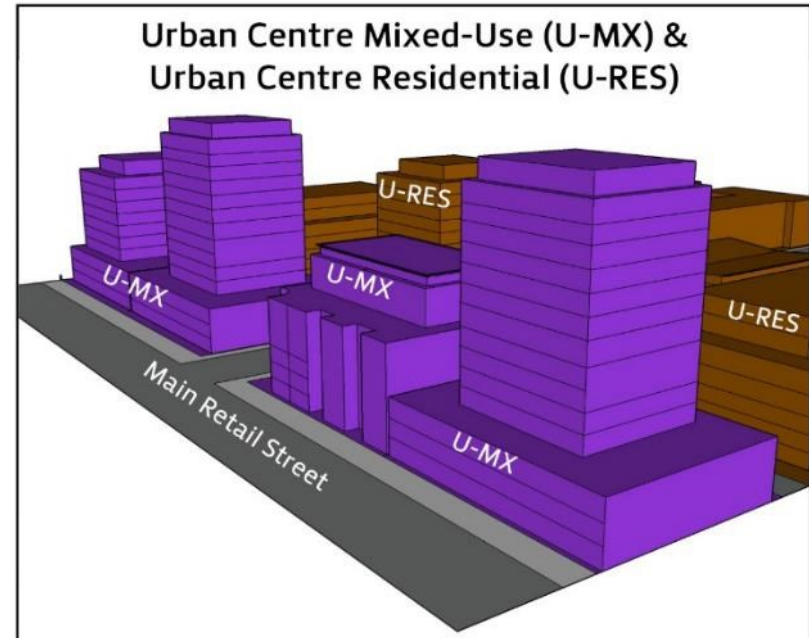
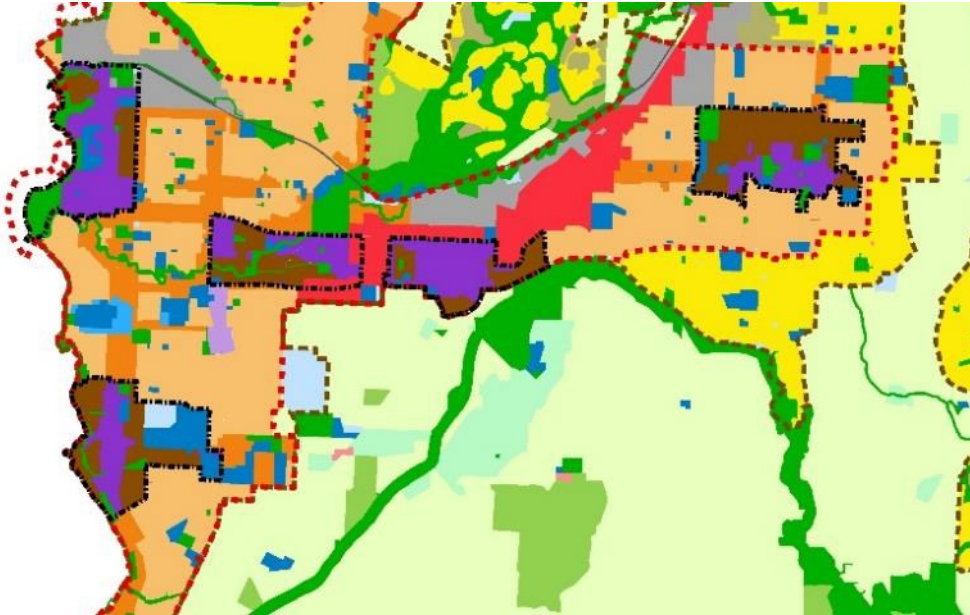
Multimodal Urban Corridors



Core Area Corridor

- Range of rowhousing and low rise apartments
- Mixed use encouraged in key locations and intersections
- Supports transit and active transportation

Urban Centres



Urban Centre Mixed Use

- Shopping, employment and entertainment mixed with residential

Urban Centre Residential

- High density residential
- Context specific densities and heights

Village Centres



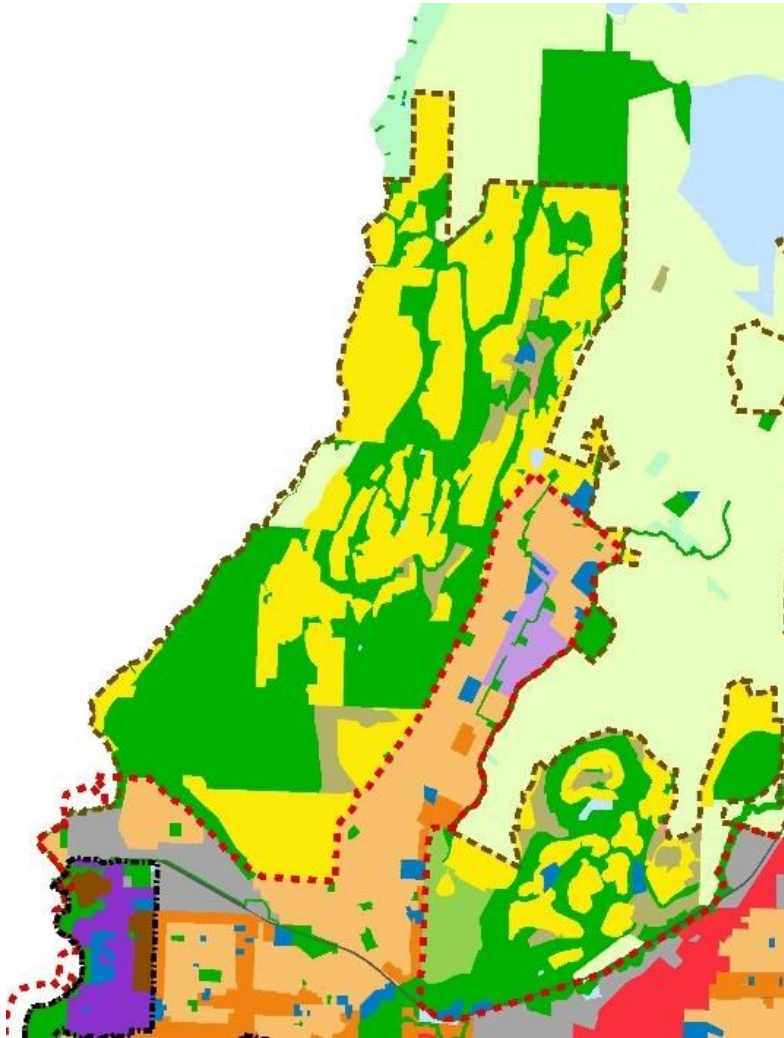
Village Centre Designation

- Shopping, employment and entertainment mixed with residential at lower scale

New Lakeshore Road Village Centre

- Lakeshore Road between Mission Creek and Lexington/Bluebird Road

Suburban Neighbourhoods



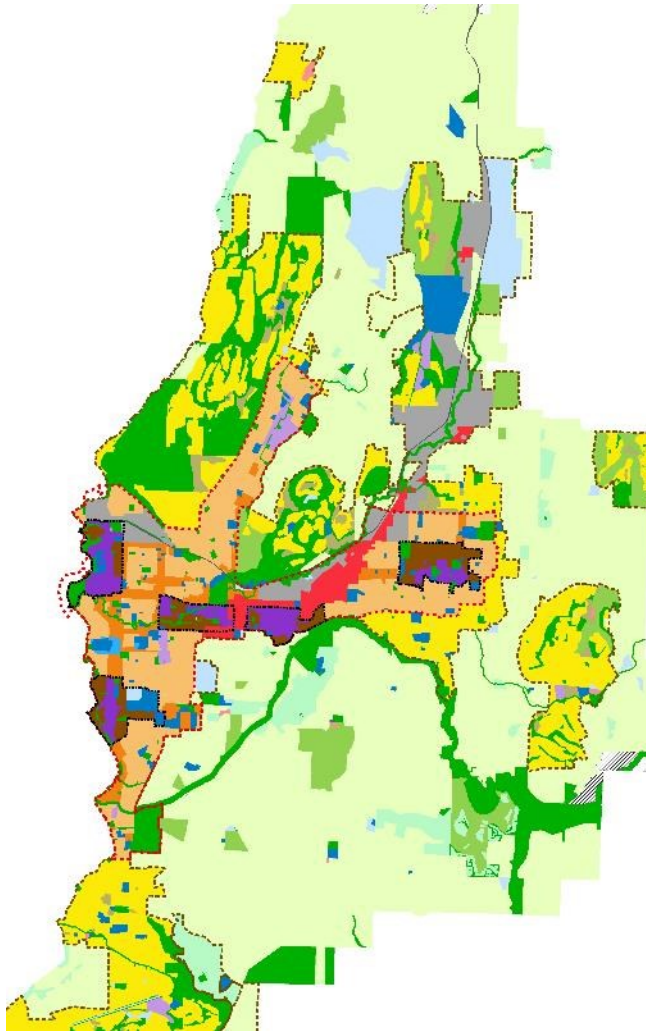
Suburban Residential

- Single and two unit housing in suburban and hillside neighbourhoods

Suburban Multi Unit

- Ground oriented and apartment housing, consistent with ASPs

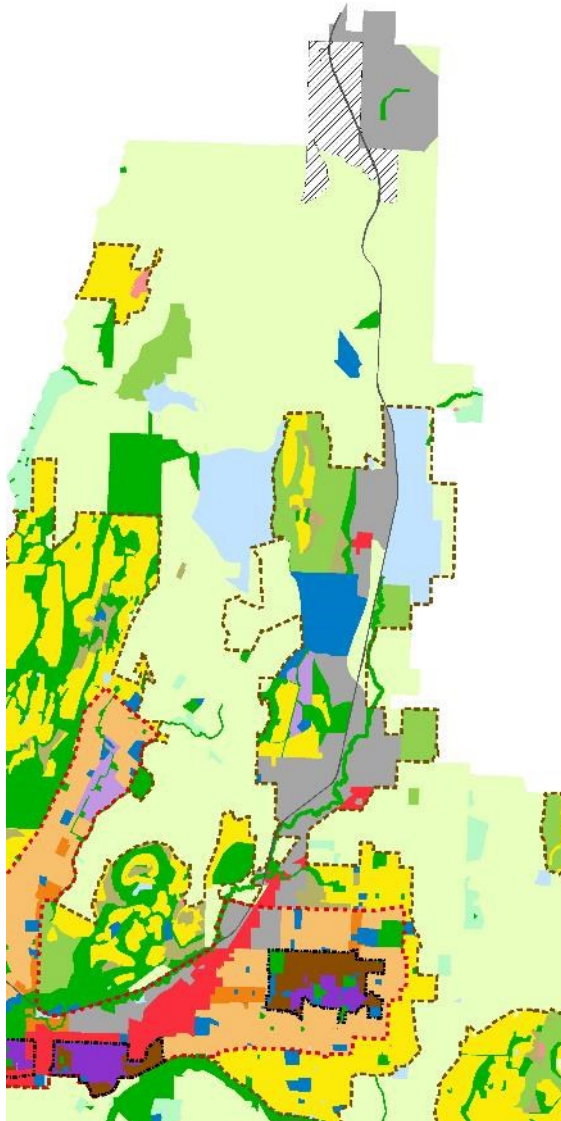
Permanent Growth Boundary



Proposed Boundary Changes

- McKinley Beach
- Crawford Estates
- Kelowna International Airport

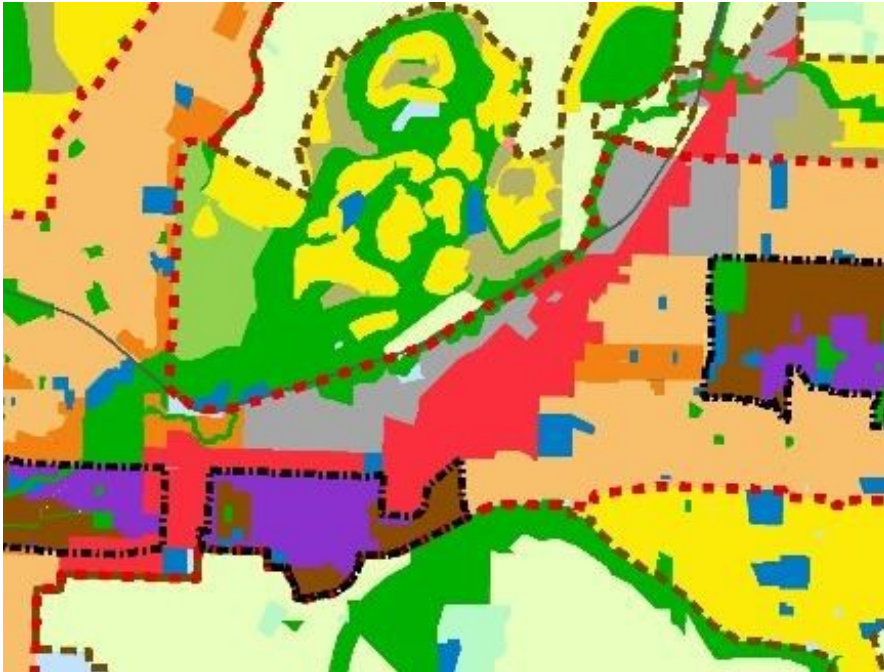
Industrial Lands



Industrial Designation

- Combines three existing designations
- Policy direction would vary by Growth Strategy District

Regional Commercial



Regional Commercial Designation

- Large format commercial development
- Combines retail commercial with service commercial

Parks and Open Space

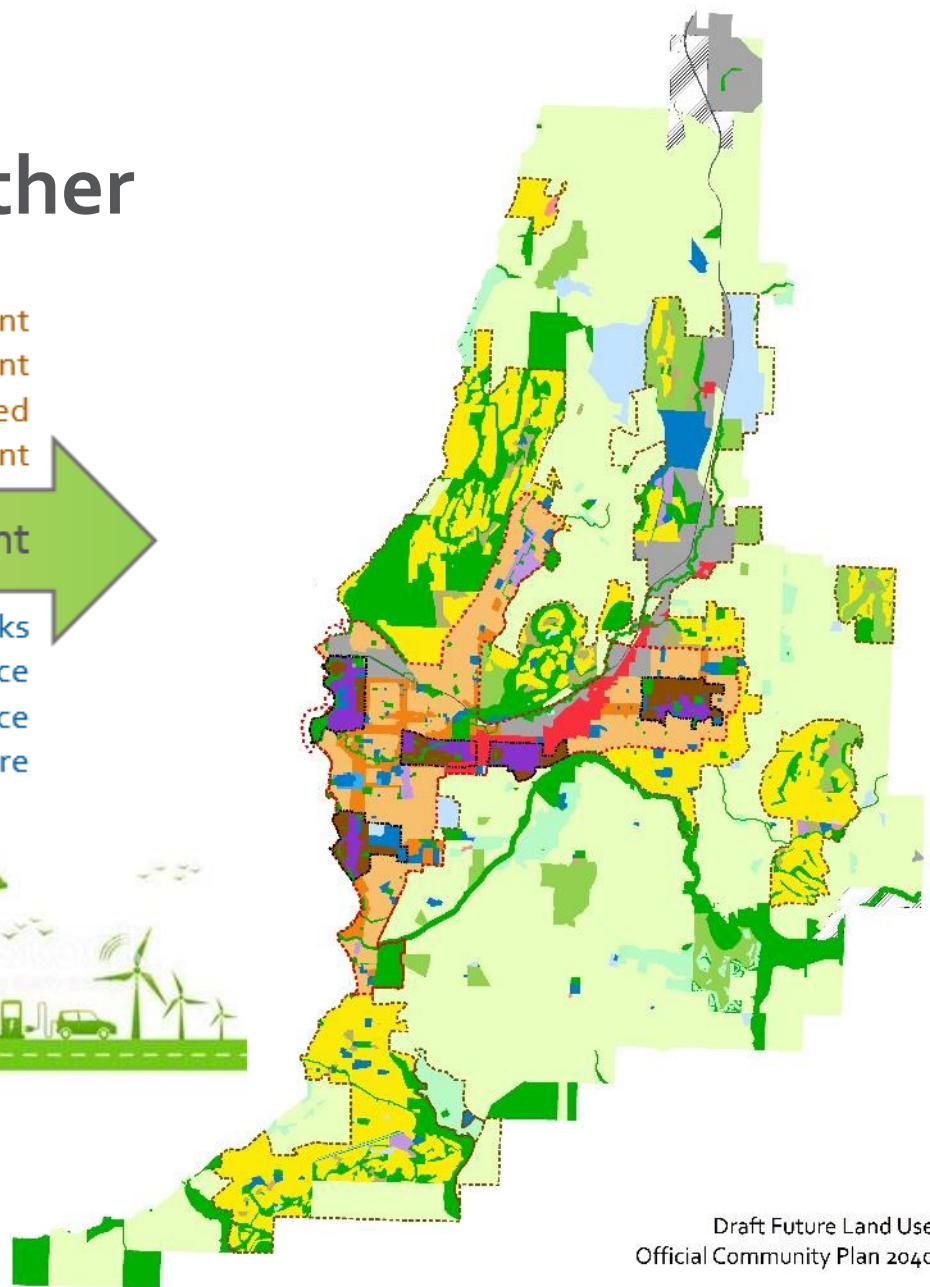


How It All Fits Together

Medium and high density development
Mixed use development
Key destinations focused
Transit-oriented development

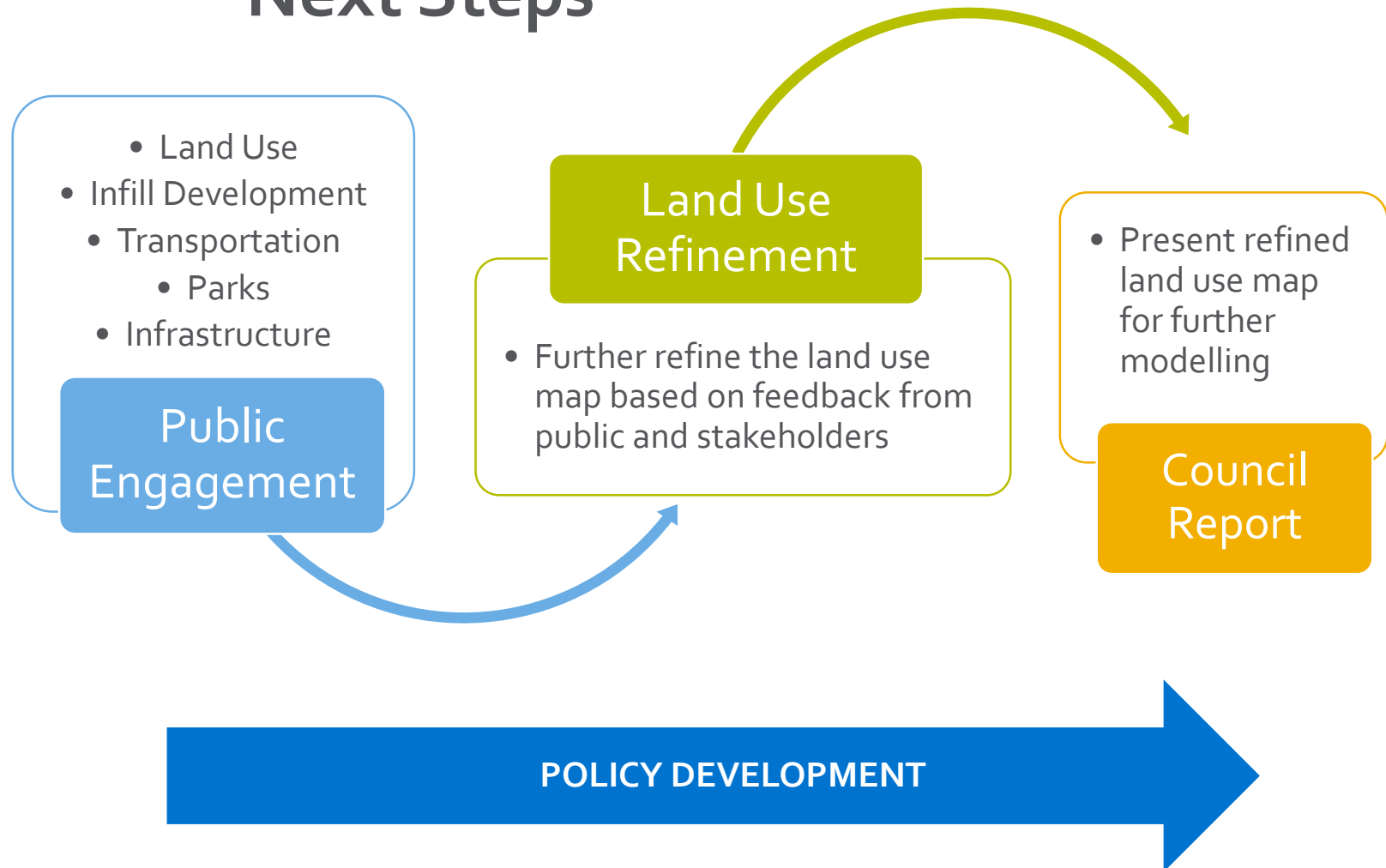
Compact urban development

Robust ped and cycling networks
High quality transit service
Greater transportation choice
Concentrated infrastructure



Draft Future Land Use
Official Community Plan 2040

Next Steps







Questions?



Our Kelowna as we Grow 2040



Report to Council



Date: August 12, 2019

File: 0710-40

To: Council

From: City Manager

Subject: Healthy Housing Strategy Implementation - Rental Housing Incentives Update

Recommendation:

THAT Council receives, for information, the Report from the Long Range Policy Planning Department dated August 12, 2019 with respect to the policy and bylaw updates to support the City's Healthy Housing Strategy;

AND THAT Council Policy No. 335, being the Rental Housing Grants Policy, be revised as outlined in the Report from the Planner Specialist dated August 12, 2019;

AND THAT Bylaw No. 11911 being Amendment No. 6 to Housing Opportunities Reserve Fund Bylaw No. 8593, be forwarded for reading consideration;

AND FURTHER THAT Bylaw No. 11912 being Amendment No. 7 to Revitalization Tax Exemption Bylaw No. 9561, be forwarded for reading consideration.

Purpose:

To update the Council Policy 335: Rental Housing Grants Policy, the Housing Opportunities Reserve Fund Bylaw 8593 and the Revitalization Tax Exemption Bylaw 9561 to align with the recommendations of the Healthy Housing Strategy.

Background:

On June 25, 2018 Council endorsed the Healthy Housing Strategy (HHS). This Strategy aims to address the community's most pressing housing issues through the following vision: "*the housing needs of all Kelowna residents are met through affordable, accessible and diverse housing options*".

The Healthy Housing Strategy (HHS) includes four key directions and 19 actions to be implemented over the next five years. The four key directions include:

1. Promote and protect rental housing;
2. Improve housing affordability and reduce barriers for affordable housing;
3. Build the right supply; and
4. Strengthen partnerships and align investments.

The first key direction, “Promote and protect rental housing” was created in recognition of the vital role that rental housing plays in Kelowna’s housing system. As the ownership market becomes more difficult to enter due to escalating prices, the rental market is expected to grow in importance. The Housing Needs Assessment (HNA), endorsed by Council on November 20, 2017, recommends an annual target of 400 purpose-built rental units to meet the growing demand in Kelowna. As a result, investments in purpose-built rental housing are required to ensure a healthy supply of rental housing. The HNA identified the need for affordable rental housing that is most often provided by non-profit housing providers. Achieving the Healthy Housing Strategy’s rental housing objectives will require action and investment on a number of fronts, two of which are the subject of this report: (1) the Rental Housing Grants Program, and (2) the Revitalization Tax Exemption program.

1.0 Rental Housing Grants Program

One of the Healthy Housing Strategy actions is to “reduce the cost of building affordable, purpose-built rental housing”. This action relates to the upfront Development Cost Charges (DCC) that are triggered for new housing when a Building Permit application is submitted. Ranging from about \$13,000-\$22,000 per unit, these costs cover the city-wide infrastructure needed to service new development. For non-profit, purpose-built rental housing projects, DCC’s represent a significant financial cost and can act as a barrier to the financial feasibility of projects.

The City’s Rental Housing Grant Program currently offers all market and non-market purpose-built rental housing the opportunity to obtain grant funding as a measure to offset DCC’s. The funding source for the Rental Housing Grant Program is \$120,000 via taxation and a \$200,000 appropriation from the Housing Opportunities Reserve Fund (HORF).

While market rental projects and non-profit rental projects are treated equally in the grants program today, each group values the grants differently. In staff discussions with market rental housing providers, these grants are not seen as vital to a project’s viability. In the case of non-profit rental projects (often providing affordable housing or supportive housing), every incentive available is highly important to decisions about whether a project will proceed.

Over the last four years, the Rental Housing Grants program has seen a significant uptick in interest. The increase in the number of applications corresponds to a spike in the construction of new purpose-built rental housing. However, the vast majority of applications to the program were for market rental housing projects with no links to affordability. Due to the large number of projects applying to the

program, the grant amounts for each individual application decreased substantially (in order for each applicant to benefit from the program). Over the last four years, the grants have covered 5-10 per cent of a project's DCCs. In short, the rapid influx of market rental housing applications has resulted in the distribution of many small grants, but fewer meaningful contributions to support non-market rental housing projects.

Proposed Updates to the Rental Housing Grants Program

The Healthy Housing Strategy recommended shifting the \$200,000 annual HORF allocation from grants to land acquisition. In 2019 this change was made through the budget process to restore the historical contribution to the HORF reserve, ensuring that the City can build a balance in the reserve for affordable housing partnerships in the future.

Because of the \$200,000 HORF allocation being shifted from the rental housing grants program to land acquisition, the grants program was left with only \$120,000. Accordingly, through 2019 budget process an annual operating budget allocation was approved, resulting in a new annual funding allocation of \$180,000 that will be phased in over the next three years to offset the shift of the HORF allocation, ensuring the rental housing grant program remains essentially unchanged at \$300,000. Also, to increase the impact of the \$300,000 in rental housing grants, staff is recommending the eligibility criteria be shifted to focus on a smaller pool of non-market rental projects.

To maximize the program's benefits and to align with the Healthy Housing Strategy recommendations, Staff are proposing three main changes to the Rental Housing Grant Program:

1. Separate the funding so that land acquisition and DCC relief are not combined;
2. Change the project eligibility requirements to focus exclusively on non-profit housing developers / providers that are delivering non-market rentals; and
3. Change the project eligibility requirements to focus affordable rental projects in areas where the City plans to invest in frequent transit and active transportation infrastructure.

The proposed changes to the program will require an update to Council Policy 335: Rental Housing Grants Policy (see attached for updated Policy) and minor changes to the Housing Opportunities Reserve Fund Bylaw (attached for bylaw amendments).

The primary goal of these changes (see Attachment A for more detail) is to ensure that the grants are aiding those projects that have the greatest impact on promoting affordability in the long-term rental market. It is anticipated that this change will provide the greatest positive impact to the non-market units that are operated by a non-profit, and in practice, are the most difficult to finance given increasing land and construction costs.

2.0 Rental Housing Tax Exemption Program

Another key action of the Healthy Housing Strategy is to "revise tax incentives for purpose-built rental housing". This action directly addresses the City's primary incentive to encourage new long-term, purpose-built rental housing: The Rental Housing Revitalization Tax Exemption Program. Over the last

five years, roughly 25 Revitalization Tax Exemption Agreements for rental housing were approved, with a mix of both market and non-market rental projects utilizing the program. Based on discussions with market rental housing providers and a financial analysis of the program, staff has determined that the rental housing tax exemption is a much more important incentive for rental housing providers. Recent staff analysis shows that on average, a mid-market rental housing project receives roughly \$7,000 per door over the ten-year lifespan of the tax exemption. In comparison, the rental housing grants ranged from \$500 to \$3,000 per door depending on unit type and number of applications over the last four years.

Proposed Changes to the Rental Housing Tax Exemption

Staff continue to see a role for the rental housing tax exemption program to support both market and non-market rentals given the rental housing market in Kelowna remains constrained with a vacancy rate below 2 per cent. However, staff are proposing minor changes to the rental housing tax exemption program to implement the HHS recommendations and to ensure the Rental Housing Grants Program is aligned with the Rental Housing Revitalization Tax Exemption program. These changes include amending the application criteria, updating the definition of 'affordable rental housing', and to provide clarity for geographic locations for land acquisitions pursued with the Housing Opportunities Reserve Fund. The proposed changes (see Attachment A for more detail) will encourage continued investment in rental housing and ensure rental housing development locates in areas that are well served by transit and other key amenities (shops, services, parks and schools).

Summary

With more and more of the population relying on rental housing, purpose-built rental housing in Kelowna will only become more important to ensure residents have access to attainable and affordable housing. The bylaw and policy updates proposed in this report are a key step toward the Healthy Housing Strategy goal of promoting and protecting the supply of long-term rental housing. As other Healthy Housing Strategy recommendations are implemented, further momentum will be achieved to promote affordable and attainable housing options in Kelowna.

Internal Circulation:

Planning & Development Services Divisional Director
Department Manager, Policy and Planning
Manager, Long Range Policy Planning
Department Manager, Development Planning
City Clerk
Budget Supervisor
Revenue Supervisor

Existing Policy:

Council Policy 335: Rental Housing Grants Policy

Submitted by:
R. Soward, Planner Specialist

Approved for inclusion:



James Moore, Long Range Policy Planning Manager

Attachments:

Attachment A: Summary of Proposed Updates to Rental Incentives
Draft Council Policy 335: Rental Housing Grants Eligibility
Existing Council Policy 335: Rental Housing Grants Policy



Rental Incentives Update

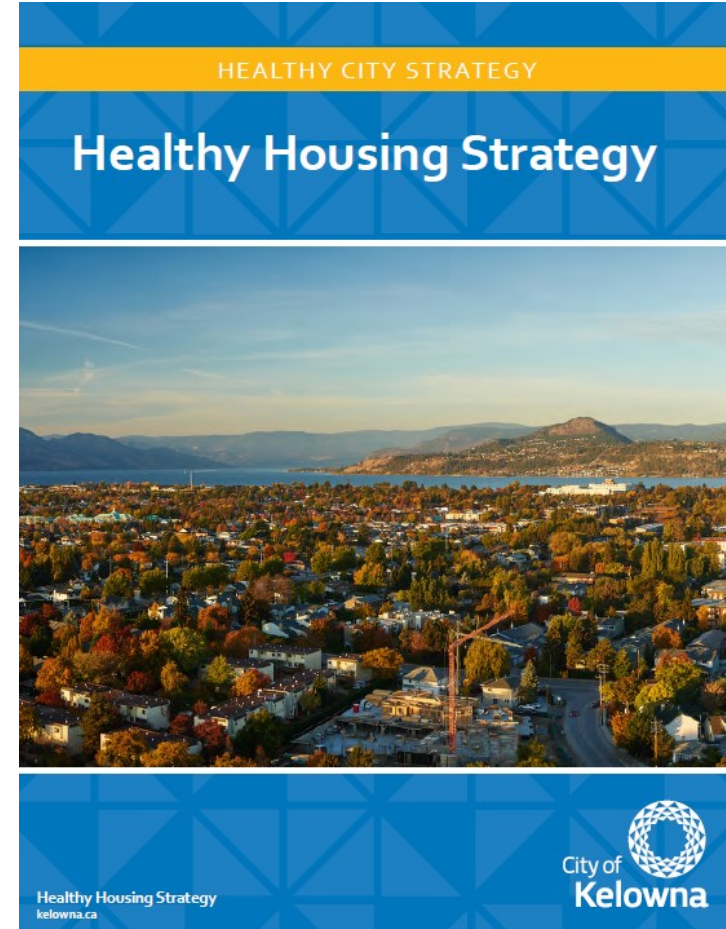
Healthy Housing Strategy Implementation
August 12, 2019



Healthy Housing Background

► *Council Endorsement of 4 key directions*

- 1) Promote and protect rental housing;
- 2) Improve housing affordability and reduce barriers for affordable housing;
- 3) Build the right supply; and
- 4) Strengthen partnerships and align investments.



Current Rental Housing Incentives

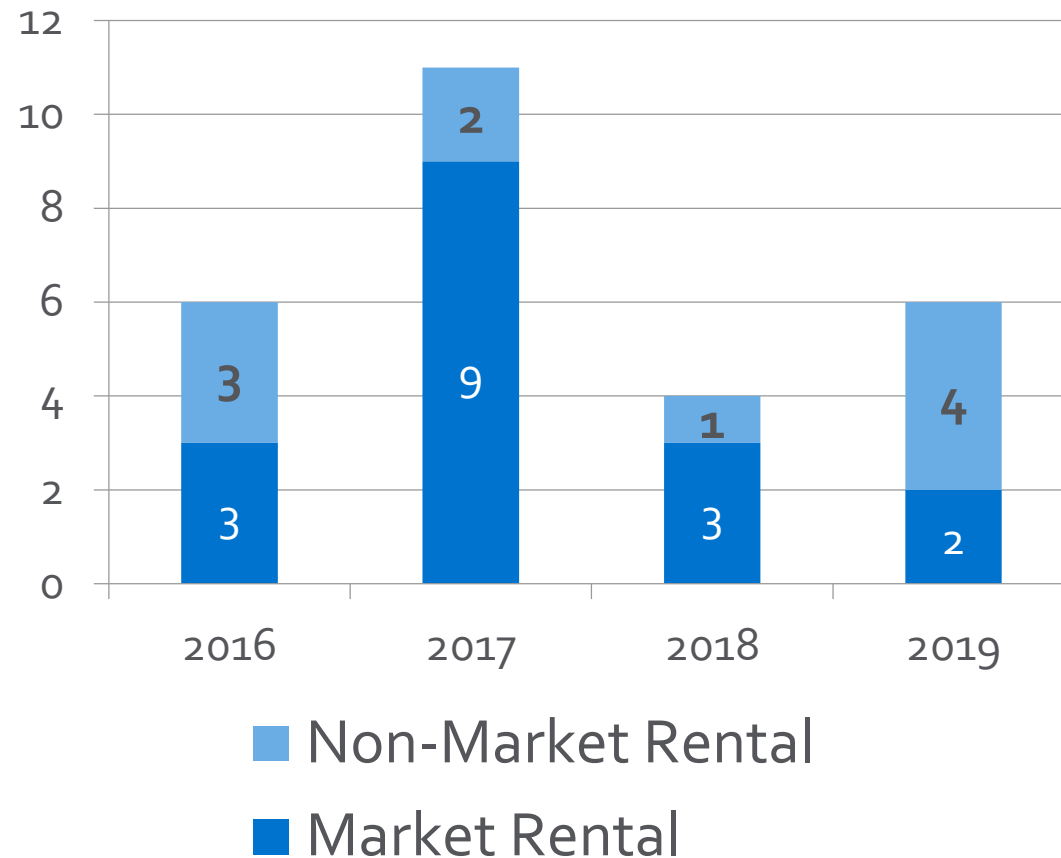
1. *Rental Housing Grants*
2. *Rental Housing Tax Exemptions*



Current Program Rental Housing Grants

- ▶ Annual grant funding via DCC credits
- ▶ All long-term rental projects eligible
- ▶ Any location in city
- ▶ Housing agreement with City required
- ▶ Many small grants disbursed to market & non-market projects

Number of Applications 2016-19



Proposed Updates to Rental Housing Grants

1. Shift focus to non-market long-term rental projects;
2. Require projects to be located in areas with transit and amenities.
3. Lift requirement of City of Kelowna housing agreement for projects with long-term operating agreement with BC Housing.



Non-market rentals (Pleasantvale Project)

Current Rental Tax Exemption Program

- ▶ Mix of market and non-market projects, anywhere in city
- ▶ Vacancy rate must be 3 per cent or lower
- ▶ High number of rental tax agreements approved over last 5 years
- ▶ Mid-market rental projects receive roughly \$7,000 / unit over the 10-year exemption
- ▶ Key incentive for market rental providers



Market Rental Project at 1155 Pacific Ave

Proposed Updates to Rental Tax Exemption Program

- ▶ Eliminate 3 per cent vacancy requirement
- ▶ Introduce location requirement, focus on areas with transit and amenities
- ▶ Remove requirement of City of Kelowna housing agreements for projects with long-term(+15 years) agreements with BC Housing



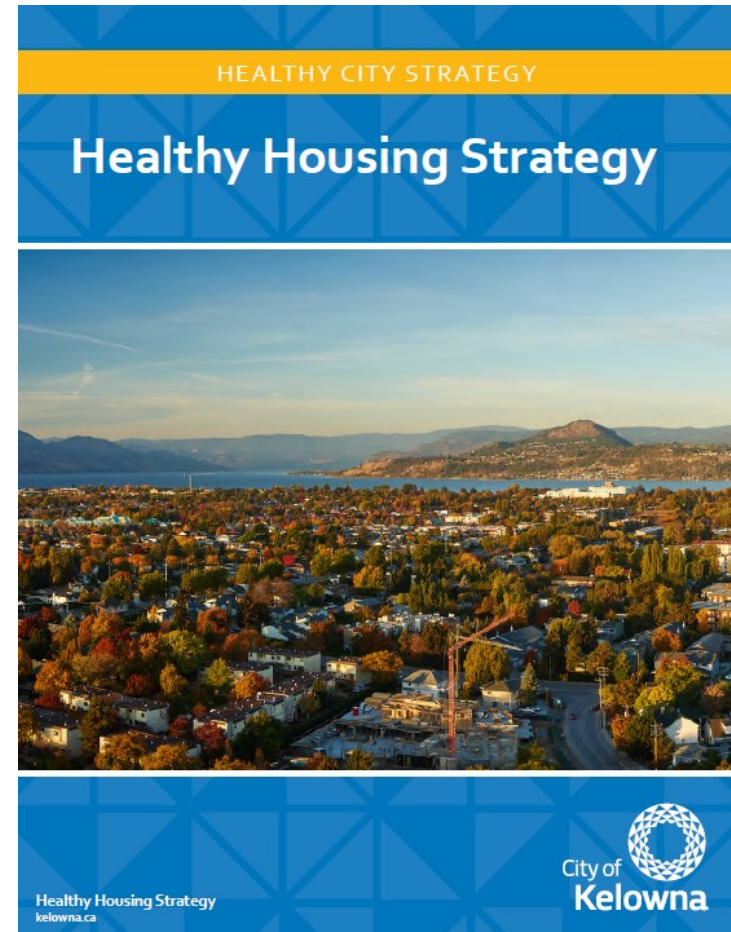
Conclusion

- ▶ Growing proportion of residents rely on long-term rental
- ▶ Implementing Healthy Housing Strategy recommendations
- ▶ Key to promoting investments in attainable & affordable housing options



Next Steps for Healthy Housing

- ▶ Long-term strategy
- ▶ Forthcoming report on rental housing inventory



Attachment A: Summary of Key Changes to Rental Incentives Program

Council Policy 335: Rental Housing Grants Policy

Staff are recommending that the eligibility requirements for the *Rental Housing Grants Program* be changed to focus on the following non-market rental projects:

1. Non-market rental housing units where a non-profit housing provider¹ is the applicant;
2. Non-market rental housing units where a long-term operating agreement is in place between a for-profit developer and a non-profit housing provider for a minimum of 10 per cent of the units within a rental housing project. Only the units that are managed by a non-profit will be eligible for the grant;
3. Non-market rental housing units where a long-term operating agreement (greater than 15 years) is in place between a for-profit developer and the Provincial Rental Housing Corporation (BC Housing);

Staff are recommending that eligibility requirements be updated, requiring all rental housing grants projects to be located in areas with a range of transportation options and amenities in close proximity:

4. Non-market rental housing units located in the Core Area or within the University South or Glenmore Village Centres as defined by the Official Community Plan;

Housing Opportunities Reserve Fund Bylaw 8593

Staff are recommending the following updates to the bylaw:

1. Remove the requirement for rental housing projects to enter into a housing agreement with the City of Kelowna in cases where a long-term operating agreement (greater than 15 years) is in place with the Provincial Rental Housing Corporation (BC Housing);
2. Align definition of Affordable Rental housing with the non-market housing definition proposed for the Rental Housing Grants Council Policy;
3. Require all monies from the Housing Opportunities Reserve Fund to be used for land acquisition in the Core Area, Glenmore Valley Village Centre and the University South Village Centre as defined in the Official Community Plan.

Revitalization Tax Exemption Bylaw 9561

Staff are recommending the following updates to the bylaw to implement the Healthy Housing Strategy and align with the Rental Housing Grants program:

1. Remove the 3 per cent vacancy requirement, allowing eligible purpose-built rental housing projects to apply for the tax exemption anytime;
2. Revise the eligibility criteria for the rental housing tax exemption program to require all eligible purpose-built rental housing projects to be in the Core Area or within the University South or Glenmore Village Centres as defined by the Official Community Plan;
3. Remove the requirement for rental housing projects to enter into a housing agreement with the City of Kelowna in cases where a long-term operating agreement (greater than 15 years) is in place with the Provincial Rental Housing Corporation (BC Housing).

¹. Non-profit housing providers are associations, clubs, or societies that are organized and operated exclusively for affordable housing, social welfare, civic improvement, or any other purpose except profit.



City of Kelowna
1435 Water Street
Kelowna, BC V1Y 1J4
250 469-8500
kelowna.ca

Council Policy

Rental Housing Grants Eligibility

ESTABLISHED: 07/02/05

Contact Department: Policy & Planning

Guiding Principle

The Rental Housing Grants Eligibility Policy aligns with the goal to encourage the construction of affordable long-term rental housing as established by the Healthy Housing Strategy, endorsed in June 2018.

Purpose

The policy describes the management of the Rental Housing Grants Program, establishing who is eligible to apply for the grants and how the development cost-charge credits are disbursed annually.

Application

To be eligible for the grant funding, affordable rental projects must be located within the Core Area, Glenmore Valley Village Centre or the University South Village Centre as defined by the OCP Bylaw No. 10500 and meet one of the following requirements:

- Non-market rental housing units where a non-profit housing provider¹ is the applicant;
- Non-market rental housing units where a long-term operating agreement² is in place between a for-profit developer and a non-profit housing provider for a minimum of 10 per cent of the total units within any individual rental housing project. Only the units that are managed by a non-profit are eligible for the grant;
- Non-market rental housing units where a long-term operating agreement is in place between a for-profit developer and the Provincial Rental Housing Corporation (BC Housing);

Micro-suite units will not be eligible to receive the rental housing grants as they are not required to pay development cost-charges;

Policy Statements

1. Complete applications must be submitted to the Policy & Planning Department by October 31st of each year;
2. The Policy & Planning Department will review all applications and prepare a report to Council no later than January 31st of the subsequent year, summarizing the recommended eligible dwelling units for rental housing grants;
3. All rental housing grant funding allocations must be approved by Council;
4. Any funds that are not paid out in grants in a given year will be added to the Housing Opportunities Reserve Fund to be used for other purposes set out in the HORF Bylaw No. 8593.
5. Grants may be provided to housing providers up to a maximum level of:
 - a) \$8,000 per three-bedroom unit,
 - b) \$4,000 per two-bedroom unit,
 - c) \$2,000 per one-bedroom or bachelor unit.
6. Projects where the City has contributed land at little or no cost, by lease or sale, in order to secure affordable housing, would qualify to apply for the funding identified under 5 (a) (b) and (c) above, up to the maximum dollar amount

¹ Non-profit housing providers are associations, clubs, or societies that are organized and operated exclusively for affordable housing, social welfare, civic improvement, or any other purpose except profit.

² Operating agreement must be for a minimum of 15 years.

equivalent to the total development cost charges payable, less the market value of the land contributed by the City;

7. The amount available per dwelling unit will be determined based on the number of eligible units that have applied and the amount of funds allocated through the annual budget process and available in the given year;
8. Consideration of each application for funding is contingent on there being a reasonable expectation that the eligible units will be constructed in the calendar year of approval. Where a development is being phased, each distinct phase will be treated as a separate application;
9. If dwellings authorized for funding under this policy do not reach building permit stage in the year that the funding was authorized, a carryover request may be accepted for up to a maximum of one year following the year the grants were authorized.
10. At the time of building permit issuance, the authorized funding will be applied as a deduction from the total development cost charges payable;
11. Projects receiving housing grants will be required to secure their dwelling units through a housing agreement with the City of Kelowna. In cases where the project has a long-term (15 years or greater) operating agreement in place with the Provincial Rental Housing Corporation (BC Housing), a housing agreement with the City of Kelowna will not be required;

Amendments

Resolutions listed here with associated action



City of Kelowna
1435 Water Street
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250 469-8500
kelowna.ca

Council Policy

Rental Housing Grants Policy

APPROVED July 24, 2006

RESOLUTION: R450/12/05/14

REPLACING: R143/07/02/05, R738/06/07/24

DATE OF LAST REVIEW: May 2012

1. Applications for funding under this policy will be accepted until September 30th of each year. Applications should be submitted to the Community Planning Manager;
2. All applications will be reviewed and a report to Council will summarize eligible dwellings for rental housing grants by October 31st annually;
3. Funding to be awarded will be determined by April 30th each year;
4. Sources of funding will be an amount allocated through the annual City Financial Plan for the Housing Opportunities Reserve Fund and an annually budgeted amount to provide some relief from development cost charges for affordable rental housing;
5. Funds allocated annually from taxation to the Housing Opportunities Reserve Fund (HORF) will be awarded up to the maximum annual budget allocation. Any funds that are not paid out in grants in a given year will return to the HORF to be used for other purposes set out in the HORF By-law;
6. Un-used portions of the funding for the grants assigned to provide additional relief from Development Cost Charges in any given year will be carried over and added to the next year's budget for rental housing grants;
7. Funds may be awarded as follows, subject to the amount of money allocated annually:
 - (a) Up to \$5,000 per dwelling that qualifies as affordable rental housing, as defined in the City's Official Community Plan;
 - (b) An additional amount that, when combined with funds provided under part (a), is not to exceed the equivalent of the applicable development cost charge per unit of rental housing;
 - (c) All units that qualify for funding, based on the City's current definitions of affordable rental housing or core needs housing in the Official Community Plan, will need to be identified by a housing *agreement with the City registered on the title of the property to confirm that the units will remain as affordable, rental housing.*
8. The amount available per dwelling unit, up to the identified maximums defined in 7, above, will be determined based on the number of eligible units that have been applied for and the amount of money available in the given year;
9. Projects where the City has contributed land at little or no cost, by lease or sale, in order to secure affordable housing, would qualify to apply for the funding identified under 7 (a) (b) and (c), above, up to a maximum dollar amount equivalent to the total development cost charges payable, less the market value of the land contributed by the City;
10. Consideration of each application for funding is contingent on there being a reasonable expectation that the eligible units will be constructed in the calendar year of approval. Where a development is being phased, each distinct phase will be treated as a separate application;
11. If dwellings authorized for funding under this policy do not reach building permit stage in the year that the funding was authorized, the applicant can apply to have the authorized amount carried over to the next year. Carryover requests will only be accepted for one year following the year the grants were authorized. Any carryover requests beyond one year will be treated as a new application for funding;
12. Authorization of funding and the amount applicable will be confirmed in writing by a letter from the City;

13. At time of building permit issuance, the authorized funding will be applied as a deduction from the total development cost charges payable;
14. The Financial Services Department will track funding under this policy against budget information annually and advise the Community Planning Manager accordingly;
15. As set out in 4.4 of the Housing Opportunities Reserve Fund By-law 8593, any request to lift the notice of a housing agreement from a rental dwelling that received a grant under this policy will be subject to repayment of the grant to the City directed to the Housing Opportunities Reserve Fund.
16. The Community Planning Manager will report to Council on the distribution of available funding by April 30th , each year.

REASON FOR POLICY

Long term (“purpose-built”) rental housing is in short supply in Kelowna. If secured by a housing agreement, the OCP defines it as a form of affordable housing. This Policy provides the City with resources to partner with senior government and private sector funding to assist in the supply of lower cost housing. The City has a defined and significant need for affordable rental housing. The goal is to maximize affordable rental units that can be generated with this approach where other sources of funding may be subject to a contribution by the City.

LEGISLATIVE AUTHORITY

Section 905 of the Local Government Act sets the parameters for housing agreements. Sections 188 and 189 of the Community Charter governs the use of reserve funds. Policies in the City’s Official Community Plan define housing affordability and guide City initiatives for affordable housing. By-law 8593 - A Bylaw to Establish a Housing Opportunities Reserve Fund governs the source of funding for the reserve, defines affordable housing and provides direction for spending of these funds.

PROCEDURE FOR IMPLEMENTATION

Through the Community Planning Manager annually as authorized by Council through this policy.

CITY OF KELOWNA

BYLAW NO. 11911

Amendment No. 6 to Housing Opportunities Reserve Fund Bylaw No. 8593

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Housing Opportunities Reserve Fund Bylaw No. 8593 be amended as follows:

1. THAT **Section 1 – TITLE**, 1.2 be amended by:

a) deleting sub-paragraph (a) that reads:

“(a) in the case of rental housing:

- i. that which is available for rent at or below the average rent for Kelowna, as determined annually by the Canada Mortgage and Housing Corporation’s “Rental Market Report”; or
- ii. Purpose-Built Rental Housing. A self-contained building(s) containing five or more Dwelling Units that are intended to be used for rental housing and does not include buildings that are stratified. Purpose-built rental housing meets an identified need for affordable housing in the city. Since rent is controlled within rental buildings under the Residential Tenancy Act, this is a form of affordable housing.”

And replace it with the following:

“(a) in the case of rental housing means a self-contained building(s) containing five or more Dwelling Units that are intended to be used for non-market rental housing that meets one of the following descriptions:

- i. Non-market rental housing units where a non-profit housing provider is the applicant;
- ii. Non-market rental housing units where a long-term operating agreement is in place between a for-profit developer and a non-profit housing provider for a minimum of 10 per cent of the total units within any individual rental housing project. Only the units that are managed by a non-profit are eligible for the grant.
- iii. Non-market rental housing units where a long-term operating agreement is in place between a for-profit developer and BC Housing.”

b) deleting the “(a)” before “in the case of owner-occupied housing” and replace it with (b);

c) deleting the definition ‘**Core Needs Housing**’ that reads:

‘**Core Needs Housing**’ means housing for households that, based on data from Statistics Canada or Canada Mortgage and Housing Corporation (CMHC) or British Columbia Housing Management Corporation (BCMHC), meet one or all of the following criteria:

- (a) household meets the definitions of core needs used by the British Columbia Housing Management Commission for Kelowna;
- (b) household income falls at or below Low Income Cut-Off level defined by Statistics Canada for urban areas greater than 100,000 people, based on household size;
- (c) household is receiving BC income assistance for Welfare to Work or Disability Benefits or qualifies for Targeted Rent Subsidy Programs (TRSP) operated by BC Housing, or dwelling is built with senior government funding to be affordable.

2. AND THAT **Section 4 USE OF RESERVE MONIES**, be amended by:

a) deleting in sub-section 4.1 (a) the following:

"(a) within, or in proximity, to an Urban Town Centre as defined in the **Official Community Plan**;
and

and replacing it with;

"(a) Within the Core Area, Glenmore Valley Village Centre or the University South Village Centre as defined in the **Official Community Plan**; and"

b) adding a new sub-section 4.6 in its appropriate location that reads:

"Projects receiving housing grants will be required to secure their dwelling units through a 10-year housing agreement with the City of Kelowna. In cases where the project has a long-term operating agreement in place with BC Housing, an agreement with the City of Kelowna will not be required."

3. This bylaw may be cited for all purposes as "Bylaw No. 11911, being Amendment No. 6 to Housing Opportunities Reserve Fund Bylaw No. 8593."

4. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

CITY OF KELOWNA

BYLAW NO. 11912

Amendment No. 7 to Revitalization Tax Exemption Program Bylaw No. 9561

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Revitalization Tax Exemption Bylaw No. 9561 be amended as follows:

1. THAT Section 4 be amended by deleting the phrase "entered into" following "relevant Agreement" and before "between the City".

2. AND THAT Section 5, sub-paragraph e be deleted that reads:

"For Purpose-Built Rental Housing Projects throughout the City, 100% of the Revitalization Amount on the parcel, for projects that are subject to a Housing Agreement (for up to 10 years) and where the proposed project is in compliance with the Official Community Plan Future Land Use designation at the time of the Revitalization Tax Exemption application. A tax incentive for rental housing will only be considered when the vacancy rate is at or below 3%,"

And replace it with;

"For Purpose-Built Rental Housing Projects within the Core Area, Glenmore Valley Village Centre and University South Village Centre as defined by the Official Community Plan (OCP), 100% of the Revitalization Amount on the parcel, for projects that are subject to a Housing Agreement (for a minimum of 10 years) and where the proposed project is in compliance with the OCP Future Land Use designation at the time of Revitalization Tax Exemption application."

3. AND THAT Section 6, be amended by adding a new sub-paragraph h in its appropriate location that reads:

"Projects pursuing exemptions for purpose-built rental housing will be required to secure their dwelling units through a 10-year housing agreement with the City of Kelowna. In cases where the project has a long-term operating agreement (15 years or greater) in place with the Provincial Rental Housing Corporation (BC Housing), an agreement with the City of Kelowna will not be required."

4. AND THAT **SCHEDULE "B" Revitalization Tax Exemption Agreement** be amended by deleting in No. 6, sub-paragraph e that reads:

"For Purpose-Built Rental Housing Projects throughout the City, 100% of the Revitalization Amount on the Parcel where the project is subject to a Housing Agreement (for up to 10 years) and is in compliance with the OCP Future Land Use designation as at May 30, 2011. A tax incentive for rental housing will only be considered when the vacancy rate is at or below 3%,"

And replace it with;

"For Purpose-Built Rental Housing Projects within the Core Area, Glenmore Valley Village Centre and University South Village Centre as defined by the OCP, 100% of the Revitalization Amount on the parcel, for projects that are subject to a Housing Agreement (for a minimum of 10 years) and where the proposed

project is in compliance with the OCP Future Land Use designation at the time of Revitalization Tax Exemption application."

5. AND THAT **SCHEDULE "C" Tax Exemption Certificate** be deleted in its entirety and replaced with a new Schedule "C" Tax Exemption Certificate.
6. This bylaw may be cited for all purposes as "Bylaw No. 11912, being Amendment No. 7 to Revitalization Tax Exemption Program Bylaw No. 9561."
7. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

Schedule "C"

Revitalization Tax Exemption Agreement No. _____

Date of Issuance by Revenue Department _____

Tax Exemption Certificate

In accordance with the City of Kelowna Revitalization Tax Exemption Program Bylaw No. 9561 (the "Bylaw"), and in accordance with a Revitalization Tax Exemption Agreement dated for reference the ____ day of _____, 20__ (the "Agreement") entered into between the City of Kelowna (the "City") and _____ (the "Owner"), the registered owner(s) of [insert legal description of property] _____ (the "Parcel):

A.) This certificate certifies that the Parcel is subject to a Revitalization Tax Exemption, for each of the taxation years 20__ to 20__ inclusive, equal to [\[choose one from below and insert applicable wording\]](#):

1. "Tax Incentive Area 1", 100% of the Revitalization Amount attributed to Building Permit No _____ between 20__ (the calendar year before the commencement of construction of the project) and 20__ (the calendar year in which the Revitalization Tax Exemption Certificate is issued);
2. "Tax Incentive Area 2,"
 - a. 100% of the Revitalization Amount attributed to Building Permit No _____ between 20__ (the calendar year before the commencement of construction of the project) and 20__ (the calendar year in which the Revitalization Tax Exemption Certificate is issued);
 - b. 75% of the Revitalization Amount attributed to Building Permit No _____ between 20__ (the calendar year before the commencement of construction of the project) and 20__ (the calendar year in which the Revitalization Tax Exemption Certificate is issued) which can be attributed to a residential land use,

and/or 50% of the Revitalization Amount attributed to Building Permit No _____ between 20__ (the calendar year before the commencement of construction of the project) and 20__ (the calendar year in which the Revitalization Tax Exemption Certificate is issued) which can be attributed to a commercial land use;
3. [deleted]
4. "Tax Incentive Area 3," 100% of the Revitalization Amount attributed to Building Permit No _____ between 20__ (the calendar year before the commencement of construction of the project) and 20__ (the calendar year in which the Revitalization Tax Exemption Certificate is issued);
5. Purpose-Built Rental Housing Project, 100% of the Revitalization Amount attributed to Building Permit No _____ between 20__ (the calendar year before the commencement of construction of the project) and 20__ (the calendar year in which the Revitalization Tax Exemption Certificate is issued).

B.) Any construction of a new improvement or alteration of an existing improvement, on the Parcel described above, undertaken prior to the application for a Revitalization Tax Exemption will not be eligible for consideration;

C.) The maximum Revitalization Tax Exemption authorized must not exceed the increase in the assessed value of improvements on the property resulting from the construction or alterations attributed to Building Permit No

_____ between 20__ (the calendar year before the commencement of construction of the project) and 20__ (the calendar year in which the Revitalization Tax Exemption Certificate is issued);

D.) The Property's assessed value of improvements must not be reduced below the amount assessed in the calendar year prior to construction or alteration, as a result of the Revitalization Tax Exemption.

The Revitalization Tax Exemption is provided under the following conditions:

1. The Owner does not breach any term, condition or provision of, and performs all obligations set out in, the Agreement and the Bylaw;
2. The Owner has not sold all or any portion of his or her equitable or legal fee simple interest in the Parcel without the transferee taking an assignment of the Agreement, and agreeing to be bound by it;
3. The Owner, or a successor in title to the Owner, has not allowed the property taxes for the Parcel to go into arrears or to become delinquent;
4. The Exempt Use (as defined in the Agreement) of the Project is not discontinued.

E.) If the Owner is subject to an operating agreement with the Provincial Rental Housing Corporation, the owner must comply with the terms of the operating agreement with the Provincial Rental Housing Corporation.

F.) If any of these conditions are not met, the Council of the City of Kelowna may cancel this Revitalization Tax Exemption Certificate. If such cancellation occurs, the Owner of the Parcel, or a successor in title to the Owner as the case may be, shall remit to the City an amount equal to the value of the exemption received after the date of the cancellation of the certificate.

Report to Council



Date: August 12, 2019
File: 0600-20
To: Council
From: City Manager
Subject: Subdivision, Development and Servicing Bylaw 7900 - Schedule 4 and 5 Update - Stormwater

Recommendation:

THAT Council receives, for information, the report from the Infrastructure Engineering Manager dated August 12, 2019, with respect to amending the Subdivision, Development and Servicing Bylaw 7900;

AND THAT Bylaw No. 11913, being Amendment No. 20 to Subdivision, Development and Servicing Bylaw No. 7900 be forwarded for reading consideration.

Purpose:

To amend the Subdivision, Development and Servicing Bylaw 7900 Design so it aligns with industry best practice and ensure construction of quality and long-lasting infrastructure.

Background:

Schedules 4 and 5 of Bylaw 7900 Subdivision, Development & Servicing Bylaw set the minimum standards and specifications for works and services in connection with developing and servicing lands within the City boundaries. Schedule 4 outlines the design standards and Schedule 5 outlines the construction standards for municipal infrastructure like water distribution, sanitary sewer, drainage, roads, sidewalks, traffic signals, street lighting and landscaping. Staff's goal is to maintain Bylaw 7900 as a living document

The City periodically updates these standards and provide regular updates to assure that the infrastructure installed is high quality, long lasting and supports service delivery expectations from our community. These standards are also reviewed against industry best practice and practices in other communities to ensure the City is obtaining best value.

A working committee was formed with representation from various internal groups, including Infrastructure Engineering and Delivery, Development Engineering and Civic Operations. The working

group meets regularly to review sections of the Bylaw, engage with internal and external stakeholders (like the Urban Development Institute) and decide on changes to bring forward to Council. The changes identified below form part of the major components as part of this revision.

Summary of changes to Schedule 4 of Bylaw 7900 – Design Standards include:

Chapter 0 - General

1. Minor grammatical changes.
2. Change to chapter 3 – Stormwater Management in title.

Chapter 1 - Water

1. Section 1.5 – Fire Flows
 - This change to on-site Fire Flow requirements was required to address building fire flow requirements at the building permit approvals stage. Off-site infrastructure sizing will remain based on area zoning. On-site fire flow requirements at the Building Permit stage are now consistent with Section 3.2.5.7 of the BC Building Code. This modification is significant for those buildings with automated sprinkler systems constructed to NFPA 13 criteria. The calculated value must be confirmed to the City by an experienced professional. The Owner or Developer must report to the City that the calculated fire flow does not exceed the minimum requirements for that zoned area.

Chapter 3 – Stormwater Management

1. The old Chapter 4 – Drainage in previous version has been replaced and revised.
2. The new Chapter 3 is now called Stormwater Management. The section is formatted for consistency with the Master Municipal Construction Documents (MMCD) Design Guidelines.
3. Climate change is now incorporated into the standard. Stormwater quantities will include a rainfall intensity increase of 15% in the calculations. This will confirm the capacity of critical structures and assure that minimum standard sizing is adequate for such items as mainlines, structures or culverts.
4. Stormwater quality has more emphasis with measures connected to health of Okanagan Lake, our main City water supply. Some of the important features include:
 - a. No permanent pumping of groundwater will be permitted into the City sewer system. Where permanent pumping cannot be avoided, water licensing and separate private works will require approval from both the City and the Province.
 - b. Water quality will be measured against Provincial recreational water quality guidelines.
 - c. No construction will be permitted below the proven high-water table.
5. Hillside or Hazardous Area infrastructure requirements have been changed to focus on safe conveyance of stormwater. This includes
 - a. Eliminating infiltration to ground in most instances.
 - b. All roof leaders and site drainage must connect to the City storm system.
 - c. Reliance on hydrogeologists has been reduced to special applications.
 - d. A new map has been added to identify areas of allowable infiltration to ground.
6. All stormwater design must clearly demonstrate a path that does not impact downstream properties.
7. Erosion and Sediment Control Guidelines have been strengthened to assure that responsibilities on construction sites is clearly defined.
8. Stormwater infrastructure details have been updated. This includes, but not limited to:

- a. Minor Systems: manholes, catch basins, service connections, mainline sizing, roof leaders, curved sewers, modelling requirements,
- b. Detention Controls: parking lot storage, underground storage, detention ponds, outlet Controls, etc.

Summary of changes to Schedule 5 of Bylaw 7900 – Construction Standards include:

- SS-S50 - Catch Basin Lead Sizing – Chart adjusted for minimum 200 mm leads.
- SS-S56 - IDF Curves – City of Kelowna at Kelowna International Airport (YLW) - Revised intensity duration curves from Environment Canada (generated from 2014 data).
- SS-S58 - Groundwater Recharge Suitability Map

The working group continues to work closely to align other service areas like roads, traffic signals, landscaping, agricultural irrigation and wastewater with MMCD where practical. Updates on this work will be brought forward to Council at a later date.

Internal Circulation:

City Clerk
Community Engagement Manager
Development Engineering Manager
Infrastructure Delivery Department Manager
Infrastructure Operations Department Manager
Legislative Coordinator
Project Manager - Water Integration Project
Utility Planning Manager

Legal/Statutory Authority:

Community Charter section 8.

Considerations not applicable to this report:

Legal/Statutory Procedural Requirements
Existing Policy
Financial/Budgetary Considerations
Personnel Implication
Communications Comments
Alternate Recommendation

Submitted by:

Joel Shaw, P.Eng., Infrastructure Engineering Manager

Approved for inclusion:



A. Newcombe, Division Director, Infrastructure

Attachment 1 – Amendment 20 – Revised Bylaw 7900

Attachment 2 – Bylaw 7900 – Schedule 4 and 5 Update Presentation

cc: City Clerk

Bylaw 7900 – Subdivision, Development and Servicing Bylaw 2019 Updates

Joel Shaw, P. Eng. & Rod MacLean, P. Eng. | August 12, 2019

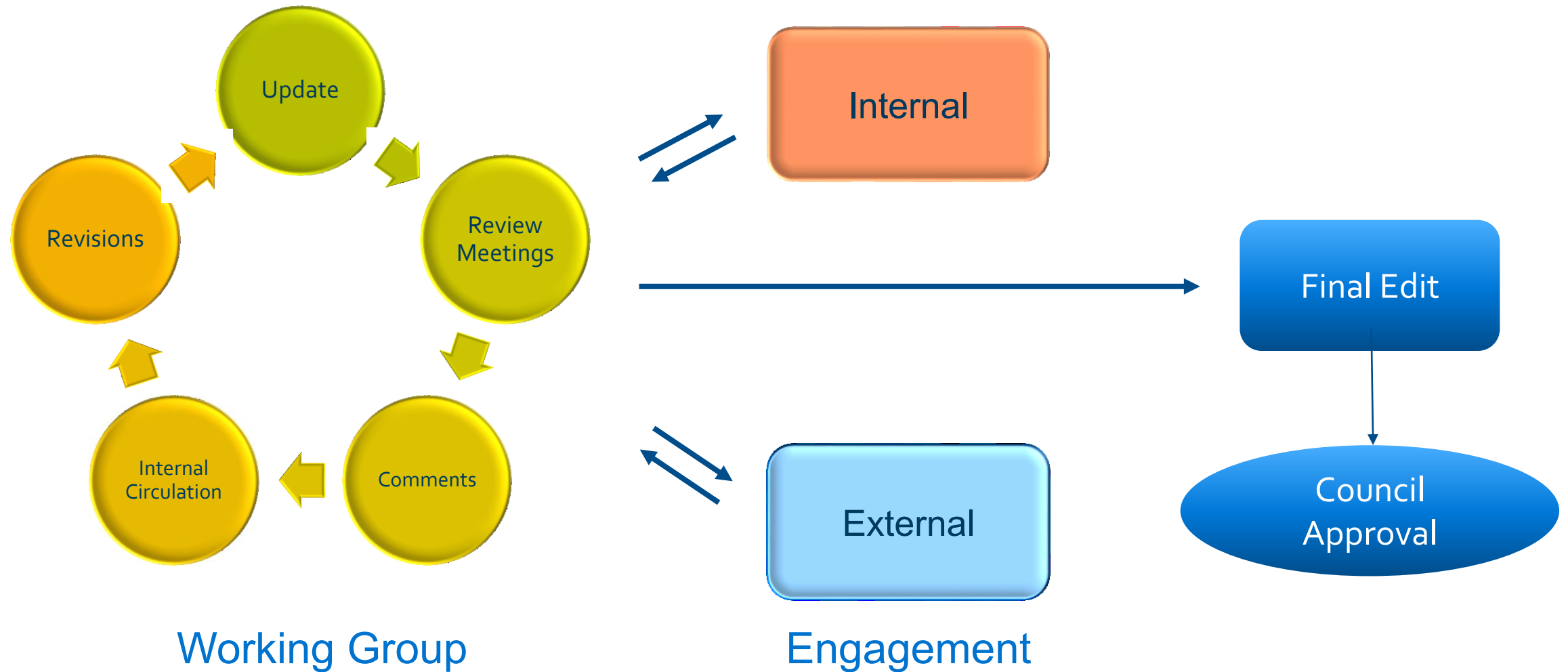
Subdivision, Development and Servicing (Bylaw 7900)

- ▶ Sets out subdivision application procedures
- ▶ Provides Design and Construction Standards for works and services
 - ▶ Schedule 4 – Design Standards
 - ▶ Schedule 5 – Construction Standards
- ▶ Approved Products List (Policy 266)

Objectives

- ▶ Update Design and Construction Standards in Bylaw 7900
 - ▶ Format to Municipal Infrastructure Design Guidelines - (MMCD, 2014)
- ▶ Ensure quality and long-lasting infrastructure
- ▶ “Living Document”
- ▶ Complements:
 - ▶ MMCD Specifications and Standard Detailed Drawings.
 - ▶ City - Schedule 5 – Supplementary Specifications and Supplementary Standard Detailed Drawings.
 - ▶ City - Policy 265 (Engineering Drawing Submission Requirements).

Bylaw 7900 Review Workflow



Design and Construction Standards

- ▶ Water
- ▶ Sanitary Sewer
- ▶ Drainage
- ▶ Transportation
- ▶ Electrical, Street Lighting
- ▶ Landscaping & Irrigation
- ▶ Hillside Development Standards

Design and Construction Standards

- ▶ Water
- ▶ Sanitary Sewer
- ▶ ~~Drainage~~ Stormwater Management
- ▶ Transportation
- ▶ Electrical, Street Lighting
- ▶ Landscaping & Irrigation
- ▶ Hillside Development Standards

Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ Improve Water Quality
- ▶ Address Climate Change
- ▶ Hillside and Hazardous Slope Issues



Focus in 2019 - Stormwater

► Protect Okanagan Lake

- Route minor systems to natural watercourses
- Eliminate permanent groundwater pumping into the City storm sewer.
 - Reduce capacity issues near outlets to lake
 - Eliminate reliance on deeper water table
 - Improve groundwater quality into Okanagan Lake.

► Improve water quality

► Address Climate Change

► Hillside and Hazardous Slope Issues



Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ **Improve water quality**
 - ▶ Emphasis on water quality to Okanagan Lake.
 - ▶ Must meet Provincial (MOE) Recreational Water Quality Guidelines.
 - ▶ Control manholes added
 - ▶ Expanded Erosion & Sediment Control process
- ▶ Address Climate Change
- ▶ Hillside and Hazardous Slope Issues



Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ Improve water quality
- ▶ **Address Climate Change**
 - ▶ Consultant to consider climate change in calculations
 - ▶ Flow conditions/debris flow
 - ▶ Minimum 15% capacity increase
 - ▶ Response required.
- ▶ Hillside and Hazardous Slope Issues



Focus in 2019 - Stormwater

- ▶ Protect Okanagan Lake
- ▶ Improve water quality
- ▶ Address climate change
- ▶ **Hillside and Hazardous Slope Issues**
 - ▶ Focus on safe conveyance of stormwater to natural watercourse
 - ▶ Roof/site drainage direct to Storm
 - ▶ No infiltration to ground (except foundation)



Focus in 2019

- ▶ Fire Flows (Section 1.5)
 - ▶ Development Permit Stage
 - ▶ No change – Use table.

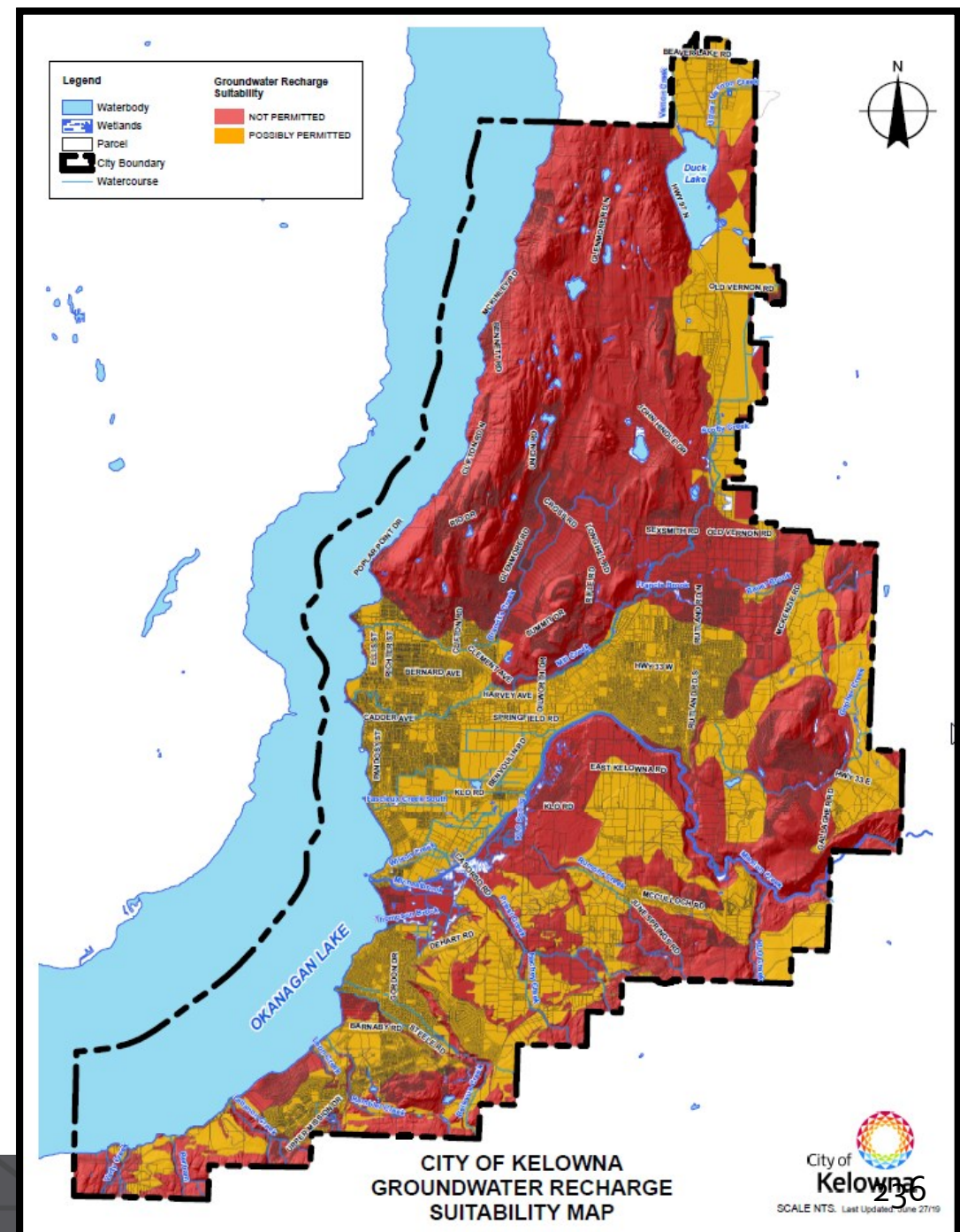
- ▶ Building Permit Stage
 - ▶ Section 3.2.5.7 of the BC Building Code.
 - ▶ Automated sprinkler systems to NFPA 13
 - ▶ Confirmation required.
 - ▶ Calculated fire flow must not exceed the minimum requirements for that zoning



Drawing Updates

New

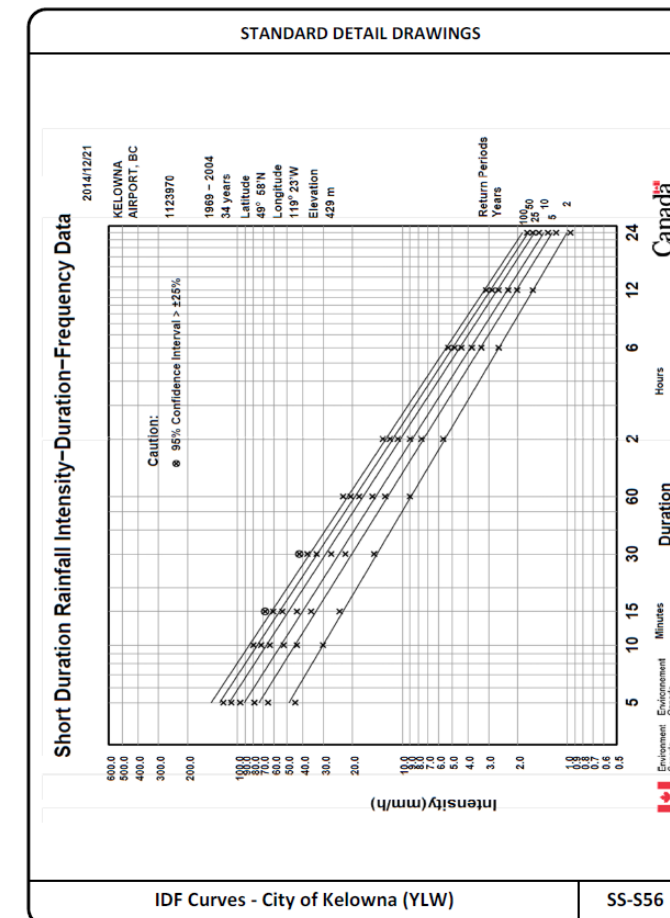
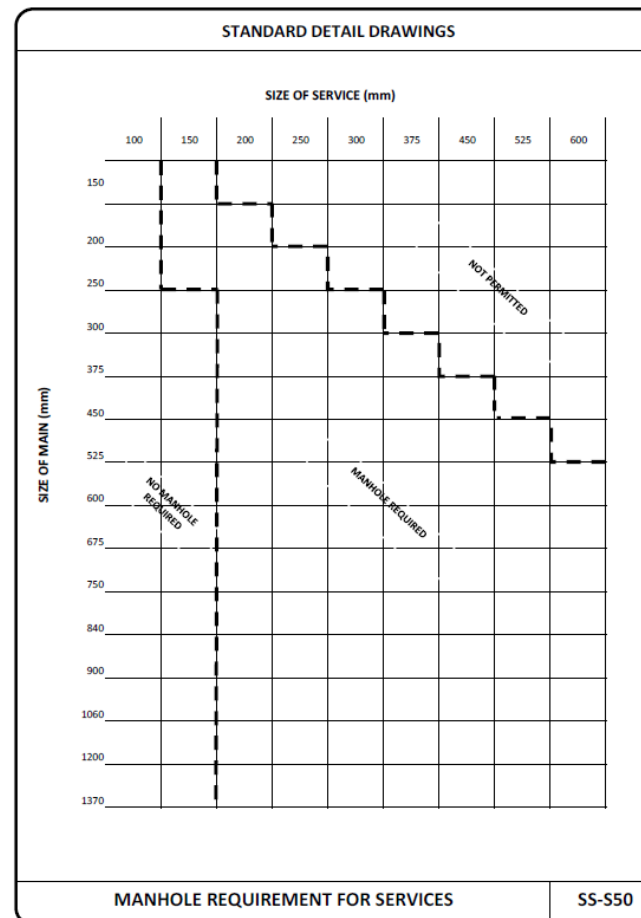
- Map identifying where Infiltration to Ground is suitable.



Drawing Updates

Updated

- IDF Curve
- Service/Manholes Sizing



Thank you



CITY OF KELOWNA

BYLAW NO. 11913

Amendment No. 20 to Subdivision, Development and Servicing Bylaw No. 7900

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Subdivision, Development and Servicing Bylaw No. 7900 be amended as follows:

1. THAT **SCHEDULE 4 – DESIGN STANDARDS**, title page be amended by deleting the title for Section 3 that reads "**3. DRAINAGE**" and replace it with a new title that reads "**3. STORMWATER MANAGEMENT**";
2. AND THAT **SCHEDULE 4 – DESIGN STANDARDS, Section 1 – Water Distribution, 1.5 Fire Flows** be deleted that reads:

"1.5 Fire Flows

Fire flows shall be determined in accordance with the requirements of the current edition of "Water Supply for Public Fire Protection - A Guide to Recommended Practice", published by Fire Underwriters Survey.

The following minimum fire flows must be met for the noted zones under peak daily flow conditions (Table 1.5):

Table 1.5 Minimum Fire Flow Requirements

Developments (without sprinklers)	Minimum Fire Flow
Single Family & Two Dwelling Residential	60 L/s
Modular / Mobile Home	60 L/s
Three & Four Plex Housing	90 L/s
Apartments, Townhouses	150 L/s
Commercial	150 L/s
Institutional	150 L/s
Industrial	225 L/s

The Design shall not use a fire flow greater than those listed in Table 1.5 to design their onsite fire protection systems. The maximum available fire flow for site development is the lesser of the actual available fire flow at the service connection or the fire flows in Table 1.5.

Subdivisions and main extensions may utilize hydraulic information from water model as provided by the City.

Actual required fire flows shall be determined for all new developments.”

And replacing it with:

“1.5 Fire Flows

Fire flows are subject to the following minimum requirements (Table 1.5) for all offsite works.

Table 1.5 Minimum Required Fire Flow by Zoning Designation

General Zoning Designation	Minimum Fire Flow*
Single Family & Two Dwelling Residential	60 L/s
Modular / Mobile Home	60 L/s
Three & Four Plex Housing	90 L/s
Apartments, Townhouses	150 L/s
Commercial	150 L/s
Institutional	150 L/s
Industrial	225 L/s

*Off-site fire flow requirements are calculated in accordance with the requirements of the current edition of "Water Supply for Public Fire Protection - A Guide to Recommended Practice", published by Fire Underwriters Survey.

Subdivisions and main extensions must utilize hydraulic information from water model results provided by the City.

Onsite requirements are defined during the Building Permit process:

- a) Fire flow requirements for structures are to be calculated based on the worst-case requirement consistent with Section 3.2.5.7 of the BC Building Code.
- b) Where a structure design includes an automated sprinkler system to NFPA 13 as per Section 3.2.5.12 of the BC Building Code, then:
 - i. The NFPA 13 fire flow result for the worst-case building shall be the fire flow requirement on site.
 - ii. Confirmation of meeting the NFPA 13 requirement must be provided to the City.
- c) The Owner or Developer must report to the City that the calculated fire flow does not exceed the minimum requirements for that zoning found in Table 1.5.”

3. AND THAT **SCHEDULE 4 – DESIGN STANDARDS**, Section 3 - be deleted in its entirety and replaced with a new Section 3 – Stormwater Management as attached to and forming part of this bylaw as Appendix A;
4. AND THAT **SCHEDULE 5 – CONSTRUCTION STANDARDS, 2. STANDARD DRAWINGS**, be amended by deleting the standard detailed drawings for **MANHOLE REQUIREMENT FOR SERVICES DRAWING SS-S50 and IDF Curves – City of Kelowna (YLW) SS-S56** and replacing the standard detailed drawings for **MANHOLE REQUIREMENT FOR SERVICES DRAWING SS-S50 and IDF Curves – City of Kelowna (YLW) SS-S56** as attached to and forming part of this bylaw as Appendix B and C;
5. AND THAT **SCHEDULE 5 – CONSTRUCTION STANDARDS, 2. STANDARD DRAWINGS** be amended by adding a standard detailed drawing for **GROUNDWATER RECHARGE SUITABILITY MAP DRAWING SS-S58** as attached to and forming part of this bylaw as Appendix D;
6. This bylaw may be cited for all purposes as "Bylaw No.11913, being Amendment No. 20 to Subdivision, Development and Servicing Bylaw No. 7900."
7. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

3 Stormwater Management

3.1 General

3.2 Stormwater Flow Control

3.3 On-Site Stormwater Management and Practice

3.4 Runoff Analysis

3.5 Site and Lot Grading

3.6 Minimum Building Elevations (MBE)

3.7 Rational Method

3.8 Hydrograph Method

3.9 Minor System Design

3.10 Major System Design

3.11 Runoff Controls

3.12 Outlet Controls

3.13 Drainage Pump Stations

3.14 Erosion and Sediment Control (ESC)

3 Stormwater Management

3.1 General

The City stormwater system integrates surface water flows collected through the City's infrastructure and the natural watercourses that flow into Okanagan Lake. Proper integrated stormwater management practice mitigates impacts with the goal of maintaining Okanagan Lake as a high quality water source, with an abundant water supply, and with a balanced ecosystem. While urban, agricultural and natural areas all benefit from Okanagan Lake, drainage impacts from our systems must be mitigated, as well as be resilient to flood hazard and a changing climate.

The presence of an existing stormwater management facility does not imply that there is adequate capacity to receive the design flow, nor does it imply the facility is necessarily acceptable to the City. Where required, stormwater facilities must be upgraded to accommodate the appropriate flow as specified in this standard.

3.1.1 Outcomes

With respect to stormwater, the City's goals are to:

- a) Improve and protect water quality from creek flows, outfalls and groundwater entering Okanagan Lake.
- b) Reduce the risk of health hazard, life, and damage to property and infrastructure from

- flooding, and provide strategies to attenuate peak flows and volumes.
- c) Preserve and protect aquatic and riparian habitat and provide opportunity for restoration.
 - d) Minimize risks to the Okanagan Lake drinking water source.
 - e) Increase the resiliency of our watersheds to climate change impacts.

This stormwater management standard applies the latest Best Management Practices (BMP) and processes in use in British Columbia. New systems and development within the City are to use the practices described within this Section as a *minimum* standard.

All flows must be routed through sewer pipe, ditching, water courses, riparian areas, or road allowances with the required capacity and right of way access for operation and maintenance. The City requires that major system flows must be safely routed downstream to an adequately sized municipal drain or natural watercourse without impacting private property.

3.1.2 Regulations

Stormwater management designs must conform to this standard, City of Kelowna bylaws, regulations and policies; in addition to federal and provincial statutes where applicable. These include but are not limited to the following:

- Supplementary Design Criteria
- Existing Master Drainage Plans,
- Local Government Act
- Fisheries Act of BC
- Water Sustainability Act
- BC Water Act
- Navigable Waters Protection Act
- Canada Wildlife Act
- Migratory Birds Convention Act
- Dike Maintenance Act
- Standards and Best Practices for Instream Works (Canada/BC)
- Land Development Guidelines for the Protection of Aquatic Habitat (Canada/BC)
- Urban Runoff Quality Control Guidelines for British Columbia
- National Guide to Sustainable Municipal Infrastructure (Canada)
- Canadian Dam Association Dam Safety Guidelines

3.1.3 Climate Change

The City accepts that climate patterns are changing, and that its customers are impacted by creek flooding, lake rises, temperature fluctuations and fire. The design standards for infrastructure outlined in this bylaw are to be considered a minimum expectation. The City requires that design professionals consider impacts of climate change, through potential changing weather patterns or water levels when implementing a design; particularly in components where critical and long term design decisions are being made, or in areas where the consequence of failure is high.

To account for a changing climate, the capacity of storm works will include an additional 15 percent upward adjustment, and applied to the rainfall intensity curve stage (IDF) in Section 3.7.2. This is consistent with recommendations in EGBC (2018): Legislated Flood Assessments in a Changing Climate in BC.

The design professional will be required to consider debris flow and flow management as a result of higher peak flows.

On larger projects, basin characteristics are required elements of the Stormwater Management Plan (See Section 3.2.1). Developers will need to anticipate this form of analysis as part of their overall cost strategy.

3.1.4 Hillside or Hazardous Areas

Where the City identifies an area in the OCP as a Hillside or Hazardous Development Area, the City focus will be on safe conveyance. Roof or site drainage must discharge directly to the storm system. This City will not permit infiltration to ground except for foundation drainage. Where storm drains are not available, the City will require a hydrogeological review provided by a qualified Professional (P.Eng. or P.Geo.) to ensure that site infiltration does not exceed pre-development conditions. impact slope stability or off-site seepage, or impact downhill properties. The terms of reference and review must be confirmed by the City Engineer and approved as a condition for obtaining a Development Permit.

3.2 Stormwater Flow Control

The City's Stormwater Management system consists of two main components:

- a) **The Minor System** consists of sewer pipes, gutters, catch basins, driveway culverts, open channels, watercourses and storm water management BMPs designed to capture, convey, treat or modify flows up to a 5-year return design event as directed by the City.
- b) **The Major System** consists of surface flood paths, roadways, roadway culverts, watercourses and storm water management facilities designed to capture, convey, treat or modify larger flows up to a 100-year return design event. A piped minor system may be enlarged or supplemented to accommodate major flows. Major roads and arterials, bridges and creek protection armouring are to be designed for the 1 in 200 year event. This is discussed further in Section 3.10.

3.2.1 Stormwater Management Plan

Stormwater Management Plans are required for all municipal development. A plan should include the following:

- a) Tributary areas in the catchment which identify existing and potential land uses or current development.
- b) References to applicable Area Stormwater Drainage Plans.
- c) Details indicating how the proposed site relates to the Master Plan and its recommendations. Contours at 0.5 m elevation intervals.
- d) Conceptual lot grading patterns.

- e) Existing watercourses, including environmental classifications and/ or fish presence information, if available.
- f) Layouts of existing and proposed drainage systems.
- g) Major flow paths to a municipal drain or natural watercourse without impacting private property.
- h) Proposed control features to meet the water quantity and quality targets identified in the applicable Master Plan
- i) Locations, sizes, design flows, volumes, and capacities of all existing and proposed works.
- j) Capacity assessment of receiving downstream works, or reference to the applicable Master Plan demonstrating adequate capacity. The City will provide the required stormwater area plans upon request.
- k) Minor and Major hydraulic grade line elevations on profiles for all proposed works.
- l) Proposed service connection locations and their associated minimum building elevations (MBE). Pre and post development flows both entering and leaving the subject lands.
 - i. Pre development is defined as the natural condition prior to any development changes, including those resulting from past development activities.
- m) The City may exempt plan requirements for development in rural or agricultural areas upon request or determination by the City Engineer.

3.3 On-Site Stormwater Management and Practice

3.3.1 Storm Effluent Limitations to City Storm System

The City will not allow permanent pumping of groundwater into the City storm sewer system and natural watercourses for building structures designed or constructed below the proven high groundwater table.

- a) For structures designed or constructed above the proven high groundwater table, intermittent stormwater pumping will be permissible to the City stormwater system where approved by the City Engineer. All operations and testing must be consistent with the requirements in Sanitary Sewer/Storm Drain Regulation Bylaw 6618.
- b) Where structures are designed or constructed below the proven high groundwater table, groundwater pumping will not be permitted. The City will only approve designs that include provisions for eliminating, over the long term, groundwater penetration into the structure, as well as assurances for addressing buoyancy concerns. These design aspects must be reviewed and approved by the City Engineer.
- c) Refer to the latest BC Building code for drainage discharge requirements in parkades.

3.3.2 Water Quality

Whether water is routed through creeks, pipelines or infiltration into ground, the City will require consideration for treatment, emergency management and maintenance of the

stormwater infrastructure and water quality. Stormwater designs on private property must meet or exceed minimum water quality guidelines prior to entering the City storm system. Water quality for a minor system flow (50% of the 1 in 2-year) must meet minimum BC Ministry of Environment Recreational Water Quality Guidelines and as per Sanitary Sewer/Storm Drain Regulation Bylaw 6618 .

3.3.3 Construction Sites

The City storm system can be used for temporary site water management provided the water quality exiting the property meets BC Ministry of Environmental Protection and Sustainability Recreational Water Quality Guidelines. This temporary use must be approved by the City prior to issuance of the Development Permit and/or Building Permit, following a confirmation of capacity within the downstream system, and adequacy of the quality of storm effluent. There must be no discharge to the sanitary sewer system.

3.3.4 High Density Residential, Commercial and Industrial Storm Systems

- a) A control manhole is to be installed within 3 metres of the property line, and downstream of any water quality enhancement system. The manhole will include provision for isolating runoff into the City Storm system.
- b) The City requires access to the structure in an emergency and inspection. An SROW is required. Provisions must be considered for response to emergency toxic spills on site. Any costs associated with emergency response are the responsibility of the property owner.
- c) Water quality enhancement systems such as oil/grit separators, fuel/water separator (where required), naturalized storm ponds or other approved systems are the responsibility of the site owner, and must be maintained on a regular basis. The City can request regular maintenance records.
- d) Minor system flows must meet water quality guidelines described above prior to discharging to a creek or city storm system.
- e) On industrial sites where perforated storm systems or dry wells are used, the design must include provisions to manage emergency spills on site and minimize groundwater impacts.

3.4 Runoff Analysis

Storm drainage design should be carried out using one or both of the following methods. Calculations are to be submitted with designs.

- a) **Rational Method:** To be used only for hydrologically simple and uniform areas with contributing area less than 10 Ha.
- b) **Hydrograph Method:** Applicable for all larger areas or more hydrologically complex catchments, or where stormwater management systems require more than basic conveyances. Use SWMM based models or approved equivalent to analyze these processes. Each model must include a level of complexity dependent on the watershed and the hydrologic processes that need to be considered (e.g., detention, groundwater recharge and infiltration, evapotranspiration, continuous simulation, etc.).

For all modelling, use the rainfall Intensity Duration Frequency (IDF) curves found in standard drawing **SS-S56**. Both historical data as well as climate change information must be incorporated into the runoff analysis.

3.5 Site and Lot Grading

Grading is to comply with the BC Building Code and the following:

- a) Swales and site drainage must be constructed to prevent ponding within lots, with runoff routed, where possible, to storm services in public streets or other appropriate stormwater management system for the site.
- b) Grade lots to drain to an approved City drainage system or roadway. Use 1% minimum grade. Grading directly to a natural drainage path must include adequate erosion control and water quality improvement measures.
- c) Avoid drainage across adjacent lots. Where cross-lot drainage is unavoidable, provide adequate measures such as channelling, swales, inlets or piped connections to direct flow appropriately. A statutory right of way in favour of the City or private easement is required for unobstructed access.
- d) Positive drainage is required for buildings and foundations.
- e) Set building elevations above the hydraulic grade line (HGL) of the major drainage system as per Minimum Building Elevations (MBE) guidelines below.

3.6 Minimum Building Elevations (MBE)

The MBE applies to the elevation of the lowest floor slab in a building or the underside of the floor joists where the lowest floor is constructed over a crawl space. Crawl space is defined as the space between a floor and the underlying ground having a maximum height of 1.2 m to the underside of the joists and not used for the storage of goods or equipment damageable by flood waters.

The MBE is to be at least 0.60 m above the storm sewer service connection invert and 0.30 m above the major drainage system hydraulic grade line (HGL), whichever governs except where permissible on hillside development where:

- foundation drains are disconnected from the storm main; or
- intermittent foundation pumping has backflow prevention.

MBE must not be below elevation 343.66m, or the 1 in 200 flood elevation for Okanagan Lake plus 600mm of freeboard. For developments within close proximity to the Okanagan Lake shoreline, further consideration shall be given to wind and wave action when setting the required MBE.

For sites near a watercourse where a floodplain elevation has been established through flood mapping, the MBE is to be a minimum of 300mm above the 200-year return period peak flood elevation or as per City of Kelowna Mill Creek Flood Plain Bylaw No. 10248. Where a flood elevation has not been established, setbacks are to be as per the Provincial guidelines or 1.5 metres above the natural boundary of any watercourse, lake, marsh or pond.

3.7 Rational Method

The Rational Method for calculation of peak flows is as follows:

$$Q = R A I N$$

Where:

Q = Peak flow in cubic metres per second (m³/s)
R = Runoff Coefficient (C) x Adjustment Factor (C_{AFs})
A = Area of catchment in hectares (ha)
I = Intensity of rainfall (mm/hr)
N = 1/360

Factors for use in the Rational Formula are indicated below.

3.7.1 Runoff Coefficients (C)

The following runoff coefficients are for use with the Rational Formula. These coefficients are for general application only. Design values are subject to verification by the designer and approval by the City. Higher values may be applicable in those areas which experience rainfall during the winter when the ground is frozen.

Table 3.7.1 Runoff Coefficients (C)

Land Use	Percent Impervious	C	
		Minor Storm (1:5 year)	Major Storm (1:100 Year)
Residential			
• Suburban Residential (Lots>0.4 ha)	20%	0.35	0.40
• Low Density (Single Family)	40%	0.50	0.55
• Medium (Multi-Units Detached)	65%	0.60	0.65
• High Density (Multi-Units Attached)	90%	0.85	0.90
Commercial	90%	0.85	0.90
Industrial	90%	0.85	0.90
Institutional (e.g. Schools)	80%	0.75	0.80
Parks/Grasslands	20%	0.20	0.30
Cultivated Fields	30%	0.30	0.40

Runoff Coefficient Adjustment Factor (C_{AF})

An adjustment factor is to be applied to the runoff coefficient to reflect variations in soil permeability and slope.

Table 3.7.2 Runoff Coefficient - Soil Adjustment Factor (C_{AF})

Soil type and Slope	C_{AF}
Sandy soil with flat slope (up to 5%)	0.9
Sandy soil with steep slope (over 5%)	1.0
Clayey soil with flat slope (up to 5%)	1.0
Clayey soil with steep slope (over 5%)	1.1
Rock	1.1

Note: The above runoff coefficient adjustment factors are subject to verification by the designer. The product of C and C_{AF} can not exceed 1.0.

3.7.2 Rainfall Intensity (I)

Rainfall intensity for use in the Rational Method should be determined using the rainfall IDF curve in standard drawing **SS-S56** for the City of Kelowna. This curve was developed from the Atmospheric Environment Service recording station located at the Kelowna international Airport. To account for climate change, as noted in Section 3.1.3, a **15 percent increase** will be applied to the intensity derived from the IDF curve. The duration is equal to the Time of Concentration (T_c), as calculated below.

Time of Concentration (T_c)

The time of concentration is the time required for runoff to route from the most remote part of the catchment area under consideration to the design outlet node. The time of concentration can be calculated using the following formula:

$$T_c = T_i + T_t$$

Where:

T_c = time of concentration (minutes)

T_i = inlet or overland flow time (minutes)

T_t = travel time in sewers, ditches, channels or watercourses (minutes).

Inlet or Overland Flow Time (T_i)

Typical inlet times for urban areas, assuming BMP's are not applied, are as follows:

- a) Single Family Lot 10 minutes

- | | |
|----------------------------------------|-----------|
| b) Multi-Family Lot | 8 minutes |
| c) Commercial/Industrial/Institutional | 5 minutes |

For relatively flat areas, the inlet time for larger areas can be calculated using the "Airport Method" as follows:

$$T_i = \frac{3.26 (1.1 - C) L^{0.5}}{S^{0.33}}$$

Where:

T_i = inlet time (minutes)

C = runoff coefficient (See above)

L = travel distance (Maximum length = 300 m)

S = slope of travel path (%)

Travel Time

The travel time for routing in sewers, ditches, channels or watercourses can be estimated using the Modified Manning formula:

$$T_t = \frac{L n}{60 R^{0.667} S^{0.5}}$$

Where:

T_t = travel time (minutes)

L = length of flow path (m)

n = Manning roughness coefficient:

0.050 Natural channels

0.030 Excavated ditches

0.013 Pipe and concrete lined channels.

R = Hydraulic radius = Area/Wetted Perimeter (m)

S = slope (m/m)

3.7.3 Design Summary Sheet

All design calculations are to be tabulated and shown on the design drawings, or in a report and summarized on design drawings.

3.8 Hydrograph Method

Analysis using the Hydrograph Method requires computer modeling capable of analyzing the hydrologic characteristics of the watershed and generating runoff hydrographs.

For City applications, SWMM based models are appropriate. The City of Kelowna must be consulted before selecting a more specialized software program.

3.8.1 Modelling Procedures

Modelling results are to be calibrated using observed historical rainfall and flow data from the

design watershed. Sensitivity of the model predictions to variations of key parameters should be tested and the findings used to develop a realistic and conservative model.

At a minimum, post-development hydrographs are to be generated at key points of the drainage system for a 5-year and 100 year design storm with durations of 1, 2, 6, 12, and 24 hours for each development condition. A different range of storm durations may be appropriate, subject to City approval. This will identify the critical storm event to be used in designing the system component. Note that the storm durations that generate the critical peak flow may be different from the durations that generate the critical storage volume.

Systems with a number of interconnected ponds or with restricted outlet flow capacity may require a more detailed analysis for sequential storm events or modelling with a continuous rainfall record.

Detailed designs should include hydraulic grade lines (HGLs) of the minor and major systems plotted on profiles of the minor system components and compared with MBE to demonstrate flood protection.

3.8.2 Submission of Modelling Results

Modelling results are to be submitted to the City in a report or drawing containing at least the following information:

- a) Stormwater Control Plan as defined in Section 3.2,
- b) Name and version of modelling program(s)
- c) Parameters and simulation assumptions.
- d) Design precipitation details.
- e) Pre-development and post-development hydrographs.

3.9 Minor System Design

The minor system includes all drainage works that collect, convey, detain, divert and intercept design storm runoff. The minor design event must be the 5-year design storm.

3.9.1 Pipe and Channel Capacity

Use Manning's formula.

$$Q = \frac{A R^{0.667} S^{0.5}}{n}$$

Where:

A = Cross sectional area in m²

R = Hydraulic radius (area/wetted perimeter) in m

S = Slope of hydraulic grade line in m/m

n = Roughness coefficient:

0.013 for all smooth pipes.

0.024 for corrugated pipes and culverts.

3.9.2 Flow Velocities

a) Pipes/Culvert Flow

- i. Minimum design velocity for pipes flowing full or half full: 0.60 m/s.
- ii. Where grades are greater than 10%, measures are required to prevent pipe erosion and movement such as control structures and/or tie-backs and anchor blocks.
- iii. Where a storm sewer discharges into a watercourse, provide riprap bank protection and, if necessary, energy dissipation facilities. Avoid discharge perpendicular to stream flow.

b) Conveyance channels must be armoured and sized for a 1:100-year event. For riprap design chart see standard drawing **SS-S57**.

c) Road Ditches

- i. Maximum road ditch velocity is 0.5 m/s without armouring.
- ii. Ditch Inlets - Ditch inlets to storm sewers must include wing wall structures, safety grillage for large pipes (>600 mm diameter), debris screens and sedimentation basins.

3.9.3 Alignment

Except as indicated for Curved Sewers, horizontal and vertical alignments are to be straight lines between manholes.

3.9.4 Minimum Pipe Diameter

• Storm Sewers	250 mm
• Culverts crossing roads	450 mm
• Culverts crossing driveways	300 mm
• Catch Basin Leads	200 mm
• Double Catch Basin Leads	250 mm
• Service Connection - Residential	150 mm
• Service Connection - Commercial/Industrial	150 mm

Downstream pipe sizes are not to be reduced unless the downstream pipe is 600 mm diameter or larger and increased grade provides adequate capacity. Detailed hydraulic analysis is required. The maximum reduction is one standard pipe size.

3.9.5 Minimum Grade

Minimum grades of storm sewers are as required to obtain the minimum velocity of 0.6 m/s at design flow except for catch basin leads and service connections, for which minimum grades are as indicated in Section 3.9.12, Service Connections.

3.9.6 Curved Sewers

Where permitted by the City, horizontal and vertical curves may be formed using pipe joint deflections as follows:

- a) The radius of the curve is to be no less than the recommended manufacturer's minimum radius of curvature at a constant radius.
- b) Horizontal curves must be parallel to the centre line of road at a constant offset.
- c) Only one horizontal curve is permitted between manholes, unless the mainline is installed and appropriately anchored outside the road on a steep hill slope requiring multiple vertical curves.
- d) Where the pipe curve does not have a consistent offset from a road centre line, the offsets must be properly referenced on Record Drawings.
- e) Subject to City Engineer approval, curved storm sewer systems larger than 600 mm diameter may include deflections formed by mitred bends to a maximum mitre of 45°.

3.9.7 Depth

The minimum depth of the sewer must be sufficient to provide all service connection piping with a minimum cover of 1.2m to the top of the service, anywhere within the finished right-of-way. In no instance shall the cover over the crown of the sewer main be less than 1.2m when installed in travelled areas. The depth of course can be reduced to 1.0m when installed outside of travelled areas.

- a) The maximum depth of cover must be 4.5m, except under special circumstances and with permission of the City Engineer.
- b) For catch basin leads, the minimum depth of cover is 0.90m.

3.9.8 Pipe Joints

All pipe joints are to be watertight.

3.9.9 Perforated Storm Pipe

- a) The City will only consider the installation of perforated storm sewers and/or dry wells to discharge water back to the ground where soil conditions, slope and water table elevation are suitable. The perforated pipe system design must be designed to provide surcharge conditions.
- b) Perforated pipes can only be installed in areas of the City described as "Possibly Suited" in the Groundwater Recharge Suitability Map in Standard Drawing **SS-S58** and confirmed by a hydrogeotechnical site investigation.

3.9.10 Manholes

- a) Manholes are required at:
 - i. Every 150m or less.
 - ii. Every change of pipe size.

- iii. Every change in grade, except on curvilinear pipe alignments.
 - iv. Every change in direction, except on curvilinear pipe alignments.
 - v. All terminal sections.
 - vi. Every sewer main intersection.
- b) Placement of manholes in existing or future wheel paths must be avoided.
- c) Manhole sizes must be in accordance with the Standard Drawings: Manhole connection details as per MMCD S₃ & S₄, or City of Kelowna supplemental standard drawing **SS-S1a**.
- d) Hydraulics: Crown elevations of inlet sewers not lower than crown elevation of outlet sewer. When connecting a collector sewer main to a trunk sewer 300 mm or greater, the invert of the collector main must not connect lower than 0.75D ($\frac{3}{4}$ of the pipe diameter).
- e) Minimum drop in invert elevations across manholes:
- i. Straight run: 10 mm drop
 - ii. Deflections up to 45 degrees: 25 mm drop
 - iii. Deflections 45 to 90 degrees: 50 mm drop
- f) Drop manhole and ramp structures should be avoided where possible by steepening inlet sewers. Where necessary, provide drop structures as follows (table 3.9.10):

Table 3.9.10 Drop Structures

Invert Difference	Structure
Up to 0.45m	Inside Ramp
0.45 to 0.90 m	Outside Ramp
Greater than 0.90 m	Outside Drop*

*Inside drop may be used if specifically approved by the City Engineer.

- g) Drop manholes and outside ramps must be installed in accordance with standard drawings.
- h) Hydraulic losses are to be calculated for manholes with significant change of grade or alignment. For high velocity flows, particularly for pipes 600 mm or larger, detailed analysis is required using the Froude number, or utilizing appropriate computer models. The Manning's equation should not be relied on for pipe slopes above 10%. For low to moderate velocities and smaller pipes, use the following formula:

$$H_L = k v^2 / 2g$$

Where:

H_L = head loss (m)

v = flow velocity entering junction (m/s)

g = gravitational acceleration (9.81 m/s²)

k = head loss coefficient (1.0 for channelled 90° bends and tees, to 1.5 without channelized benching)

Where benching is used, the minimum drops listed above are applicable for velocities below 1 m/s. Where flows exceed 1 m/s, H_L should be specifically computed and used as the drop across the junction.

3.9.11 Catch Basins

- a) Catch basins are required at regular intervals along roadways, at intersections and at low points to:
 - i. Prevent overflows to driveways, boulevards, sidewalks and private property.
 - ii. Avoid interference with crosswalks.
 - iii. Avoid low points in curb returns at intersections.
- b) Catch basin leads are minimum 200 mm diameter.
- c) Minimum grade of a catch basin lead is 1%.
 - i. Catch basin leads require a 0.9 m minimum cover. If 0.9 m is not available, design to protect from freezing and traffic loads; design calculations must be provided.
- d) Spacing is to provide sufficient inlet capacity to collect the entire minor flow or major flow, where required, into the sewer system.
- e) Local suppliers are required to provide rating curves for available catch basin grates. As a general rule, space catch basins to drain maximum impervious areas of:
 - i. 500 m² on roads with grades up to 4%,
 - ii. 400 m² on roads with grades greater than 4% at 100 m maximum.
- f) Lawn basins are required on boulevards and private properties where necessary to prevent ponding or flooding of sidewalks, boulevards, driveways, buildings and yards.
- g) Double or twinned catch basins must not be connected directly together, rather one basin will be wye'd into the lead of the other. Maximum lead length to the mainline must be 30 meters and be minimum 250mm diameter. Each CB will have a trapping hood (standard drawing **SS-S54**).
- h) Double or twinned catch basins are to be provided at all sag points or sump locations as a minimum. Inlet calculations are required where the major storm needs to be accommodated, such as downhill cul-de-sacs or where there is potential for excessive ponding or overflow onto private property.

- i) Oversized grates and/or secondary emergency inlets must be considered where leaves and/or debris collection is anticipated.

3.9.12 Service Connections

Service connections to the City storm system are required for all multi-family, commercial, industrial and institutional land uses.

Single Family Residential service connections to the City Storm system are required in instances where site conditions do not provide for safe infiltration or dispersal of storm water on site. The safe use of infiltration is to be confirmed by a qualified Professional.

a) Where a service connection is required:

- i. The minimum diameter of a residential storm service is 150 mm.
- ii. Inspection chambers (IC's) are required and to be installed on the property line complete with service box. Where this is not possible, identify offset on the record drawings
 - An IC is not required on residential connections where the service is less than 2.5 m long from a manhole.
 - Colour of IC covers
 - Red – Sanitary
 - Green - Storm
- iii. For all commercial, industrial and multi-family (5 units or more) storm service connections where the size of the service is 250 mm in diameter or greater (refer to standard drawing **SS-S50**), a manhole is to be installed at the private side of the property line and main connection as required.
- iv. Service connections are permitted into manholes.
- v. Depth to be minimum 1.2 m.
- vi. Minimum grade from property line to storm sewer main is 2%.
- vii. Wye fittings are permitted for service connections to City storm sewers. Insertable tees are permitted into 250mm or larger existing mains.

b) Roof Leaders (drains):

- i. Where permissible and not in Hillside or Hazardous Areas, roof water is expected to be contained on site as part of best management practices to meet requirements for pre-development storm rate. Acceptable best management practices include splash-pad onto green space, rain harvesting systems or appropriately sized rock pits where soil infiltration parameters permit.
- ii. Roof leaders are not permitted to be directed to any infiltration device or soak away pit near or part of an engineered retaining wall or reinforced earth structure.
- iii. Roof leaders or inlets from downward sloping driveways in Hillside or Hazardous Areas must be connected to the City storm sewer.

c) Perimeters Drains

- i. Perimeter drains for buildings are required as per the British Columbia Building Code.
- ii. Discharge may be to the surface or a soak away pit.
- iii. Foundation perimeter drains are not permitted to be directed to any infiltration device or soak away pit near or part of an engineered retaining wall or reinforced earth structure.
- iv. Foundation perimeter drains can be routed by gravity through a storm service to the storm sewer provided that:
 - the elevation of the basement/crawlspace floor is at least 600 mm above the MBE (Section 3.6), or
 - 600 mm above the anticipated or known high ground water table, or
 - 600 mm above the 100 year hydraulic grade line within the sewer main at that point, whichever is higher.
- v. Where a sump pump is required, a backflow prevention device must be installed as part of the mechanical configuration to prevent backflow into a basement from the City Storm sewer.
- vi. As per Section 3.3.1, permanent groundwater pumping is not permitted to City storm sewers.

3.9.13 Perforated Sub-Drains

Perforated subsurface drainage systems designed for the purpose of permanent groundwater level reduction are not permitted to be connected to the City Storm sewer system.

3.9.14 Locations and Corridors

Wherever possible, storm sewers and service connections should be located within the public road right of way. Side or rear yard easements should be avoided where possible. Where it can't be avoided, statutory right-of-ways will be required for permanent City access.

3.10 Major System Design

The major drainage system includes all drainage pathways that convey, detain and/or intercept flows in excess of the capacity of the minor system. Its primary purpose is to provide flood protection for the 1:100 year return event. The major system generally includes surface flow paths such as ditches, swales, sewers, roadways, plus roadway culverts and watercourses.

3.10.1 Surface Flow Routing

All surface flows should have specially designed routes that are preserved and protected by right-of-ways and are accessible for maintenance. Design criteria include:

- a) HGL is to be at least 600 mm below the MBE of adjacent buildings.

- b) Maximum flow depth on roadways: 300 mm. Boulevards and intersecting driveway profiles will need to be set such that roadway surface flows are contained within the public right-of-way.
- c) One lane, or a 3.5 m width at the crown of each roadway, is to be free from flooding.
- d) Where a roadway is used as a major flow path, the road grades are to be designed to accommodate and control the flow at intersections.
- e) Flood routing is not permitted on to private property except in engineered flow channels or sewers protected in a statutory right-of-way.
- f) Overflow routes are required at all sags and low points in roadways and other surface flow routes.
- g) Major flood routes are required to exit down-slope in cul-de-sacs with Statutory Rights of Way established.

3.10.2 Surface Flow Capacity

Flow capacity of road surfaces and swales can be calculated using the Manning formula, presented in Section 3.9.2, Time of Concentration. Typical values of the Manning Roughness Coefficient "n" are:

- a) 0.018 for paved roadway
- b) 0.03 for grassed boulevards and swales
- c) 0.04 to 0.10 for irregular or treed channels.

Design detail is to include consideration of flow velocities and the potential requirement for erosion control measures. Ditches should be designed using a low n-value to determine velocity and provide the basis for stable channel design and a high n-value to determine ditch capacity and free board to prevent flooding or submergence of adjacent roadway subgrades.

3.10.3 Piped System

As noted in Section 3.2.1, the minor drainage system may be enlarged or supplemented to accommodate major flows in special circumstances. Modifications to the design criteria must be included in Stormwater Management Plan. Design considerations include:

- a) Provision of adequate inlets to accommodate major flows. Capacity calculations are to be provided in the Stormwater Management Plan.
- b) The requirement for surface overflow routes at potential surface ponding locations.
- c) Flow depth and velocity.
- d) Where applicable, design in accordance with minor drainage system guidelines.

3.10.4 Culverts and Bridges

The following service levels are to be used for design:

Road Class	Design Flood Frequency for Bridges and Culverts
Arterial and Collector	1:200 Year Flood
Local	1:100 Year Flood + provision for overflow if on major channel

The fishery value (aquatic classification) of the watercourse will establish the design requirements for the crossing. Particular designs will apply if fish passage is needed. Approvals are required under the BC Water Act and the Federal Fisheries Act, and may be required under the federal Navigable Waters Protection Act.

Culvert design is to be in accordance with the procedures outlined in an applicable design manual including but not limited to:

- American Concrete Pipe Association - Concrete Pipe Design Manual
- Corrugated Steel Pipe Institute - Handbook of Steel Drainage and Highway Construction Products.
- Standards and Best Practices for Instream Works - Culverts, Province of British Columbia and DFO.

Inlet and outlet protection is required for all major system culverts. Design considerations are to include inlet control and outlet control conditions, energy dissipation and erosion control measures.

The City requires all municipal channel culverts 500mm or greater to be constructed with headwalls, endwalls and safety grillage as per Standard Drawings.

3.10.5 Watercourses

Natural watercourses are integral components of both the major drainage system and the ecological system. Riparian areas are to be preserved and/or enhanced to sustain habitat for aquatic and other wildlife as well as convey storm runoff.

Increases in peak storm flows and volumes to major watercourses and receiving waters shall be minimized. Consideration must be given to fish bearing streams and to streams presently at capacity.

Designers must consider all federal, provincial and municipal laws, regulations and guidelines noted above, and must obtain comments and approvals from the appropriate agencies.

3.11 Runoff Controls

Runoff controls are required to meet the objectives indicated previously. The controls may include:

3.11.1 Detention Storage

Detention storage is used to capture and store water on site to assure that storm releases are limited to the pre-development release rate for a 1 in 5 year storm. Drainage Basin Plans are available upon request to the City Engineer.

As a guideline, detention storage is not required on any lands west of Richter Street between Bernard Avenue to the north and Wardlaw Avenue to the South unless approved by the City Engineer. Where peak flow rates or volumes are increased and will cause detrimental impacts, provisions for downstream improvements must be provided in order to mitigate the impacts.

Detention storage options and design guidelines include the following:

3.11.2 Parking Lot Storage

- a) Requires detailed lot grading design to ensure proper drainage, pedestrian safety and convenience, and major flow paths .
- b) Maximum ponding depth: 300 mm outside vehicle stalls, 150 mm within vehicle stalls, however, also with consideration to frequency of ponding and impact to users of the parking lot.

3.11.3 Underground Storage

- a) Facilities include tanks and oversized pipes, with outlet controls.
- b) Tanks, fencing and graded slopes to be constructed off-line and on-site.
- c) Cross sections and inlet and outlet locations should be designed to minimize maintenance requirements.
- d) Structural design to accommodate traffic loads and groundwater pressure.
- e) Maintenance access provisions required.

3.11.4 Dry Detention Ponds

- a) Intended to provide storage only during severe storm events.
- b) May be on-line or off-line, although off-line is preferred. Fencing and graded slopes required.
- c) May accommodate active recreational uses.
- d) Overflow elevations to be coordinated with MBEs.
- e) Emergency overflow spillway to be constructed for 1:100yr storm event.
- f) Design details, other than discharge rates should be in accordance with current technologies as outlined in Land Development Guidelines for Protection of Aquatic Habitat (Canada/BC).
- g) Provide warning signage indicating facility is a stormwater detention structure subject to

flooding or rapid water level changes. Signs to be posted at all public access points or road frontages.

3.11.5 Wet Detention Ponds

- a) Intention is to provide on-line detention storage and maintain a permanent minimum water levels.
- b) Catchment area must be large enough to provide sufficient base flow to ensure wet storage and is sustained without becoming stagnant (based on local hydrologic characteristics).
- c) Generally located off-site, and must include fencing and graded slopes on-site.
- d) Can provide a public amenity within a passive park.
- e) Overflow elevations to be coordinated with MBEs.
- f) Design details, other than discharge rates, should be in accordance with current technologies as outlined in Land Development Guidelines for the Protection of Aquatic Habitat (Canada/BC), and related documents.
- g) Provide warning signage indicating facility is a stormwater detention structure subject to flooding or rapid water level changes. Signs to be posted at all public access points or road frontages.

3.11.6 Subsurface Disposal / Infiltration Systems

- a) These systems are intended to promote stormwater retention and groundwater recharge.
- b) Suitable for high permeability soils with low groundwater elevation. Geotechnical investigation is required.
- c) Design details should be in accordance with current technologies as outlined in Infiltration systems guidelines in Land Development Guidelines for the Protection of Aquatic habitat (Canada/BC), and related documents.
- d) Stormwater infiltration basins planned for Hillside or Hazardous Development Areas must be designed by a qualified Professional with experience in hydrogeology. The design must be reviewed and confirmed by the City Engineer. See Section 3.1.4.

3.12 Outlet Controls

Outlet controls for storage facilities may be designed using the standard orifice and weir equations:

Orifice Equation:

$$Q = C A (2 g h)^{0.5}$$

Where:

Q = release rate (m³/s)

C = orifice coefficient (0.62 for sharp or square edge, 0.85 for rounded edge)

A = area of orifice (m²)

g = gravitational acceleration (9.81 m/s^2)
 h = net head on orifice (m)

Weir Equation:

$$Q = CLH^{1.5}$$

Where:

Q = release rate (m^3/s)
 C = weir coefficient
 L = effective length of weir crest (m)
 H = net head on weir crest (m)

Larger storage facilities are to include provisions for discharges at rates greater than the design release rate (i.e., major storm event and emergency conditions). Rapid drawdown of the water level may be necessary for emergency purposes or to restore the available storage to accommodate subsequent storm events. Simple reducers are permitted on smaller facilities.

Orifices shall be fixed and designed to pre-development outflow rate. Adjustable mechanisms such as slide gates or removable orifice plates are not permitted unless approved by the City Engineer.

Design of inlet and outlet structures is to include consideration of energy dissipation and erosion control. Safety grates are required over all inlet and outlet openings larger than 500 mm diameter. Locks for access hatches are required.

The following is an introductory list of some runoff controls focused on water quality treatment.

- a) Bio-filtration Swales and Constructed Wetlands
- b) Intended to provide bio-filtration and sediment removal.
- c) May be designed to provide on-line detention storage as well as quality treatment.
- d) May be located on-site or off-site.
- e) Qualified professional required for design.
- f) Design requires consideration of climatic conditions.

3.12.1 Oil and Grit Separators

Oil and Grit Separators are required:

- a) On site with parking for 50 or more vehicles (does not apply to parkades).
- b) On all industrial zoned properties, unless it can be proven that there is no risk of storm water contamination.
- c) Supplier design details are required.

Design criteria for Oil and Grit Separators must include:

- a) Devices must have a current Canadian Environmental Technology Verification (ETV) or ISO

14034 ETV verification.

- b) A target Total Suspended Solids removal of 60% of the ETV Particle Size Distribution.
- c) Performance predictions for all proposed units.
- d) A maintenance plan and commitment from all Owners. This will be included in the business license renewal.
- e) A location on-site, including a Statutory Right of Way or covenant on title should the City need to inspect the unit.

3.12.2 Oil/Water Separators

- a) Required for gas stations, vehicle service areas and storage areas for highway vehicles and construction equipment.
- b) Design details in accordance with current technologies as outlined in Urban Runoff Quality Control Guidelines for British Columbia.

3.13 Drainage Pump Stations

Drainage pump stations are not commonly used in the City. Where drainage pumping is required, the designer must review the design concept and proposed guidelines with the City, submit a pre-design report and obtain approval of the City before proceeding with design. At a minimum, the pre-design report should include the following:

- a) Delineated catchment area map
- b) Estimated flows and HGL
- c) Pump station location
- d) Connection to existing infrastructure.

3.14 Erosion and Sediment Control (ESC)

All construction projects in the City require an Erosion and Sediment Control (ESC) Plan approved by the City. Storm water runoff from construction sites commonly contains significantly higher contaminant concentrations than storm water from developed sites. Poor construction practices and lack of attention to detail are contributors to sediment transport, in turn impacting both downstream infrastructure, aquatic habitats and Okanagan Lake.

Erosion and Sediment Control will be managed as a separate process with a cost identified as a separate line item in the development planning process

The following policies will be administered:

- a) No Person may cause, or permit another Person to cause, sediment or sediment-laden water to discharge into the storm system, with concentrations greater than 75 milligrams per litre (ppm) of total suspended solids (TSS). A sample measuring greater than 60 nephelometric turbidity units (NTU) will be the trigger point where the sample must also be sent to the lab for analysis.

- b) A Security Deposit for ESC Works equal to 3% of the Consulting Engineer's opinion of probable costs of civil earthworks and infrastructure will be added to the Servicing Agreement.
 - i. The Security Deposit submitted is to secure the full and proper compliance with the provisions of the By-law. In the event, that the Owner, Developer, or Person Responsible has not complied with the provisions of this By-law, the necessary funds from the security deposit may be drawn down, at the City's option, and the money used either by the City or its agents to protect the storm system from sediment or sediment-laden water in adherence with the terms and conditions of this By-law. Notwithstanding, the City is under no obligation to initiate or complete remedial works in or under the Land.
 - ii. If the amount of the security deposit is insufficient for the City to complete the ESC Facilities, the Owner and Developer jointly and severally will pay any deficiency to the City on demand.
- c) The Owner must retain a Qualified Professional (P.Eng, RPBio, P.Ag, ASCT, CPESC, CISEC or CESCL) responsible for inspecting and monitoring the ESC Facilities weekly and after any rain event which exceeds the intensity of 25mm of total rainfall depth in a 24-hour period. All records and data must be made available to the City upon request. Should a site be determined to be non-compliant, the Professional will be responsible for submitting notification and presenting a remediation plan to the City within two days of the event.
- d) The ESC will include a construction plan and site management plan. ESC features must be installed before any clearing, excavation, or soils mobilization takes place.
- e) The fundamental approaches to effective ESC include:
 - i. reduce clearing and grading and preserve natural vegetation as much as possible;
 - ii. phase construction to limit soil exposure at any one time, particularly in wet seasons;
 - iii. stabilize exposed soils as quickly as possible, whether temporary or permanent;
 - iv. protect slopes and cuts;
 - v. prepare the site to limit soil tracked off-site by haul vehicles;
 - vi. sweep off-site streets when dirt is tracked;
 - vii. filter runoff water before it leaves the site;
 - viii. install filters or barriers to protect downstream drains and inlets;
 - ix. adjust ESC plan to suit changing weather and construction phasing;
 - x. assess ESC practices after rain event; and
 - xi. maintain the works throughout construction.

Ideally, practices and features are put in place to prevent erosion from occurring in the first place, but realistically some degree of erosion and sediment transport will occur. When it does, other practices and features are to intercept and capture the sediment before reaching vulnerable areas. As such, the following sub-sections introduce ESC practices in two core categories; erosion control and sediment control.

3.14.1 Erosion Control

Rainfall and wind can aggressively displace and transport soil, although rainfall tends to be the more damaging in BC climates. The soil composition has a significant bearing on its erosion potential. The first line of defense is to either maintain or provide protective cover to the soil. Ideally, natural vegetative cover is maintained for areas that do not need to be disturbed. Where soils do need to be exposed or stockpiled, temporary covers should be applied when rainfall events are imminent.

For exposed site areas, straw mulch is the most common form and can be effective with low cost. However, it is commonly not applied thick enough or replenished frequently enough. It is important that a uniform blanket be provided and refreshed as the straw decays or is displaced. For the most part, bare soil should not be visible.

For steeper slopes, or for areas exposed and inactive for considerable time, manufactured erosion control blankets may be most appropriate. There are many products available and local suppliers should be consulted for the selection of the appropriate one. While they have a higher purchase cost, with proper selection and installation they will provide longer and more effective service with far less maintenance than straw mulch.

For soil stockpiles, poly tarps should be applied when the stockpile is inactive, including short overnight periods if there is any threat of precipitation. If inactive for considerable time, other measures such as temporary seeding, mulching, or matting may be considered.

Once disturbance to an area is complete, permanent cover practices should be established as soon as possible. Top dressing the area with topsoil having high organic content in itself can be a significant benefit; a minimum of 100 mm should be applied for purposes of erosion control. Greater depth is often required to meet landscape growing medium and hydrologic management needs. Sodding, broadcast seeding, hydro-seeding, and drill seeding are acceptable methods to re-establish a blanket of vegetative.

Aside from maintaining good quality ground cover, there are a number of other techniques that can be applied as erosion control, including the following, but not necessarily limited to those below. They should be selected based upon the specific conditions and requirements of the site.

Construction of stable haul roads for transport vehicles coming and going from the site is required.

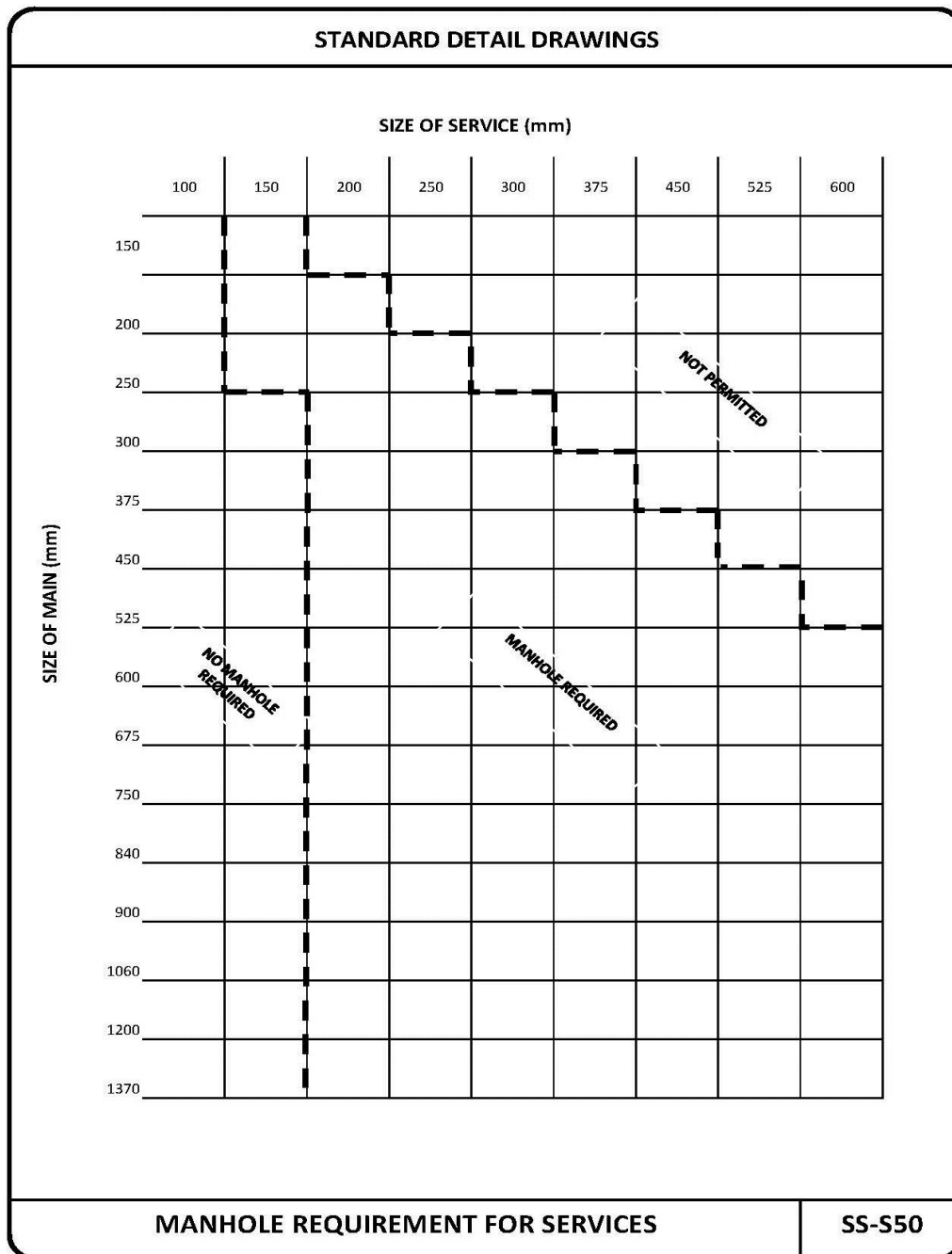
At a minimum, haul roads include 200 mm of a coarse granular running surface, but strong consideration for underlying filter fabric, and potentially geogrid reinforcing in weak soils, should be given;

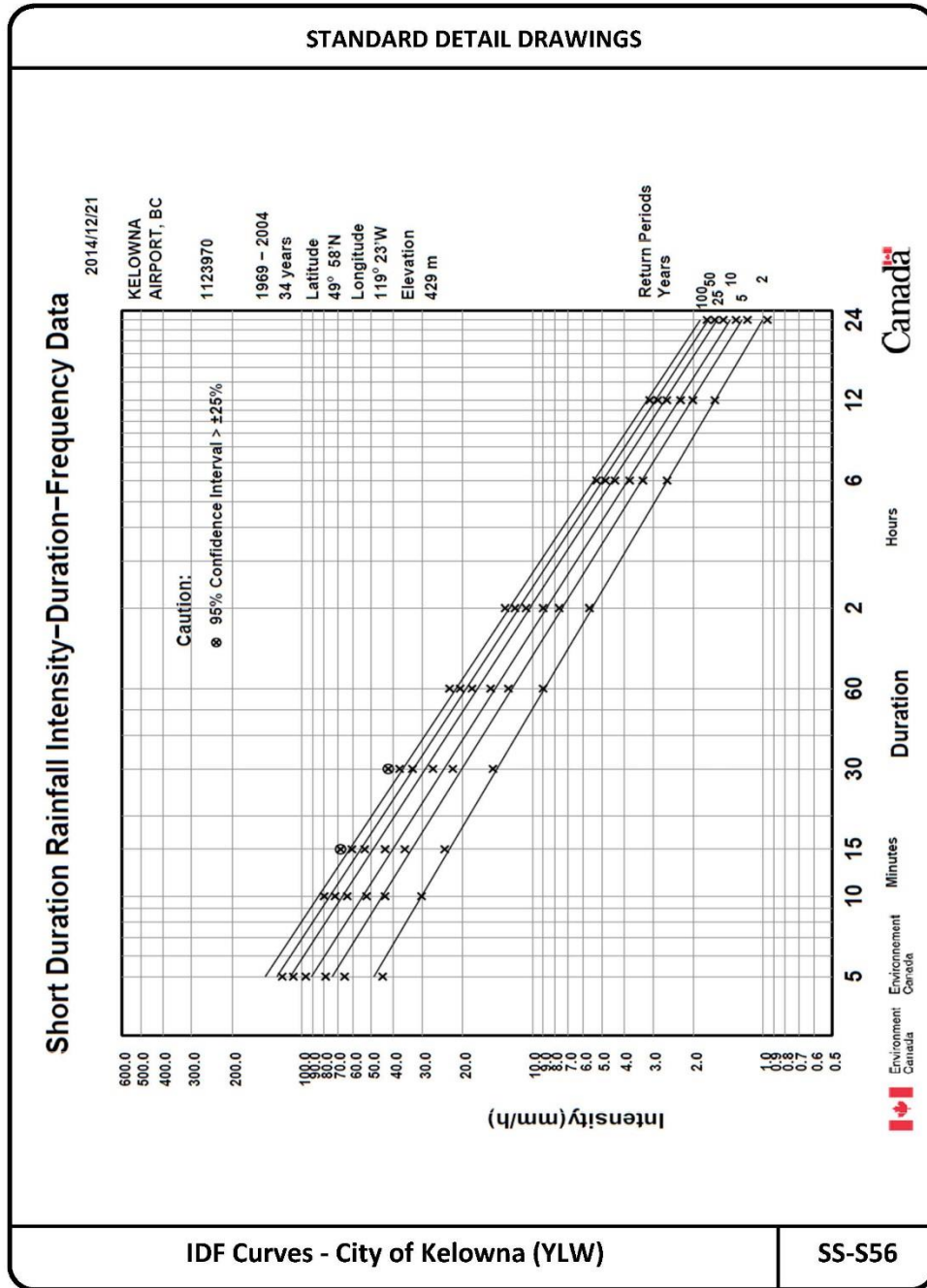
- a) Intercept trenches on the upstream edges of the working area to redirect runoff;
- b) Terracing steeper slopes;
- c) Scarifying the soil surface;
- d) Bio-engineered protection of very steep slopes;
- e) Rip-rap with appropriate underlying filter.

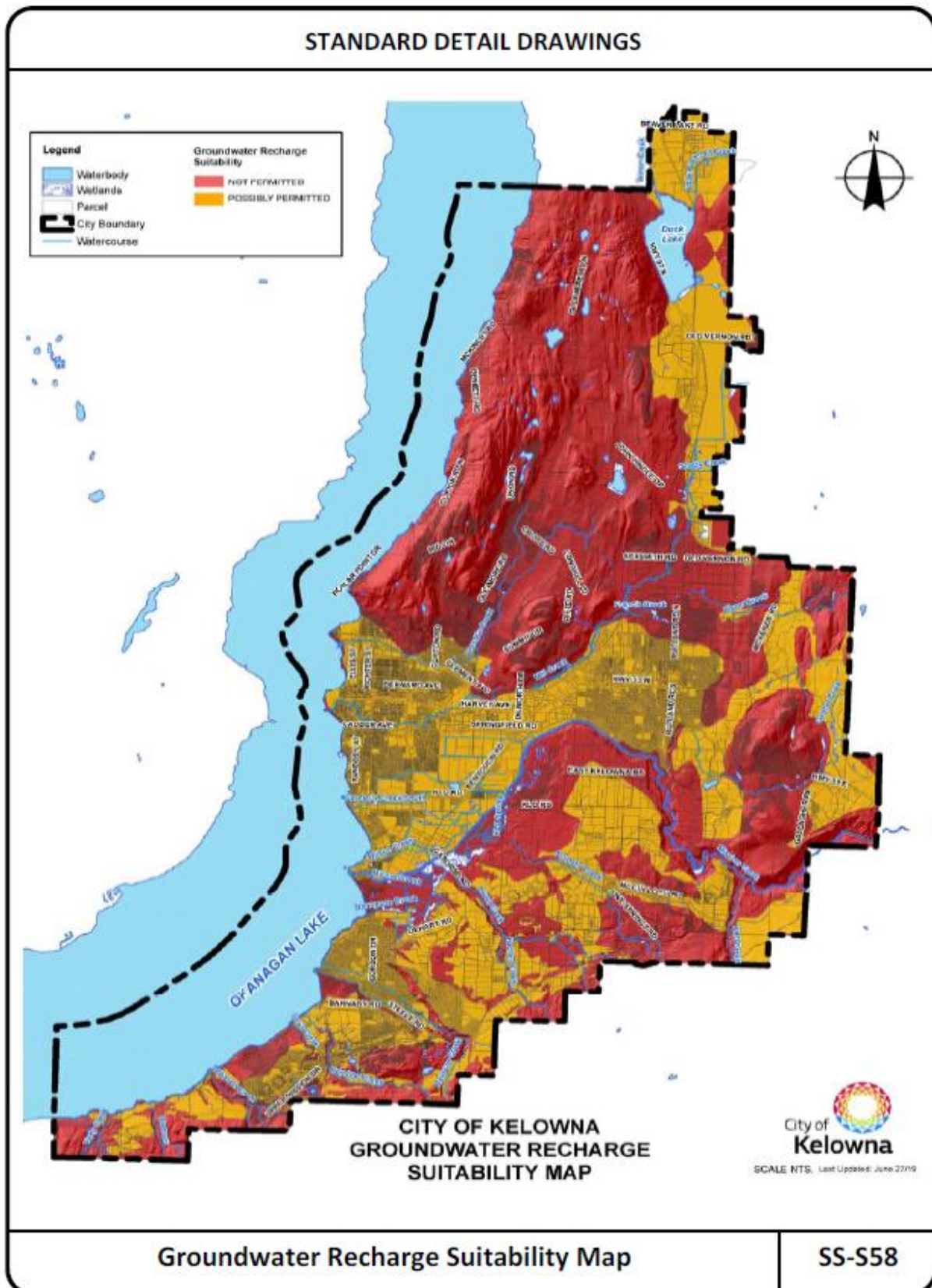
3.14.2 Sediment Control

Silt fences can be an effective barrier to contain soil, but are not an effective filter of sediment laden runoff. Their permeability is insufficient to allow water to pass through, and therefore more commonly act as a dam which is then often undermined or circumvented by the flow of water. When used appropriately as a soil containment barrier, they must be sufficiently installed and maintained. Design criteria include: stakes should be > 7.5cm in diameter and > 1.5m long and driven > 40cm into the ground; stakes should be < 2.4m apart unless wire backing is used; and bottom should be buried in a trench > 20cm.

- a) Storm drains and catch basins potentially receiving site runoff are to be protected with filters.
- b) Straw bales and gravel berms are to be used within flow paths to slow water and promote trapping of coarse sediment. Note that these are less effective for fine sediment.
- c) Dust control is required at all times.
- d) Soil transport from vehicles coming and going from the site must be controlled. Where a wheel wash facility is constructed, wash water must be appropriately contained and treated prior to release off-site.
- e) Sediment ponds (or basins) are generally applied to larger construction sites (> 2 hectares) to settle suspended sediments larger than 0.02mm. The outlet should consist of a perforated riser pipe with a gravel jacket. Internal gravel baffles are to be installed to create individual cells to reduce velocities and prevent short circuiting of flow to the outlet. As a design guideline, ponds should be sized to accommodate 125 m³/ha of site area. Of this volume, at least 20% should be dedicated to a forebay. The remainder, as a permanent pool, should measure 1.3-1.8m in average depth, and not exceed 2.4m.
- f) Sediment traps are similar to sediment ponds, but designed for small sites. Generally fed by swales, these facilities are located on the low-side of the site to receive site runoff water and allow settling of solids before discharge off-site.







Report to Council



Date: August 12, 2019

File: 0610-50

To: Council

From: City Manager

Subject: 2019 Financial Plan Amendment - Kelowna International Airport

Report Prepared by: Mira Malkowsky, Airport Corporate Services Manager

Recommendation:

THAT Council receives, for information, the report from the Airport Finance & Corporate Services Department dated August 12, 2019, with respect to a budget amendment for \$695,000;

AND THAT the 2019 Financial Plan be amended to increase facility maintenance fees by \$695,000 to be funded from the Airport's Airside, Groundside and Terminal reserves.

Purpose:

To obtain Council's approval to amend the 2019 Financial Plan.

Background:

In 2013, Bouygues Energies and Services Canada Limited ("Bouygues") was the successful proponent in the City's negotiated request for proposal process to provide building and technical services at Kelowna International Airport ("YLW"). As part of this agreement, Bouygues is responsible for maintenance of YLW facilities and infrastructure which includes, but is not limited to the following:

- preventative and reactive maintenance on buildings, building systems, life safety systems and equipment including: electrical, the baggage handling system, passenger bridges, aviation safety systems, and HVAC;
- airfield electrical work;
- surface maintenance;
- snow and ice control;
- janitorial maintenance;
- environmental services;
- energy management; and
- minor works.

The original term of the Building and Technical Services Master Agreement (the "Agreement") was May 1, 2013 to April 30, 2018 with two optional five-year renewals subject to the mutual agreement of both parties.

In the winter of 2017, and prior to the first optional five-year renewal, Bouygues and YLW agreed that the Agreement, which was comprised of approximately 2,346 pages and included the negotiated request for proposal document, Bouygues' proposal and the base contract, was difficult to manage and was not operationally effective. As a result, YLW and Bouygues worked together to consolidate and condense the Agreement, which is now under 75 pages in length. YLW and Bouygues also worked together to update certain aspects of the Agreement, which had changed during the initial five-year term due to changes in YLW's operations. These changes were completed with involvement from the City's Purchasing Department. Due to the time required to complete this exercise, the current Agreement is in over-holding and, subject to budget approval, the five-year extension will be executed in August 2019.

The following table provides a summary of the changes to the fee that result in the request for a budget amendment. It is important to note that the first three months of the contract will be a hybrid fee to allow Bouygues the time to adjust its workforce. The Proposed Fee in the table below represents the fee commencing in month 4 of the Agreement.

Year 5 Fee	\$2,311,435
Proposed Changes	
Change in YLW Infrastructure ^a	494,656
Growth of YLW ^b	223,480
Indexation ^c	116,723
Office Space ^d	38,089
Labour ^e	31,462
Snow clearing ^f	(200,733)
Other	(9,540)
Subtotal	694,137
Proposed Fee	3,005,572

- a. Mainly due to the completion of the Outbound Baggage Hall in 2017.
- b. YLW's passenger numbers have increased 38% since 2013.
- c. Based on BC CPI.
- d. YLW is not able to provide office space within the air terminal building.
- e. Market-based increase to attract talent.
- f. Proposed to be paid for directly by YLW rather than being incorporated in the base fee.

Internal Circulation:

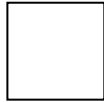
Phillip Elchitz, Senior Airport Operations Manager
Lori Evans, Senior Buyer
Jessica Hewitt, Airport Communications Advisor
Jackie Dueck, Controller

Financial/Budgetary Considerations: N/A
Considerations not applicable to this report: N/A
Legal/Statutory Authority: N/A
Legal/Statutory Procedural Requirements: N/A
Existing Policy: N/A
Personnel Implications: N/A
External Agency/Public Comments: N/A
Communications Comments: N/A
Alternate Recommendation: N/A

Submitted by:

S. Dyrdal, Senior Airport Finance & Corporate Services Manager

Approved for inclusion:



S. Samaddar, Airport Director
D. Gilchrist, City Manager

cc: Genelle Davidson, Divisional Director, Financial Services
Darren Tompkins, Purchasing Manager
Phillip Elchitz, Senior Airport Operations Manager
Lori Evans, Senior Buyer
Jessica Hewitt, Airport Communications Advisor
Jackie Dueck, Controller

CITY OF KELOWNA

BYLAW NO. 11851

Amendment No. 34 to Traffic Bylaw No. 8120

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Traffic Bylaw No. 8120 be amended as follows:

1. THAT PART 1 – INTRODUCTIONS, 1.4 Definitions, 1.4.1 Definitions be amended by:

- a) adding new definitions in their appropriate locations as follows:

“Block-face” means either side of a **highway** between two consecutive features intersecting that **highway**. The features can be named **highways** or boundaries of standard geographic areas, not including **lanes** and **driveways**.

“Car Share Organization” means operations that allow members of the public to book cars on a short-term as-needed basis, paying only for the time they use the car and the mileage they drive. The operators are responsible for maintenance, insurance and vehicle booking through an online application.

“Car Share Vehicle (Classic)” means any **vehicle** owned or leased by a **car share organization** where the vehicle is returned to the same location at the end of every booking to a designated parking area.

“Car Share Vehicle (One-Way)” means any **vehicle** owned or leased by a **car share organization** which is shared among the organizations members and has no fixed or dedicated parking space.

“Car Share Vehicle Parking Space” means a parking space reserved for the exclusive use of a **Car Share Vehicle (Classic)** or **Car Share Vehicle (One-Way)**.

“Community Based Car Share Vehicle” means any **vehicle** owned or leased by a member-owned organization, co-operative association, or non-profit organization incorporated under the laws of British Columbia.

“Off-Season” Parking means rates and restrictions that are applicable on any date/time between September 16th and May 14th.

“Summer Season” Parking means rates and restrictions that are applicable on any date/time between May 15th and September 15th.

- b) deleting the name **“Pay by Phone”** and replacing it with **“Virtual Parking Payment”** and placing it in its appropriate location;

c) deleting all references to **"Pay by Phone"** and replacing it with **"Virtual Parking Payment"**.

d) deleting the definition for **Permit** that reads:

"Permit" means a document in writing issued pursuant to this Bylaw.

and replacing it with:

"Permit" means a document issued in writing or electronically linked to a licence plate pursuant to this Bylaw.

2. AND THAT **Part 4 – PARKING REGULATIONS**, 4.1.2 Parking Prohibitions be amended as follows:

a) Deleting sub-section (cc) that reads:

"on any public parking lot operated by the **City** where **traffic control devices** indicate the length of time allowed for parking, in contravention of the length of time indicated on the applicable **traffic control device**,"

And replacing it with:

"on any public parking lot operated by the **City** where a **traffic control device** indicates the length of time allowed for parking, after the time allowed has been exceeded, measured from the time when the **vehicle** was first parked on the same calendar day, regardless of whether the **vehicle** has moved,"

b) Deleting sub-section (ff) that reads:

"on any **highway** where the length of time allowed for a parking space is controlled by a **traffic control device** and where a **vehicle** has been parked at a parking space for any period of time, after having moved the **vehicle**, the **owner** or operator of such **vehicle** shall not, having left such a parking space, permit the immediate parking of such **vehicle** in the same or any other parking space on either side of the same block,"

And replacing it with:

"anywhere within a **block-face** where a **traffic control device** indicates the length of time allowed for parking, after the time allowed has been exceeded, measured from the time when the **vehicle** was first parked on the same calendar day, regardless of whether the **vehicle** has moved,"

- c) Adding a new sub-section (uu) in its appropriate location that reads:

"on public property while displaying a licence plate that is partially or completely obstructed with dirt or foreign material, so that the numbers and letters on it cannot be plainly seen and read at all times and so that the numbers and letters may be accurately photographed using a licence plate recording device or camera used for monitoring compliance with this bylaw or to record evidence of a violation."

- d) Adding a new sub-section (vv) in its appropriate that reads:

"in a **block-face** within a residential zoned area of the City Centre, South Padosy or Capri-Landmark Urban Centre areas, as defined in the Official Community Plan, between the hours of 9:00 a.m. and 6:00 p.m. on weekdays (excluding Statutory Holidays), for more than 3 hours without a valid **permit** effective as of May 15, 2020."

3. AND THAT **Part 4 – PARKING REGULATIONS** be amended by:

- a) Adding a new sub-section **4.6 Car Sharing** in its appropriate location that reads:

"4.6 Car Sharing

- 4.6.1 Car Share Permits. A **Car Share Vehicle (Classic)** or **Car Share Vehicle (One-Way)** operated by a **Car Share Organization** with a valid **On-Street Parking Permit** or **Reserved Off-Street Parking Permit** shall not be subject to the penalties provided for the breach of Subsections 4.1.2(bb), (v) or (vv) of this Part so long as that permit(s) remains valid and subsisting.

- (a) The **Car Share Organization** shall register the current licence plate number for each authorized **Car Share Vehicle (Classic)** or **Car Share Vehicle (One-Way)** with the **City**, or
- (b) The **Car Share Organization** shall ensure each **vehicle** is parked in a location designated for exclusive or non-exclusive use by **vehicles** registered to the **Car Share Organization** when not booked.
- (c) The **City** reserves the right to limit the number of permits issued to **Car Share Vehicles (Classic)** or **Car Share Vehicles (One-Way)**, either by type of permit/vehicle or area.
- (d) Permits will be linked to the plate number registered to each **Car Share Vehicle (Classic)** or **Car Share Vehicle (One-Way)** and are invalid if used with any other **vehicle**/licence plate."

4. AND THAT **SCHEDULE "A" FEES, PART 4 – PARKING REGULATIONS** Subsection 4.3.1 (d) Reserved On-Street Parking Permit Fees be deleted that reads:

"Subsection 4.3.1(d) Reserved On-Street Parking Permit Fee

- a) Reserved On-Street Parking permit fee: \$11.00 per day per stall/permit
 \$11.00 per day per meter hood
- b) Security Deposit (Per Permit/Meter Hood): \$25.00 each"

And replacing it with:

"Subsection 4.3.1(d) Reserved On-Street Parking Permit Fees

PERMIT AREA	FEE (Per calendar day/per stall)
A (Summer)	\$21.00*
A (Off-Season)	\$18.00*
B (Summer)	\$14.00*
B (Off-Season)	\$12.00*
C	\$12.00*
D	\$12.00*
E	\$12.00*
F	\$12.00*
G	\$16.00*
H	\$16.00*

* Applicable taxes included

Security Deposit (if Required)	\$25.00 per meter bag
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5. AND THAT SCHEDULE "A" FEES, PART 4 – PARKING REGULATIONS Subsection 4.3.3 On-Street Pay Parking & Short Term Parking Lot be deleted that reads:

"Subsection 4.3.3 On-Street Pay Parking & Short Term Parking Lot**

FEE	PERMITTED PARKING TIME
\$0.25*	12 Minutes
\$0.50*	24 Minutes
\$0.75*	36 Minutes
\$1.00*	48 Minutes
\$1.25*	60 Minutes (1 hour)
\$1.50*	72 Minutes
\$2.00*	96 Minutes
\$2.50*	120 Minutes (2 hours)

* Applicable taxes are included.

\$ 12.00** 1 Month – Bicycle Locker Rental

**Plus Applicable taxes.

** *Designated Off-Street Pay Parking - "Short Term Parking Lots":*

- Water Street Boat Launch Lot (1354 Water Street)
- Ellis & Lawrence Lot A (483 Lawrence Avenue)
- Museum Parking Lot (470 Queensway – Access from Ellis Street)"

And replacing it with:

"Subsection 4.3.3 On-Street Pay Parking & Short Term Parking Lot**

ZONE	FEE			
	<u>1st and 2nd Hours</u>	<u>3rd to Maximum Hours</u>	<u>Evening Hours (As Posted)</u>	<u>Weekend Hours (As Posted)</u>
A (Summer) <i>Downtown</i> (Effective May 15, 2020)	\$1.75*	\$3.50*	\$1.50*	\$1.50*
A (Off-Season) <i>Downtown</i> (Including Summer 2019)	\$1.50*	\$3.00*	\$1.25*	\$1.25*
B (Summer) <i>Downtown</i> (Effective May 15, 2020)	\$1.50*	\$3.00*	No Charge	\$1.25*
B (Off-Season) <i>Downtown</i> (Including Summer 2019)	\$1.25*	\$2.50*	No Charge	\$1.25*
C <i>Downtown</i>	\$1.25*	\$1.25*	No Charge	\$1.25*

D <i>South Pandosy</i>	\$1.25*	Not Available	No Charge	No Charge
E <i>South Pandosy</i>	\$1.25*	\$1.25*	No Charge	No Charge
F <i>South Pandosy</i>	\$1.25*	\$1.25*	No Charge	No Charge
G <i>Hospital Area</i>	\$1.25*	\$1.25*	\$1.25*	\$1.25*
H <i>Hospital Area</i>	\$1.25*	\$1.25*	No Charge	\$1.25*
1 (OS Lots)	\$1.25*	\$1.25*	\$1.25*	\$1.25*
2 (OS Lots)	\$1.25*	\$1.25*	No Charge	No Charge

* Applicable taxes included.

DESCRIPTION	TERM	FEE
Bicycle Locker	1 Calendar Month	\$ 12.00**

**Plus Applicable taxes.

Zone 1 - Off-Street Pay Parking – Designated Short Term Parking Lots:

- Water Street Boat Launch Lot 602 (1354 Water Street)
- Museum Parking Lot 608 (470 Queensway/1420 Ellis Street)

Zone 2 - Off-Street Pay Parking – Designated Short Term Parking Lots:

- Ellis & Lawrence Lot 614 (483 Lawrence Avenue)"

6. AND THAT **SCHEDULE "A" FEES, PART 4 – PARKING REGULATIONS subsection 4.4.2 (a) – Off Street Pay Parking (Long Term Parking Lot)** be deleted that reads:

"Subsection 4.4.2 (a) - Off-Street Pay Parking (Long Term Parking Lot)

<u>FEE</u>	<u>PERMITTED PARKING TIME</u>
\$ 1.00*	1 Hour
\$ 6.00*	1 Day (Standard Vehicle - Regular Operating Hours for Lot)***
\$ 3.00*	1 Day (Motorcycle – Regular Operating Hours for Lot)
\$ 7.00*	Special Event Parking
	*Applicable taxes are included.
\$ 12.00**	1 Month – Bicycle Locker Rental
\$ 78.08**	1 Month – Random Parking
\$ 117.62**	1 Month – Reserved Stall***
	**Plus Applicable taxes.

Above Rates apply to all City Owned Parking Lots not otherwise specified in this bylaw.

**** Not Available at all parking lots."*

And replacing it with:

"Subsection 4.4.2 (a) - Off-Street Pay Parking (Long Term Parking Lot)

DESCRIPTION	TERM	FEE (Zone 3)	FEE (Zone 4)
Hourly	1 Hour	\$1.00 ¹	\$1.00 ¹
Daily (Vehicle) ³	1 Calendar Day	\$6.50 ¹	\$6.50 ¹
Daily (Motorcycle) ³	1 Calendar Day	\$3.00 ¹	\$3.00 ¹
Special Events ³	As/when Indicated On-Site	Hourly/Daily	\$7.00 ¹
Bicycle Locker ³	1 Calendar Month	\$12.00 ²	\$12.00 ²
Random Parking Permit ³	1 Calendar Month	\$81.79 ² (Sept 1, 2019)	\$81.79 ² (Sept 1, 2019)
		\$90.00 ² (June 1, 2020)	\$90.00 ² (June 1, 2020)
Reserved Stall Permit ³	1 Calendar Month	\$150.00 ²	\$150.00 ²

¹ Applicable taxes included.

² Plus Applicable taxes.

³ Not Available at all locations.

Zone 3 - Off-Street Pay Parking – Long Term Parking Lots:

- City Hall Lot 605 (1435 Water Street) – *Monthly Parking Restricted to Facility Staff*
- Police Services Lot 643 (1190 Richter Street) – *Monthly Parking Restricted to Facility Staff*

Zone 4 - Off-Street Pay Parking – Long Term Parking Lots:

- Applies to all City Owned Parking Lots not otherwise specified in this bylaw."

7. AND THAT SCHEDULE "A" FEES, PART 4 – PARKING REGULATIONS **subsection 4.4.2 (b) – Off-Street Pay Parking – Peripheral Lot (Long Term Parking Lot)** be deleted that reads:

"Subsection 4.4.2(b) – Off-Street Pay Parking – Peripheral Lot (Long Term Parking Lot)

<u>FEE</u>	<u>PERMITTED PARKING TIME</u>
\$ 1.00*	1 Hour
\$ 6.00*	1 Day (Standard Vehicle - Regular Operating Hours for Lot)***
\$ 3.00*	1 Day (Motorcycle – Regular Operating Hours for Lot)
\$ 5.00*	24 Hour Parking for Vehicle with Boat Trailer Only (Cook Rd Boat Launch)
\$ 7.00*	Special Event Parking
	*Applicable taxes are included.
\$ 12.00**	1 Month – Bicycle Locker Rental
\$ 62.87**	1 Month – Random Parking
	**Plus Applicable taxes.

*** Not Available at all parking lots.

Available only in designated **Off-Street Pay Parking** – Peripheral Lots located at:

- Waterfront Park Lot (1200 Water Street)
- City Park Lot (1600 Abbott St) – *Monthly Parking in Off-Season Only*
- Cook Rd Boat Launch (3786 Lakeshore Rd) – *Monthly Parking in Off-Season Only*

And replacing it with:

"Subsection 4.4.2(b) – Off-Street Pay Parking – Peripheral Lot (Long Term Parking Lot)"

DESCRIPTION	TERM	FEE (Zone 5)	FEE (Zone 6)
Hourly (Vehicle)	1 Hour	\$1.00 ¹	\$1.00 ¹
Daily (Vehicle) ³	1 (Calendar) Day in the Summer Season	\$6.00 ¹	\$10.00 ¹
Daily (Vehicle) ³	1 (Calendar) Day in the Off-Season	\$6.00 ¹	\$6.00 ¹
Daily (Motorcycle) ³	1 (Calendar) Day	\$3.00 ¹	\$3.00 ¹
Vehicle with Boat Trailer	1 Hour in the Summer Season	Not Available	\$5.00 ¹ (Effective May 15, 2020)
Vehicle with Boat Trailer	12 Hours in the Summer Season	Not Available	\$25.00 ¹ (Effective May 15, 2020)
Vehicle with Boat Trailer	24 Hours in the Summer Season	Not Available	\$40.00 ¹ (Effective May 15, 2020)
Vehicle with Boat Trailer	1 Hour in the Off-Season	Not Available	\$2.00 ¹ (Including Summer 2019)
Vehicle with Boat Trailer	12 Hours in the Off-Season	Not Available	\$12.00 ¹ (Including Summer 2019)
Vehicle with Boat Trailer	24 Hours in the Off-Season	Not Available	\$20.00 ¹ (Including Summer 2019)
Special Events ³	As/when Indicated On-Site	Hourly/Daily	Hourly/Daily
Bicycle Locker ³	1 Calendar Month	\$12.00 ²	\$12.00 ²
Random Parking Permit ³	1 Calendar Month	\$65.86 ² (Sept 1, 2019)	\$65.86 ² (Sept 1, 2019)
		\$75.00 ² (June 1, 2020)	\$75.00 ² (June 1, 2020)

¹ Applicable taxes included.

² Plus Applicable taxes.

³ Not Available at all locations.

Zone 5 - Off-Street Pay Parking – Peripheral Lots:

- City Park Lot 616 (1600 Abbott St) – *Monthly Parking in Off-Season Only*
- Gyro Beach Park Lot 639 (3360 Lakeshore Rd) – *Monthly Parking in Off-Season Only*
- Rotary Beach Park Lot (3726 Lakeshore Rd) – *Monthly Parking in Off-Season Only*

Zone 6 - Off-Street Pay Parking – Peripheral Lots:

- Cook Rd Boat Launch Lot 618 (3786 Lakeshore Rd) – *Monthly Parking in Off-Season Only*

8. AND THAT SCHEDULE "A" FEES, PART 4 – PARKING REGULATIONS, subsection **4.4.3 Off-Street Pay Parking (Structured Parking Garage)** be deleted that reads:

"Subsection 4.4.3 Off-Street Pay Parking (Structured Parking Garage)"

FEE

PERMITTED PARKING TIME

\$ 1.00*	1 Hour
\$ 6.00*	1 Day (Standard Vehicle - Regular Operating Hours for Lot)***
\$ 3.00*	1 Day (Motorcycle – Regular Operating Hours for Lot)
\$ 5.00*	Special Event Parking (Library Plaza and Chapman Parkades)
\$ 3.00*	Special Event Parking (Memorial Parkade Only)

*Applicable taxes are included.

\$ 12.00**	1 Month – Bicycle Locker Rental
\$ 78.08**	1 Month – Random Parking
\$ 117.62**	1 Month – Reserved Stall***

**Plus Applicable taxes.

*** Not Available at all parking garages."

And replacing it with:

"Subsection 4.4.3 Off-Street Pay Parking (Structured Parking Garage)"

DESCRIPTION	TERM	FEE (Zone 7)	FEE (Zone 8)
Hourly	1 Hour	\$1.00 ¹	\$1.00 ¹
Daily (Vehicle) ³	1 Calendar Day	\$6.00 ¹	\$6.00 ¹
Daily (Motorcycle) ³	1 Calendar Day	\$3.00 ¹	\$3.00 ¹
Special Events	As/When Indicated On-Site	\$5.00 ¹	\$3.00 ¹
Bicycle Locker ³	1 Calendar Month	\$12.00 ²	\$12.00 ²
Random Parking Permit ³	1 Calendar Month	\$81.79 ² (Sept 1, 2019)	\$81.79 ² (Sept 1, 2019)
		\$90.00 ² (June 1, 2020)	\$90.00 ² (June 1, 2020)
Reserved Stall Permit ³	1 Calendar Month	\$150.00 ²	Not Available

¹ Applicable taxes included.

² Plus Applicable taxes.

³ Not Available at all locations.

Zone 7 – Structured Parking Garages:

- Library Plaza Parkade - Lot 604 (1360 Ellis Street)
- Chapman Parkade - Lot 613 (345 Lawrence Avenue)

Zone 8 – Structured Parking Garages:

- Memorial Parkade - Lot 641 (1420 Ellis Street)”

9. AND THAT **SCHEDULE “A” FEES, PART 4 – PARKING REGULATIONS** be amended by adding in its appropriate location the following:

a) **Subsection 4.6.1(a) Car Share Parking Fees (Community Based Car Share Vehicles)**

DESCRIPTION	TERM	FEE
On-Street Pay Parking Area	1 Calendar Month	50%¹ of On-Street Pay Parking Daily Permit Fee (Subsection 4.3.1(d))
Off-Street Pay Parking – Long Term Lot	1 Calendar Month	50%¹ of Off-Street Pay Parking (Long Term Lot) Fee (Subsection 4.4.2(a))
Off-Street Pay Parking – Long Term (Peripheral) Lot	1 Calendar Month	50%¹ of Off-Street Pay Parking (Long Term Peripheral Lot) Fee (Subsection 4.4.2(b))
Off-Street Pay Parking – Structured Garage	1 Calendar Month	50%¹ of Off-Street Pay Parking (Structured Parking Garage) Fee (Subsection 4.4.3)
On-Street Non-Pay Parking Area, Reserved Space	1 Calendar Month	25%¹ of Off-Street Pay Parking (Long Term Peripheral Lot) Fee (Subsection 4.4.2(b))
On-Street Non-Pay Parking Area, Random Permit (Non-Reserved)	1 Year	Admin Fee - 2 x Current Rate for a 12-month Residential Permit Fee in Subsection 4.3.2
Off-Street Non-Pay Parking Area, Random Permit (Non-Reserved)	1 Year	Admin Fee - 2 x Current Rate for a 12-month Residential Permit Fee in Subsection 4.3.2

¹ First year rate will be 50% of posted fees to encourage adoption of newly added vehicles.

b) **Subsection 4.6.1(b) Car Share Parking (All Other/For Profit)**

DESCRIPTION	TERM	FEE
On-Street Pay Parking Area	1 Calendar Month	80%¹ of On-Street Pay Parking Daily Permit Fee (Subsection 4.3.1(d))
Off-Street Pay Parking – Long Term Lot	1 Calendar Month	80%¹ of Off-Street Pay Parking (Long Term Lot) Fee (Subsection 4.4.2(a))
Off-Street Pay Parking – Long Term (Peripheral) Lot	1 Calendar Month	80%¹ of Off-Street Pay Parking (Long Term Peripheral Lot) Fee (Subsection 4.4.2(b))
Off-Street Pay Parking – Structured Garage	1 Calendar Month	80%¹ of Off-Street Pay Parking (Structured Parking Garage) Fee (Subsection 4.4.3)

On-Street Non-Pay Parking Area, Reserved Space	1 Calendar Month	40% ¹ of Off-Street Pay Parking (Long Term Peripheral Lot) Fee (Subsection 4.4.2(b))
On-Street Non-Pay Parking Area, Random Permit (Non-Reserved)	1 Year	Admin Fee - 4 x Current Rate for a 12-month Residential Permit Fee in Subsection 4.3.2
Off-Street Non-Pay Parking Area, Random Permit (Non-Reserved)	1 Year	Admin Fee - 4 x Current Rate for a 12-month Residential Permit Fee in Subsection 4.3.2

¹ First year rate will be 50% of posted fees to encourage adoption of newly added vehicles.

10. AND THAT **SCHEDULE "A" FEES, PART 4 – PARKING REGULATIONS**, subsection **4.6 Parking Fees and Charges – Annual CPI Adjustment** be deleted that reads:

"Subsection 4.6 Parking Fees and Charges – Annual CPI Adjustment"

Parking rates in this Schedule are based upon the 2016 **Consumer Price Index (CPI)**. **Annual Parking Rate Adjustments** will be made annually on June 1st, based on the **CPI** (British Columbia, all items) value from the preceding year. Any adjustments to short-term parking rates (hourly, daily, special event) will be rounded to the nearest \$0.25 for ease of use by customers paying with cash/coin. Annual adjustment calculations that do not result in an amendment will be compounded until such time an increase is indicated."

And replacing it with:

"Subsection 4.7 Parking Fees and Charges – Annual CPI Adjustment"

Parking rates and fees shown in Part 4 of Schedule A are based upon the 2016 **Consumer Price Index (CPI)**. **Annual Parking Rate Adjustments** to all fees and charges in Part 4 of Schedule A will be made each year on June 1st, based on the **CPI** (British Columbia, all items) value from the preceding year. Any adjustments to short-term parking rates (hourly, daily, special event) will be rounded to the nearest \$0.25 for ease of use by customers paying with cash/coin. Annual adjustment calculations that do not result in an amendment will be compounded until such time an increase is indicated."

11. AND THAT **SCHEDULE "A" FEES, PART 5 – HIGHWAY USE REGULATIONS**, subsection **5.4.2 Road Usage and Hoarding Permit (RUP & HP)** be deleted that reads:

Meter Bags	\$0	\$5.00 Per day per bag	\$25.00 per bag
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And replacing it with:

Meter Bags	\$0	Refer to SCHEDULE "A" FEES, Subsection 4.3.1(d) - Reserved On-Street Parking Permit Fees for rates
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12. This bylaw may be cited for all purposes as "Bylaw No. 11851, being Amendment No. 34 to Traffic Bylaw No. 8120."
13. This bylaw shall come into full force and effect and be binding on all persons as of as of the date of adoption.

Read a first, second and third time by the Municipal Council this 29th day of July, 2019.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

CITY OF KELOWNA

BYLAW NO. 11852

Amendment No. 25 to Bylaw Notice Enforcement Bylaw No. 10475

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Bylaw Notice Enforcement Bylaw No. 10475 be amended as follows:

1. THAT **Schedule "A", Traffic Bylaw No. 8120, PART 4 – PARKING REGULATIONS** be amended by:

- a) deleting Section 4.1.2 (ff) that reads:

8120	4.1.2 (ff)	Parked in same block	\$20.00	\$15.00	\$25.00	No
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And replacing it with:

8120	4.1.2 (ff)	Parked in same block-face	\$30.00	\$25.00	\$35.00	No
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- b) adding new Sections 4.1.2 (uu) and 4.1.2 (vv) in their appropriate location that reads:

8120	4.1.2 (uu)	Obstructed licence plate	\$30.00	\$25.00	\$35.00	No
8120	4.1.2 (vv)	Parked over 3 hours within urban centre residential area (effective May 15, 2020)	\$30.00	\$25.00	\$35.00	No

- c) deleting Section 4.5.1 that reads:

8120	4.5.1	Tamper with parking meter	\$100.00	\$90.00	\$110.00	No
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And replacing it with:

8120	4.5.1	Tamper with parking meter	\$500.00	\$450.00	\$500.00	No
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2. This bylaw may be cited for all purposes as "Bylaw No. 11852 being Amendment No. 25 to Bylaw Notice Enforcement Bylaw No. 10475."
3. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first, second and third time by the Municipal Council this 29th day of July, 2019.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

CITY OF KELOWNA

BYLAW NO. 11853

Amendment No. 2 to Payment in Lieu of Parking Bylaw No. 8125

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts that the City of Kelowna Payment in Lieu of Parking Bylaw No. 8125 be amended as follows:

1. THAT Section 2 be deleted in its entirety that reads:

"This bylaw applies to all lands designated as being within an Urban Town Centre as established by Kelowna Official Community Plan (1994-2013) Bylaw No. 7600."

And replaced with:

"This bylaw applies to all lands designated as being within an Urban Centre as established by Kelowna 2030 – Official Community Plan Bylaw No. 10500."

2. AND THAT Section 3 be deleted in its entirety that reads:

"An owner or occupier of a parcel of land within an Urban Town Centre, in respect of which off-street parking facilities are required under the City of Kelowna Zoning Bylaw, 1976, No. 4500, as amended or replaced from time to time, may, in lieu of providing the required off-street parking spaces, pay to the City a sum as set out in Schedule "A" of this bylaw for all or any portion of the required number of off-street parking spaces."

And replacing it with:

"An owner or occupier of a parcel of land within an Urban Centre Area, in respect of which off-street parking facilities are required under the City of Kelowna Zoning Bylaw No. 8000, as amended or replaced from time to time, may, in lieu of providing the required off-street parking spaces, pay to the City a sum as set out in Schedule "A" of this bylaw for all or any portion of the required number of off-street parking spaces."

3. AND THAT a new Section 4 be added and subsequent Sections be renumbered that reads:

"Where a complete development application was received by the City prior to September 1, 2019 and the building permit for that application is issued before September 1, 2020, the fee per off-street parking space in effect on August 31, 2019 will apply."

4. AND THAT Schedule "A" be deleted in its entirety and replaced with a new Schedule "A" as attached to and forming part of this bylaw.
5. This bylaw may be cited for all purposes as "Bylaw No.11853, being Amendment No 2 to Payment in Lieu of Parking Bylaw No. 11853."

6. This bylaw shall come into full force and effect and is binding on all persons as of September 1, 2019.

Read a first, second and third time by the Municipal Council this 29th day of July, 2019.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

SCHEDULE "A"

URBAN CENTRE	FEE PER OFF-STREET PARKING SPACE
City Centre (Downtown) Urban Centre Area	\$ 33,000*
Midtown Urban Centre Area	\$ 33,000*
South Pandosy Urban Centre Area	\$ 33,000*
Rutland Urban Centre Area	\$ 9,500*
Capri / Landmark Urban Centre Area	\$ 33,000*

***Annual CPI Adjustment**

Payment in Lieu of Parking Fees in this Schedule are based upon the 2019 Consumer Price Index (CPI). Fee adjustments will be made annually on June 1st, based on the CPI (British Columbia, all items) value from the preceding year and adjustments will be rounded to the nearest \$1.00.