

City of Kelowna

Regular Council Meeting

AGENDA



Monday, January 22, 2018
1:30 pm
Council Chamber
City Hall, 1435 Water Street

Pages

1. Call to Order

This meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

2. Confirmation of Minutes

5 - 14

PM Meeting - January 15, 2018

3. Committee Reports

3.1 Journey Home Task Force Terms of Reference Amendment and Additional Appointments

15 - 21

To obtain Council's approval of the amendment to the Journey Home Task Force Terms of Reference and additional recommendations for appointments to the Journey Home Task Force.

3.2 Journey Home Task Force Update

22 - 37

To provide Council with an update on the progress of the Journey Home Task Force and work plan to develop Kelowna's long-term plan to address homelessness.

4. Development Application Reports & Related Bylaws

4.1 Moberly Rd 3439, Z17-0090 - Steven and Colleen Hall

38 - 45

To rezone the subject property to facilitate the construction of a carriage house.

4.2 Moberly Rd 3439, BL11538 (Z17-0090) - Steven and Colleen Hall

46 - 46

To give Bylaw No. 11538 first reading in order to rezone the subject property from the RU2 - Medium Lot Housing zone to the RU2c - Medium Lot Housing with Carriage House zone.

| | | |
|-------------|---|----------------|
| 4.3 | Christleton Ave 344, Z17-0102 - Christopher and Sara Eddy | 47 - 51 |
| | To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house. | |
| 4.4 | Christleton Ave 344, BL11539 (Z17-0102) - Christopher and Sara Eddy | 52 - 52 |
| | To give Bylaw No. 11539 first reading in order to rezone the subject property from the RU1 - Large Lot Housing zone to the RU1c - Large Lot Housing with Carriage House zone. | |
| 4.5 | Maple St 1847, Z17-0070 - Robert and Marianne Law | 53 - 56 |
| | To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house. | |
| 4.6 | Maple St 1847, BL11541 (Z17-0070) - Robert and Marianne Law | 57 - 57 |
| | To give Bylaw No. 11541 first reading in order to rezone the subject property from the RU1 - Large Lot Housing zone to the RU1c - Large Lot Housing with Carriage House zone. | |
| 4.7 | Pitcairn Ct 911, Z17-0084 - Glen and Charmaine Manlove | 58 - 62 |
| | To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house. | |
| 4.8 | Pitcairn Ct 911, BL11542 (Z17-0084) - Glen and Charmaine Manlove | 63 - 63 |
| | To give Bylaw No. 11542 first reading in order to rezone the subject property from the RU1 - Large Lot Housing zone to the RU1c - Large Lot Housing with Carriage House zone. | |
| 4.9 | Treadgold Ct 5059, Z17-0029 - Travis Shipka | 64 - 68 |
| | To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house. | |
| 4.10 | Treadgold Ct 5059, BL11543 (Z17-0029) - Travis Shipka | 69 - 69 |
| | To give Bylaw No. 11543 first reading in order to rezone the subject property from the RU1 - Large Lot Housing zone to the RU1c - Large Lot Housing with Carriage House zone. | |
| 4.11 | Bernard Ave 565, DP17-0242 - MKK Property Corp Inc. | 70 - 95 |
| | To review the Form & Character Development Permit of exterior façade renovations and building cladding updates for the Town Centre Mall. | |

| | | |
|-------------|---|------------------|
| 4.12 | Stewart Road West 4500, Farm Help Development Permit Application FH17-0001 - Stewart Brothers Winery Ltd. Inc. | 96 - 115 |
| | To consider a request for a Farm Help Development Permit for two proposed temporary residences on non-permanent foundations to accommodate 22 temporary agricultural workers on the subject property. | |
| 5. | Bylaws for Adoption (Development Related) | |
| 5.1 | Grenfell Rd 934, Z17-0058 - Allan and Annette Lipkovits | 116 - 116 |
| | To adopt Bylaw No. 11494 in order to rezone the subject property from the RU1 - Large Lot Housing zone to the RU6 - Two Dwelling Housing zone. | |
| 6. | Non-Development Reports & Related Bylaws | |
| 6.1 | Rental Housing Grant Recommendation 2018 | 117 - 129 |
| | To consider approval of four purpose-built rental housing projects for rental housing grants in 2018, subject to final budget approval. | |
| 6.2 | Budget Adjustment Required for Highway 97 Sanitary Manhole Upgrades | 130 - 131 |
| | To transfer budget from the identified 2017 capital project, to meet financial commitments for the Highway 97 Sanitary Manhole Upgrades projects. | |
| 6.3 | Kelowna Bikeshare Proposal | 132 - 192 |
| | To present Council with a proposal for an 18-month bikeshare pilot in Kelowna. | |
| 6.4 | Road Closure and Sale – Adjacent to 1969 Cross Road | 193 - 195 |
| | To close a 0.148-hectare portion of excess laneway adjacent to 1969 Cross Road. | |
| 6.5 | BL11534 - Road Closure and Removal of Hwy Dedication - Portions of Cross Road | 196 - 197 |
| | To give Bylaw No. 11534 first, second and third readings in order to authorize the City to permanently close and remove the highway dedication of a portion of highway on Cross Road. | |

7. Bylaws for Adoption (Non-Development Related)

7.1 BL11516 - Road Closure and Removal of Hwy Dedication - Portions of Queensway Ave and Water St 198 - 199

Mayor to invite anyone in the public gallery who deems themselves affected by the proposed road closure to come forward.

To adopt Bylaw No. 11516 in order to authorize the City to permanently close and remove the highway dedication of a portion of highway on Queensway Ave and Water St.

8. Mayor and Councillor Items

9. Termination



City of Kelowna

Regular Council Meeting

Minutes

Date: Monday, January 15, 2018
 Location: Council Chamber
 City Hall, 1435 Water Street

Council Members Present: Mayor Colin Basran and Councillors Ryan Donn, Gail Given, Tracy Gray, Charlie Hodge, Brad Sieben and Luke Stack

Council Members Absent: Councillors Maxine DeHart and Mohini Singh

Staff Present: City Manager, Ron Mattiussi*; City Clerk, Stephen Fleming; Divisional Director, Community Planning & Strategic Investments, Doug Gilchrist*; Community Planning Department Manager, Ryan Smith*; Planner, Melanie Stepphun*; Planner Specialist, Laura Bentley*; Urban Planning Manager, Terry Barton*; Policy & Planning Department Manager, Danielle Noble-Brandt*; Planner Specialist, Ross Soward*; Financial Analyst, Matt Friesen*; Infrastructure Operations Department Manager, Ian Wilson*; Parks Services Manager, Blair Stewart*; Divisional Director, Infrastructure, Alan Newcombe*; Utility Planning Manager, Rod MacLean*; FOI-Legislative Coordinator, Sandi Horning

(*denotes partial attendance)

1. Call to Order

Mayor Basran called the meeting to order at 1:31 p.m.

Mayor Basran advised that the meeting is open to the public and all representations to Council form part of the public record. A live audio and video feed is being broadcast and recorded by CastaNet and a delayed broadcast is shown on Shaw Cable.

2. Confirmation of Minutes

Moved By Councillor Hodge/Seconded By Councillor Donn

R030/18/01/15 THAT the Minutes of the PM Meeting of January 8, 2018 be confirmed as circulated.

Carried

3. Committee Reports

3.1 Agricultural Advisory Committee – Annual Report to Council

Staff:

- Displayed a PowerPoint presentation and provided a summary of agriculture related initiatives and files.
- Introduced, John Janmaat, the Chair of the Agricultural Advisory Committee.

John Janmaat, Chair, Agricultural Advisory Committee Chair:

- Displayed a PowerPoint presentation summarizing the annual report.
- Responded to questions from Council.

Moved By Councillor Hodge/Seconded By Councillor Gray

R031/18/01/15 THAT Council receives, for information, the Report from the Agricultural Advisory Committee dated December 11, 2017 with respect to an annual report to Council.

Carried

4. Development Application Reports & Related Bylaws

4.1 OCP Amendment (OCP17-0027) & Rezoning Application (Z17-0107) – Boundaries Amendments

Staff:

- Displayed a PowerPoint Presentation summarizing the application and responded to questions from Council.

Moved By Councillor Stack/Seconded By Councillor Sieben

R032/18/01/15 THAT Official Community Plan Map Amendment Application No. OCP17-0027 to amend Map 4.1 in the Kelowna 2030 – Official Community Plan Bylaw No. 10500 by changing the Future Land Use designation of several properties as indicated the Future Land Use

Designation Discrepancies Table and shown on Maps "A" to "H" attached to the Report from the Community Planning Department dated January 8, 2018 be considered by Council;

AND THAT the Official Community Plan Map Amending Bylaw be forward to a Public Hearing for further consideration;

AND THAT Council considers the Public Hearing process to be appropriate consultation for the purpose of Section 475 of the *Local Government Act*;

AND THAT Rezoning Application No. Z17-0107 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of several properties as indicated the Future Land Use Designation Discrepancies Table and shown on Maps "I" to "T" attached to the Repot from the Community Planning Department dated January 8, 2018 be considered by Council;

AND FURTHER THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration.

Carried

4.2 Various Addresses, BL11527 (OCP17-0027) - City of Kelowna

Moved By Councillor Donn/Seconded By Councillor Gray

R033/18/01/15 THAT Bylaw No. 11527 be read a first time;

AND THAT the bylaw has been considered in conjunction with the City's Financial Plan and Waste Management Plan.

Carried

4.3 Various Addresses, BL11536 (Z17-0107) - City of Kelowna

Moved By Councillor Donn/Seconded By Councillor Gray

R034/18/01/15 THAT Bylaw No. 11536 be read a first time.

Carried

4.4 McIntosh Rd 360, Dougall Rd N 345 & 365 - Z17-0041 - Unik-Town Development Inc.

Staff:

- Displayed a PowerPoint Presentation summarizing the application and responded to questions from Council.

Moved By Councillor Sieben/Seconded By Councillor Hodge

R035/18/01/15 THAT Rezoning Application No. Z17-0041 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of each of the following properties:

- Lot A Sec 26 Twp 26 ODYD Plan 6977, located at 360 McIntosh Road, Kelowna, BC;
- Lot B Sec 26 Twp 26 ODYD Plan 6977, located at 345 Dougall Road, Kelowna, BC
- Lot C Sec 26 Twp 26 ODYD Plan 6977, located at 365 Dougall Road, Kelowna, BC;

from the RU1 – Large Lot Housing zone to the RM5 – Medium Density Multiple Housing zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Community Planning Department dated January 23, 2017;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered in conjunction with Council's consideration of a Development Permit, Development Variance Permit for the subject property.

Carried

4.5 McIntosh Rd 360, Dougall Rd N 345 & 365 - BL11528 (Z17-0041) - Unik-Town Development Inc.

Moved By Councillor Donn/Seconded By Councillor Hodge

R036/18/01/15 THAT Bylaw No. 11528 be read a first time.

Carried

4.6 Lakeshore Rd 5076, LUCT16-0001 & Z16-0027 - Stewart Storie and Gloria Heung

Staff:

- Displayed a PowerPoint Presentation summarizing the application.

Moved By Councillor Given/Seconded By Councillor Gray

R037/18/01/15 WHEREAS the BC Provincial Government has mandated that all Land Use Contracts under the jurisdiction of a local government and in the Province of British Columbia be terminated by 2024;

AND WHEREAS the BC Provincial Government has provided a legislated process for the early termination of land use contracts when the local government has adopted a zoning bylaw that will apply to the land at the time the termination bylaw comes into force;

AND THAT Application No. LUCT16-0001 to terminate LUC77-1012 from Lot A and Lot B, Section 23, Township 28, SDYD, Plan 30063, located on 5064 and 5076 Lakeshore Road, Kelowna, BC, be considered by Council;

AND WHEREAS the underlying A1 – Agriculture 1 zone in the City of Kelowna Zoning Bylaw No. 8000 applies to Lot A and Lot B, Section 23, Township 28, SDYD, Plan 30063, located on 5064 and 5076 Lakeshore Road, Kelowna, BC under Land Use Contract LUC77-1012;

THEREFORE, BE IT RESOLVED THAT as the underlying A1 – Agriculture 1 zone Lot A and Lot B, Section 23, Township 28, SDYD, Plan 30063, located on 5064 and 5076 Lakeshore Road, Kelowna, BC under Land Use Contract LUC77-1012 does not meet the land use requirements under City of Kelowna Zoning Bylaw No. 8000;

AND THAT Rezoning Application No. Z16-0027 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification for Lot A and Lot B, Section 23, Township 28, SDYD, Plan 30063, located on 5064 and 5076 Lakeshore Road, Kelowna, BC from the A1 - Agriculture 1 zone to the RR2 – Rural Residential 2 zone be considered by Council;

AND FURTHER THAT the Land Use Contract Termination Bylaw and Rezoning Bylaw be forwarded to a Public Hearing for further consideration.

Carried

4.7 Lakeshore Rd 5076, BL11529 (LUCT16-0001) - Stewart Storie and Gloria Heung

Moved By Councillor Hodge/Seconded By Councillor Donn

R038/18/01/15 THAT Bylaw No. 11529 be read a first time.

Carried

4.8 Lakeshore Rd 5076, BL11531 (Z16-0027) - Stewart Storie and Gloria Heung

Moved By Councillor Gray/Seconded By Councillor Hodge

R039/18/01/15 THAT Bylaw No. 11531 be read a first time.

Carried

4.9 DeHart Rd 644, Z17-0097 - Ricardo & Patricia Tomic

Staff:

- Displayed a PowerPoint Presentation summarizing the application.

Moved By Councillor Given/Seconded By Councillor Gray

R040/18/01/15 THAT Rezoning Application No. Z17-0097 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 1 District Lot 358 Osoyoos Division Yale District Plan 39262, located at 644 DeHart Road, Kelowna, BC from the RU1 - Large Lot Housing zone to the RU6 – Two Dwelling Housing zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Community Planning Department dated January 15, 2018.

Carried

4.10 DeHart Rd 644, BL11533 (Z17-0097) - Ricardo & Patricia Tomic

Moved By Councillor Hodge/Seconded By Councillor Gray

R041/18/01/15 THAT Bylaw No. 11533 be read a first time.

Carried

4.11 Glenmore Rd 621, Z17-0106 - Mehdi Tehrani & Mandana Ghanyei

Staff:

- Displayed a PowerPoint Presentation summarizing the application.

Moved By Councillor Stack/Seconded By Councillor Donn

R042/18/01/15 THAT Rezoning Application No. Z17-0106 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot A Section 32 Township 26 ODYD Plan 6835 Except Plan KAP71534, located at 621 Glenmore Road, Kelowna, BC from the A1 – Agriculture 1 zone to the Ru6 – Two Dwelling Housing zone, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Community Planning Department dated January 15, 2018;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered subsequent to the issuance of a Preliminary Layout Review Letter by the Approving Officer.

Carried

4.12 Glenmore Rd 621, BL11535 (Z17-0106) - Mehdi Tehrani and Mandana Ghanyei

Moved By Councillor Sieben/Seconded By Councillor Stack

R043/18/01/15 THAT Bylaw No. 11535 be read a first time.

Carried

4.13 Rutland Rd N 1308 & 1324, Z17-0047 - 1121911 BC Ltd

Staff:

- Displayed a PowerPoint Presentation summarizing the application and responded to questions from Council.

Moved By Councillor Sieben/Seconded By Councillor Stack

R044/18/01/15 THAT Rezoning Application No. Z17-0047 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of each of the following properties:

- Lot 2, Sec 35, Twp 26, ODYD, Plan 14663, located at 1324 Rutland Rd N, Kelowna, BC;
- Lot 3, Sec 35, Twp 26, ODYD, Plan 14663, located at 1308 Rutland Rd N, Kelowna, BC

from the RU1 – Large Lot Housing zone to the RM3 – Low Density Multiple Housing zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Community Planning Department dated January 15, 2018;

AND FURTHER THAT final adoption of the Rezoning Bylaw be considered in conjunction with Council's consideration of a Development Permit for the subject property.

Carried

4.14 Rutland Rd N 1308 & 1324, BL11537 (Z17-0047) - 1121911 BC Ltd

Moved By Councillor Stack/Seconded By Councillor Sieben

R045/18/01/15 THAT Bylaw No. 11537 be read a first time.

Carried

4.15 Richter Street 1800, DP17-0179 - Meiklejohn Architects Inc.

Staff:

- Displayed a PowerPoint Presentation summarizing the application.
- Noted the OCP future land use should be identified as medium density residential rather than mixed use (residential/commercial) as indicated on page 1 of the staff report.
- Responded to questions from Council.

Moved By Councillor Stack/Seconded By Councillor Sieben

R046/18/01/15 THAT Council authorizes the issuance of Development Permit No. DP17-0179 for Lot A District Lots 14 and 139 ODYD Plan EPP18764, located at 1800 Richter Street, Kelowna, BC subject to the following:

1. The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";
2. The exterior design and finish of the building to be constructed on the land, be in accordance with Schedule "B";
3. Landscaping to be provided on the land be in accordance with Schedule "C";
4. The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect;

AND THAT the applicant be required to complete the above noted conditions of Council's approval of the Development Permit Application in order for the permits to be issued;

AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

Carried

5. Non-Development Reports & Related Bylaws

5.1 Capri-Landmark - Preferred Concept Plan

Staff:

- Displayed a PowerPoint Presentation summarizing the preferred concept plan and responded to questions from Council.

Moved By Councillor Given/Seconded By Councillor Donn

R047/18/01/15 THAT Council receives, for information, the Report from the Planner Specialist dated January 15, 2018, with respect to the Preferred Concept Plan for the Capri-Landmark area;

AND THAT Council approves the Preferred Concept Plan in principle and directs staff to move forward with the development of the final plan and implementation strategy as outlined in the report from the Planner Specialist dated January 15, 2018;

AND FURTHER THAT Council directs staff to further explore the feasibility of the funding options outlined in the report from the Planner Specialist dated January 15, 2018.

Carried

Councillor Stack – Opposed.

5.2 Transit 2017/2018 Amended Annual Operating Agreement

Staff:

- Displayed a PowerPoint Presentation summarizing the amendments to the 2017/2018 Transit Annual Operating Agreement.

Moved By Councillor Donn/Seconded By Councillor Gray

R048/18/01/15 THAT Council approves the City entering into Amendment #1 to the Kelowna Regional 201/2018 Annual Operating Agreement between the City of Kelowna and BC Transit for conventional, community and custom transit;

AND THAT the Mayor and City Clerk be authorized to execute Amendment #1 to the Kelowna Regional 2017/2018 Annual Operating Agreement on behalf of the City.

Carried

5.3 Urban Deer in Kelowna

Staff:

- Provided an overview of the staff report and responded to questions from Council.

Moved By Councillor Sieben/Seconded By Councillor Donn

R049/18/01/15 THAT Council receives, for information, the January 10, 2018 Report from the Park Services Manager regarding urban deer in Kelowna;

AND THAT Council directs staff to forward a request to the Regional District of Central Okanagan to consider becoming a member of WildSafe BC.

Carried

5.4 Sanitary Sewer Connection Area 28 - Stellar Drive Extension Charges

Staff:

- Provided an overview of the staff report.

Moved By Councillor Stack/Seconded By Councillor Hodge

R050/18/01/15 THAT Council receives for information, the report from the Utility Planning Manager dated January 15, 2018, with respect to sanitary sewer servicing work completed on Stellar Drive within Sanitary Sewer Connection Area 28;

AND THAT Council authorizes the expenditure of \$172,100 from the Sewer Connection Area 28 reserve to pay for the sanitary sewer servicing;

AND FURTHER THAT the 2018 Financial Plan be amended to include the \$172,100 Stellar Drive Extension project, with funding from the Sanitary Sewer Connection Area 28 Reserve.

Carried

6. Mayor and Councillor Items

Councillor Gray:

- Acknowledged the three (3) firefighters who were injured and wished them best wishes on their recovery.

-

Councillor Sieben:

- Offered condolences to the family of the person who died in the mobile home fire.

Mayor Basran:

- Thanked Councillor Gray for being Deputy Mayor in his absence last week.
- Looking forward to a busy 2018.

7. Termination

This meeting was declared terminated at 4:42 p.m.

Mayor Basran

[Signature]

City Clerk

/slh

Report to Council



Date: January 22, 2018

File: 0615-20-02

To: City Manager

From: Co-Chairpersons, Journey Home Task Force

Subject: Journey Home Task Force Terms of Reference Amendment and Additional Appointments

Prepared by: Social Development Manager, Active Living & Culture

Recommendation:

THAT Council receives, for information, the Report from the Journey Home Task Force Co-Chairpersons dated January 22, 2018 with respect to the Journey Home Task Force;

AND THAT Council approves the amended Journey Home Task Force Terms of Reference to increase the membership from 21 to 23 members in the form attached as Appendix A to the Report from the Journey Home Task Force Co-Chairpersons dated January 22, 2018;

AND THAT Council appoint as members Westbank First Nation and Ki-Low-Na Friendship Society, through their representatives, to the Journey Home Task Force.

Purpose:

To obtain Council's approval of the amendment to the Journey Home Task Force Terms of Reference and additional recommendations for appointments to the Journey Home Task Force.

Background:

At the regular meeting on September 11, 2017, Council approved the Terms of Reference for the Journey Home Task Force and the appointment of the Task Force Members.

The current membership of the Journey Home Task Force consists of 21 members: the two Co-Chairpersons (Dr. Myrah and Mr. Bell); six organization representatives (Central Okanagan Foundation, BC Housing, Ministry of Social Development & Poverty Reduction, Interior Health, Okanagan Nation Alliance, and RCMP); and thirteen community members appointed to represent a wide variety of sectors, experiences, skills and knowledge areas.

As the work of the Journey Home Task Force proceeds, it has come to the attention of the Task Force Co-Chairpersons that strengthening the Task Force's knowledge in relation to challenges faced by the local Indigenous population is essential. In conversation with the appointed Okanagan Nation Alliance

representative, it was noted that with Indigenous peoples making up approximately one quarter of the local population of people experiencing homelessness, additional Indigenous perspectives are vital to the work of the Task Force. As a result, the Co-Chairpersons recommend that the Journey Home Task Force Terms of Reference to be amended to expand the number of members from 21 to 23 to include two additional organizations, Westbank First Nation with representative Diane Roy and Ki-Low-Na Friendship Society with representative Tina Larouche.

Current Task Force Members:

In addition to the Co-Chairpersons, there are currently 19 appointed members on the Journey Home Task Force (* represent the six appointed organizations):

1. BC Housing – Ann Howard (Housing) *
2. Central Okanagan Foundation – Cheryl Miller (Funding) *
3. Interior Health Authority – John Yarschenko (Health) *
4. Ministry of Social Development & Poverty Reduction (Public Systems) *
5. Okanagan Nation Alliance – Jennifer Houde/Shawna Whitney (Cultural) *
6. RCMP – Brent Mundle (Public Systems) *
7. Theresa Arsenault, QC (Private)
8. Gaelene Askeland (Housing; Social Services)
9. Tom Dyas (Private)
10. Diane Entwistle (Social Services)
11. Mike Gawliuk (Housing; Health)
12. David Krysko (Housing)
13. Scott Lanigan (Community)
14. Carrie McDonald (Community)
15. Lisa McHaffie (Public Systems)
16. Pam Moore (Community)
17. Brenda Plitt (Community)
18. Kelly Taverner (Community)
19. Shane Worman (Housing)

The Journey Home Task Force membership was selected using a skills and knowledge inventory matrix to ensure a diversity of perspectives that will strengthen the development of a collective strategy. This collaborative approach is vital to successfully integrate Kelowna's housing and homelessness services. The goal of the strategy is to assist residents in achieving housing stability and improve their well-being while ensuring the overall safety, health, and sustainability of the community. To successfully develop and implement the Journey Home Strategy, additional Indigenous representation is essential.

Internal Circulation:

J. Gabriel, Divisional Director, Active Living & Culture
K. Needham, Deputy City Clerk
L. Ruether, Communications Advisor

Considerations not applicable to this report:

Legal/Statutory Authority

Legal/Statutory Procedural Requirements
Existing Policy
External Agency/Public Comments
Communications Comments
Alternate Recommendation
Financial/Budgetary Considerations
Personnel Implications

Submitted by:

Journey Home Task Force Co-Chairpersons

Approved for inclusion: J. Gabriel, Divisional Director, Active Living & Culture

Cc: Divisional Director, Active Living & Culture
Divisional Director, Community Planning & Real Estate
Divisional Director, Corporate & Protective Services

Attachments:

Appendix A: City of Kelowna, Amended Terms of Reference - Journey Home Task Force



City of Kelowna Terms of Reference Journey Home Task Force

INTRODUCTION

The City of Kelowna is currently facing the same challenges that cities across the nation are experiencing related to homelessness and a lack of non-market and affordable housing to meet community needs. Cities are often the first line of response to ensure locally appropriate and effective action-focused strategies are in place to address the complex social issues that impact the safety, health, and wellbeing of our community.

Drawing from best practices that are demonstrating positive results in addressing homelessness, the City of Kelowna recognizes the importance of local leadership in building and strengthening the capacity of the services and systems that serve the homeless in the community. The intent of this Task Force is to focus on applying best practice principles to the local context to achieve a redesign of the functioning of the system of homeless-serving services. Current best practices demonstrate that collaboration and coordination of services designed to address the root causes of vulnerability is at the foundation of building a safe, healthy, and sustainable community.

On May 15, 2017 Council resolved:

- THAT Council receive, for information, the report from the Social Development Manager, dated May 15, 2017, outlining a proposed governance structure to support the development of a long-term strategy to address homelessness;
- AND THAT Council approve in principle the Journey Home Governance Structure as outlined in the report from the Social Development Manager, dated May 15, 2017;
- AND THAT Council direct staff to prepare a Journey Home Task Force Terms of Reference for consideration.

OBJECTIVE

The objective of the Task Force is to develop a “Journey Home Strategy” (Strategy) for Council’s consideration and endorsement;

The Strategy must:

- be based on a Homeless-Serving Systems Planning approach;
- be driven by local and lived experience knowledge in the application of best practices principles through a community-wide collaborative process; and
- present a locally-informed solution-focused approach that is: person-centred, coordinated, and outcome-focused to guide decision-making on addressing affordable housing and homelessness issues.

SCOPE OF WORK

To achieve this objective, the Task Force will be provided with background orientation, readiness preparation and training to undertake the following:

- establish vision and goals to guide strategy development;
- review community research including housing needs assessment and consultation processes;
- establish priority focus areas to inform Strategy development;
- prepare draft strategy that includes implementation plan; and
- finalize strategy for Council consideration by June, 2018.

MEMBERSHIP

In order to provide diverse representation, the Task Force will be comprised of stakeholders representing defined sectors selected based on their skills, knowledge, experience, and strong interest in homelessness prevention and the provision of affordable housing.

- 23 members will be appointed by Council for one-year term
- Members will be selected from the following six (6) sectors:
 - Private/Funding
 - Social Services
 - Health
 - Housing
 - Public Systems
 - Cultural/Community

APPOINTMENT AND TERM

Members shall be appointed by Council and shall serve for a one-year term, beginning June 2017 until the final report is presented to Council in June of 2018.

The Task Force may appoint sub-committees to deal with any special matters coming within the scope and jurisdiction of the Task Force. Any sub-committee so created will report to the Task Force.

Council may, at any time, remove any member of the Task Force. A member of the Task Force may resign at any time upon sending written notice to the Co-Chairpersons of the Task Force.

Task Force members who are absent for three consecutive meetings shall forfeit their appointment, unless such absence is authorized by resolution of the Task Force.

Members of the Task Force shall serve without remuneration.

In the event of a vacancy occurring during the one-year term, the vacancy may be filled upon resolution of Council.

CHAIR

Co-Chairpersons of the Task Force will be appointed by Council.

In the event that a Co-Chairperson is unable to complete the one-year term, a replacement may be appointed by Council.

MEETING PROCEDURES

The Co-Chairs shall call meetings of the Committee monthly, or on an as needed basis.

A special meeting may be called by the Chairperson or at the request of any three members of the Task Force. Notice of the day, hour, and place of special meeting shall be given at least three days prior to the meeting, by providing a copy of the notice to each member of the Task Force and by ensuring a copy of the notice is posted by the Office of the City Clerk.

A majority of the Task Force shall represent a quorum. A meeting shall not proceed if a quorum cannot be achieved. Members must notify the City at least two (2) working days before the meeting if they are unable to attend.

Unless otherwise authorized by the Community Charter or City of Kelowna Council Procedure Bylaw No. 9200, all meetings will be held in open session and in a location accessible to the public.

Unless otherwise authorized by the Task Force, the public shall only address the Task Force when they are a scheduled delegation on the Task Force meeting agenda.

The order of business is to be set out in an agenda package to be provided to the members Electronically, and posted for the public on Kelowna.ca at least three complete working days prior to the meeting date. Minutes of the meeting will be prepared by the Office of the City Clerk for adoption at the following meeting.

Task Force members have a responsibility to make decisions based on the best interests of the City-at-large.

Voting:

- all members of the Task Force, including the Co-Chairpersons, vote on every question.
- any member who does not indicate how they vote, or has left the meeting temporarily, is counted as having voted in favour.

Co-chairpersons are the recognized spokespersons for the Task Force. Members of the Task Force will direct public or media enquiries to the Co-Chairpersons.

CONFLICT OF INTEREST

Task Force members must abide by the conflict of interest provisions of the *Community Charter* and City of Kelowna Council Procedure Bylaw No. 9200. Members who have a direct or indirect pecuniary interest in a matter under discussion are not permitted to participate in the discussion of the matter or to vote on a question in respect of the matter. They must declare their conflict and state the general nature of their conflict, and then leave the meeting or that part of the meeting where the matter is under discussion. The member's declaration must be recorded in the minutes, and the Task Force member must not attempt

in any way, whether before, during or after the meeting to influence the voting on any question in respect of the matter.

REPORTING TO COUNCIL

Recommendations of the Task Force must be adopted by Task Force resolution prior to presentation to Council.

Task Force Co-Chairpersons will report to Council on behalf of the Task Force.

BUDGET

The routine operations of the Task Force and the cost of the Facilitator shall be funded by the Active Living & Culture Division budget.

STAFF SUPPORT

The Social Development Manager shall serve as staff liaison to the Task Force.

The Office of the City Clerk will provide meeting management support for the Task Force. Typical support functions include the following:

- help to organize and prepare the agenda, in conjunction with staff liaison;
- post agenda as a public notice;
- distribute the agenda package to the Task Force members;
- prepare draft minutes in conjunction with staff liaison; and
- provide support and training related to areas including governance, chairing, and meeting management.

Report to Council



Date: January 22, 2018
File: 0615-20-02
To: City Manager
From: Co-Chairpersons, Journey Home Task Force
Subject: Journey Home Task Force Update
Prepared by: Social Development Manager, Active Living & Culture

Recommendation:

THAT Council receives, for information, the update report from the Journey Home Task Force Co-Chairpersons dated January 22, 2018.

Purpose:

To provide Council with an update on the progress of the Journey Home Task Force and work plan to develop Kelowna's long-term plan to address homelessness.

Background:

At the regular meeting on September 11, 2017, Council approved the Terms of Reference for the Journey Home Task Force and the appointment of the Task Force members. The Task Force began meeting in late September and Turner Strategies was engaged to support the work of the Task Force near the end of November.

Journey Home Consulting Team:

The principal of Turner Strategies, Dr. Alina Turner, will be leading the process to build the Journey Home Strategy, bringing expertise in system planning, performance management, and program evaluation specific to Housing First implementation. Dr. Turner is recognized as a leading expert working on homelessness issues and has extensive experience working with communities across Canada to support homelessness strategy development and implementation, including Edmonton, Calgary, Medicine Hat, Yellowknife, Fredericton, St. John's, Lethbridge, and Red Deer.

As the community of Kelowna was embarking on the development of the Journey Home Strategy to address the issue of homelessness in our community, the need to address the distinct needs of youth who are facing homelessness was clear. Working with the A Way Home (AWH) – Kelowna Steering Committee, the approach has been designed to integrate and embed a strategy to address youth homelessness into the overarching Journey Home Strategy. This will ensure that the unique responses required to effectively address youth homelessness are prioritized.

The approach of embedding a strategy to address youth homelessness within the Journey Home Strategy caught the attention of both the Canadian Observatory on Homelessness (COH) and A Way Home (AWH) – Canada. Their interest is in working with our community to extrapolate learnings to assist in developing a model to be promoted nationally. As a result, Turner Strategies has partnered with the COH and AWH - Canada to prepare the Journey Home plan.

The Observatory is a leader in Canada in forming partnerships between academics, policy and decision makers, service providers, and people with lived experience of homelessness to conduct research for more effective solutions to homelessness. The Observatory, led by Dr. Stephen Gaetz will be engaged throughout the scope of the project in an expert advisory role to ensure the approach is grounded in leading edge research while developing locally informed solutions.

A Way Home is a national movement, bringing together a coalition of partners dedicated to preventing and ending youth homelessness in Canada. Led by Melanie Redman, the co-founder and President, A Way Home Canada believes that by aligning the strategies and resources of key stakeholders, we will end youth homelessness in Canada once and for all.

Journey Home Task Force:

The work of the Task Force to move the Journey Home vision to reality is well underway with a goal to present the Strategy for Council's consideration by late June 2018. The Strategy is being designed to transform Kelowna's housing and homelessness services into an integrated, people-centered, results-focused service system.

The objective of the Task Force, as outlined in the approved Terms of Reference, is to develop a Journey Home Strategy for Council's consideration for adoption. The strategy will:

1. be based on a Homeless-Serving Systems Planning approach;
2. be driven by local and lived experience in the application of best practice principles to ensure a community-wide collaborative process; and
3. become a guide for local decision-making on addressing affordable housing and homelessness issues.

Journey Home Deliverables:

The Journey Home Strategy will deliver a multi-year implementation plan outlining community priorities, system coordination, and integrated information and data system development. The implementation plan will be critical to the success of the Strategy through clear identification of the associated costs, and the resources and funding investment required. The implementation plan will provide a clear and necessary foundation for a strategy to secure senior government and other funding that will be required to bring Journey Home to reality.

The Journey Home Strategy will deliver three components to guide local work to address homelessness over the next five years:

1. *Community Priorities Plan Development*

This first component of the Journey Home Strategy will include the development of community vision and principles. Through a process of community and stakeholder engagement, it will outline the key priorities in the community to reduce and prevent homelessness. Grounded in evidence-based approaches, the Priorities Plan will guide collaborative efforts in Kelowna over the next five years by defining community priorities with clear, measurable, and achievable outcomes encouraging a foundation of inclusion, innovation, collective impact, and partnership.

2. *Systems Coordination Framework*

This component of the Strategy is based on developing a local coordinated approach to housing and supports. The goal is to apply a system planning approach that considers the homeless-serving system, made up of multiple agencies and government services, as an integrated whole, comprised of defined program components working towards a common end moving local responses from managing homelessness and its impacts, to focus on the collective responses to end homelessness.

3. *Integrated Information and Data System Development*

This component of the Strategy will address the development of an integrated information system designed to leverage community-level research to promote effective investment priorities and opportunities. The goal is to develop a system-wide data collection system that will guide the development of performance indicators to measure progress and ongoing effectiveness of the local systems coordination. This will promote system-wide data-driven decision-making and ensure the alignment and effectiveness of all resources and financial investments.

Journey Home Project Roadmap and Key Dates:

The process to develop the Journey Home strategy is focused on providing the community with opportunities to increase their understanding of the underlying issues related to homelessness and to contribute to locally-grown solutions to inform the creation of a realistic plan to address homelessness in Kelowna.

The Journey Home work plan reflects this approach, outlining Journey Home processes through to the completion of the Strategy in June, 2018. The work plan is further outlined in Appendix A.

Summary

The Task Force has engaged Turner Strategies in the development of a long-term strategy to ensure people experiencing homelessness in Kelowna have a clear path to the help they need, when they need it. The approach to embed the Youth Homelessness Plan, A Way Home -Kelowna, within the broader Strategy, will provide an opportunity to understand and address the factors that contribute to youth homelessness. Working with COH and A Way Home - Canada will provide Kelowna with the opportunity to contribute to the national work to end youth homelessness across the country.

The Journey Home project roadmap has been designed to ensure that the Strategy is community-driven and inclusive, representing a variety of voices, including those with lived experience of homelessness. The focus is on community strengths, bringing together knowledge, skills, and

resources to transform the local homeless-serving sector into a coordinated, person-centred and outcomes focused system that will be more effective in meeting the needs of community members experiencing, or at-risk of homelessness.

Internal Circulation:

J. Gabriel, Divisional Director, Active Living & Culture
K. Needham, Deputy City Clerk
L. Ruether, Communications Advisor

Considerations not applicable to this report:

Legal/Statutory Authority
Legal/Statutory Procedural Requirements
Existing Policy
External Agency/Public Comments
Communications Comments
Alternate Recommendation
Financial/Budgetary Considerations
Personnel Implications

Submitted by:

Journey Home Task Force Co-Chairpersons

Approved for inclusion: J. Gabriel, Divisional Director, Active Living & Culture


Cc: Divisional Director, Active Living & Culture
Divisional Director, Community Planning & Real Estate
Divisional Director, Corporate & Protective Services

Attachments:

Appendix A: Journey Home Project Roadmap and Key Dates:
Appendix B: Journey Home Task Force Update Presentation

Appendix A: Journey Home Project Roadmap and Key Dates:

| Strategy Phase | Date | Description |
|---|---|--|
| Research | | |
| | December and January | Gathering of local research and analyzing the data to determine priorities for discussion. These priorities have helped to inform the design of the Summits. |
| Community Engagement | | |
| Youth Homelessness Summit | January 19 | Brought together interested and essential stakeholders with national experts from COH and AWH-Canada to learn more about how we 'Make the Shift' from managing youth homelessness to focusing on prevention and supporting homeless youth to achieve housing stability and full participation in their communities. |
| Journey Home Community Summit | Tuesday, January 23 | The Community Summit will gather key Kelowna stakeholders to develop key tenets for the Journey Home plan. Approximately 100 people are expected to attend representing service providers, funders, government, private sector and those with lived experience. |
| Public Engagement Survey | February 5 – March 5 | Citizens will have the opportunity to contribute to the Journey Home strategy development through an online survey. Additional public engagement opportunities will be developed based on consultation learnings. |
| Stakeholder Design Labs | 2-week series (Feb 19 -23 rd , March 5 – 9 th) | Design Lab topics will be informed by engagement processes, and will be designed to further understand local barriers and issues, and to explore local solutions. |
| Lived Experience Circle | Jan - June | The creation of a Lived Experience Circle is underway. This includes those who are currently experiencing or have experienced homelessness to provide input in the development of the Journey Home plan. Through open sharing and understanding, the goal is to garner lived expertise and understand perspectives, barriers and potential solutions to inform the plan. |
| Community Feedback & Revisions and Design of the Journey Home Strategy | | |
| | April and May | Community priorities will be gathered from consultation processes and presented for feedback. Revisions and design of the final Strategy will be based on feedback. |
| Council Consideration and Launch of the Journey Home Strategy | | |
| | June | Next Council Updates: <ul style="list-style-type: none"> ○ April 9th – Summary of Consultation Outcomes ○ June 25th - Final Strategy for Council Consideration |



TURNER | STRATEGIES

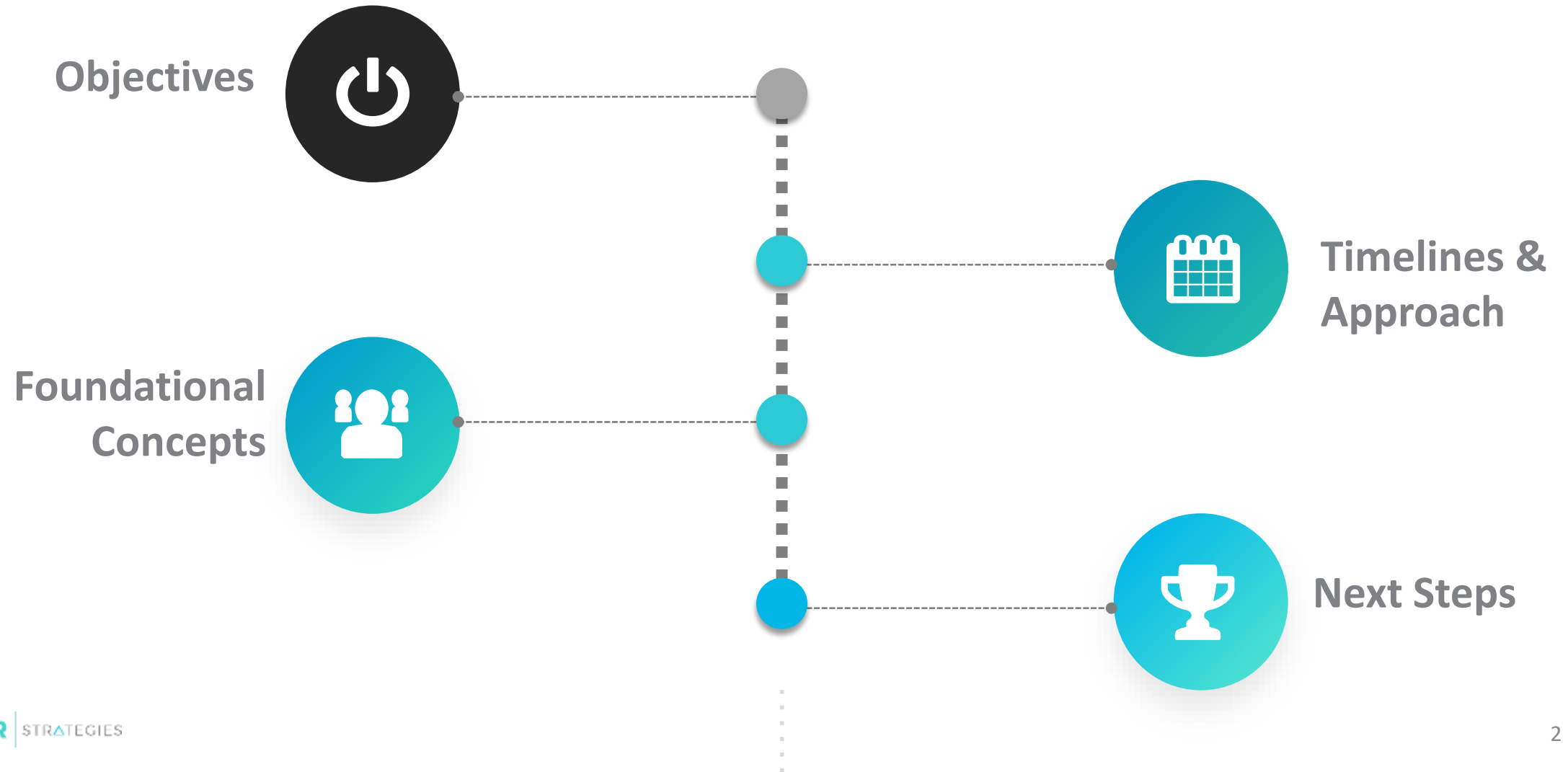
Journey **HOME**

**BUILDING KELOWNA'S PLAN TO END
HOMELESSNESS**

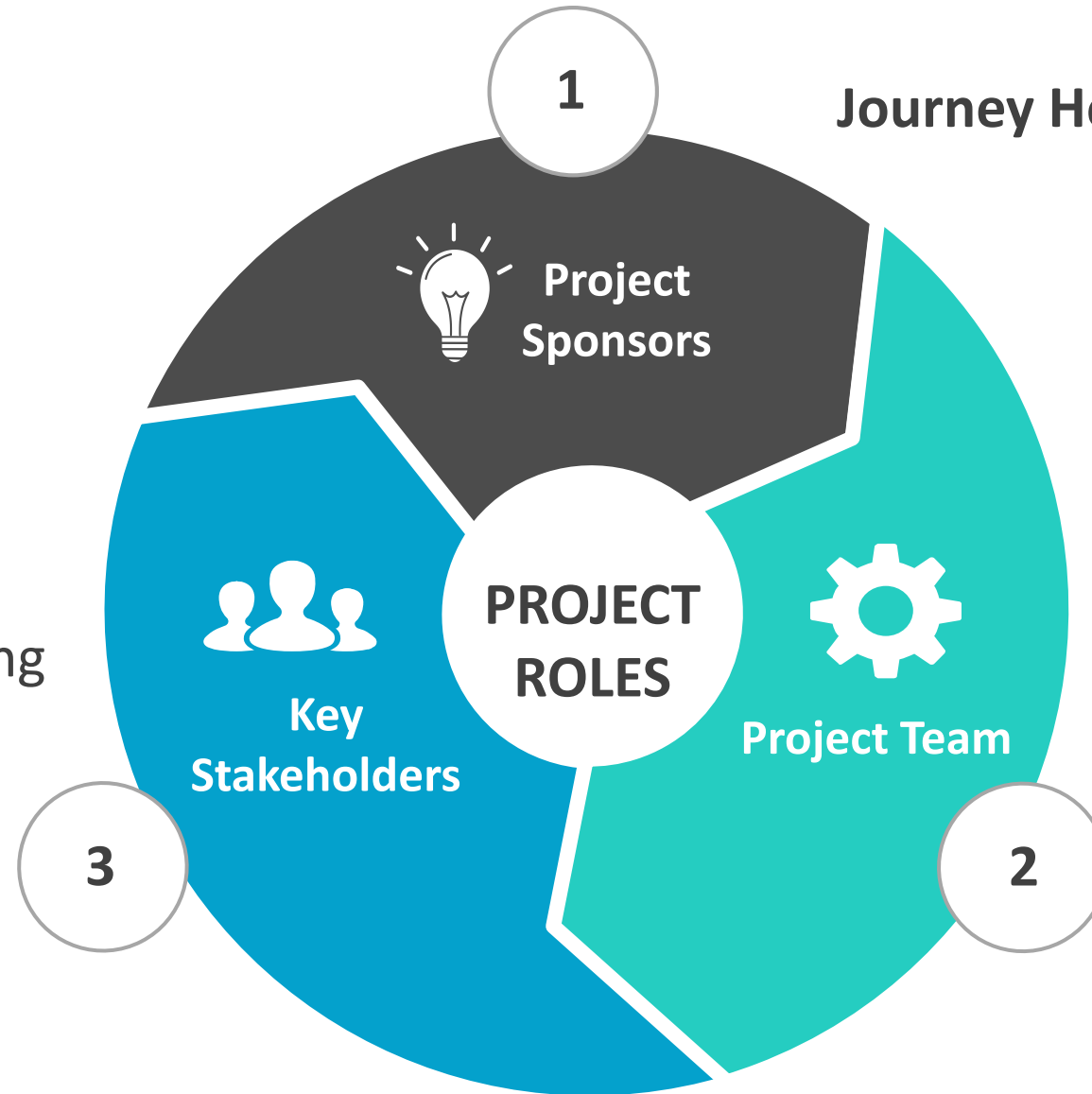
City Council Update

January 22, 2018

Overview



- Public
- Service Providers
- Funders
- City Council
- Lived Experience Circle
- Youth Plan Steering Committee
- Community Advisory Board (CAB)
- Media



Journey Home Task Force

**Turner Strategies, with
COH, AWH support
City of Kelowna**

Project Team

Turner Strategies



- **DR. ALINA TURNER**
- JAIME ROGERS, MSW
- CHANTAL HANSEN, MGIS

Canadian Observatory on Homelessness

- **DR. STEVE GAETZ – DIRECTOR & PROFESSOR, YORK U**
- DR. JOHN ECKER – DIRECTOR OF RESEARCH & EVALUATION
- DR. ERIN DEJ – POSTDOCTORAL FELLOW
- SARAH HOLDEN, MSW – RESEARCH & EVALUATION OFFICER
- STEPHANIE VASKO – DIRECTOR OF COMMUNICATIONS

A Way Home - Canada

- MELANIE REDMAN, CEO & PRESIDENT

Significant opportunity to showcase nationally embedding Youth focus within community plans from the start.

Kelowna's Plan will...

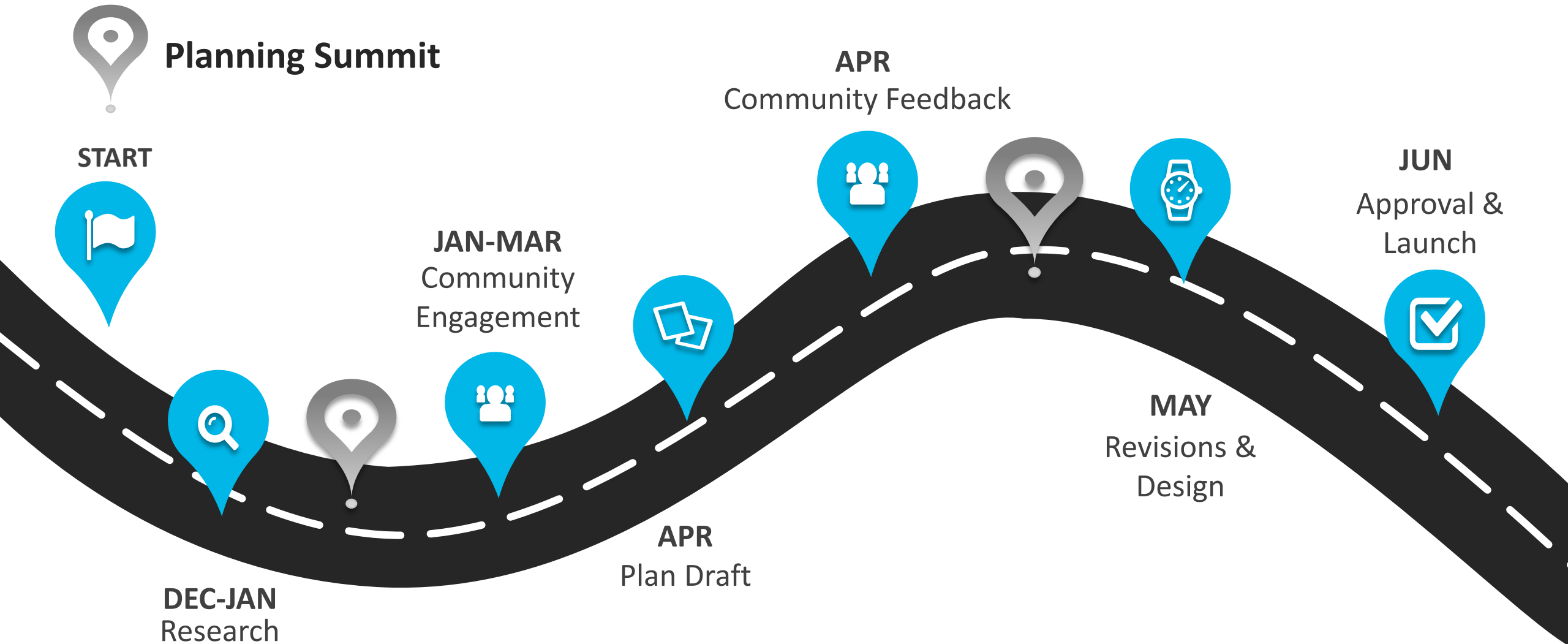
- Provide a blueprint with **concrete actions that are realistic**
- Result in **measurable, positive change** for those experiencing homelessness, at risk AND residents, businesses, public systems
- Be supported & reflective of **diverse perspectives & needs**
- Build awareness and **shared accountability** across stakeholders



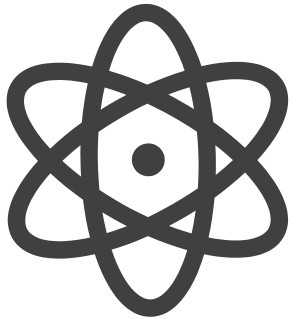
- Advance a **systems planning** approach, grounded in evidence
- Identify accountabilities, **targets, and resources needs** for implementation
- Be solutions focused & **strengths-based, building on what works**
- Will balance short, medium and longer term interventions for diverse groups (**Indigenous, youth, women, rough sleepers**, etc.)

- Embed the **AWH Kelowna Youth Strategy** from the start – setting a national example.

Project Road Map



Foundational Concepts



1

System planning response focuses on both ending homelessness and preventing future homelessness.

2

Uses the concept of **functional zero** as the measurement for ending homelessness which means that homelessness is prevented whenever possible, and that experiences of homelessness are rare, brief, and non-recurring.

3

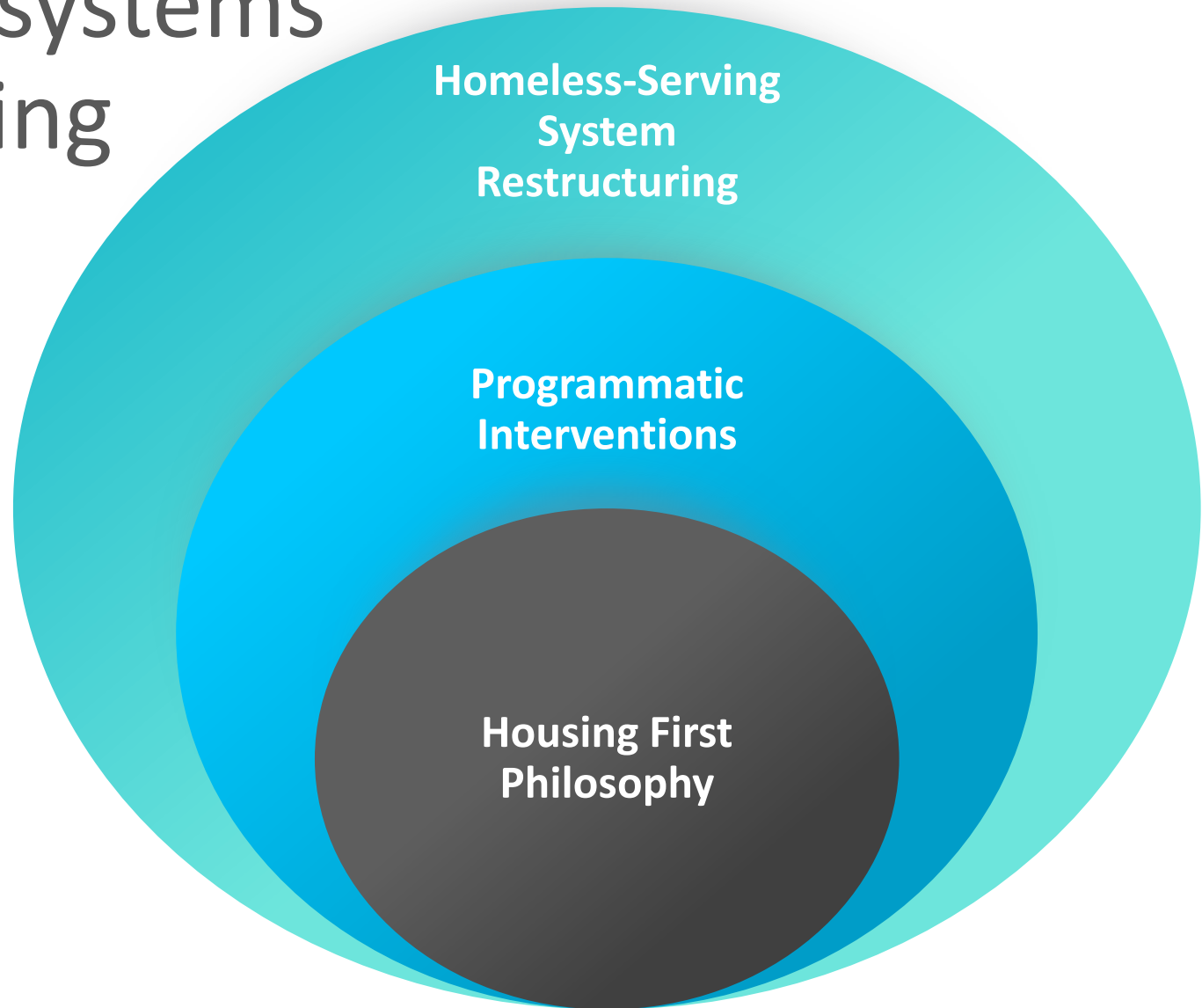
Ending and preventing homelessness require renewed **leadership & accountability** across stakeholders and investment in what works.

4

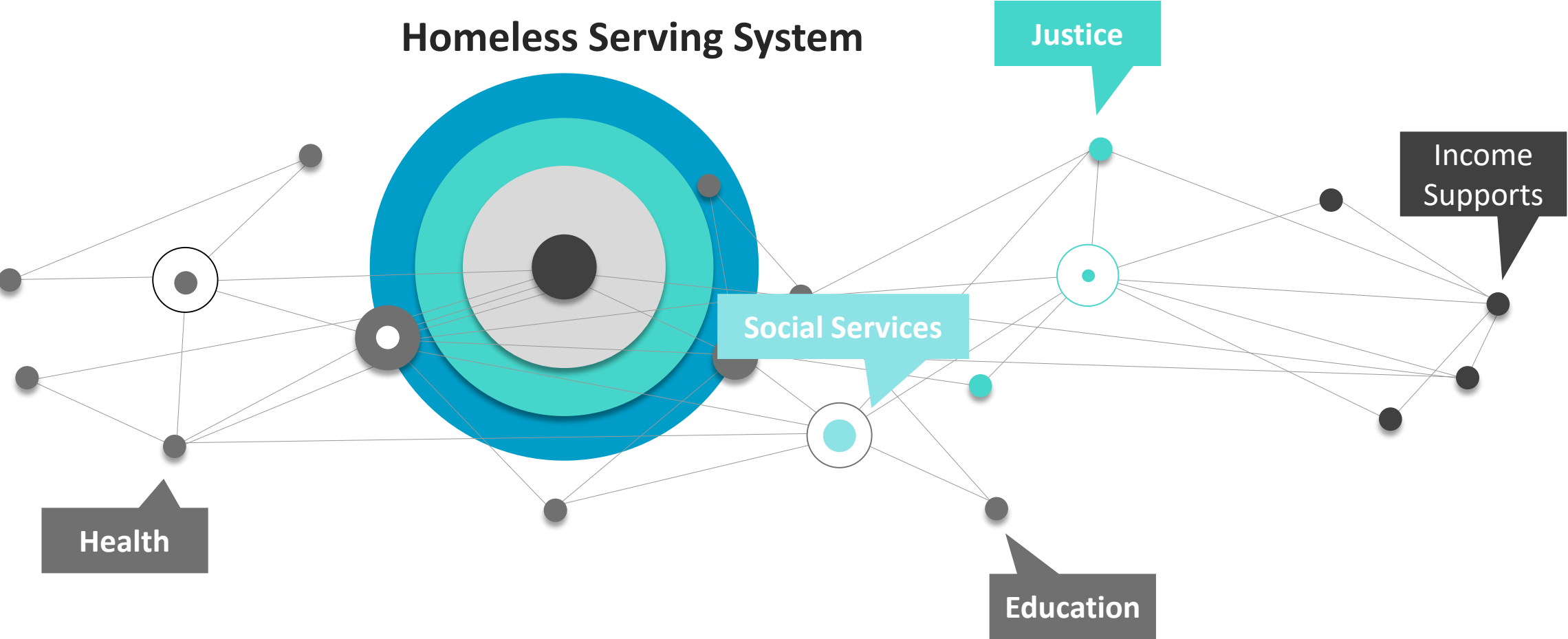
Critical need to increase **permanent supportive and affordable housing supply**, and a greater focus on prevention and diversion, including **longer term supports** where appropriate.

Shifting from a program-by-program to a systems approach to ending homelessness.

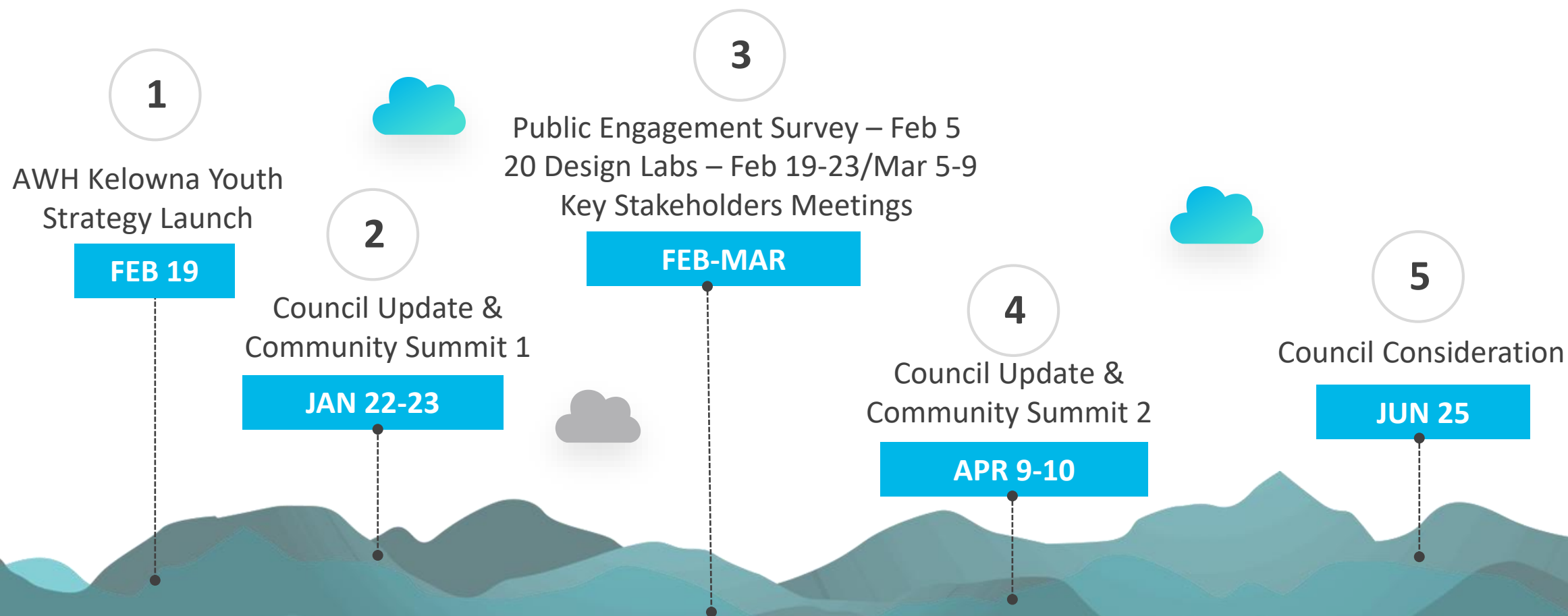
Restructuring our approach to homelessness following the Housing First philosophy.



Rethinking Integration



Key Dates



THANK YOU



DR. ALINA TURNER



403-827-8722



alina@turnerstrategies.org



turnerstrategies.org

REPORT TO COUNCIL



Date: January 22, 2018

RIM No. 1250-30

To: City Manager

From: Community Planning Department (EW)

Application: Z17-0090 **Owners:** Steven James Hall & Colleen May Hall

Address: 3439 Moberly Rd **Applicant:** Urban Options Planning & Permits

Subject: Rezoning Application

Existing OCP Designation: S2RES – Single/Two Unit Residential

Existing Zone: RU2 – Medium Lot Housing

Proposed Zone: RU2c – Medium Lot Housing with Carriage House

1.0 Recommendation

THAT Rezoning Application No. Z17-0090 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 8 District Lot 135 ODYD Plan 39812, located at 3439 Moberly Rd, Kelowna, BC from the RU2 – Medium Lot Housing zone to the RU2c – Medium Lot Housing with Carriage House zone, be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration.

2.0 Purpose

To rezone the subject property to facilitate the construction of a carriage house.

3.0 Community Planning

Community Planning Staff support the proposed rezoning application to facilitate the future construction of a carriage house on the subject property. Rezoning the subject property to add the 'c' designation meets several City policy objectives including fostering a mix of housing forms, concentrating growth with the Permanent Growth Boundary and achieving density through sensitive infill. The proposed rezoning is also consistent with the property's future land use designation and the property is connected to City sanitary sewer.

To fulfill Council Policy No. 367, the applicant submitted a Neighbour Consultation Summary Form to staff on October 26, 2017, outlining that the neighbours within 50 m of the subject property were notified.

4.0 Proposal

4.1 Background

The applicant would like to construct a 1 ½ storey carriage house by converting the existing detached garage on the subject property. The building permit for the existing double garage was issued in 1990 with storage space on the upper storey. At this time staff are not tracking any variances for the conversion to a carriage house.



Figure 1. Photo of existing double garage with existing dwelling to the right.

4.2 Site Context

The 817m² subject property is located in the South Pandosy – KLO City Sector and in the South Pandosy City Centre, east of Richter St and south of Lanfranco Rd. The neighbourhood has a mix of residential zones and is in close proximity to commercial properties fronting Richter St and Lakeshore Rd.

Specifically, adjacent land uses are as follows:

| Orientation | Zoning | Land Use |
|-------------|--------------------------|-------------|
| North | RU2 – Medium Lot Housing | Residential |
| East | RU2 – Medium Lot Housing | Residential |
| South | RU2 – Medium Lot Housing | Residential |
| West | RU2 – Medium Lot Housing | Residential |

Subject Property Map: 3439 Moberly Rd



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Development Process

Compact Urban Form.¹ Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Sensitive Infill.² Encourage new development or redevelopment in existing residential areas to be sensitive to or reflect the character of the neighborhood with respect to building design, height and siting.

Carriage Houses & Accessory Apartments³. Support carriage houses and accessory apartments through appropriate zoning regulations.

6.0 Technical Comments

6.1 Building & Permitting Department

- Full plan check for Building Code related issues will be done at time of Building Permit application.

¹ City of Kelowna Official Community Plan, Policy 5.3.2 (Development Process Chapter).

² City of Kelowna Official Community Plan, Policy 5.22.6 (Development Process Chapter).

³ City of Kelowna Official Community Plan, Policy 5.22.12 (Development Process Chapter).

6.2 Development Engineering Department

- See Attachment 'C' – Development Engineering Memorandum dated October 12, 2017.

6.3 Bylaw Services

- There are no outstanding/open Bylaw Enforcement files pertaining to property location: 3439 Moberly Rd.

7.0 **Application Chronology**

Date of Application Received: September 11, 2017

Date Public Consultation Completed: October 26, 2017

Report prepared by: Emily Williamson, Planner

Reviewed by: Terry Barton, Urban Planning Manager

Approved for Inclusion: Ryan Smith, Community Planning Department Manager

Attachments:

Attachment 'A' – Applicant's Rationale Letter

Attachment 'A' – Carriage House Site Plan

Attachment 'A' – Development Engineering Memorandum dated October 12, 2017



Steve and Colleen Hall
3439 Moberly Road
Kelowna, BC
V1W 3H9

September 10, 2017

City of Kelowna
Urban Planning Department
1435 Water Street
Kelowna, BC



RE: Rezoning proposal at 3439 Moberly Road Zone: RU2 – Medium Lot Housing Zone

Dear Planning Staff:

We would like to rezone the property at 3439 Moberly Road to the RU2c– Medium Lot Housing with carriage house zone with the intention of converting the existing 2 vehicle garage to a carriage house. The garage is located on the left side of the site behind the principal dwelling and is 1 ½ storeys in height. The situation is ideal for a carriage house as there is ample parking and a large private open space area behind the garage.

The principal dwelling has a single car garage that is used for storage and converting the garage to livable space makes sense. The proposed suite will have a bedroom and full bath on the upper ½ storey and half the lower portion planned for the living area. One of the garage doors will become a large window. The entrance is planned for the west side of the building allowing easy access to the outdoor space.

The large yard space, which is fenced and well landscaped for the owner's enjoyment, will be retained by the principal dwelling. Since no new construction is proposed, there will be no negative impact on all mature landscaping, specifically the large trees and shrubs along the rear lot line.

The property is located within in the lower Mission sector of the City, close to amenities such as schools, shopping and beaches. The request for the RU2c zone is consistent given there are RU1c and RU6 zoned properties within the immediate neighbourhood. It meets the policies objectives in the Official Community Plan and is anticipated to be good infill development that is easily integrated on this large property and into the existing form of the neighbourhood, adding much needed quality housing stock to the area.

Regards,

A handwritten signature in black ink, appearing to read 'Birte Decloux', written in a cursive style.

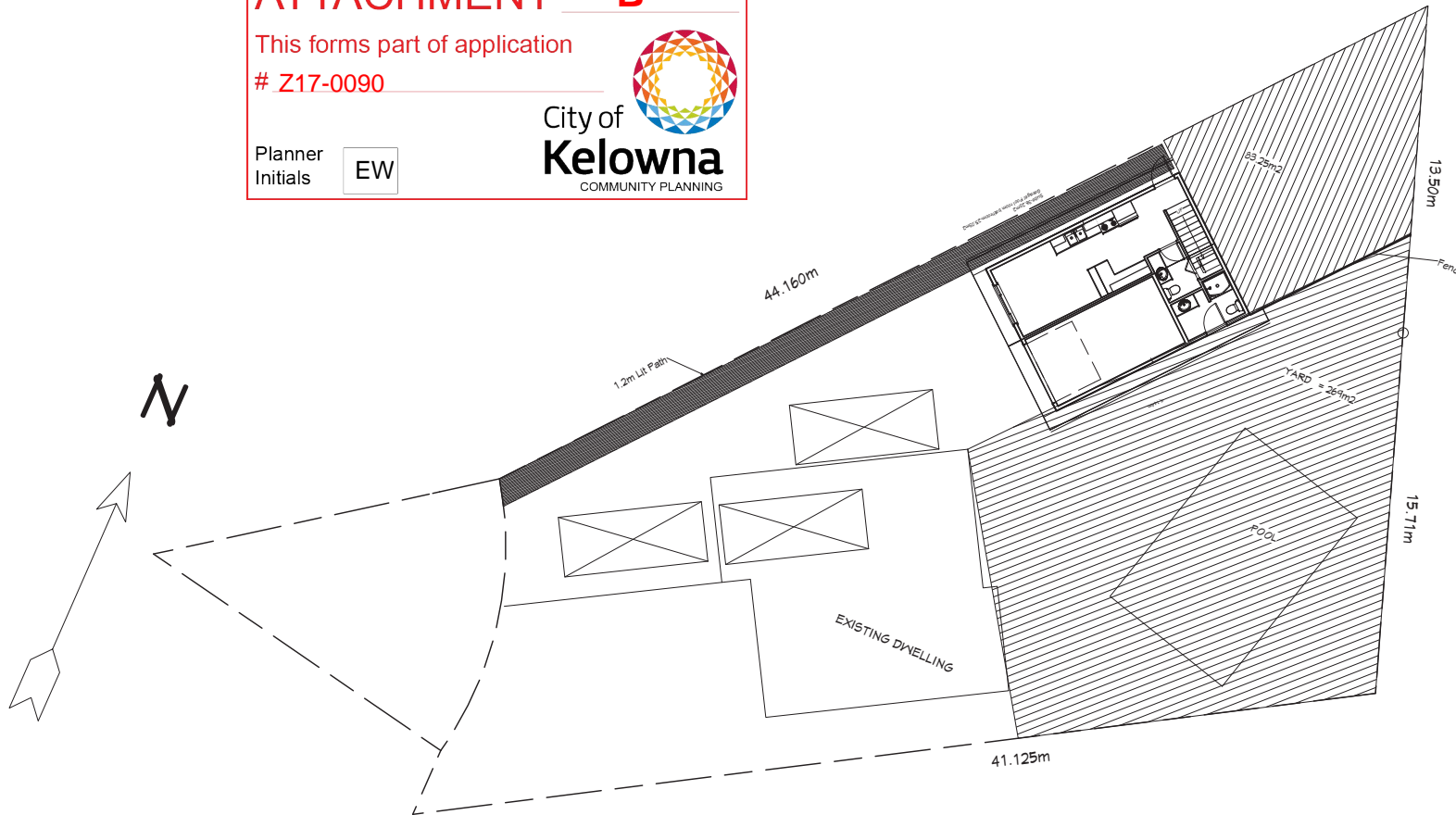
Birte Decloux, on behalf of S & C Hall

ATTACHMENT B

This forms part of application
Z17-0090

Planner
Initials EW

City of
Kelowna
COMMUNITY PLANNING

3439 Moberly
Site plan with parking and private open space

CITY OF KELOWNA

MEMORANDUM

Date: October 12, 2017
File No.: Z17-0090

To: Urban Planning (EW)

From: Development Engineering Manager (JK)

Subject: 3439 Moberly Rd

RU2 to RU2c

Development Engineering Department have the following comments and requirements associated with this application. The road and utility upgrading requirements outlined in this report will be a requirement of this development.

The Development Engineering Technologist for this project is Jason Angus.

1. Domestic Water and Fire Protection

- a) Our records indicate that this property is currently serviced with a 19mm-diameter water service which is adequate for this application.

2. Sanitary Sewer

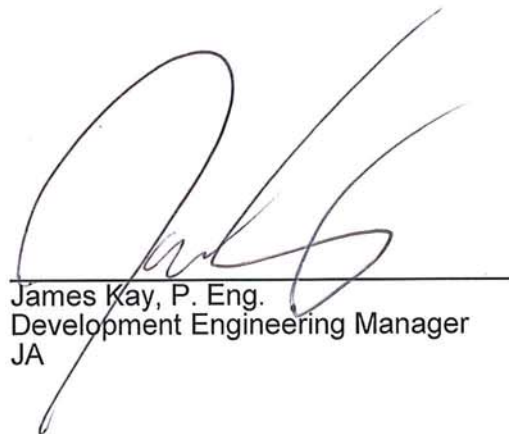
- a) Our records indicate that this property is currently serviced with a 100mm-diameter sanitary sewer service which is adequate for this application.

3. Development Permit and Site Related Issues

- a) Direct the roof drains onto splash pads
- b) Maximum one (1) driveway access per fronting road as per bylaw.

4. Electric Power and Telecommunication Services

- a) It is the applicant's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for service upgrades to these services which would be at the applicant's cost.



James Kay, P. Eng.
Development Engineering Manager
JA

ATTACHMENT C

This forms part of application
Z17-0090

Planner
Initials EW


**City of
Kelowna**
COMMUNITY PLANNING

CITY OF KELOWNA
BYLAW NO. 11538
Z17-0090 - 3439 Moberly Road

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 8 District Lot 135 ODYD Plan 39812 located on Moberly Road, Kelowna, B.C., from the RU2 – Medium Lot Housing zone to the Ru2c – Medium Lot Housing with Carriage House.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time and adopted by the Municipal Council this

Mayor

City Clerk

REPORT TO COUNCIL



Date: January 22, 2018

RIM No. 1250-30

To: City Manager

From: Community Planning Department (TA)

Application: Z17-0102 **Owner:** Christopher Darryl Eddy
Sara Green Eddy

Address: 344 Christleton Avenue **Applicant:** Harmony Co-Ordination
Services Ltd

Subject: Rezoning Application

Existing OCP Designation: S2RES – Single/Two Unit Residential

Existing Zone: RU1 – Large Lot Housing

Proposed Zone: RU1c – Large Lot Housing with Carriage House

1.0 Recommendation

THAT Rezoning Application No. Z17-0029 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 7 DL 14 ODYD Plan 3451 located at 344 Christleton Avenue, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Community Planning Department dated January 22, 2018.

2.0 Purpose

To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

3.0 Community Planning

Community Planning supports the proposed rezoning from RU1 – Large Lot Housing to RU1 c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

The Official Community Plan (OCP) Future Land Use is designated as S2RES – Single/Two Unit Residential, which supports this modest increase in density and the property is located within the Permanent Growth Boundary. The concept of the carriage house is aligned with the OCP Urban Policy of Compact Urban Form – increasing density where infrastructure already exists.

Should Council approve the rezoning, the applicant may apply for a building permit provided there are no variances requested.

4.0 Proposal

4.1 Background

The subject property has a single family dwelling located on the property that will be retained through this development.

4.2 Project Description

The applicant has provided preliminary designs for the carriage house at 1 ½ storeys in height and located along the rear lane. The proposed carriage house does not trigger any variances at this time.

4.3 Site Context

The subject property is located south of the Kelowna General Hospital in an area that is well serviced with amenities including public beach accesses, South Padosy Urban Centre, and transit along Padosy Street. It earns a walkscore of 56 which means it is somewhat walkable.

Specifically, adjacent land uses are as follows:

| Orientation | Zoning | Land Use |
|--------------------|--|---|
| North | HD ₁ – Kelowna General Hospital | Hospital |
| East | RU ₁ – Large Lot Housing | Single Dwelling Housing |
| South | RU ₁ – Large Lot Housing | Single Dwelling Housing |
| West | RU _{1c} – Large Lot Housing with Carriage House | Single Dwelling Housing with Carriage House |

Subject Property Map: 344 Christleton Avenue**5.0 Current Development Policies****5.1 Kelowna Official Community Plan (OCP)****Development Process**

Compact Urban Form.¹ Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Urban Uses.² Direct urban uses to lands within the urban portion of the Permanent Growth Boundary, in the interest of reducing development and speculative pressure on agricultural lands.

Housing in Agricultural Areas.³ Discourage residential development (both expansions and new developments) in areas isolated within agricultural environments (both ALR and non-ALR).

Sensitive Infill.⁴ Encourage new development or redevelopment in existing residential areas to be sensitive to or reflect the character of the neighbourhood with respect to building design, height and siting.

¹ City of Kelowna Official Community Plan, Policy 5.2.3 (Development Process Chapter).

² City of Kelowna Official Community Plan, Policy 5.33.3 (Development Process Chapter).

³ City of Kelowna Official Community Plan, Policy 5.33.8 (Development Process Chapter).

⁴ City of Kelowna Official Community Plan, Policy 5.22.6 (Development Process Chapter).

6.0 Technical Comments

6.1 Building & Permitting Department

- Full plan check will be done at time of Building Permit.

6.2 Development Engineering Department

- Please see attached Development Engineering Memorandum (Schedule "A").

7.0 Application Chronology

Date of Application Received: October 31, 2017

Date Public Consultation Completed: December 11, 2017

Report prepared by: Trisa Atwood, Planner

Reviewed by: Terry Barton, Urban Planning Manager

Approved for Inclusion: Ryan Smith, Community Planning Department Manager

Attachments:

Schedule "A": Development Engineering Memorandum



CITY OF KELOWNA

MEMORANDUM

Date: November 17, 2017
File No.: Z17-0102
To: Community Planning (TB)
From: Development Engineering Manager (JK)
Subject: 344 Christleton Ave Lot 7 Plan 3451 RU1 to RU1c Carriage House

Development Engineering has the following comments and requirements associated with this application. The utility upgrading requirements outlined in this report will be a requirement of this development.

1. Domestic Water and Fire Protection

Our records indicate that this property is currently serviced with a 13mm water service. The applicant will arrange for the disconnection of existing service and the installation of a new service. The disconnection of the existing small diameter water services and the tie-in of a larger new service can be provided by City forces at the applicant's Cost. **The applicant will be required to sign a Third Party Work Order for the cost of the service upgrade.** For estimate inquiry's please contact Jason Angus, by email jangus@kelowna.ca or phone, 250-469-8783

2. Sanitary Sewer

Our records indicate that this property is currently serviced with a 100mm-diameter sanitary sewer service. An inspection chamber (IC) complete with brooks box must be installed on the service at the owner's cost. Service upgrades can be provided by the City at the applicant's cost. **The applicant will be required to sign a Third Party Work Order for the cost of the service upgrade.** For estimate inquiry's please contact Jason Angus, by email jangus@kelowna.ca or phone, 250-469-8783.

3. Development Permit and Site Related Issues

Direct the roof drains onto splash pads.

Driveway access permissible is one (1) per property as per bylaw

4. Electric Power and Telecommunication Services

It is the applicant's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for service upgrades to these services which would be at the applicant's cost.

James Kay, P. Eng.
Development Engineering Manager

JA

CITY OF KELOWNA
BYLAW NO. 11539
Z17-0102 – 344 Christleton Avenue

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 7 District Lot 14 ODYD Plan 3451 located on Christleton Avenue, Kelowna, B.C., from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: January 22, 2018

RIM No. 1250-30

To: City Manager

From: Community Planning Department (TA)

| | | | |
|---------------------|----------|---------------|---|
| Application: | Z17-0070 | Owner: | Robert Andrew Law Marianne Elizabeth Law |
|---------------------|----------|---------------|---|

| | | | |
|-----------------|-------------------|-------------------|---------------------|
| Address: | 1847 Maple Street | Applicant: | TI Developments Ltd |
|-----------------|-------------------|-------------------|---------------------|

Subject: Rezoning Application

Existing OCP Designation: S2RES – Single/Two Unit Residential

Existing Zone: RU1 – Large Lot Housing

Proposed Zone: RU1c – Large Lot Housing with Carriage House

1.0 Recommendation

THAT Rezoning Application No. Z17-0070 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot B DL 14 ODYD Plan 4368, located on 1847 Maple Street, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration.

2.0 Purpose

To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

3.0 Community Planning

Community Planning supports the proposed rezoning from RU1 – Large Lot Housing to RU1 c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

The Official Community Plan (OCP) Future Land Use is designated as S2RES – Single/Two Unit Residential, which supports this modest increase in density and the property is located within the Permanent Growth Boundary. Increasing density within the Permanent Growth Boundary reduces development pressure on agricultural land and is aligned with the OCP Urban Policy of Compact Urban Form – increasing density where infrastructure already exists. Carriage house regulations in general meet the OCP Policy of Sensitive Infill,

which involves designing so that height and massing is sensitive to the existing fabric of the neighbourhood. Zoning Bylaw No. 8000 restricts carriage houses from being stratified and sold, so a carriage house rezoning is one method of increasing the rental inventory within the City.

Should Council approve the rezoning, Staff will work with the applicant to issue a Heritage Alteration Permit for the form and character of the carriage house as the subject property is located within the Heritage Conservation Area.

4.0 Proposal

4.1 Background

The applicant initially applied in May 2017 with an incomplete application that required a variance. Revised drawings were provided on October 17, 2017 that eliminated the variance and allowed the application to proceed.

4.2 Project Description

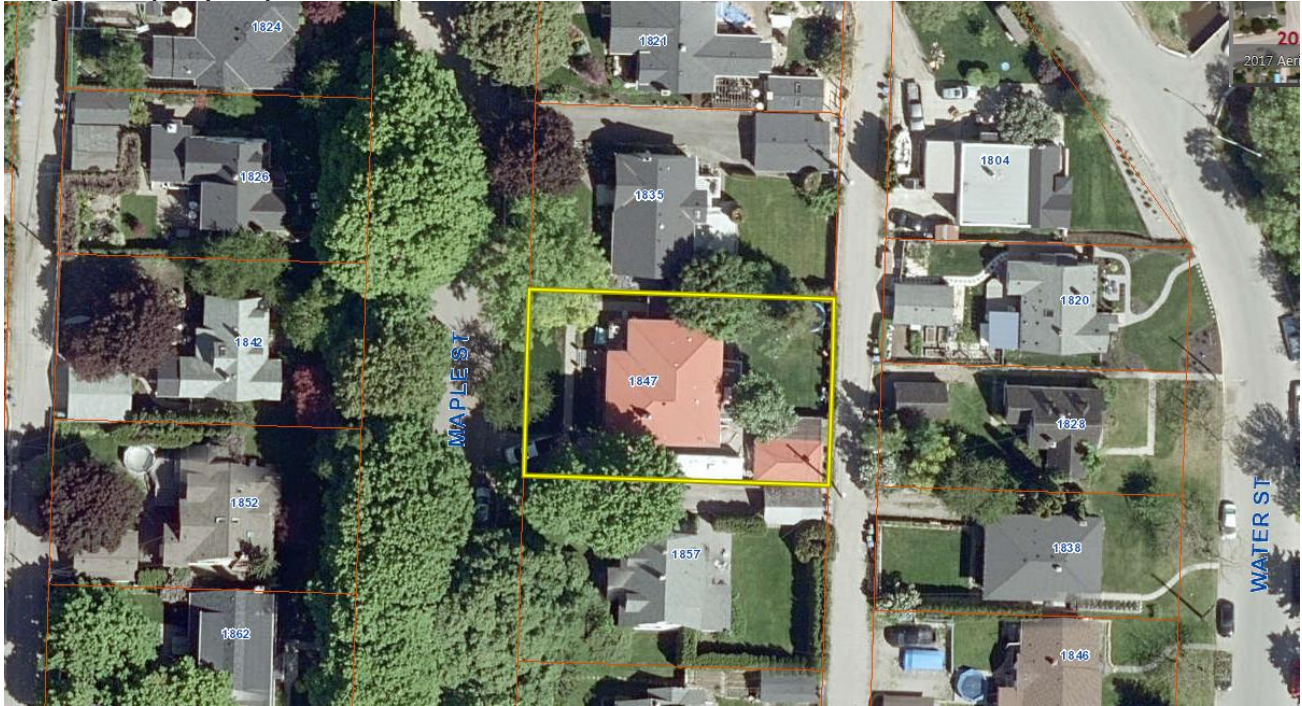
The applicant has provided preliminary designs for the carriage house at 1 ½ storeys in height and located along the rear lane. The proposed carriage house does not trigger any variances at this time. Should Council support the rezoning, Staff will work with the applicant to issue a Heritage Alteration Permit for the form and character of the carriage house.

4.3 Site Context

The subject property is located in the Heritage Conservation area in an amenity rich neighbourhood. It is within walking distance to Downtown, several parks and beaches, and transit is available on nearby Pandosy Street. It has a walkscore of 87 meaning it is very walkable. There are several other RU1c rezonings in the area as the neighbourhood shifts to higher density.

Specifically, adjacent land uses are as follows:

| Orientation | Zoning | Land Use |
|--------------------|-------------------------|-------------------------|
| North | RU1 – Large Lot Housing | Single Dwelling Housing |
| East | RU1 – Large Lot Housing | Single Dwelling Housing |
| South | RU1 – Large Lot Housing | Single Dwelling Housing |
| West | RU1 – Large Lot Housing | Single Dwelling Housing |

Subject Property Map: 1847 Maple Street**5.0 Current Development Policies****5.1 Kelowna Official Community Plan (OCP)****Development Process**

Compact Urban Form.¹ Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Urban Uses.² Direct urban uses to lands within the urban portion of the Permanent Growth Boundary, in the interest of reducing development and speculative pressure on agricultural lands.

Housing in Agricultural Areas.³ Discourage residential development (both expansions and new developments) in areas isolated within agricultural environments (both ALR and non-ALR).

¹ City of Kelowna Official Community Plan, Policy 5.2.3 (Development Process Chapter).

² City of Kelowna Official Community Plan, Policy 5.33.3 (Development Process Chapter).

³ City of Kelowna Official Community Plan, Policy 5.33.8 (Development Process Chapter).

Sensitive Infill.⁴ Encourage new development or redevelopment in existing residential areas to be sensitive to or reflect the character of the neighbourhood with respect to building design, height and siting.

6.0 Technical Comments

6.1 Building & Permitting Department

Full plan check will be done at time of Building Permit.

6.2 Development Engineering Department

All Development Engineering requirements have been satisfied.

7.0 Application Chronology

| | |
|-------------------------------------|------------------|
| Date of Application Received: | May 18, 2017 |
| Date of Revised Drawings Received: | October 17, 2017 |
| Date Public Consultation Completed: | December 5, 2017 |

Report prepared by: Trisa Atwood, Planner

Reviewed by: Terry Barton, Urban Planning Manager

Approved for Inclusion: Ryan Smith, Community Planning Department Manager

⁴ City of Kelowna Official Community Plan, Policy 5.22.6 (Development Process Chapter).

CITY OF KELOWNA
BYLAW NO. 11541
Z17-0070 – 1847 Maple Street

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot B District Lot 14 ODYD Plan 4368 located on Maple Street, Kelowna, B.C., from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Approved under the Transportation Act this

(Approving Officer – Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: January 22, 2018

RIM No. 1250-30

To: City Manager

From: Community Planning Department (TA)

Application: Z17-0084 **Owner:** Glen D'Arcy Manlove
Charmaine Ann Katherine Manlove

Address: 911 Pitcairn Ct **Applicant:** Kyle Turner, Gord Turner
Renovations Ltd.

Subject: Rezoning Application

Existing OCP Designation: S2RES – Single/Two Unit Residential

Existing Zone: RU1 – Large Lot Housing

Proposed Zone: RU1c – Large Lot Housing with Carriage House

1.0 Recommendation

THAT Rezoning Application No. Z17-0084 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 6 Section 29 TWP 26 ODYD Plan 18323 located at 911 Pitcairn Court, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Community Planning Department dated January 22, 2018.

2.0 Purpose

To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

3.0 Community Planning

Community Planning supports the proposed rezoning from RU1 – Large Lot Housing to RU1 c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

The Official Community Plan (OCP) Future Land Use is designated as S2RES – Single/Two Unit Residential, which supports this modest increase in density and the property is located within the Permanent Growth Boundary. The concept of a carriage house is also aligned with the OCP Urban Policy of Compact Urban Form – increasing density where infrastructure already exists.

Should Council approve the rezoning Staff will bring forward a Development Variance Permit application to consider a variance to the flanking yard setback.

4.0 Proposal

4.1 Background

The subject property has a single family dwelling located on the property that will be retained through this development.

4.2 Project Description

The applicant has provided proposed designs for the single storey carriage house, and is requesting a variance to the flanking side yard setback. Access to the proposed carriage house will be from Tronson Dr.

4.3 Site Context

The subject property is located near Glenmore Elementary School on the corner of Pitcairn Ct and Tronson Dr. It is a predominantly single family residential neighbourhood with several other RU1c properties in the nearby vicinity.

Specifically, adjacent land uses are as follows:

| Orientation | Zoning | Land Use |
|--------------------|-------------------------|-------------------------|
| North | RU1 – Large Lot Housing | Single Dwelling Housing |
| East | RU1 – Large Lot Housing | Single Dwelling Housing |
| South | RU1 – Large Lot Housing | Single Dwelling Housing |
| West | RU1 – Large Lot Housing | Single Dwelling Housing |

Subject Property Map: 911 Pitcairn Court



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Development Process

Compact Urban Form.¹ Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Urban Uses.² Direct urban uses to lands within the urban portion of the Permanent Growth Boundary, in the interest of reducing development and speculative pressure on agricultural lands.

Housing in Agricultural Areas.³ Discourage residential development (both expansions and new developments) in areas isolated within agricultural environments (both ALR and non-ALR).

Sensitive Infill.⁴ Encourage new development or redevelopment in existing residential areas to be sensitive to or reflect the character of the neighbourhood with respect to building design, height and siting.

¹ City of Kelowna Official Community Plan, Policy 5.2.3 (Development Process Chapter).

² City of Kelowna Official Community Plan, Policy 5.33.3 (Development Process Chapter).

³ City of Kelowna Official Community Plan, Policy 5.33.8 (Development Process Chapter).

⁴ City of Kelowna Official Community Plan, Policy 5.22.6 (Development Process Chapter).

6.0 Technical Comments

6.1 Building & Permitting Department

- Full plan check will be done at time of Building Permit.

6.2 Development Engineering Department

- Please see attached Development Engineering Memorandum (Schedule "A").

7.0 Application Chronology

Date of Application Received: September 11, 2017

Date Public Consultation Completed: October 20, 2017

Report prepared by: Trisa Brant, Planner

Reviewed by: Terry Barton, Urban Planning Manager

Approved for Inclusion: Ryan Smith, Community Planning Department Manager

Attachments:

Schedule "A": Development Engineering Memorandum

CITY OF KELOWNA
MEMORANDUM

Date: September 25, 2017
File No.: Z17-0084
To: Community Planning (TB)
From: Development Engineering Manager (JK)
Subject: 911 Pitcairn Ct Lot 6 Plan 18323 RU1 to RU1c Carriage House

Development Engineering has the following comments and requirements associated with this application. The utility upgrading requirements outlined in this report will be a requirement of this development.

1. Domestic Water and Fire Protection

Our records indicate that this property is currently serviced with a 19mm-diameter water service which is adequate for this application.

2. Sanitary Sewer

Our records indicate that this property is currently serviced with a 100mm-diameter sanitary sewer service. An inspection chamber (IC) complete with brooks box must be installed on the service at the owner's cost. Service upgrades can be provided by the City at the applicant's cost. **The applicant will be required to sign a Third Party Work Order for the cost of the service upgrade.** For estimate inquiry's please contact Jason Angus, by email jangus@kelowna.ca or phone, 250-469-8783.

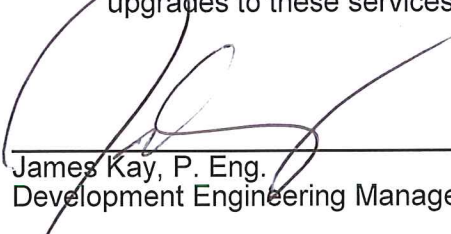
3. Development Permit and Site Related Issues

Direct the roof drains onto splash pads.

Maximum one (1) driveway access per fronting road as per bylaw.

4. Electric Power and Telecommunication Services

It is the applicant's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for service upgrades to these services which would be at the applicant's cost.


James Kay, P. Eng.
Development Engineering Manager

JA

CITY OF KELOWNA
BYLAW NO. 11542
Z17-0084 – 911 Pitcairn Court

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 6 Section 29 Township 26 ODYD Plan 18323 located on Pitcairn Court, Kelowna, B.C., from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: January 22, 2018

RIM No. 1250-30

To: City Manager

From: Community Planning Department (TA)

Application: Z17-0029 **Owner:** Travis Austin Shipka

Address: 5059 Treadgold Ct **Applicant:** Travis Austin Shipka

Subject: Rezoning Application

Existing OCP Designation: S2RES – Single/Two Unit Residential

Existing Zone: RU1 – Large Lot Housing

Proposed Zone: RU1c – Large Lot Housing with Carriage House

1.0 Recommendation

THAT Rezoning Application No. Z17-0029 to amend the City of Kelowna Zoning Bylaw No. 8000 by changing the zoning classification of Lot 21 Section 24 Township 28 SDYD Plan KAP76347 located at 5059 Treadgold Court, Kelowna, BC from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone be considered by Council;

AND THAT the Rezoning Bylaw be forwarded to a Public Hearing for further consideration;

AND THAT final adoption of the Rezoning Bylaw be considered subsequent to the outstanding conditions of approval as set out in Schedule "A" attached to the Report from the Community Planning Department dated January 22, 2018.

2.0 Purpose

To rezone the subject property from RU1 – Large Lot Housing to RU1c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

3.0 Community Planning

Community Planning supports the proposed rezoning from RU1 – Large Lot Housing to RU1 c – Large Lot Housing with Carriage House to facilitate the development of a carriage house.

The Official Community Plan (OCP) Future Land Use is designated as S2RES – Single/Two Unit Residential, which supports this modest increase in density and the property is located within the Permanent Growth Boundary. Increasing density within the Permanent Growth Boundary reduces development pressure on

agricultural land and is align with the OCP Urban Policy of Compact Urban Form – increasing density where infrastructure already exists. Carriage house regulations in general meet the OCP Policy of Sensitive Infill, which involves designing so that height and massing is sensitive to the existing fabric of the neighbourhood. Zoning Bylaw No. 8000 restricts carriage houses from being stratified and sold, so a carriage house rezoning is one method of increasing the rental inventory within the City.

Should Council approve the rezoning, the applicant may apply for a building permit provided there are no variances requested.

4.0 Proposal

4.1 Background

The subject property has a single family dwelling that will be retained as a function of this rezoning.

4.2 Project Description

The applicant has proposed to rezone the subject property without a firm design for the carriage house. Should Council support the rezoning, the applicant would be required to submit plans for building permit. Should the design or siting of the carriage house require a variance, Staff would bring forward a Development Variance Permit for Council consideration.

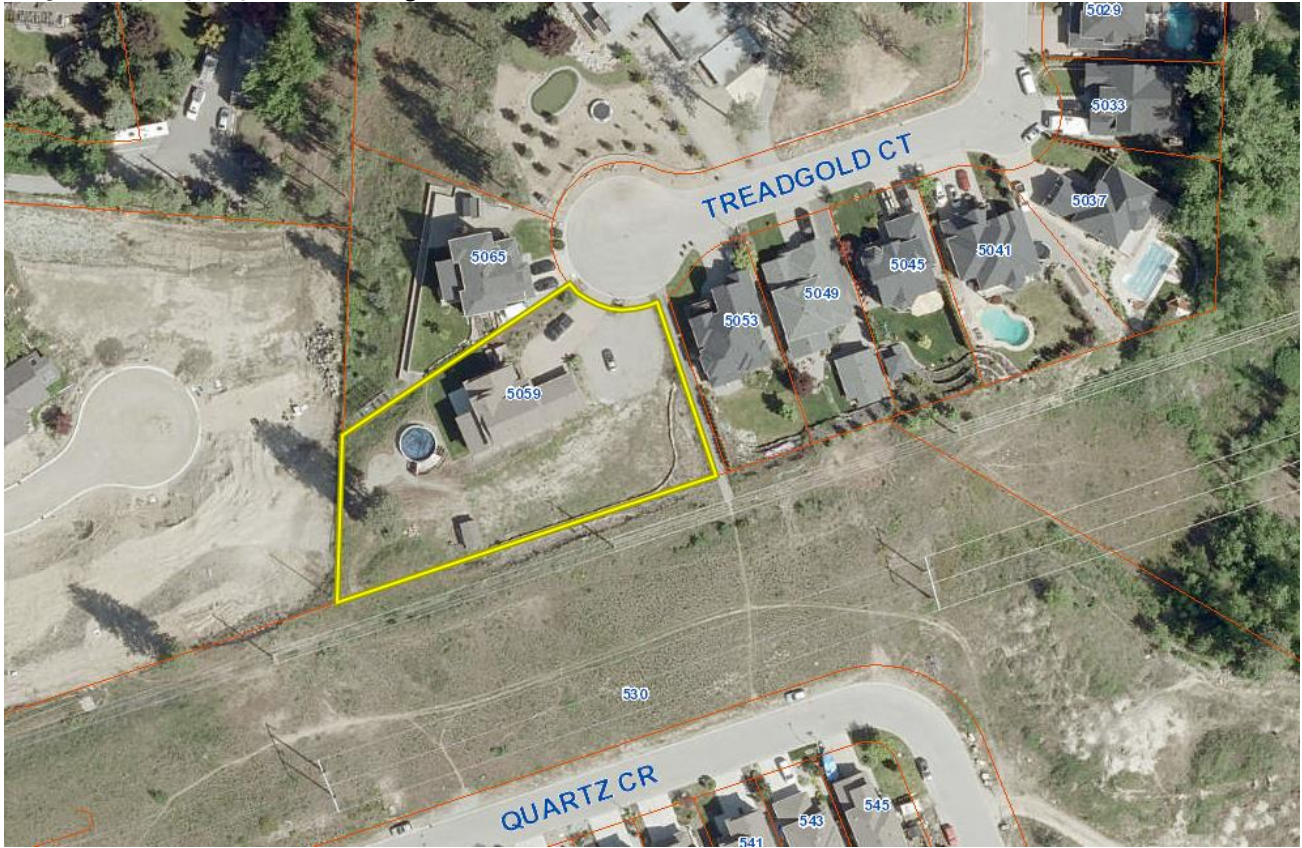
4.3 Site Context

The subject property is located on Treadgold Ct, near the edge of the Permanent Growth Boundary. The property earns a walkscore of 7, meaning it is car-dependent.

Specifically, adjacent land uses are as follows:

| Orientation | Zoning | Land Use |
|--------------------|--|-------------------------|
| North | RU ₁ – Large Lot Housing | Single Dwelling Housing |
| East | RU ₁ – Large Lot Housing | Single Dwelling Housing |
| South | P ₃ – Parks & Open Space | Park |
| West | RU _{1h} – Large Lot Housing (Hillside Area) | Single Dwelling Housing |

Subject Property Map: 5059 Treadgold Ct



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Development Process

Compact Urban Form.¹ Develop a compact urban form that maximizes the use of existing infrastructure and contributes to energy efficient settlement patterns. This will be done by increasing densities (approximately 75 - 100 people and/or jobs located within a 400 metre walking distance of transit stops is required to support the level of transit service) through development, conversion, and re-development within Urban Centres (see Map 5.3) in particular and existing areas as per the provisions of the Generalized Future Land Use Map 4.1.

Urban Uses.² Direct urban uses to lands within the urban portion of the Permanent Growth Boundary, in the interest of reducing development and speculative pressure on agricultural lands.

Housing in Agricultural Areas.³ Discourage residential development (both expansions and new developments) in areas isolated within agricultural environments (both ALR and non-ALR).

Sensitive Infill.⁴ Encourage new development or redevelopment in existing residential areas to be sensitive to or reflect the character of the neighbourhood with respect to building design, height and siting.

¹ City of Kelowna Official Community Plan, Policy 5.2.3 (Development Process Chapter).

² City of Kelowna Official Community Plan, Policy 5.33.3 (Development Process Chapter).

³ City of Kelowna Official Community Plan, Policy 5.33.8 (Development Process Chapter).

6.0 Technical Comments

6.1 Building & Permitting Department

Full plan check will be done at time of Building Permit.

6.2 Development Engineering Department

Please see attached Development Engineering Memorandum (Schedule "A").

7.0 Application Chronology

Date of Application Received: March 15, 2017

Date Public Consultation Completed: October 6, 2017

Report prepared by: Trisa Atwood, Planner

Reviewed by: Terry Barton, Urban Planning Manager

Approved for Inclusion: Ryan Smith, Community Planning Department Manager

Attachments:

Schedule "A": Development Engineering Memorandum

⁴ City of Kelowna Official Community Plan, Policy 5.22.6 (Development Process Chapter).



CITY OF KELOWNA

MEMORANDUM

Date: April 27, 2017
File No.: Z17-0029

To: Land Use Management Department (TB)

From: Development Engineering Manager

Subject: 5059 Treadgold Ct Lot 21 Plan 76347 RU1c Carriage House

Development Engineering has the following requirements associated with this application.

1. Domestic Water and Sanitary Sewer

This property is currently serviced with a 19mm-diameter water service. The service will be adequate for this application. One metered water service will supply both the main residence and the suite.

2. Sanitary Sewer

Our records indicate that this property is currently serviced with a 100mm-diameter sanitary sewer service complete with inspection chamber (IC). No service upgrades are required for this application.

The property is within Sewer Connection Area No 28.
The developer is required to make payment for half of one equivalent dwelling unit.
An additional \$11,000.00 (0.5 of \$ 22,000 charge) is required.

3. Electric Power and Telecommunication Services

It is the applicant's responsibility to make a servicing application with the respective electric power, telephone and cable transmission companies to arrange for service upgrades to these services which would be at the applicant's cost.

4. Access and Parking Requirements

The proposed parking module location for the Carriage House must meet bylaw requirements.

Steve Muenz, P. Eng.
Development Engineering Manager
JF/jf

CITY OF KELOWNA
BYLAW NO. 11543
Z17-0029 – 5059 Treadgold Court

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 21 Section 24 Township 28 SDYD Pan KAP76347 located on Treadgold Court, Kelowna, B.C., from the RU1 – Large Lot Housing zone to the RU1c – Large Lot Housing with Carriage House zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this

Considered at a Public Hearing on the

Read a second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

REPORT TO COUNCIL



Date: Jan 22nd 2018

RIM No. 0940-00

To: City Manager

From: Community Planning Department (AC)

Application: DP17-0242

Owner: MKK Property Corp Inc. No.
1097313

Address: 565 Bernard Ave

Applicant: Collabor8 – Devon Skinner

Subject: Development Permit

OCP Designation: MXR – Mixed Use (Residential / Commercial)

Existing Zone: C7 – Central Business Commercial Zone

1.0 Recommendation

THAT Council authorizes the issuance of Development Permit No. DP17-0242 for Lot A, District Lot 139, ODYD, Plan 34786, located at 565 Bernard Ave, Kelowna, BC subject to the following:

1. The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A,"
2. The exterior design and finish of the building to be constructed on the land, be in accordance with Schedule "B";

AND THAT the outstanding conditions of approval as set out in Attachment "A" attached to the Report from the Community Planning Department dated January 22nd 2018 be completed prior to Building Permit issuance.

AND THAT the applicant be required to complete the above noted conditions of Council's approval of the Development Permit application in order for the permits to be issued;

AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

2.0 Purpose

To review the Form & Character Development Permit of exterior façade renovations and building cladding updates for the Town Centre Mall.

3.0 Community Planning

3.1 Development Permit

Staff are recommending support for the proposed Development Permit due to the proposal's consistency with the majority of the Official Community Plan's (OCP) urban design guidelines, compliance with the Zoning Bylaw, and the proposal contains no variances.

The development permit application is for the property located at 565 Bernard Avenue. Currently known as the "Town Centre Mall," the site is home to a number of local retail outlets, business services, and office spaces. The intent of this development permit is to update the exterior appearance of the building and to create a visual landmark on the east side of downtown Kelowna.

The applicant's objective of the renovation is to ensure that the character of the building is attractive to residents and visitors in order to create a draw on the east side of downtown, and to elevate the design of the building to the standard of the recently completed Bernard Ave revitalization. The interior public space of the mall will be updated significantly to provide a brighter and more inviting experience.

The main entrance on Bernard Avenue will be enhanced to create greater identity and visual landmark. Overall, the project will improve the quality of development within the downtown. The project meets all the relevant design guidelines (see section 5.1 for details).

4.0 Proposal

4.1 Project Description

The existing canopy, stucco, and slate tile on the Bernard Avenue elevation will be demolished and replaced with new signage, updated stucco cladding, and new brick cladding. The applicant is proposing a combination of canvas awnings with signage and an open trellis canopy at the main entrance on Bernard Avenue. This trellis will be made from engineered Glued Laminated beams (Glulam) or similar.

Incorporated into the wood canopy, the applicant is proposing two (2) large vertical Glulam signs that reference the nature of the classic marquee sign on the Paramount theatre building (now Craft Beer Market) on the west end of Bernard Ave. The vertical marquee signs will cantilever from the face of the building and extend upwards to provide a visual landmark on the east side of Bernard. The applicant is proposing a new backlit signage that will be integral to the design of the marquee.

The remaining elevations will be cleaned and remediated where necessary, and will be updated with fresh paint to match the new color scheme along Bernard Avenue. The entrances on the east and west sides of the building will have their canopies updated with new wood cladding and updated backlit signage. The Lawrence Avenue elevation will be further enhanced with updated canvas awnings to suit the updated color scheme. The stucco cornice at the roof line of the building will be re-clad in metal panels to suit the updated color scheme. All storefront doors and windows will remain as is. At this time, the applicant is not proposing any changes to the landscaping, hard surfaces, or existing parking.

4.2 Site Context

The site is approximately 1.47 acres and is located within the downtown Urban Centre. Specifically, the adjacent land uses are as follows:

| Orientation | Zoning | Land Use |
|-------------|----------------------------------|------------|
| North | C7 – Central Business Commercial | Commercial |
| East | C7 – Central Business Commercial | Commercial |
| South | C7 – Central Business Commercial | Commercial |
| West | C7 – Central Business Commercial | Commercial |

Subject Property Map: 565 Bernard Ave



5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

DEVELOPMENT PERMIT GUIDELINES

Consideration has been given to the following guidelines as identified in Section 14.A. of the City of Kelowna Official Community Plan relating to Comprehensive Development Permit Areas:

| COMPREHENSIVE DEVELOPMENT PERMIT AREA | YES | NO | N/A |
|---|-----|----|-----|
| Authenticity and Regional Expression | | | |
| Do landscaping and building form convey a character that is distinct to Kelowna and the Central Okanagan? | x | | |
| Are materials in keeping with the character of the region? | x | | |
| Are colours used common in the region's natural landscape? | x | | |
| Does the design provide for a transition between the indoors and outdoors? | x | | |

| COMPREHENSIVE DEVELOPMENT PERMIT AREA | YES | NO | N/A |
|---|-----|----|-----|
| Context | | | |
| Does the proposal maintain the established or envisioned architectural character of the neighbourhood? | x | | |
| Does interim development consider neighbouring properties designated for more intensive development? | | | x |
| Are façade treatments facing residential areas attractive and context sensitive? | x | | |
| Are architectural elements aligned from one building to the next? | x | | |
| For exterior changes, is the original character of the building respected and enhanced? | x | | |
| Is the design unique without visually dominating neighbouring buildings? | X | | |
| For developments with multiple buildings, is there a sense of architectural unity and cohesiveness? | | | X |
| Relationship to the Street | | | |
| Do buildings create the desired streetscape rhythm? | X | | |
| Are parkade entrances located at grade? | | | X |
| For buildings with multiple street frontages, is equal emphasis given to each frontage? | | X | |
| Massing and Height | | | |
| Does the design mitigate the actual and perceived mass of buildings? | X | | |
| Does the height consider shading and view impacts for neighbouring properties and transition to less intensive areas? | | | X |
| Human Scale | | | |
| Are architectural elements scaled for pedestrians? | X | | |
| Are façades articulated with indentations and projections? | X | | |
| Are top, middle and bottom building elements distinguished? | X | | |
| Do proposed buildings have an identifiable base, middle and top? | X | | |
| Are building facades designed with a balance of vertical and horizontal proportions? | x | | |
| Are horizontal glazed areas divided into vertically proportioned windows separated by mullions or building structures? | X | | |
| Does the design incorporate roof overhangs and the use of awnings, louvers, canopies and other window screening techniques? | X | | |
| Is the visual impact of enclosed elevator shafts reduced through architectural treatments? | | | X |
| Exterior Elevations and Materials | | | |
| Are buildings finished with materials that are natural, local, durable and appropriate to the character of the development? | X | | |

| COMPREHENSIVE DEVELOPMENT PERMIT AREA | YES | NO | N/A |
|--|------------|-----------|------------|
| Are entrances visually prominent, accessible and recognizable? | X | | |
| Are higher quality materials continued around building corners or edges that are visible to the public? | X | | |
| Are a variety of materials used to create contrast, enhance the pedestrian environment and reduce the apparent mass of a building? | X | | |
| Are elements other than colour used as the dominant feature of a building? | X | | |
| Public and Private Open Space | | | |
| Does public open space promote interaction and movement through the site? | X | | |
| Are public and private open spaces oriented to take advantage of and protect from the elements? | X | | |
| Is there an appropriate transition between public and private open spaces? | X | | |
| Are amenities such as benches, garbage receptacles, bicycle stands and community notice boards included on site? | X | | |
| Site Access | | | |
| Is the safe and convenient movement of pedestrians prioritized? | X | | |
| Are alternative and active modes of transportation supported through the site design? | X | | |
| Are identifiable and well-lit pathways provided to front entrances? | X | | |
| Do paved surfaces provide visual interest? | X | | |
| Is parking located behind or inside buildings, or below grade? | | | X |
| Are large expanses of parking separated by landscaping or buildings? | | | X |
| Are vehicle and service accesses from lower order roads or lanes? | | | X |
| Do vehicle and service accesses have minimal impact on the streetscape and public views? | | | X |
| Is visible and secure bicycle parking provided in new parking structures and parking lots? | | | X |
| Environmental Design and Green Building | | | |
| Does the proposal consider solar gain and exposure? | | | X |
| Are green walls or shade trees incorporated in the design? | | | X |
| Does the site layout minimize stormwater runoff? | | | x |
| Are sustainable construction methods and materials used in the project? | unknown | | |
| Are green building strategies incorporated into the design? | | x | |
| Decks, Balconies, Rooftops and Common Outdoor Amenity Space | | | |

| COMPREHENSIVE DEVELOPMENT PERMIT AREA | YES | NO | N/A |
|---|---------|----|-----|
| Are decks, balconies or common outdoor amenity spaces provided? | | | X |
| Does hard and soft landscaping enhance the usability of decks, balconies and outdoor amenity spaces? | | | X |
| Are large flat expanses of roof enhanced with texture, colour or landscaping where they are visible from above or adjacent properties? | | | X |
| Amenities, Ancillary Services and Utilities | | | |
| Are loading, garage, storage, utility and other ancillary services located away from public view? | | | X |
| Are vents, mechanical rooms / equipment and elevator penthouses integrated with the roof or screened with finishes compatible with the building's design? | | | X |
| Crime prevention | | | |
| Are CPTED practices as related to landscaping, siting, form and exterior design included in the design? | | | x |
| Are building materials vandalism resistant? | unknown | | |
| Universal Accessible Design | | | |
| Is access for persons with disabilities integrated into the overall site plan and clearly visible from the principal entrance? | x | | |
| Are the site layout, services and amenities easy to understand and navigate? | x | | |
| Signs | | | |
| Do signs contribute to the overall quality and character of the development? | x | | |
| Is signage design consistent with the appearance and scale of the building? | x | | |
| Are signs located and scaled to be easily read by pedestrians? | x | | |
| For culturally significant buildings, is the signage inspired by historical influences? | x | | |
| Lighting | | | |
| Does lighting enhance public safety? | x | | |
| Is "light trespass" onto adjacent residential areas minimized? | | X | |
| Does lighting consider the effect on the façade, neighbouring buildings and open spaces? | | X | |
| Is suitably scaled pedestrian lighting provided? | x | | |
| Does exterior street lighting follow the International Dark Sky Model to limit light pollution? | | | x |

6.0 Technical Comments

6.1 Building & Permitting Department

- Full Plan check for Building Code related issues will be done at time of Building Permit applications.

6.2 Development Engineering Department

- See Memo (Attachment 'A') dated November 14th 2017

6.3 Fortis BC (Electric)

- There are FortisBC Inc (Electric) ("FBC(E)") primary distribution facilities within the lane adjacent to the subject property. The applicant is responsible for costs associated with any change to the subject property's existing service, if any, as well as the provision of appropriate land rights where required.
- Otherwise, FBC(E) has no concerns with this circulation.

6.4 Fire Department

- No concerns with the exterior renovation - ensure appropriate flame spread rating of cladding.

7.0 Application Chronology

Date of Application:

November 3rd 2017

Date of Notification Letters:

n/a

Prepared by:

Adam Cseke, Urban Planner

Reviewed by:

Terry Barton, Urban Planning Manager

Approved for:

Ryan Smith, Community Planning Department Manager

Attachments:

Attachment A - Development Engineering Memo

DP17-0242

CITY OF KELOWNA

MEMORANDUM

Date: November 14, 2017
File No.: DP17-0242
To: Community Planning (AC)
From: Development Engineering Manager (JK)
Subject: 565 Bernard Ave Lot A, Plan 34786

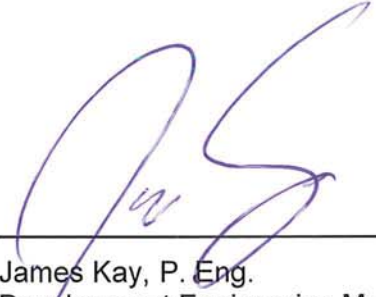
The Development Engineering Department has the following comments and requirements associated with this application to construct a temporary garden center.

1. General

This application does not trigger any frontage or infrastructure upgrades.

2. Drainage

Any drainage issues resulting from the exterior renovation and cladding upgrades as well as removing the existing canopy on Bernard Ave. and replacing with a new wood trellis canopy and canvas awning must be dealt with onsite.



James Kay, P. Eng.
Development Engineering Manager

JA

| | | |
|--------------------------------|----|---|
| ATTACHMENT | | A |
| This forms part of application | | |
| # DP17-0242 | | |
| Planner Initials | AC |  City of Kelowna <small>COMMUNITY PLANNING</small> |

Development Permit DP17-0242



This permit relates to land in the City of Kelowna municipally known as

565 Bernard Ave

and legally known as

Lot A, District Lot 139, ODYD, Plan 34786

and permits the land to be used for the development with variances to the following sections of the Zoning Bylaw 8000:

1. The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A,"
2. The exterior design and finish of the building to be constructed on the land, be in accordance with Schedule "B";

The development has been approved subject to any attached terms and conditions, and to full compliance with the approved plans bearing the stamp of approval and the above described development permit number.

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

Date of Decision: January 22, 2018

Decision By: CITY COUNCIL

Issued Date: TBD

Development Permit Area: Comprehensive Development Permit Area

File Manager: AC

This permit will not be valid if development has not commenced within 2 years of the council approved Date of Decision.

Existing Zone: C7 – Central Business Commercial Zone

Future Land Use Designation: MXR – Mixed Use (Residential / Commercial)

This is NOT a Building Permit.

In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

NOTICE

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner: MKK Property Corp Inc. No. 109731
Address: 565 Bernard Ave
City: Kelowna, BC
Phone: n/a

Ryan Smith, Community Planning Department Manager
Community Planning & Strategic Investments

Date

1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

2. CONDITIONS OF APPROVAL

- a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A";
- b) The exterior design and finish of the building to be constructed on the land be in accordance with Schedule "B";

This Development Permit is valid for two (2) years from the Council Date of Decision if applicable, or Community Planning Department Manager approval, with no opportunity to extend.

3. PERFORMANCE SECURITY

n/a.

4. Indemnification

Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:

- a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

**The PERMIT HOLDER is the CURRENT LAND OWNER.
Security shall ONLY be returned to the signatory of the
Landscape Agreement or their designates.**

TOWNE CENTRE MALL

EXTERIOR & INTERIOR RENOVATION

565 BERNARD AVE
KELOWNA, BC

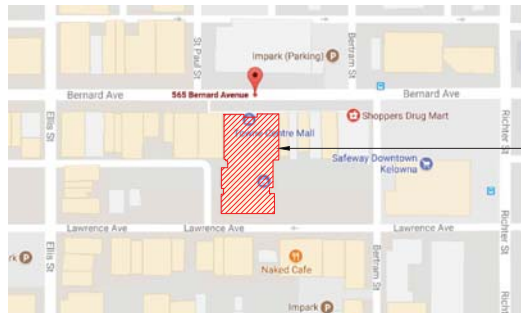
ISSUED FOR DEVELOPMENT PERMIT
OCTOBER 18, 2017

SCHEDULE A & B

This forms part of application
DP17-0242



Planner
Initials AC



1 VICINITY MAP
SCALE: N.T.S.



2 LANDUSE BYLAW MAP
SCALE: N.T.S.

LEGAL ADDRESS:
LOT A DL 139, OYD PLAN 34786

MUNICIPAL ADDRESS:
565 BERNARD AVE
KELOWNA, BC

PROJECT DESCRIPTION:
EXTERIOR AND INTERIOR RENOVATION AND CLADDING UPDATES.
REMOVING EXISTING CANOPY ON BERNARD AVENUE ELEVATION
AND REPLACING WITH A NEW WOOD TRELLIS CANOPY & CANVAS
AWNING.

PROJECT STATISTICS:
MAIN FLOOR AREA: ±3221m².

LANDUSE BYLAW REQUIREMENTS:

SITE ZONING:
C7 - CENTRAL BUSINESS COMMERCIAL

SITE AREA:
±5938 m²

SETBACKS:
FRONT YARD = 0m (<1.6m HIGH)
REAR YARD = 0m (<1.6m HIGH)
SIDE YARD = 0m (<1.6m HIGH)

BUILDING HEIGHT:
MAXIMUM HEIGHT = 26 STOREYS (76.5m)

PROPOSED BUILDING HEIGHT = 11.89m

PARKING CALCULATIONS:
44 OFF STREET STALLS, NO CHANGES TO PARKING STALLS

BICYCLE STALLS REQUIRED:
NO CHANGES TO EXISTING CONDITIONS

BICYCLE STALLS PROVIDED:
0

DRAWING LIST

ARCHITECTURAL
DP000 - COVER SHEET & PROJECT INFORMATION
DP001 - SITE PHOTOS
DP100 - SITE PLAN
DP101 - SIGNAGE DETAILS
DP200 - PROPOSED MAIN FLOOR PLAN
DP201 - PROPOSED SECOND FLOOR PLAN
DP300 - PROPOSED ELEVATIONS
DP301 - PROPOSED ELEVATIONS
DP302 - COLORED RENDERINGS
DP303 - DIGITAL COLOR BOARD
DP400 - SIGNAGE ELEVATIONS & ENLARGED PLANS
DP401 - SIGNAGE ELEVATIONS & ENLARGED PLANS

SITE LIGHTING
EO.0 - ELECTRICAL SITE PLAN
EO.1 - PROPOSED ELECTRICAL EXTERIOR LIGHTING PLAN

SITE SURVEY
S1 - TOPOGRAPHIC SITE PLAN

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drawings and designs shall be reported to the architect.

ISSUED FOR:
01 2017.10.18 ISSUED FOR DEVELOPMENT PERMIT

| | |
|--------------|--|
| CLIENT: | MKK |
| PROJECT: | TOWNE CENTRE MALL |
| ADDRESS: | 565 BERNARD AVE KELOWNA, BC LOT A DL 139, OYD PLAN 34786 |
| CONSULTANTS: | |
| SHEET TITLE: | PROJECT INFORMATION |

| | |
|-------------|-----------|
| DATE: | SEPT 2017 |
| SCALE: | AS NOTED |
| DRAWN BY: | DKW/LHR |
| PROJECT No: | 17.15.01 |

| | |
|------------|---|
| ARCHITECT: | VANGUARD |
| DESIGNER: | 404.687.3380 404.687.3380 The Martin Building 180, 100 Westport Rd. Kelowna, BC V1Y 1A8 |
| CLIENT: | 403.233.8448 403.233.8448 201, 1815 - 1st Ave NE Calgary, AB T2E 7W7 |

| |
|---------------------|
| PROJECT INFORMATION |
|---------------------|

| | |
|-------------|-----------|
| DATE: | SEPT 2017 |
| SCALE: | AS NOTED |
| DRAWN BY: | DKW/LHR |
| PROJECT No: | 17.15.01 |

| | |
|------------|---|
| ARCHITECT: | VANGUARD |
| DESIGNER: | 404.687.3380 404.687.3380 The Martin Building 180, 100 Westport Rd. Kelowna, BC V1Y 1A8 |
| CLIENT: | 403.233.8448 403.233.8448 201, 1815 - 1st Ave NE Calgary, AB T2E 7W7 |

| |
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| PROJECT INFORMATION |
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| DRAWING No: | REV: |
|-------------|------|

| |
|-------|
| DP000 |
|-------|



2 SITE PHOTO
SCALE: N.T.S.



3 SITE PHOTO
SCALE: N.T.S.



4 SITE PHOTO
SCALE: N.T.S.



5 SITE PHOTO
SCALE: N.T.S.



6 SITE PHOTO
SCALE: N.T.S.



7 SITE PHOTO
SCALE: N.T.S.

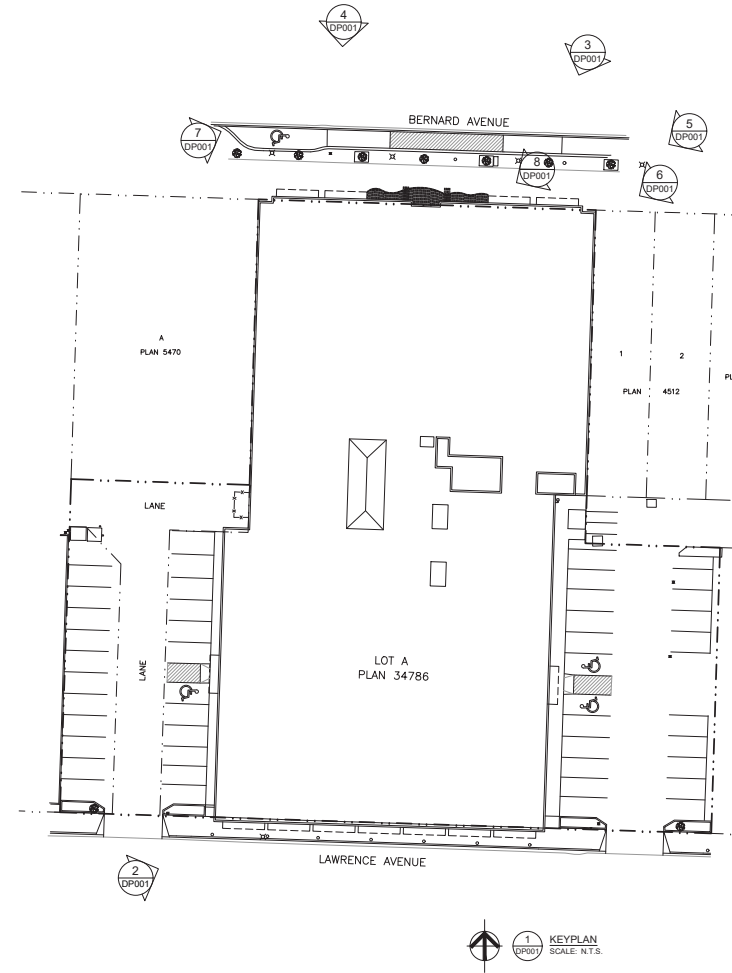


8 SITE PHOTO
SCALE: N.T.S.

SCHEDULE A & B

This forms part of application
DP17-0242

Planner
Initials AC



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| | |
|--------------|--|
| CLIENT: | MKK |
| PROJECT: | TOWNE CENTRE MALL |
| ADDRESS: | 565 BERNARD AVE KELOWNA, BC LOT A DL 139; ODDY PLANS 34786 |
| CONSULTANTS: | |
| SHEET TITLE: | SITE PHOTOS |

| | |
|-------------|-----------|
| DATE: | SEPT 2017 |
| SCALE: | AS NOTED |
| DRAWN BY: | DKW/LHR |
| PROJECT No: | 17.15.01 |

| | |
|--|--|
| <input type="checkbox"/> VANCOUVER 1 604.687.3380 2 604.687.3381 3 The Metro Building 180, 100 Westport St. Vancouver, BC Canada V6C 2B8 | <input checked="" type="checkbox"/> CALGARY 1 403.232.8448 2 403.232.8449 3 201, 1815 - 1st Ave NE Calgary, AB Canada T2E 7W7 |
|--|--|

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| DRAWING No: DP001 REV: |
|---------------------------|

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| 1 KEYPLAN SCALE: N.T.S. |
|----------------------------|

| | |
|--|---|
| 1 604.687.3380 2 604.687.3381 3 The Metro Building 180, 100 Westport St. Vancouver, BC Canada V6C 2B8 | 1 403.232.8448 2 403.232.8449 3 201, 1815 - 1st Ave NE Calgary, AB Canada T2E 7W7 |
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| DRAWING No: DP001 REV: |
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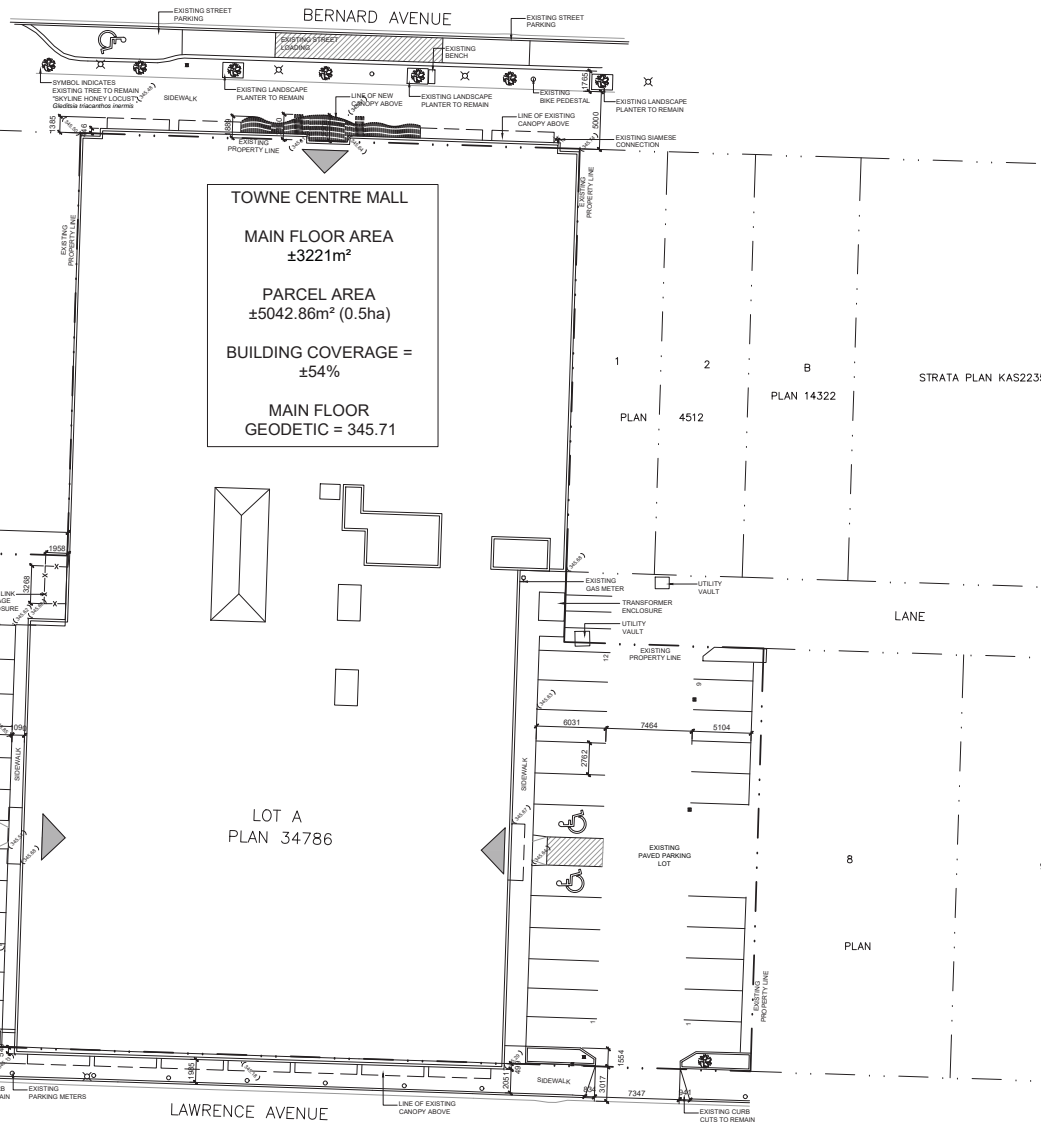




SCHEDULE A & B

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DP17-0242

Planner
Initials AC



SITE LEGEND

- PROPERTY LINE
- F.H. FIRE HYDRANT
- SIAMESE CONNECTION
- PARKING METER
- BARRIER FREE STALL
- LIGHT STANDARD
- EXISTING GRADE ELEVATION
- NEW GRADE ELEVATION
- ENTRY
- NORTH

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PROJECT: TOWNE CENTRE MALL

565 BERNARD AVE
KELOWNA, BC
LOT A DL 139/00DYD PLAN34786

CONSULTANTS:

SHEET TITLE: SITE PLAN

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKW/LHR
PROJECT No: 17.15.01

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 - 180, 100 Westport St.
 - Vancouver, BC
 - Canada V6C 2B8
- ☒ CALGARY
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 - Calgary, AB
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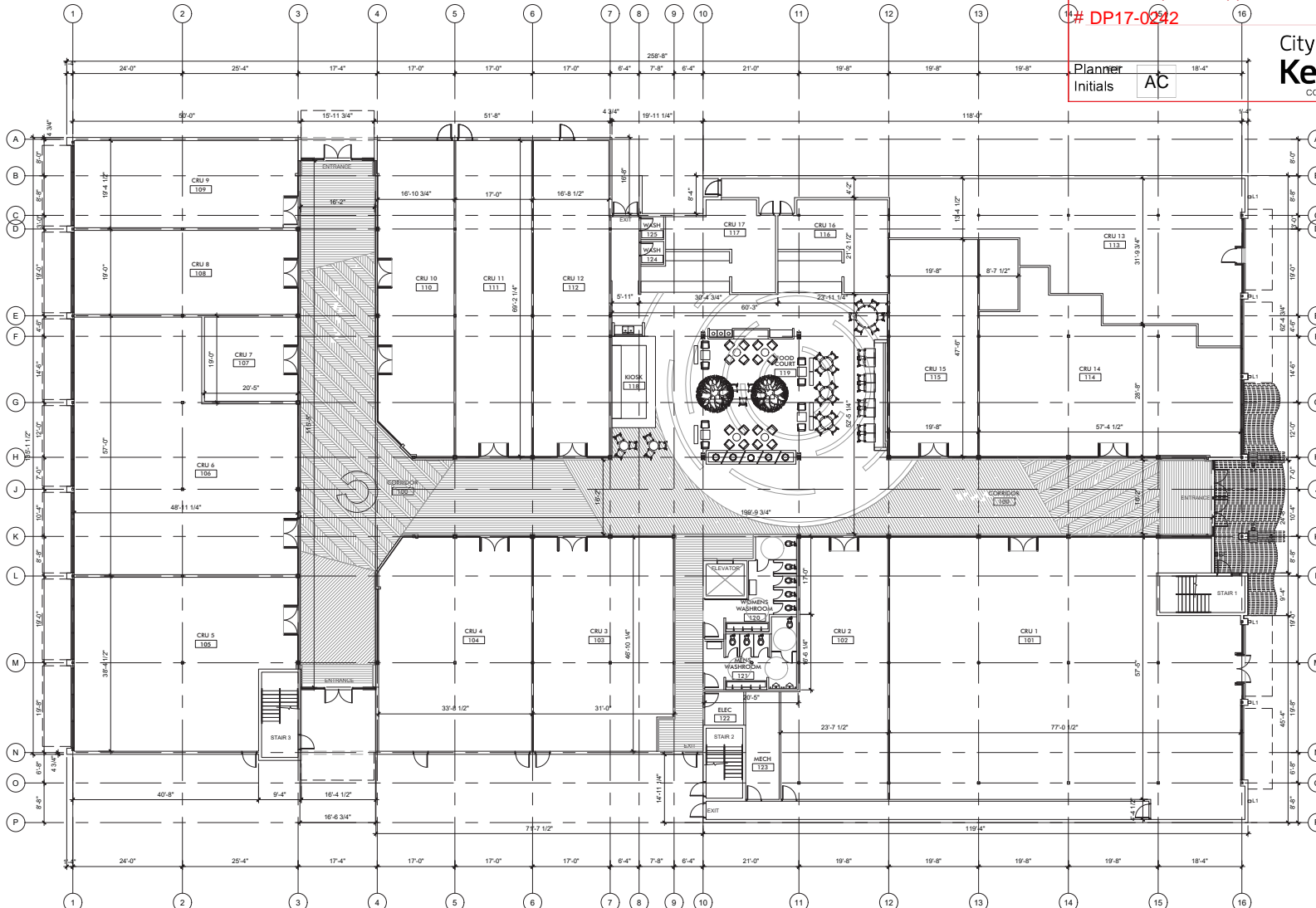
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DP100

SCHEDULE A & B

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Planner
Initials AC



1 PROPOSED MAIN FLOOR PLAN
SCALE: 3/32" = 1'-0"

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CLIENT: MKK

PROJECT: TOWNE CENTRE MALL

565 BERNARD AVE
KELOWNA, BC
LOT 4 DL 139/00DY PLAN34786

CONSULTANTS:

SHEET TITLE: PROPOSED MAIN FLOOR PLAN

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKW/LHR
PROJECT NO: 17.15.01

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• 400-681-3380
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Canada V6C 2B8

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• 403.233-8448
• 201, 1616 - 1st Ave NE
Calgary, AB
Canada T2E 7W7

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DRAWING NO: DP200 REV:

SCHEDULE A & B

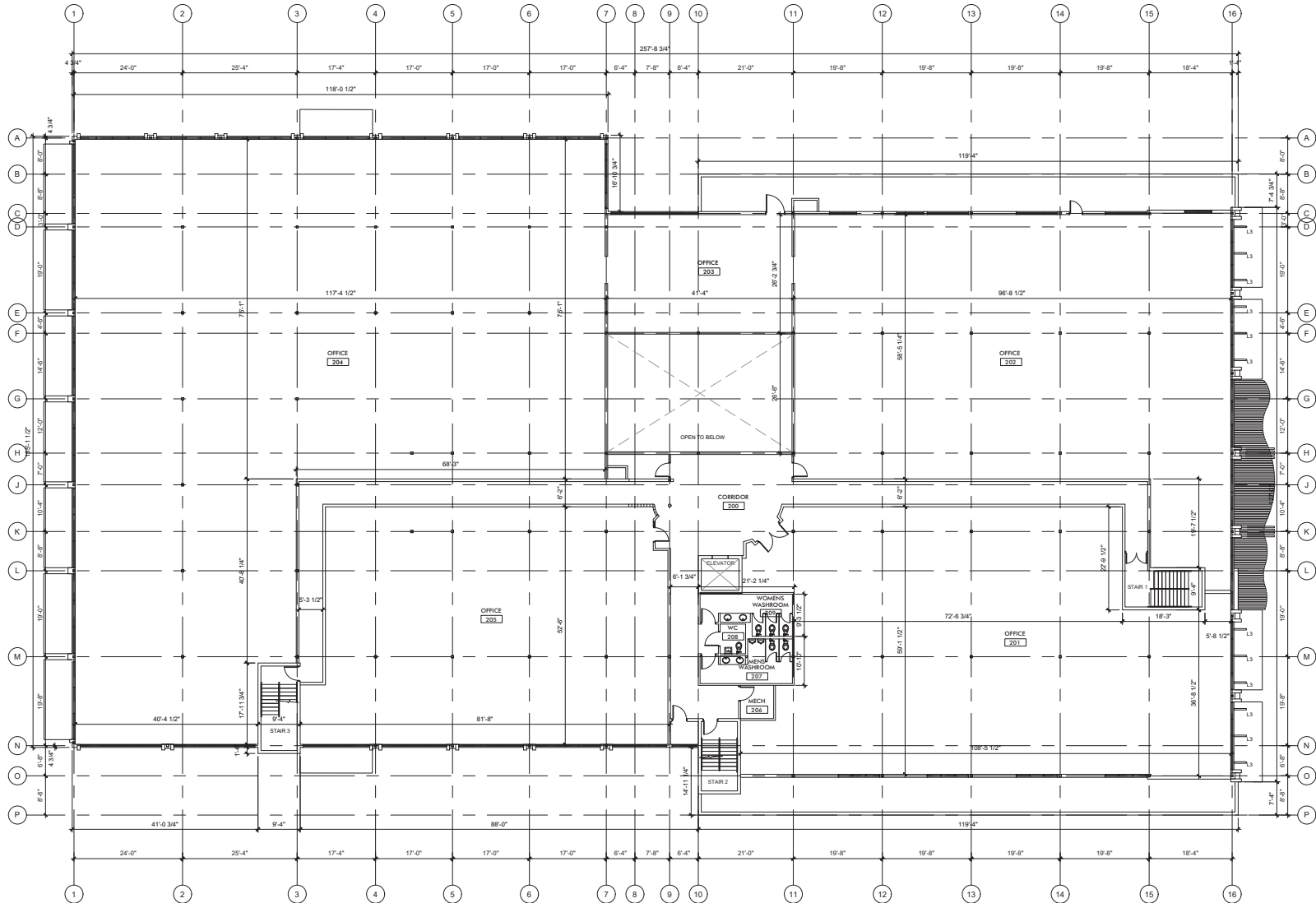
This forms part of application

DP17-0242

Planner
Initials

AC

City of
Kelowna
COMMUNITY PLANNING



1 PROPOSED SECOND FLOOR PLAN
SCALE: TBD

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ISSUED FOR:
01 2017.10.18 ISSUED FOR DEVELOPMENT PERMIT

SEAL

CLIENT:



PROJECT:

TOWNE CENTRE
MALL

565 BERNARD AVE
KELOWNA, BC
LOT A DL 139/00DY PLAN34786

CONSULTANTS:

SHEET TITLE:

PROPOSED SECOND
FLOOR PLAN

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKWL/JHR
PROJECT NO: 17.15.01

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1 604.681.3380
1000-1000
The Marine Building
180, 100 Westport Rd.
Vancouver, BC
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☒ CALGARY
1 403.233.8448
4010-100-10
201, 1015 - 1st Ave NE
Calgary, AB
Canada T2E 7W7

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DRAWING NO: REV:
DP201

SCHEDULE A & B

This forms part of application

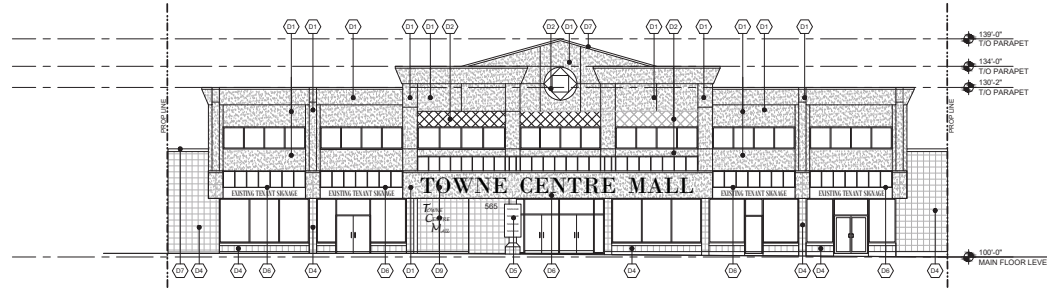
DP17-0242

Planner
Initials AC

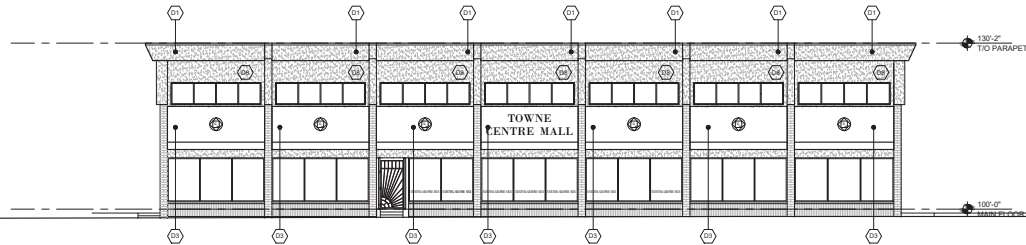


| DEMOLITION LEGEND | |
|-------------------|---|
| | EXISTING STUCCO TO BE DEMOLISHED. PATCH & REPAIR AS REQUIRED TO MAKE SURFACE READY FOR NEW FINISHES. |
| | EXISTING STUCCO BUILD OUTS AND DETAILS TO BE DEMOLISHED. PATCH & REPAIR AS REQUIRED TO MAKE SURFACE READY FOR NEW FINISHES. |
| | EXISTING AWNING FABRIC TO BE REMOVED. RETAIN FRAME FOR INSTALLATION OF NEW AWNING FABRIC. |
| | EXISTING SLATE TO BE DEMOLISHED. PATCH & REPAIR AS REQUIRED TO MAKE SURFACE READY FOR NEW BRICK FINISH. |
| | EXISTING COLUMN AND SIGNAGE TO BE DEMOLISHED. PATCH & REPAIR AS REQUIRED TO MAKE SURFACE READY FOR NEW FINISHES. |
| | EXISTING STUCCO PROJECTION, GLASS CANOPY AND SIGNAGE TO BE DEMOLISHED. PATCH & REPAIR AS REQUIRED TO MAKE SURFACE READY FOR NEW FINISHES. |
| | EXISTING CAP FLASHING TO BE REMOVED. |
| | STUCCO TO REMAIN. PATCH AND REPAIR AS REQUIRED TO RECEIVE NEW PAINT FINISH. |
| | EXISTING PROJECT SIGNAGE TO BE DEMOLISHED. |
| | EXISTING GUARD RAIL TO REMAIN. |

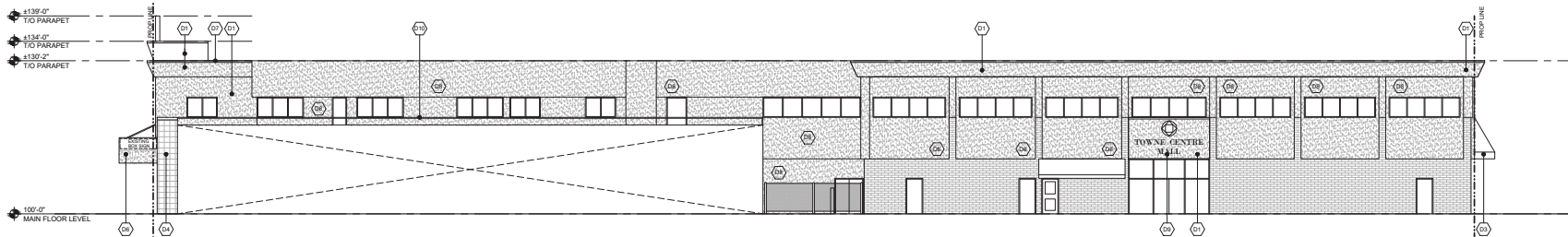
DEMOLITION NOTES:
1. ALL EXISTING STOREFRONT GLAZING & DOORS TO REMAIN AS IS.
2. ALL EXISTING BRICK AT THE REAR AND SIDE ELEVATIONS TO REMAIN.



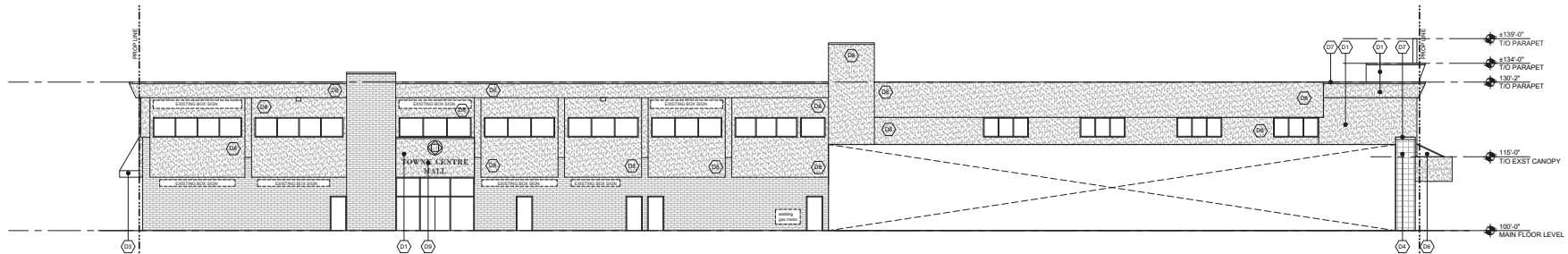
1 DEMOLITION ELEVATIONS
SCALE: TBD



2 DEMOLITION ELEVATIONS
SCALE: TBD



3 DEMOLITION ELEVATIONS
SCALE: TBD



4 DEMOLITION ELEVATIONS
SCALE: TBD

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ISSUED FOR:
01 2017.10.18 ISSUED FOR DEVELOPMENT PERMIT

CLIENT:
PROJECT:
TOWNE CENTRE MALL
565 BERNARD AVE
KELOWNA, BC
LOT A DL 139.00/DY PLAN34786



CONSULTANTS:
SHEET TITLE:
DEMOLITION ELEVATIONS

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKW/LHR
PROJECT No: 17.15.01

VANCOUVER
1 604.687.3380
• 401-100-1111
• The Marine Building
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Vancouver, BC
Canada V6C 2B8

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• 403-233-8448
• 201, 1616 - 1st Ave NE
Calgary, AB
Canada T2E 7W7

DRAWING No: DP300
REV:

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ARCHITECTURE + DESIGN INC.

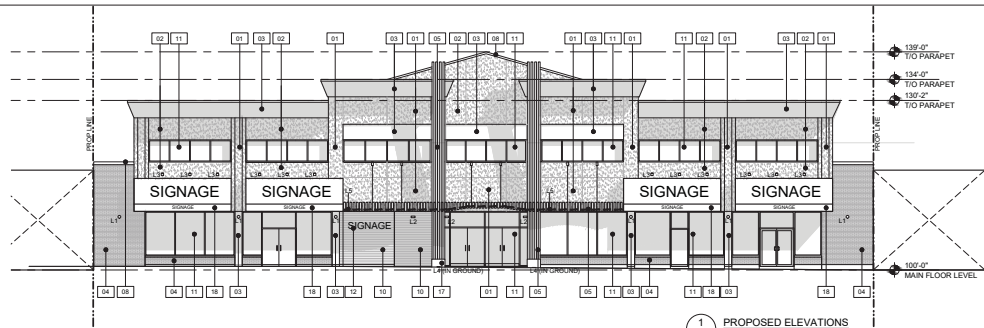
| LIGHTING LEGEND | |
|-----------------|--|
| 11 | WALL MOUNTED LIGHT FIXTURE MANUFACTURER - SIDA PART - 33 222 FINISH - POWDER COATED BLACK (BK) SUPPLIER - WOV LIGHTING, MARTA BARAHONA DE HERNADEZ |
| 12 | WALL MOUNTED LIGHT FIXTURE MANUFACTURER - SISTEMALUX PART - CO2 SQUARE LED FINISH - POWDER COATED BLACK (BK) SUPPLIER - COM2 LIGHTWORKS, MEGHAN BOYD |
| 13 | WALL MOUNTED FLOOD LIGHT PROJECTOR MANUFACTURER - SISTEMALUX PART - MOUNT ARM RECTANGULAR FINISH - POWDER COATED BLACK (BK) SUPPLIER - COM2 LIGHTWORKS, MEGHAN BOYD |
| 14 | IN-GROUND LIGHT FIXTURE MANUFACTURER - SISTEMALUX PART - 225 ROUND NAILCOVER FINISH - STAINLESS STEEL SUPPLIER - COM2 LIGHTWORKS, MEGHAN BOYD |
| 15 | FLEXIBLE LED TUBE LIGHTING FIXTURE MANUFACTURER - ACCULUM LIGHTING PART - FLEXITUBE SE 808 FINISH - FLEXIBLE SILICONE JACKET (WHITE) SUPPLIER - WOV LIGHTING, MARTA BARAHONA DE HERNADEZ |

| MATERIALS LEGEND | |
|------------------|---|
| 01 | DRIVIT STUCCO COLOR: DRIFTWOOD #133 FINISH - LYMESTONE DPR |
| 02 | DRIVIT STUCCO COLOR: WINTER EVE #618 FINISH - LYMESTONE DPR |
| 03 | ALPOLIC METAL PANEL COLOR: TR JBR BRONZE #44007/PAUBR3.5 |
| 04 | BRICK FINISH - RAVEN COLOR - MUTUAL MATERIALS CO. RUNNING BOND |
| 05 | WOOD - GLULAM ENGINEERED |
| 06 | BLACK CANVAS AWNING FABRIC ON EXISTING FRAME W/ TENANT SIGNAGE SEE DETAIL 30P41 COLOR: TURN SOLID CANVAS 548 BLACK |
| 07 | EXISTING BRICK FINISH PAINTED: COLOR TO MATCH NEW BRICK COLOR |
| 08 | NEW CAP FLASHING COLOR: TO MATCH ALPOLIC METAL PANEL FR JBR BRONZE |
| 09 | EXISTING METAL DOOR & FRAME: PAINTED COLOR: BENJAMIN MOORE HC-185 KENDALL CHARCOAL |
| 10 | NEW WOOD SIDING COLOR: TO MATCH GLULAM |
| 11 | EXISTING STOREFRONT GLAZING: TO REMAIN AS IS |
| 12 | PROJECT SIGNAGE: TBD |
| 13 | EXISTING METAL GATE TO REMAIN |
| 14 | EXISTING STUCCO TO REMAIN AND NEW ELASTOMERIC PAINT COLOR: TO MATCH DRIVIT DRIFTWOOD #133 |
| 15 | EXISTING STUCCO TO REMAIN AND NEW ELASTOMERIC PAINT COLOR: TO MATCH DRIVIT WINTER EVE #618 |
| 16 | EXISTING RED METAL DOOR & FRAME: TO REMAIN AS IS |
| 17 | NATURAL CONCRETE BASE |
| 18 | BLACK CANVAS AWNING FABRIC ON NEW BLACK STEEL FRAME W/ TENANT SIGNAGE SEE DETAIL 40P41 COLOR: TURN SOLID CANVAS 548 BLACK |

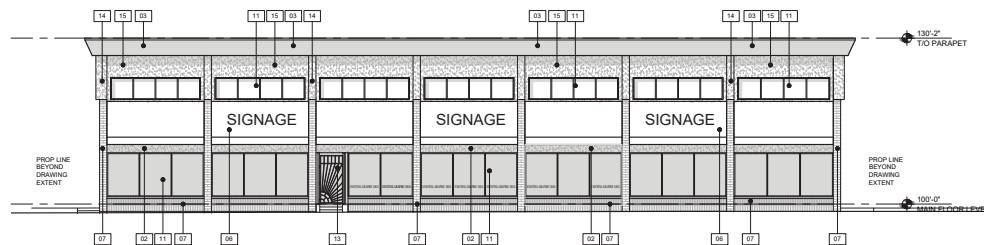
SCHEDULE A & B

This forms part of application
DP17-0242

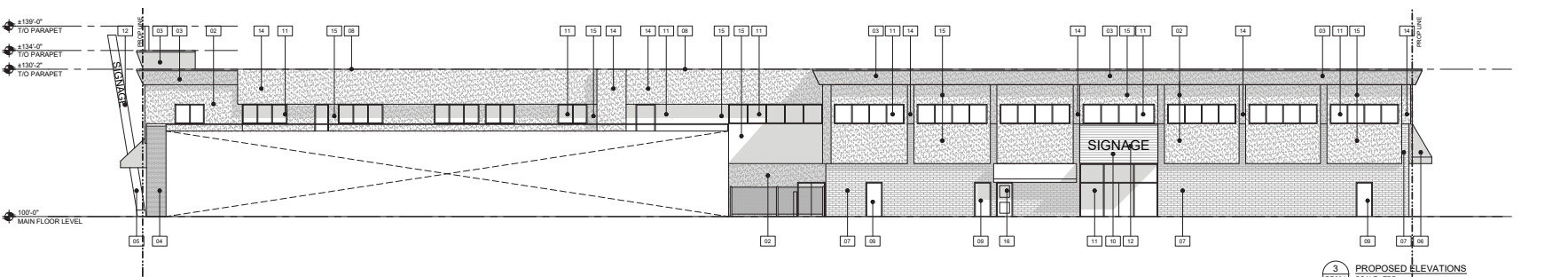
Planner
Initials AC



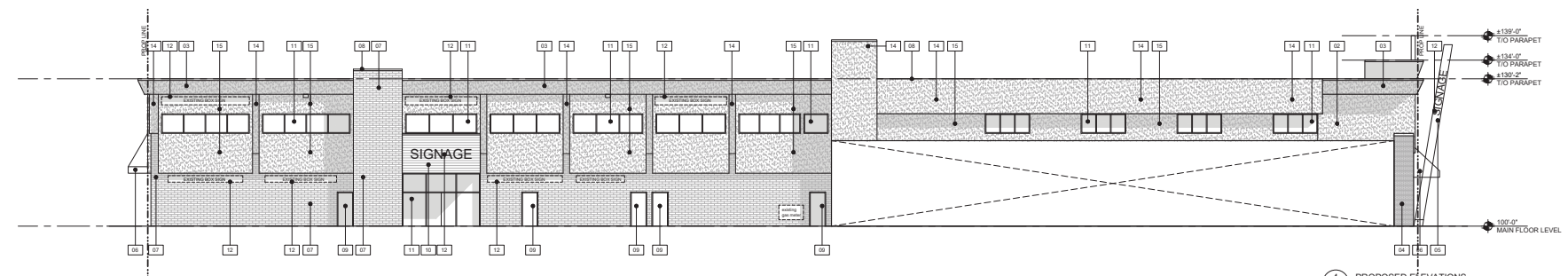
1
DP301
PROPOSED ELEVATIONS
SCALE: TBD



2
DP301
PROPOSED ELEVATIONS
SCALE: TBD



3
DP301
PROPOSED ELEVATIONS
SCALE: TBD



4
DP301
PROPOSED ELEVATIONS
SCALE: TBD

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ISSUED FOR:
01 2017.10.18 ISSUED FOR DEVELOPMENT PERMIT

CLIENT:
PROJECT:
TOWN CENTRE
MALL



PROJECT:
TOWN CENTRE
MALL

565 BERNARD AVE
KELOWNA, BC
LOT A DL 139.00/DY PLAN34786

CONSULTANTS:
SHEET TITLE:
PROPOSED
ELEVATIONS

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKWLHR
PROJECT No: 17.15.01

VANCOUVER
1 604.681.3380
• 4100-10th Ave
• The Marine Building
• 180, 100 Westport St.
Vancouver, BC
Canada V6C 2B8

CALGARY
1 403.233.8448
• 400-10th Ave SE
• 201, 1015 - 1st Ave SE
Calgary, AB
Canada T2E 7W7

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DRAWING No: DP301
REV:

SCHEDULE A & B

This forms part of application
DP17-0242

Planner
Initials AC



1 PROPOSED ELEVATIONS
SCALE: N.T.S.



2 PROPOSED ELEVATIONS
SCALE: N.T.S.



3 PROPOSED ELEVATIONS
SCALE: N.T.S.



4 PROPOSED ELEVATIONS
SCALE: N.T.S.

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SEAL



PROJECT:
TOWNE CENTRE MALL

565 BERNARD AVE
KELOWNA, BC
LOT A DL 139/00YD PLAN34786

CONSULTANTS:

SHEET TITLE:
PROPOSED COLOURED ELEVATIONS

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKW/LHR
PROJECT No: 17.15.01

☐ VANCOUVER
1 604.687.3380
• 4010-10-10-10
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180, 100 Westport St.
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Canada V6C 2B8

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1 403.233.8448
• 2010-10-10-10
• 201, 1015 - 1st Ave NE
Calgary, AB
Canada T2E 7W7

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ARCHITECTURE + DESIGN INC.

DRAWING No: DP302

| MATERIALS LEGEND | |
|------------------|---|
| 01 | DRYVIT STUCCO COLOR: DRYTWOOD #133 FINISH: LYMESTONE DPR |
| 02 | DRYVIT STUCCO COLOR: WINTER EVE #618 FINISH: LYMESTONE DPR |
| 03 | ALPOLIC METAL PANEL COLOR: FR. BR. BRONZE #80001FRBR03.5 |
| 04 | BRICK FINISH - RAVEN COLOR - MUTUAL MATERIALS CO. RUNNING BOND |
| 05 | WOOD - GLULAM ENGINEERED |
| 06 | BLACK CANVAS AWNING FABRIC ON EXISTING FRAME W/ TENANT SIGNAGE SEE DETAIL 40P01 COLOR: FURN SOLID CANVAS S48 BLACK |
| 07 | EXISTING BRICK FINISH PAINTED: COLOR TO MATCH NEW BRICK COLOR |
| 08 | NEW CAP FLASHING COLOR: TO MATCH ALPOLIC METAL PANEL FR. BR. BRONZE |
| 09 | EXISTING METAL DOOR & FRAME: PAINTED COLOR: BENJAMIN MOORE HC-186 KENDALL CHARCOAL |
| 10 | NEW WOOD SIDING COLOR: TO MATCH GLULAM |
| 11 | EXISTING STOREFRONT GLAZING: TO REMAIN AS IS |
| 12 | PROJECT SIGNAGE: TBD |
| 13 | EXISTING METAL GATE TO REMAIN |
| 14 | EXISTING STUCCO TO REMAIN AND NEW ELASTOMERIC PAINT COLOR: TO MATCH DRYVIT DRYTWOOD #133 |
| 15 | EXISTING STUCCO TO REMAIN AND NEW ELASTOMERIC PAINT COLOR: TO MATCH DRYVIT WINTER EVE #618 |
| 16 | EXISTING RED METAL DOOR & FRAME: TO REMAIN AS IS |
| 17 | NATURAL CONCRETE BASE |
| 18 | BLACK CANVAS AWNING FABRIC ON NEW BLACK STEEL FRAME W/ TENANT SIGNAGE SEE DETAIL 40P01 COLOR: FURN SOLID CANVAS S48 BLACK |

| LIGHTING LEGEND | |
|-----------------|---|
| 11 | WALL MOUNTED LIGHT FIXTURE MANUFACTURER - BEGA PART - 50.222 FINISH - POWDER COATED BLK (BLK) SUPPLIER - WOW LIGHTING, MARTA BARAHONA DE HERNANDEZ |
| 12 | WALL MOUNTED LIGHT FIXTURE MANUFACTURER - BISTEMALUX PART - COOL SQUARE LED FINISH - POWDER COATED BLK (BLK) SUPPLIER - COM2 LIGHTWORKS, MEGHAN BOYD |
| 13 | WALL MOUNTED FLOOD LIGHT PROJECTOR MANUFACTURER - BISTEMALUX PART - MOUNT ARM RECTANGULAR FINISH - POWDER COATED BLK (BLK) SUPPLIER - COM2 LIGHTWORKS, MEGHAN BOYD |
| 14 | IN-GROUND LIGHT FIXTURE MANUFACTURER - BISTEMALUX PART - 2P ROUND WALK-OVER FINISH - STAINLESS STEEL SUPPLIER - COM2 LIGHTWORKS, MEGHAN BOYD |
| 15 | FLEXIBLE LED TUBE LIGHTING FIXTURE MANUFACTURER - ACCULUM LIGHTING PART - FLEXITUBE SE RGB FINISH - FLEXIBLE SILICONE JACKET (WHITE) SUPPLIER - WOW LIGHTING, MARTA BARAHONA DE HERNANDEZ |



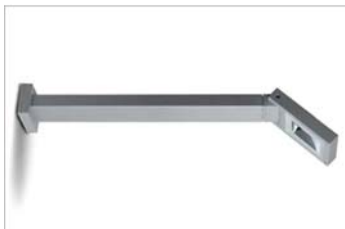
1
DP303
PROPOSED ELEVATIONS
SCALE: N.T.S.



11



12



13



14



15

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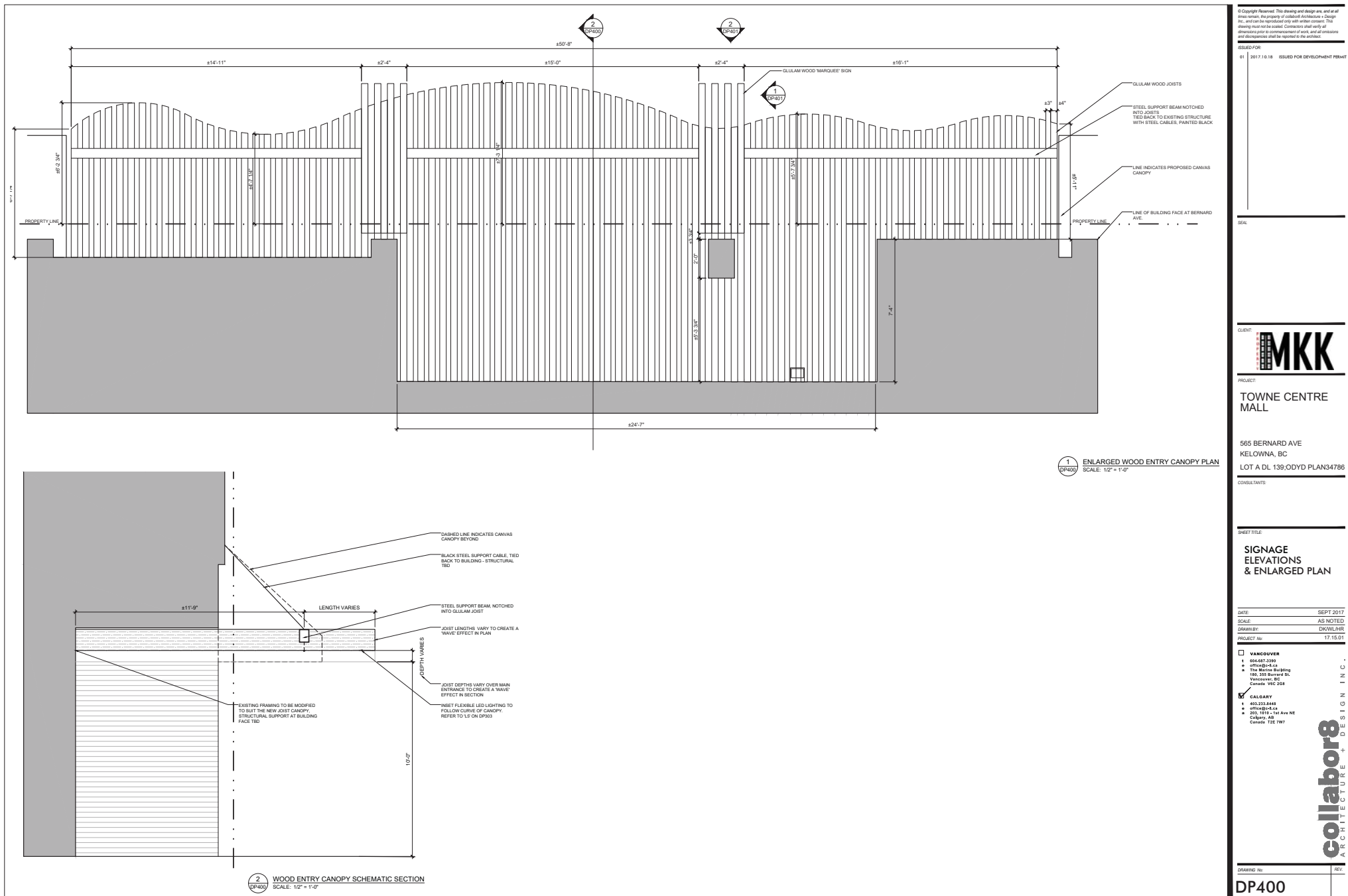
CLIENT:
MKK
PROJECT:
TOWNE CENTRE MALL
565 BERNARD AVE
KELOWNA, BC
LOT A DL 139/00DY PLAN34786
CONSULTANTS:

SHEET TITLE:
PROPOSED EXTERIOR FINISHES

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKW/JHR
PROJECT No: 17.15.01
VANCOUVER
1 604.687.3380
• 4010-101-101
• The Marine Building
180, 101 Marine Bldg
Vancouver, BC
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CALGARY
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• 4010-101-101
• 201, 101-1st Ave NE
Calgary, AB
Canada T2E 7W7

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DRAWING No: REV:
DP303



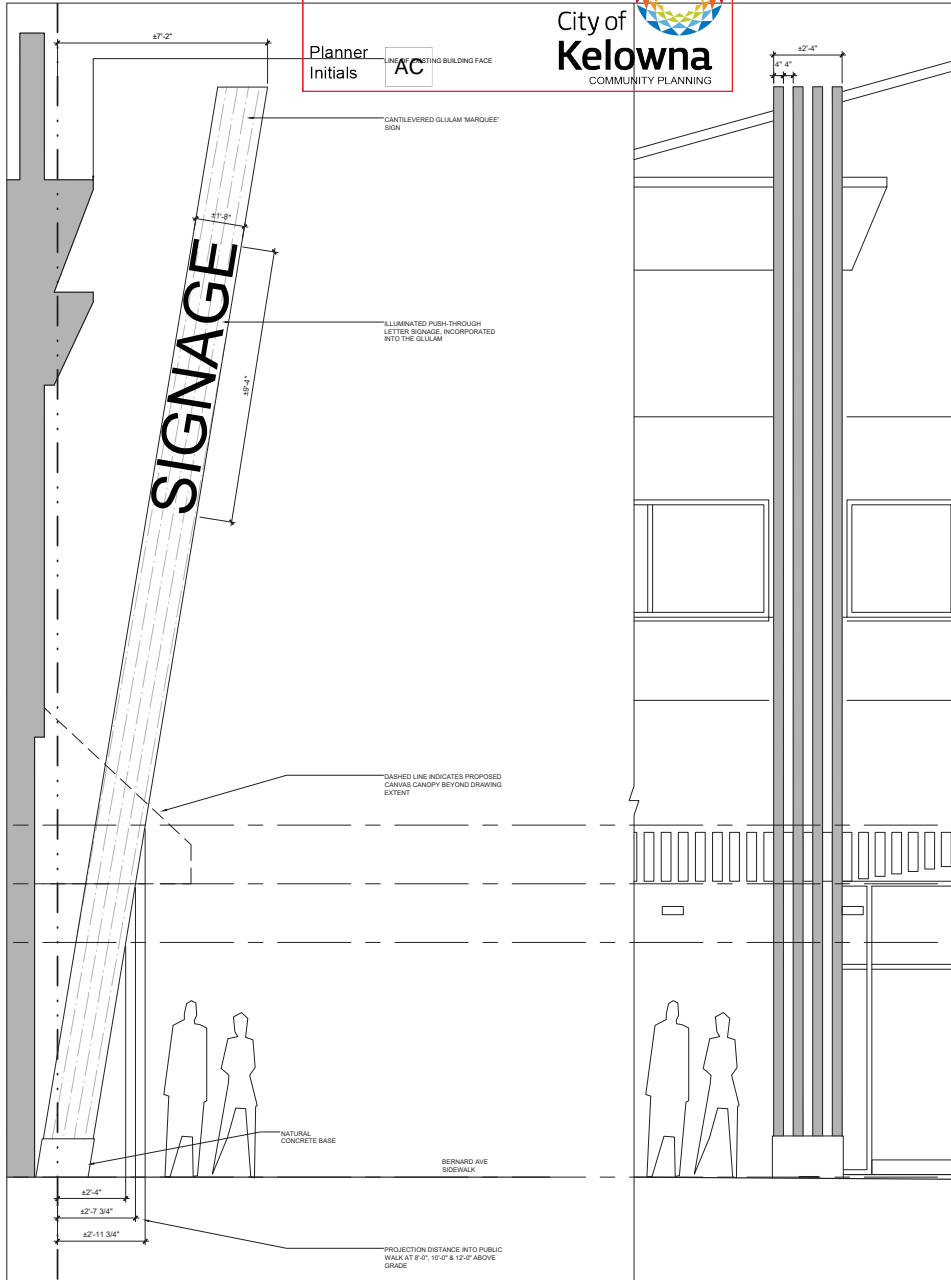
SCHEDULE A & B

This forms part of application
DP17-0242



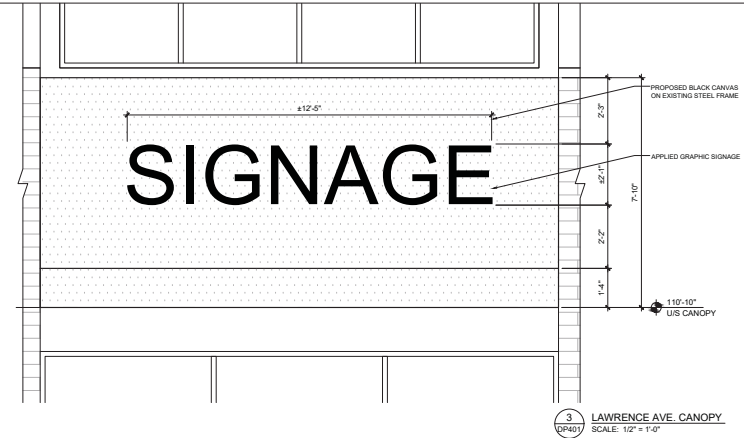
City of
Kelowna
COMMUNITY PLANNING

Planner
Initials **AC**

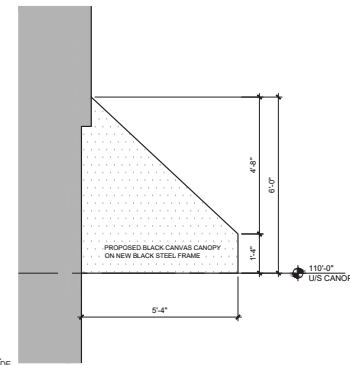


1 ENLARGED MARQUEE SIGN
SCALE: 1/2" = 1'-0"

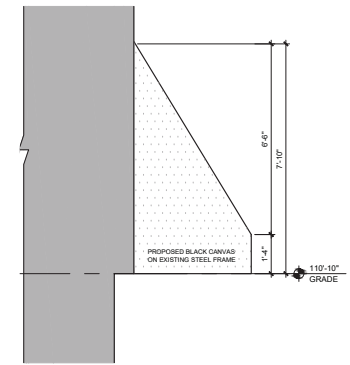
2 ENLARGED MARQUEE SIGN
SCALE: 1/2" = 1'-0"



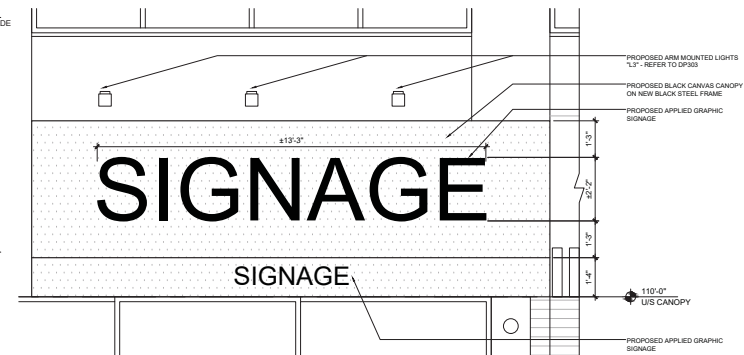
3 LAWRENCE AVE. CANOPY
SCALE: 1/2" = 1'-0"



4 BERNARD AVE. CANOPY
SCALE: 1/2" = 1'-0"



5 LAWRENCE AVE. CANOPY
SCALE: 1/2" = 1'-0"



6 BERNARD AVE. CANOPY
SCALE: 1/2" = 1'-0"

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SEAL

CLIENT:
MKK

PROJECT:
TOWNE CENTRE MALL

565 BERNARD AVE
KELOWNA, BC
LOT A DL 139/00DY PLAN34786

CONSULTANTS:

SHEET TITLE:
SIGNAGE ELEVATIONS & ENLARGED PLAN

DATE: SEPT 2017
SCALE: AS NOTED
DRAWN BY: DKW/JHR
PROJECT NO: 17.15.01

VANCOUVER
1 604.681.3380
• 4110-10-14
• The Marine Building
180, 100 West End Blvd
Vancouver, BC
Canada V6C 2B8
CALGARY
1 403.233.8448
• 2010-10-14
• 201, 1015 - 1st Ave NE
Calgary, AB
Canada T2E 7W7

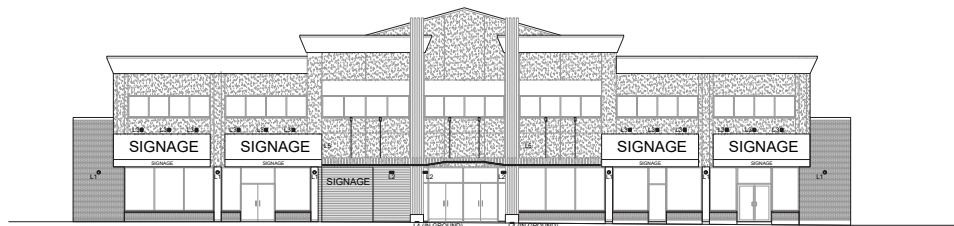
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ARCHITECTURE + DESIGN INC.

DRAWING NO: REV.
DP401

SCHEDULE A & B

This forms part of application
DP17-0242

Planner
Initials AC



NORTH ELEVATION VIEW
SCALE: 3/32"=1'-0"

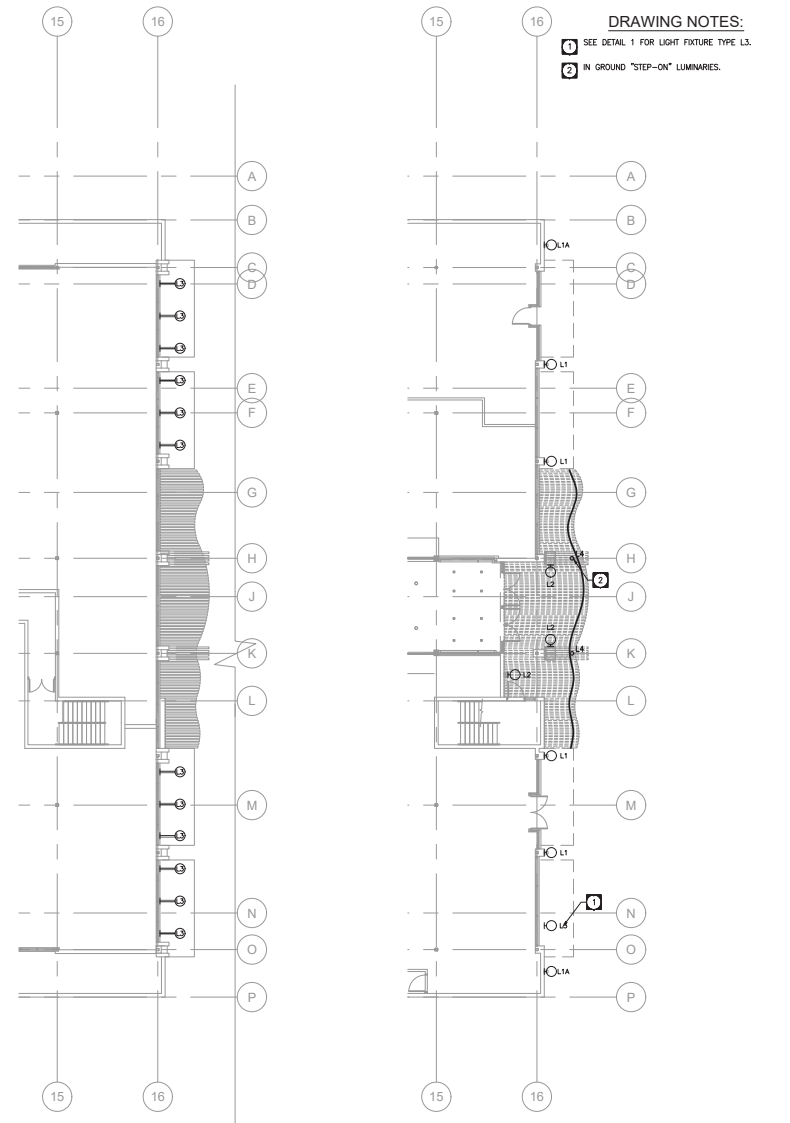
LUMINAIRE SCHEDULE

| TYPE | MANUFACTURER | CATALOGUE NO. | LAMPS NO. TYPE | VOLTS | MOUNTING | REMARKS | NOTES/ WATTS |
|------|------------------|---|-------------------|-------|----------|--|-----------------|
| L1 | SISTEMALUX | LIFT 2 WINDOW: S.5026W-UNV-14 | — LED 120 W | — | W | 3000K, UP/DOWN WIDE FLOOD, 2X60", ALUMINUM GRAY | 18.2 |
| | REBELLE | 3103-2x6L-30-120-WFL-WFL-GM | — LED 120 W | — | W | 3000K, UP/DOWN WIDE FLOOD, 2X48", GUNMETAL | 12 |
| | ARTEMIDE | T42012LW2K C/W CD-12W-700MA-BMU | — LED 120 W | — | W | 3000K, UP/DOWN WIDE FLOOD, 2X30", ANTHRACITE GRAY | 7 |
| L1A | SISTEMALUX | LIFT 2 WINDOW: S.5040W-UNV-14 | — LED 120 W | — | W | 3000K, UP/DOWN NARROW FLOOD, 2X2", ALUMINUM GRAY | 18.2 |
| | REBELLE | 3103-2x6L-30-120-SP-SP-GM | — LED 120 W | — | W | 3000K, UP/DOWN NARROW SPOT, 2X20", GUNMETAL | 12 |
| | ARTEMIDE | T42012LW2K C/W CD-12W-700MA-BMU | — LED 120 W | — | W | 3000K, UP/DOWN WIDE FLOOD, 2X30", ANTHRACITE GRAY | 7 |
| | SISTEMALUX | COOL SQUARE: S.7280W-12-UNV-14 | — LED 120 W | — | W | 3000K, UP/DOWN ASYMMETRIC AMBIENT LIGHT, ALUMINUM GRAY | 28.4 |
| L2 | OSBIT | 132AK-LED-120-120-30-SI | — LED 120 W | — | W | 3000K, UP/DOWN ASYMMETRIC AMBIENT LIGHT, SILVER | 12 |
| | BEGA | 33394 | — LED 120 W | — | W | 3000K, UP/DOWN ASYMMETRIC AMBIENT LIGHT | 36 |
| L3 | SISTEMALUX | MOVIT ARM: S.3010W-JM-BEM-14 C/W 4448-0024-048-UNV-ND | — LED 120 W | — | W | 3000K, CANTILEVERED, 4 REMOTE DRIVERS, ALUMINUM GRAY | 12.5 |
| | ECOSENSE | FORD-15-HD-30-K-80-S C/W FORD-WMA-24-C | — LED 120 W | — | W | 3000K, CANTILEVERED, 24", ALUMINUM GRAY | — |
| L4 | SISTEMALUX | ZIP ROUND: S.8864W-UNV-19 | — LED 120 R | — | R | WIDE BEAM RECESSED UPLIGHT, STAINLESS STEEL | 1.8 |
| | SOLERA | ING625-2RW-LED-120/277V-WW-E-WB-IR-TP-SS | — LED 120 R | — | R | WIDE BEAM RECESSED UPLIGHT, STAINLESS STEEL | 28 |
| | ACCLAIM LIGHTING | FTW-125-AAH-C/W ALD-800-24 & FTSELECTM (DTY120) | — LED 120 R | — | R | FLEXIBLE, COLOUR CHANGING TUBE, SIDE EMITTING | 3.3/FT |
| L5 | BL LIGHTING | M1-BB-SM-BC | — LED 120 R | — | R | FLEXIBLE, COLOUR CHANGING TUBE, BACK LEAD | 3.7/FT |
| | | | — | — | — | | — |

NOTES: 1. ALL FLUORESCENT LAMPS ARE TO BE 3500°K AND A MINIMUM #2 CRI UNLESS NOTED OTHERWISE.
2. FINISH AS SELECTED BY INTERIOR DESIGNER.

S = SURFACE W = WALL TB = T-BAR
R = RECESSED V = VALANCE DW = DRYWALL
C = CEILING P = PENDANT CH = CHAIN

LUMINAIRE SCHEDULE
SCALE: N.T.S.



PROPOSED EXTERIOR LIGHTING PLAN-2ND
SCALE: 3/32"=1'-0"

PROPOSED EXTERIOR LIGHTING PLAN-MAIN
SCALE: 3/32"=1'-0"

DRAWING NOTES:

- SEE DETAIL 1 FOR LIGHT FIXTURE TYPE L3.
- IN GROUND "STEP-ON" LUMINAIRES.

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SEAL

CLIENT

PROJECT

TOWN CENTRE MALL

565 BERNARD AVE
KELOWNA, BC

CONTRACT NO. 17000

TOWER ENGINEERING GROUP
300-1000 W. BURNETT ST. #100
VANCOUVER, BC V6C 2B8
TEL: 604.281.3300 FAX: 604.281.3302
www.towerengineering.ca

SHEET TITLE

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

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DATE: SEPT 2017

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SCALE: AS NOTED

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SCALE: AS NOTED

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SCALE: AS NOTED

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PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

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PROJECT NO. 17.10.01

DATE: SEPT 2017

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PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

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SCALE: AS NOTED

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DATE: SEPT 2017

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PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

DATE: SEPT 2017

SCALE: AS NOTED

DRAWN BY: YL

PROJECT NO. 17.10.01

REPORT TO COUNCIL



Date: January 22, 2018

RIM No. 1210-24

To: City Manager

From: Community Planning Department (KB)

Application: FH17-0001 **Owner:** Stewart Brothers Winery Ltd.,
Inc. No. BC1040444

Address: 4500 Stewart Road West **Applicant:** Protech Consulting 2012 (Grant
Maddock)

Subject: Farm Help Development Permit Application

Existing OCP Designation: REP – Resource Protection Area

Existing Zone: A1 – Agriculture 1

1.0 Recommendation

THAT Council authorizes the issuance of Farm Help Development Permit No. FH17-0001 for Lot 2 Section 32 Township 29 ODYD Plan 3093 Except Plan KAP58804, located at 4500 Stewart Road West, Kelowna, BC subject to the following:

1. The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A,"
2. Landscaping to be provided on the land be in accordance with Schedule "B";
3. The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect;
4. Registration of a Section 219 Restrictive Covenant restricting the use of the Temporary Farm Worker Housing to a maximum of twenty-two (22) farm employees only, and to a maximum occupation of ten (10) months of the year;
5. Registration of a Section 219 Restrictive Covenant restricting the use of the Temporary Farm Worker Housing to Temporary Farm Workers only;
6. Registration of a Section 219 Restrictive Covenant on all parcels within the farm unit in the City sector restricting the total number of temporary farm worker housing to sixty (60);

AND THAT Council's consideration of this Farm Help Development Permit be considered subsequent to the outstanding conditions of approval as set out in Attachment "A" attached to the Report from the Community Planning Department dated January 22, 2018;

AND THAT the applicant be required to complete the above noted conditions of Council's approval of the Development Permit Application in order for the permits to be issued;

AND FURTHER THAT this Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

2.0 Purpose

To consider a request for a Farm Help Development Permit for two proposed temporary residences on non-permanent foundations to accommodate 22 temporary agricultural workers on the subject property.

3.0 Community Planning

Community Planning Staff support the application to construct two temporary dwellings to house 22 temporary agricultural workers at 4500 Stewart Road West. The application complies with City of Kelowna and Ministry of Agriculture policy for Temporary Farm Worker Housing (TFWH) and is also consistent with the Official Community Plan Policy 5.34.2:

Temporary farm worker housing (e.g. bunkhouse accommodation on non-permanent foundations) is the preferred solution where the need for farm worker housing is justified.

The applicants have provided Community Planning Staff with copies of federal farm worker contracts and a vineyard planting plan demonstrating the need for the farm worker housing is justified. In addition, the proposal locates the TFWH on unfarmed land that has not been planted and is within a contiguous homeplate on the property, which is consistent with the City of Kelowna Farm Protection Development Permit Guidelines. The application meets other regulations, policies and guidelines for TFWH, and the proposed siting and landscaping is designed to limit the impact on agricultural land.

The Agricultural Land Commission, as well as the Ministry of Agriculture's Guide to Bylaw Development in Farming Areas, advises that temporary farm workers should be housed in temporary structures. This application complies with these standards. Further, Temporary Farm Worker Housing needs to meet the Guidelines for the Provisions of Seasonal Housing for Migrant Farm Workers in British Columbia, which details the occupancy calculations and minimum amount washroom, laundry and cooking facilities that should be provided. The applicant has submitted trailer layouts to demonstrate that these minimum standards can be met (see Schedule "A").

In keeping with the Ministry of Agriculture's Guide to Bylaw Development in Farming Areas and the City's policy for Temporary Farm Worker Housing, Community Planning Staff recommend the registration of Section 219 Restrictive Covenants which state:

- the TFWH shall be used for temporary farm workers only;
- the owner will remove the TFWH if the farm operation changes such that it is no longer required;
- the TFWH will only be used for farm workers for a specified limited number of months during the year (typically a maximum of eight months); and
- a total maximum of 60 TFWH accommodation is permitted on this farm unit within this City sector.

If supported by Council, a building permit will be required for the structures and related infrastructure elements covered by this application.

4.0 Proposal

4.1 Background

Temporary Farm Worker Housing Regulations

In 2017, Council adopted new regulations for TFWH following consultation with the Ministry of Agriculture, the farming industry, and the public. The regulations were intended to address concerns related to:

- Potential misuse of farm worker housing;
- Loss of agricultural land;
- Changes to the agricultural landscape;
- Increased demands on municipal infrastructure; and
- A perceived 'detachment' of the workers having no connection to the community.

The new regulations allow farmers to have accommodations for up to 40 temporary farm workers on parcels less than 8 ha and up to 60 temporary farm workers for parcels 8 ha or greater on farm units within each City sector. This enables farmers to have multiple TFWH locations within the City.

Seasonal Agricultural Workers Program

The Federal Government has recognized that there is a shortage of farm workers throughout Canada. As part of the Seasonal Agricultural Workers Program (SAWP) the owner must provide accommodation for their foreign workers. Seasonal dwellings for farm help are considered a 'farm use'¹ by the Agricultural Land Commission. According to the Ministry of Agriculture, seasonal worker housing should be communal and temporary in nature, with shared kitchens, and conform to SAWP / BC Agriculture Council standards.

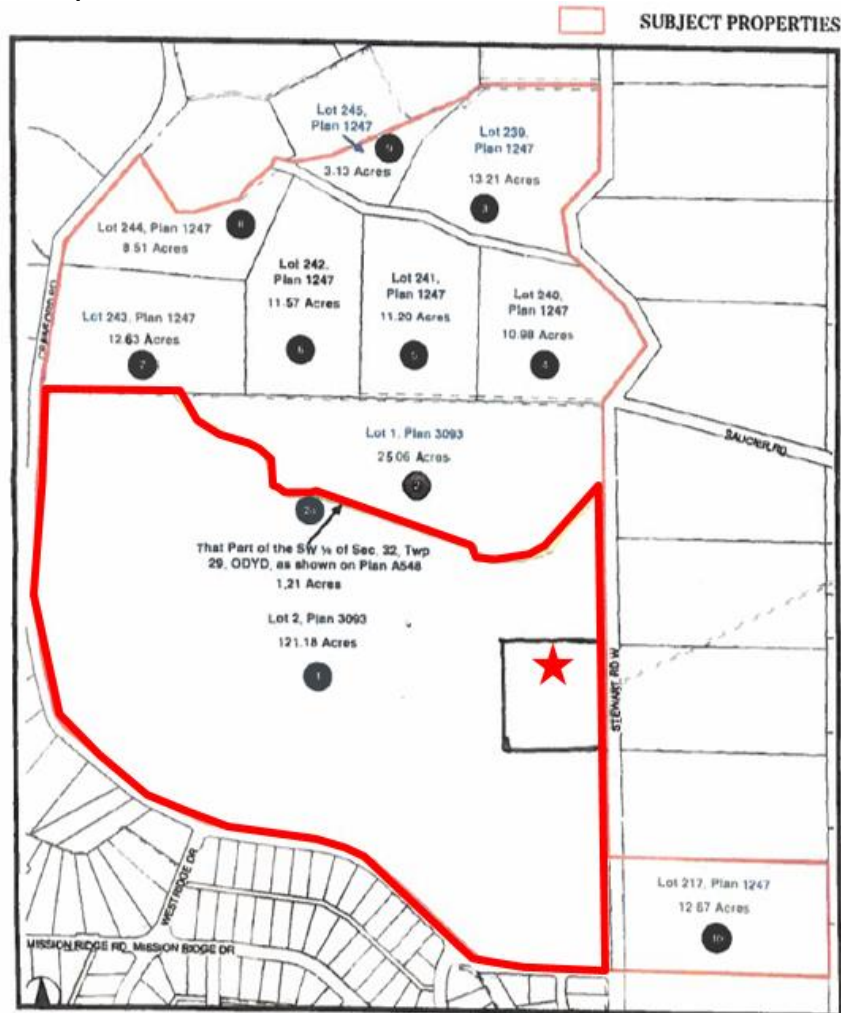
Subject Proposal

The owners, Stewart Brothers Winery Ltd. farm 4500 Steward Rd West, and nine immediately surrounding properties (the "Farm Unit"), in Southeast Kelowna. Combined, there are a total of 218.9 acres owned or controlled by Stewart Brothers Winery Ltd. in this immediate area, as shown on Map 1. On these properties, 25 acres are currently planted with mature vineyards and in 2017 40 additional acres were planted. In the next three years an additional 30 acres are proposed to be planted per year, and the remaining land area is planned on being planted in 2022. The 22 workers proposed to be housed on the subject property would mainly work at the properties on-site, but there may be some overlap with additional vineyards in West Kelowna. The owners have made significant investment over the past 18 months to improve the farming capability of the property, which was formerly used as the Stewart Brothers Nursery Site.

The owners are currently proceeding with master site development plans for a future winery processing facility, on-site retail and food services for the subject property. The owners will continue to work with Planning Department Staff and will submit for a Farm Protection Development Permit once plans are completed.

¹ Province of BC, 2002, Agricultural Land Commission Act – Agricultural Land Reserve Use, Subdivision and Procedure Regulation – Part 2.

Map 1 – Properties owned or controlled by Stewart Brothers Winery Ltd. surrounding 4500 Stewart Road West that make up the Farm Unit



4.2 Project Description

The application is for 22 Temporary Farm Workers to be housed in temporary structures on non-permanent foundations. The accommodation is proposed to be placed on the east portion of the property, which access off Stewart Road West. It is in close proximity to an existing office and warehouse building (see Map 2 for the proposed location). Each trailer includes six bedrooms (to accommodate 11 workers in each trailer), a shared kitchen, sitting area, washroom and laundry facilities. It is anticipated that the TFWH would be occupied between March and October annually.

The application includes a landscape plan, which provides a buffer between the TFWH and the vineyard. This plan includes a three metre wide buffer with a variety of native tree and shrub species. The landscape buffer would surround the trailers on the north, east and west sides, as shown on Map 2.

Map 2 – Proposed Temporary Farm Worker Housing Layout



4.3 Site and Neighbourhood Context

The subject property is within the Southeast Kelowna Sector of the City according to the Official Community Plan. The property is zoned A1 – Agriculture 1 and the Future Land Use is REP – Resource Protection. It is outside of the City's Permanent Growth Boundary and is fully located within the Agricultural Land Reserve (ALR). The subject property, where the TFWH would be located, is approximately 121 acres. The surrounding parcels that make up this farm unit, include approximately an additional 100 acres (218.9 acres total).

Parcel Summary – 4500 Stewart Road West:

Parcel Size: 49.04 ha (121.18 acres)

Zoning and land uses adjacent to the property are as follows:

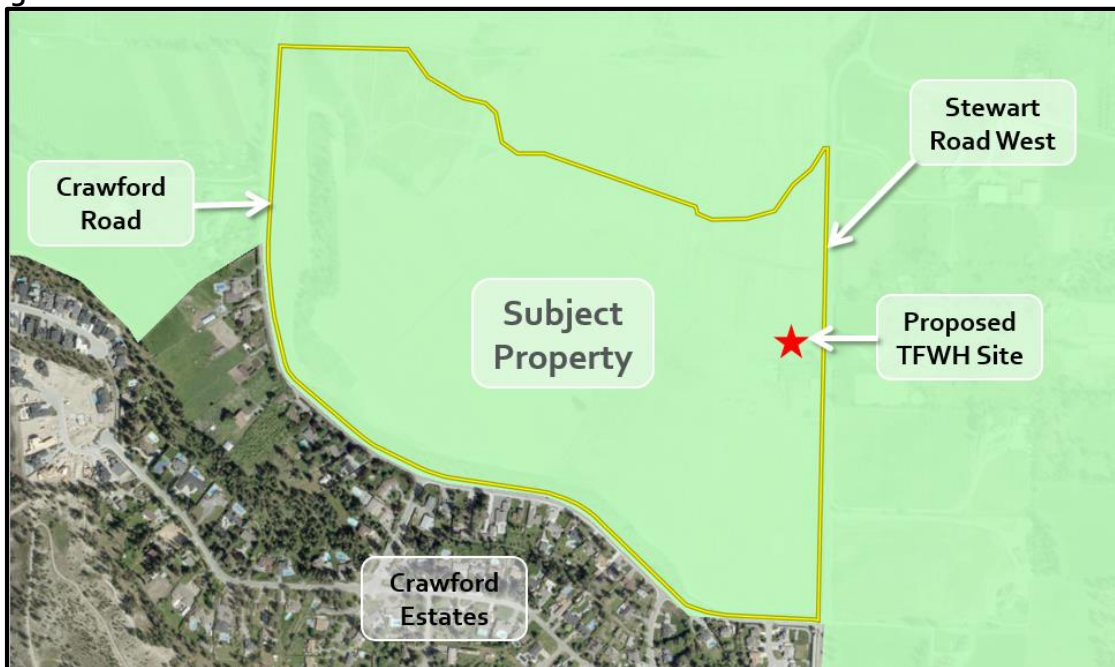
Table 1: Zoning and Land Use of Adjacent Property

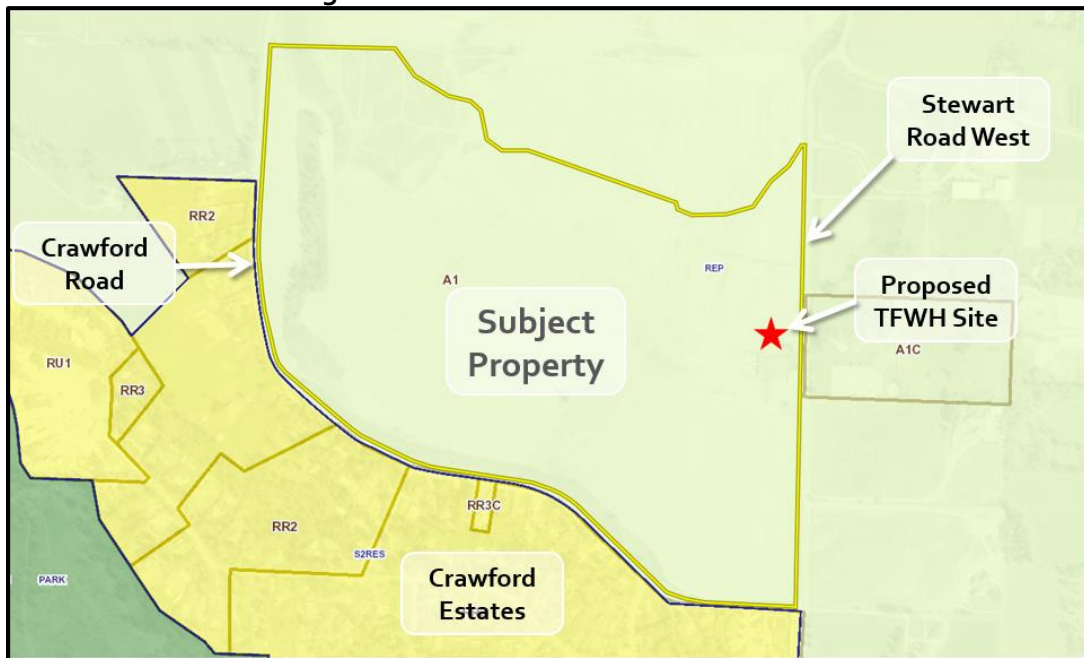
| Direction | Zoning | ALR | Land Use |
|-----------|--|---------|---------------------------|
| North | A1 – Agriculture 1 | Yes | Agriculture |
| South | RR2 – Rural Residential 2 RR3 - Rural Residential 3 RR3C - Rural Residential 3 with Carriage House | No | Residential |
| East | A1 – Agriculture 1 A1C - Agriculture 1 with Carriage House | Yes | Agriculture / Residential |
| West | A1 – Agriculture 1 RR2 - Rural Residential 2 | No/ Yes | Agriculture |

Map 3 - Subject Property Map



Map 4 - Agricultural Land Reserve



Map 5 – Future Land Use and Zoning

4.4 Neighbourhood Notification

As staff understand it, neighbourhood notification was completed by the applicant in accordance with Council Policy #367. On lands located outside of the Permanent Growth Boundary neighbourhood notification within a 300 metre radius was required. For lands within the permanent growth boundary, primarily those within the Crawford Estates neighbourhood, notification within a 50 metre radius was required.

5.0 Current Development Policies

5.1 Kelowna Official Community Plan (OCP)

Chapter 5 – Development Process - Agricultural Land Use Policies:

Objective 5.33 Protect and enhance local agriculture

Policy 5.33.1 Protect Agricultural Land - Retain the agricultural land base by supporting the ALR and by protecting agricultural lands from development, except as otherwise noted in the City of Kelowna Agricultural Plan. Ensure that the primary use of agricultural land is agriculture, regardless of parcel size.

Objective 5.34 Preserve productive agricultural land

Policy 5.34.2 Farm Help Housing - Accommodation for farm help on the same agricultural parcel will be considered only where:

- agriculture is the principal use on the parcel, and
- the applicant demonstrates that the additional housing is necessary to accommodate farm employee(s) whose residence on the farm property is considered critical to the overall operation of the farm. The primary consideration is whether the scale of the farm operation is large enough that permanent help is deemed necessary.

Temporary farm worker housing (e.g. bunkhouse accommodation on non-permanent foundations) is the preferred solution where the need for farm worker housing is justified.

Policy 5.34.3 Homeplating – Locate buildings and structures, including farm help housing and farm retail sales area and structures, on agricultural parcels in close proximity to one another and where appropriate, near the existing road frontage. The goal should be to maximize use of existing infrastructure and reduce impacts on productive agricultural lands.

Chapter 15 - Farm Protection Development Permit Guidelines:

Guideline 1.2 Homeplate - On agricultural lands, where appropriate, locate all buildings and structures, including farm help housing and farm retail sales, within a contiguous area (i.e. homeplate). Exceptions may be permitted where the buildings or structures are for farm use only.

Guideline 1.8 Temporary Farm Worker Housing – Design temporary farm worker housing such that:

- Temporary farm worker housing should use all existing dwellings within the farm unit, prior to building new temporary farm worker housing, unless the existing dwellings are used for a use consistent with the Agriculture Land Commission Act. Alternatively, the existing dwellings on the farm unit must be removed, decommissioned to an approved use or demolished including decommissioning the existing septic system, prior to the authorization of a new temporary farm worker housing structure.
- Temporary farm worker housing footprint should be contiguous with the residential footprint (i.e. homeplate) and / or within 50 metres of the road and / or located to maximize agricultural potential and limit negative impacts on the farm parcel.
- Temporary farm worker housing should have a minimum three metre wide vegetated buffer for screening to adjacent property lines and between the temporary farm worker housing and active farming areas.

5.2 Ministry of Agriculture

Purpose and goals for TFWH and associated bylaw standards:

- Meet the needs of the agriculture industry;
- Minimize the residential impact of TFWH in the agricultural area;
- Minimize loss and / or fragmentation of agricultural land due to TFWH; and
- Minimize the risk of TFWH being used for non-farm purposes.

6.0 Technical Comments

6.1 Building & Permitting Department

- Full Plan check for Building Code related issues will be done at time of Building Permit applications.

6.2 Development Engineering Department

- See Attachment “A” City of Kelowna Memorandum

6.3 Fire Department

- A fire alarm shall be installed, as per BCBC 2012 Division B section 3.2.4.1(j) in a residential occupancy with sleeping accommodation for more than 10 persons. Requirements of section 9.10.19 Smoke Alarms of the BCBC 2012 are to be met where less than 10 will be accommodated.
- CO Alarms shall be installed should there be gas fuelled appliances installed.
- Construction fire safety plan is required to be submitted and reviewed prior to construction and updated as required.
- Engineered Fire Flow calculations are required to determine Fire Hydrant requirements as per the City of Kelowna Subdivision Bylaw #7900. Should a hydrant be required on this property, it shall be private.
- A visible address must be posted as per City of Kelowna Bylaws.
- Should a fire alarm be required, A fire safety plan as per section 2.8 BCFC is required at occupancy. The fire safety plan and floor plans are to be submitted for approval in AutoCAD Drawing format on a CD or DVD to facilitate Fire Department pre-planning for this structure. The fire safety plan should clearly detail the unique requirements for this structure .
- Fire Department access is to be met as per BCBC.
- All requirements of the City of Kelowna Fire and Life Safety Bylaw 10760 shall be met.
- Contact Fire Prevention Branch for fire extinguisher requirements and placement.

6.4 Interior Health Authority

It is my understanding the housing will be serviced by community water and an onsite sewerage system. Interior Health has no objections to this proposal; however note the following:

It appears the proposed housing footprint is small and contained. This is important for the long-term ability of the farmland to produce food. Interior Health has an interest in the protection of farmland to support local agricultural capacity because it is a key aspect of local healthy food systems.

In addition, from an environmental public health perspective the following legislation applies:

- Industrial Camps Regulation - Prescribes the minimum housing standards which must be met.
- Sewerage System Regulation - Prescribes a Record of Sewerage System must be filed by an Authorized Person.

I suggest workers not be allowed to occupy the proposed housing until the City of Kelowna has received confirmation the requirements of the above legislation have been satisfied.

6.5 Southeast Kelowna Irrigation District (SEKID)

SEKID has reviewed the above referenced application and the following requirements must be met to meet requirements for water servicing:

1. The application calls for the construction of two farm help accommodations each approximately 1,300 ft² in area. A Capital Expenditure charge of \$5,000.00 must be paid for each building.
2. It is proposed the buildings be serviced from the existing domestic service located adjacent to the building site on Stewart Road West, so no additional water services are required.
3. All water use in the buildings must be metered. As it appears water will be connected in-line with the building to the west serviced from a connection to the building to the east, one water meter in the east building should be adequate to meter all water use in both buildings. If this is not the case, a second water meter may be required for the second building. An application fee of \$450.00 must be paid. This fee covers the costs of one 5/8" water meter. It is the applicant's responsibility to have the water meter installed in the building. Please contact the district office to arrange to pick up the meter at the appropriate time.
4. A new account fee of \$20.00 must be paid.

All fees must be paid prior to SEKID issuing a water letter indicating the applicant has satisfied the requirements for water servicing.

7.0 Application Chronology

Date of Application Received: October 24, 2017
Date Public Notification Completed: January 12, 2018

Agricultural Advisory Committee January 11, 2018

The above noted application was reviewed by the Agricultural Advisory Committee at the meeting held on January 11, 2018 and the following recommendations were passed:

THAT the Agricultural Advisory Committee recommends that Council support Farm Help Development Permit Application No. FH17-0001 for Lot 2, Section 32, Township 29, ODYD, Plan 3093 except Plan KAP58804, located at 4500 Stewart Road West, for Temporary Farm Worker Housing for 22 Temporary Farm Workers.

Carried - Moved by Keith Duhaime/Seconded by Dominic Rampone

ANECOTAL COMMENTS:

The Agricultural Advisory Committee commended the applicant for working with City staff to ensure that the City's policies and the Agricultural Land Commission's policies were adhered to. The Committee noted that the application provided for good buffering, the use of non-permanent foundations and that the size of the farm supports the need for temporary farm workers.

Report prepared by: Kimberly Brunet, Planner

Reviewed by: Ryan Smith, Community Planning Department Manager

Approved for Inclusion:

Attachments:

Draft Farm Help Development Permit No. FH17-0001
Attachment "A" – City of Kelowna Memorandum
Schedule "A" – Site Plan, Site Servicing Plan and Trailer Layout
Schedule "B" – Landscape Plan
Schedule "C" – Site Photos

Farm Help Development Permit

FH17-0001



This permit relates to land in the City of Kelowna municipally known as

4500 Stewart Road West

and legally known as

Lot 2 Section 32 Township 29 Osoyoos Division Yale District Plan 3093 Except Plan KAP58804

and permits the land to be used for the following development:

To House 22 Temporary Farm Workers in Two Temporary Structures on Non-Permanent Foundations (Trailers)

The development has been approved subject to any attached terms and conditions, and to full compliance with the approved plans bearing the stamp of approval and the above described development permit number.

The present owner and any subsequent owner of the above described land must comply with any attached terms and conditions.

Date of Decision: January 22, 2018

Decision By: City Council

Issued Date: January 22, 2018

Development Permit Area: Farm Help Development Permit

This permit will not be valid if development has not commenced by January 22, 2020.

Existing Zone: A1 – Agriculture 1 Future Land Use Designation: REP – Resource Protection

This is NOT a Building Permit.

In addition to your Development Permit, a Building Permit may be required prior to any work commencing. For further information, contact the City of Kelowna, Development Services Branch.

NOTICE

This permit does not relieve the owner or the owner's authorized agent from full compliance with the requirements of any federal, provincial or other municipal legislation, or the terms and conditions of any easement, covenant, building scheme or agreement affecting the building or land.

Owner: Stewart Brothers Winery Ltd., Inc. No. BC1040444

Applicant: Grant Maddock (Protech Consulting 2012)

Ryan Smith, Community Planning Department Manager
Community Planning & Strategic Investments

Date

1. SCOPE OF APPROVAL

This Development Permit applies to and only to those lands within the Municipality as described above, and any and all buildings, structures and other development thereon.

This Development Permit is issued subject to compliance with all of the Bylaws of the Municipality applicable thereto, except as specifically varied or supplemented by this permit, noted in the Terms and Conditions below.

The issuance of a permit limits the permit holder to be in strict compliance with regulations of the Zoning Bylaw and all other Bylaws unless specific variances have been authorized by the Development Permit. No implied variances from bylaw provisions shall be granted by virtue of drawing notations that are inconsistent with bylaw provisions and that may not have been identified as required Variances by the applicant or Municipal staff.

2. CONDITIONS OF APPROVAL

- a) The dimensions and siting of the building to be constructed on the land be in accordance with Schedule "A,"
- b) Landscaping to be provided on the land be in accordance with Schedule "B";
- c) The applicant be required to post with the City a Landscape Performance Security deposit in the form of a "Letter of Credit" in the amount of 125% of the estimated value of the landscaping, as determined by a Registered Landscape Architect;
- d) Registration of a Section 219 Restrictive Covenant restricting the use of the Temporary Farm Worker Housing to a maximum of twenty-two (22) farm employees only, and to a maximum occupation of ten (10) months of the year;
- e) Registration of a Section 219 Restrictive Covenant restricting the use of the Temporary Farm Worker Housing to Temporary Farm Workers only; and
- f) Registration of a Section 219 Restrictive Covenant on all parcels within the farm unit in the City sector restricting the total number of temporary farm worker housing to sixty (60);

This Development Permit is valid for two (2) years from the date of Council approval, with no opportunity to extend.

3. PERFORMANCE SECURITY

As a condition of the issuance of this Permit, Council is holding the security set out below to ensure that development is carried out in accordance with the terms and conditions of this Permit. Should any interest be earned upon the security, it shall accrue to the Developer and be paid to the Developer or his or her designate if the security is returned. The condition of the posting of the security is that should the Developer fail to carry out the development hereby authorized, according to the terms and conditions of this Permit within the time provided, the Municipality may use enter into an agreement with the property owner of the day to have the work carried out, and any surplus shall be paid over to the property own of the day. Should the Developer carry out the development permitted by this Permit within the time set out above, the security shall be returned to the Developer or his or her designate. There is filed accordingly:

- a) An Irrevocable Letter of Credit in the amount of \$16,500.00 (Landscaping Bond)

Before any bond or security required under this Permit is reduced or released, the Developer will provide the City with a statutory declaration certifying that all labour, material, workers' compensation and other taxes and costs have been paid.

4. Indemnification

Upon commencement of the works authorized by this Permit the Developer covenants and agrees to save harmless and effectually indemnify the Municipality against:

- a) All actions and proceedings, costs, damages, expenses, claims, and demands whatsoever and by whomsoever brought, by reason of the Municipality said Permit.

All costs, expenses, claims that may be incurred by the Municipality where the construction, engineering or other types of works as called for by the Permit results in damages to any property owned in whole or in part by the Municipality or which the Municipality by duty or custom is obliged, directly or indirectly in any way or to any degree, to construct, repair, or maintain.

The PERMIT HOLDER is the CURRENT LAND OWNER.
Security shall ONLY be returned to the signatory of the
Landscape Agreement or their designates.

DRAFT

CITY OF KELOWNA

MEMORANDUM

Date: November 20, 2017
File No.: FH17-0001
To: Urban Planning Management (KB)
From: Development Engineering Manager
Subject: 4500 Stewart Road W

Development Engineering Services have the following requirements associated with this rezoning application to allow a suite in the existing horse barn.

1. Domestic Water and Fire Protection

The subject property is within the service area of the South East Kelowna Irrigation District (SEKID). The developer is required to make satisfactory arrangements with the SEKID for these items. On-site servicing including the utilisation of existing or proposed services will be reviewed by Building & Permitting.

2. Sanitary Sewer

This property is currently not within the municipal service area. Sanitary sewage is to be handled by an on-site sewage disposal system.

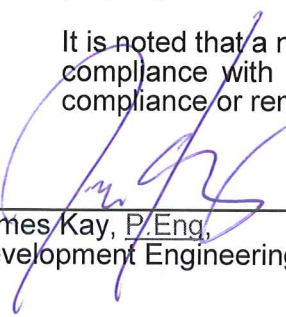
Servicing will be reviewed by Interior Health Authority as well as Building & Permitting.

3. Electric Power and Telecommunication Services

Make servicing applications to the respective Power and Telecommunication utility companies. The utility companies are required to obtain the City's approval before commencing construction.

4. The existing gravel driveway requires pavement from the road pavement edge to the property line. This is to prevent gravel from tracking onto the fronting road.

5. It is noted that a new culvert and driveway were installed without permissions, permits, or compliance with City Bylaws to Crawford Road. It will either need to be brought to compliance or removed.


James Kay, P.Eng.
Development Engineering Manager
JF



SCHEDULE

A

This forms part of application

FH17-0001

Planner Initials

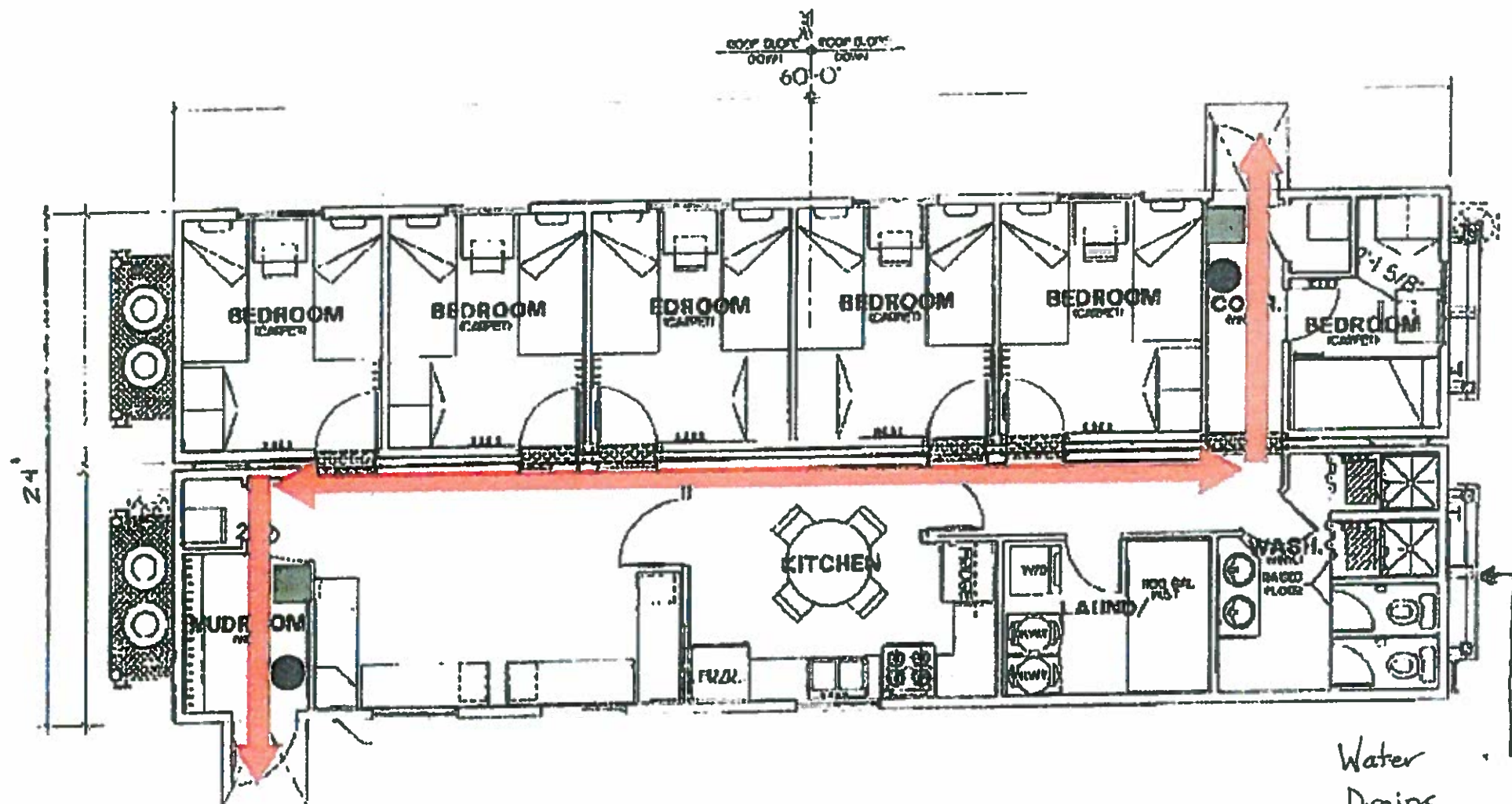
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City of Kelowna

COMMUNITY PLANNING

| | | | | | | | | | | | | | |
|---|--|--|--|---|--|--|--|---|--|---|---|--|---|
| <div>LEGEND</div> <div><div>WATER</div><div>SAN. SEWER</div><div>STORM SEWER</div><div>GAS</div><div>U/G UTILITY (ALIGNMENT)</div></div> <div><div><div><div></div></div><div></div></div><div><div><div></div></div><div></div></div><div><div><div></div></div><div></div></div><div><div><div></div></div><div></div></div><div><div><div></div></div><div></div></div></div> <div><div>EX. MANHOLE</div><div>PROP. MANHOLE</div><div>POWER POLE</div><div>LAMP STANDARD</div><div>CATCH BASIN</div><div>HYDRANT</div><div>SURVEY MONUMENT</div></div> <div><div><div></div></div><div><div></div></div><div><div></div></div><div><div></div></div><div><div></div></div><div><div></div></div><div><div></div></div></div> <div><div>WATER CURB STOP</div><div>SANITARY INSPECTION CHAMBER</div></div> | | | | <div>PROTECH CONSULTING 2012</div> <div>200 - 1461 St. Paul Street Kelowna B.C. Phone 860-1771 FAX 860-1994</div> | | | | <div>NO.</div> <div>DATE</div> <div>BY</div> <div>REVISION</div> <div>CH'KD</div> | | <div><div>DRAWN</div><div>DESIGN</div><div>APPROVED</div><div>DATE</div><div>SCALE</div><div>HORIZ.</div></div> <div><div>TWJM</div><div>TWJM</div><div>GM</div><div>06/27/17</div><div>1:250</div></div> | <div>THE CITY OF KELOWNA</div> <div>ENGINEERING DEPARTMENT</div> <div>QUAILS GATE WINERY</div> <div>SITE SERVICING PLAN</div> | | <div>CITY FILE</div> <div>DRAWING NO.</div> <div>15040-SRV</div> <div>REV. NO.</div> <div>0</div> |
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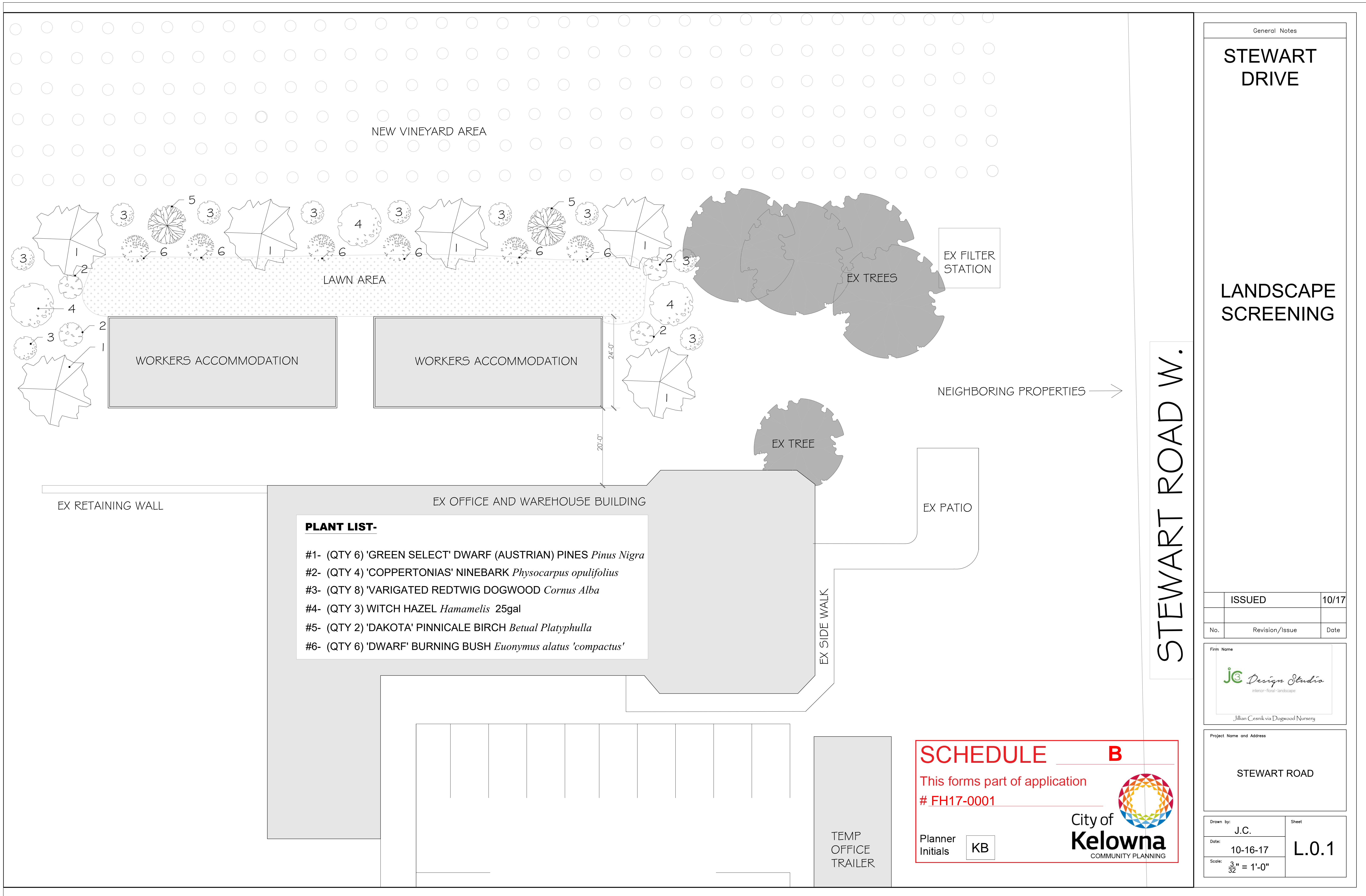
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FH17-0001

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Initials

KB

City of
Kelowna
COMMUNITY PLANNING



| | | |
|---|----------------|-------|
| General Notes | | |
| STEWART DRIVE | | |
| LANDSCAPE SCREENING | | |
| ISSUED 10/17 | | |
| No. | Revision/Issue | Date |
| Firm Name | | |
| jc Design Studio interior • floral • landscape Jillian Cesnik via Dogwood Nursery | | |
| Project Name and Address | | |
| STEWART ROAD | | |
| Drawn by: | J.C. | Sheet |
| Date: | 10-16-17 | L.0.1 |
| Scale: | 3/32" = 1'-0" | |



| | | |
|---|----|---|
| SCHEDULE | | C |
| This forms part of application # FH17-0001 | | |
| Planner Initials | KB |  City of Kelowna <small>COMMUNITY PLANNING</small> |



CITY OF KELOWNA
BYLAW NO. 11494
Z17-0058 – 934 Grenfell Road

A bylaw to amend the "City of Kelowna Zoning Bylaw No. 8000".

The Municipal Council of the City of Kelowna, in open meeting assembled, enacts as follows:

1. THAT City of Kelowna Zoning Bylaw No. 8000 be amended by changing the zoning classification of Lot 2, Section 2, District Lot 136, ODYD, Plan 39171 located on Grenfell Road from the RU1 – Large Lot Housing zone to the RU6 – Two Dwelling Housing zone.
2. This bylaw shall come into full force and effect and is binding on all persons as and from the date of adoption.

Read a first time by the Municipal Council this 16th day of October, 2017.

Considered at a Public Hearing on the 7th day of November, 2017.

Read a second and third time by the Municipal Council this 7th day of November, 2017.

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

Report to Council



Date: January 22, 2018
File: 0710-40
To: City Manager
From: Ross Soward, Planner Specialist
Subject: Rental Housing Grant Recommendation 2018

Recommendation:

THAT Council receives the report from the Planner Specialist, dated, January 22, 2018 regarding the rental housing grant recommendations for 2018.

AND THAT Council approves the 2018 Rental Housing Grants as identified in the report from the Planner Specialist, dated January 22, 2018, in accordance with the Housing Opportunities Reserve Fund Bylaw No. 8593.

Purpose:

To consider approval of four purpose-built rental housing projects for rental housing grants in 2018, subject to final budget approval.

Background:

Canada Mortgage and Housing Corporation (CMHC), identifies Kelowna as having one of the lowest primary rental housing market vacancy rates for CMAs in Canada, reporting a 0.2 per cent vacancy rate as of November 2017. The high rate of population growth in the CMA over the last several years combined with the limited amount of rental housing constructed between 2010-2015 has put substantial pressure on the City's rental housing supply. According to the CMHC housing portal data, approximately 300 purpose-built rental units received occupancy between fall 2016 and fall 2017. However, CMHC also estimates 1,373 rental housing starts in 2017 in Kelowna.¹ City of Kelowna staff estimate roughly 863 of the 1,070 units approved for rental housing grants in 2017 are under

¹ Canada Mortgage and Housing Corporation. 2017 Housing Marketing Information Portal. Kelowna Census Subdivision data for 2017. Retrieved from: <https://www03.cmhc-schl.gc.ca/hmportal/en/#Apartment>

construction. As these rental units are completed in 2018 and 2019 the region should see a moderate improvement in the vacancy rate for purpose-built rental housing.

The *City of Kelowna Housing Strategy* (2012) identified the goal of developing 300 purpose-built rental units each year as a base amount to keep pace with the City's future rental housing needs. However, from 2007-2016 roughly 1,684 purpose-built rental units were completed (an average of 168/year). The only year where more than 300 units received occupancy was 2016.² The lack of rental housing constructed over this period has resulted in a deficit of roughly 1,300 rental housing units based on the targets from the *2012 Kelowna Housing Strategy*. Over the last two years, just shy of 800 rental units were completed, but strong population growth over this time has resulted in a further decrease in the vacancy rate to 0.2 per cent.

Based on the recently completed Housing Needs Assessment, Staff have recommended an annual target of 400 purpose-built rentals to meet the growing demand. As a result, continued investments in rental housing will be required to ensure a healthy supply of rental housing exists. Purpose-built rental apartments are an important part of a healthy housing market, ensuring housing options exist for people in transitional periods and those who are not able to own their own home.

Given the significant need for rental housing, the overall amount of funding available for rental housing grants was increased from \$320,000 to \$420,000 for the 2017 and 2018 grant funding cycles. The increase was intended to support the housing sector in its efforts to address the low vacancy rates and take advantage of the strong population growth and low interest rates. As part of the Healthy Housing Strategy staff will be investigating recommendations for the City's rental housing incentive programs this spring.

In order for applicants to be eligible for the grants, a housing agreement must be signed to secure the rental dwellings for a minimum of ten years. After ten years, a request can be made to release the notice of the housing agreement. To discharge a housing agreement Council approval is required as well as a subsequent repayment of the grant funding to the City for the Housing Opportunities Reserve Fund (s. 4.4 and 4.5 of the Housing Opportunities Reserve Fund By-law 8593).

Requesting a release of the housing agreement is unlikely for provincially subsidized non-profit rental housing. Requests for stratification of purpose-built rental buildings could occur, but would only be considered if the vacancy rates for rental housing were above 3 per cent. Letters outlining this process are sent to grant recipients and the letters specify the repayment of the grant is required if the housing agreement is lifted.

² Canada Mortgage and Housing Corporation. 2017 Housing Marketing Information Portal. Kelowna - Completions by Intended Market Data from 2006-2017. Retrieved from: <https://www03.cmhc-schl.gc.ca/hmiportal/en/#Apartment>

The 2018 intake of applications demonstrates the continued response from the housing sector to address the rental housing demand in Kelowna. Notably, approximately 10 per cent of the units recommended for rental housing grants are proposed to be family-friendly three / four bedroom units. The Bylaw maximizes incentives for those units that are 3+bedroom types over those other units that offer 1-2 bedrooms.

Staff recommend the following projects for rental housing grants for 2018, as follows.

1. \$76,062 for 68 units at 555 Fuller Avenue
2. \$159,566 for 154 units at 1360 KLO Road
3. \$115,748 for 58 units at 573-603 Clement
4. \$68,622 for 22 units at 225 Rutland Road South

The rental housing grants have been established to help offset the cost of developing rental housing. The grants are applied against the DCC fees for each project. Based on the rental applications received and the preliminary DCC estimates, the grants are covering (on average) 10-20 per cent of eligible DCCs for the projects below. Subject to final Council approval, the grants and DCC estimates are summarized in the table outlined below.

| Project | Dwelling Units Supported | Recommended DCC Grant | Estimated DCC Charges | Type of Units |
|------------------------|--------------------------|-----------------------|-----------------------|---|
| 555 Fuller Avenue | 68 | \$76,062 | \$969,584 | Affordable rental |
| 1360 KLO Road | 154 | \$159,566 | \$2,250,000 | Seniors Purpose-built rental |
| 573-603 Clement Avenue | 58 | \$115,748 | \$739,304 | Affordable rental |
| 225 Rutland Road South | 22 | \$68,622 | \$307,538 | Student housing Purpose-built rental |
| Totals | 302 | \$420,000 | \$4,266,426 | |

Grant recipients will be notified by letter of the amount of the grant and advised that they need to include a copy of the letter with their building permit application in order to receive a DCC credit, on the DCC's payable at building permit stage, equal to the amount of the grant.

In addition to rental housing grants, application can also be made for a 10-year revitalization tax exemption on new purpose-built rental projects. Applications must be consistent with the OCP and the vacancy rate must be at 3 per cent or less to apply. Staff anticipate several of the grant applicants will also be applying for the rental housing tax exemptions.

Internal Circulation:

Divisional Director, Community Planning and Real Estate
 Department Manager, Policy and Planning
 Manager, Long Range Policy Planning
 Manager, Urban Planning

Department Manager, Community Planning
Divisional Director, Financial Services

Legal/Statutory Authority:

Local Government Act, Section 877. (1) (a) 933.1 (1)

Legal/Statutory Procedural Requirements:

Housing Opportunities Reserve Fund By-law No. 8593

Existing Policy:

2030 Official Community Plan

Objective 10.3 Support the creation of affordable and safe rental, non-market and /or special needs housing

Policies 10.3.1, 10.3.2, 10.3.3 & 10.3.4

Council Policy no. 355 – Rental Housing Grants

Financial/Budgetary Considerations:

Budgeted funds (subject to final approval) allow for a contribution to support 302 rental units at a rate of \$826 per 1-bedroom/studio unit, \$1,653 per 2-bedroom unit and \$3,307 per three/ four-bedroom unit.

Available funds are as follows:

| | |
|--|------------------|
| <u>Housing Opportunities Reserve Fund (HORF) – annual budget allocation:</u> | <u>\$300,000</u> |
| <u>Annual Budget Allocation to Rental Grants to provide partial relief from DCCs</u> | <u>\$120,000</u> |
| Total Funds Available | \$420,000 |

Submitted by:
Ross Soward, Planner Specialist

Approved for inclusion:



D. Noble-Brandt, Department Manager Policy & Planning



Rental Housing Grants

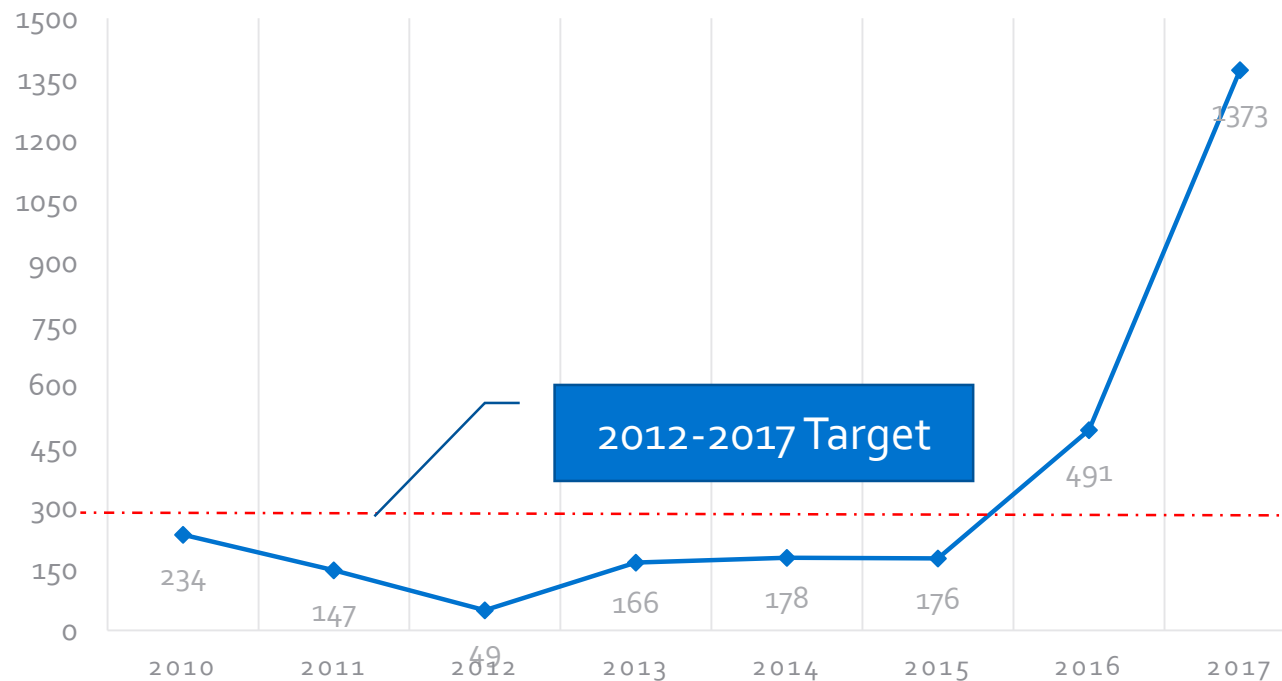
2018 Funding Recommendations
Policy and Planning Department



Rental Housing Trends

- ▶ Extremely low vacancy rates (0.2%)
- ▶ Deficit of rental housing units
- ▶ 300 Rental housing units completed from 2016 to 2017
- ▶ Significant number of rental housing starts in 2017 (1,373)

KELOWNA RENTAL HOUSING STARTS



Rental Housing Program

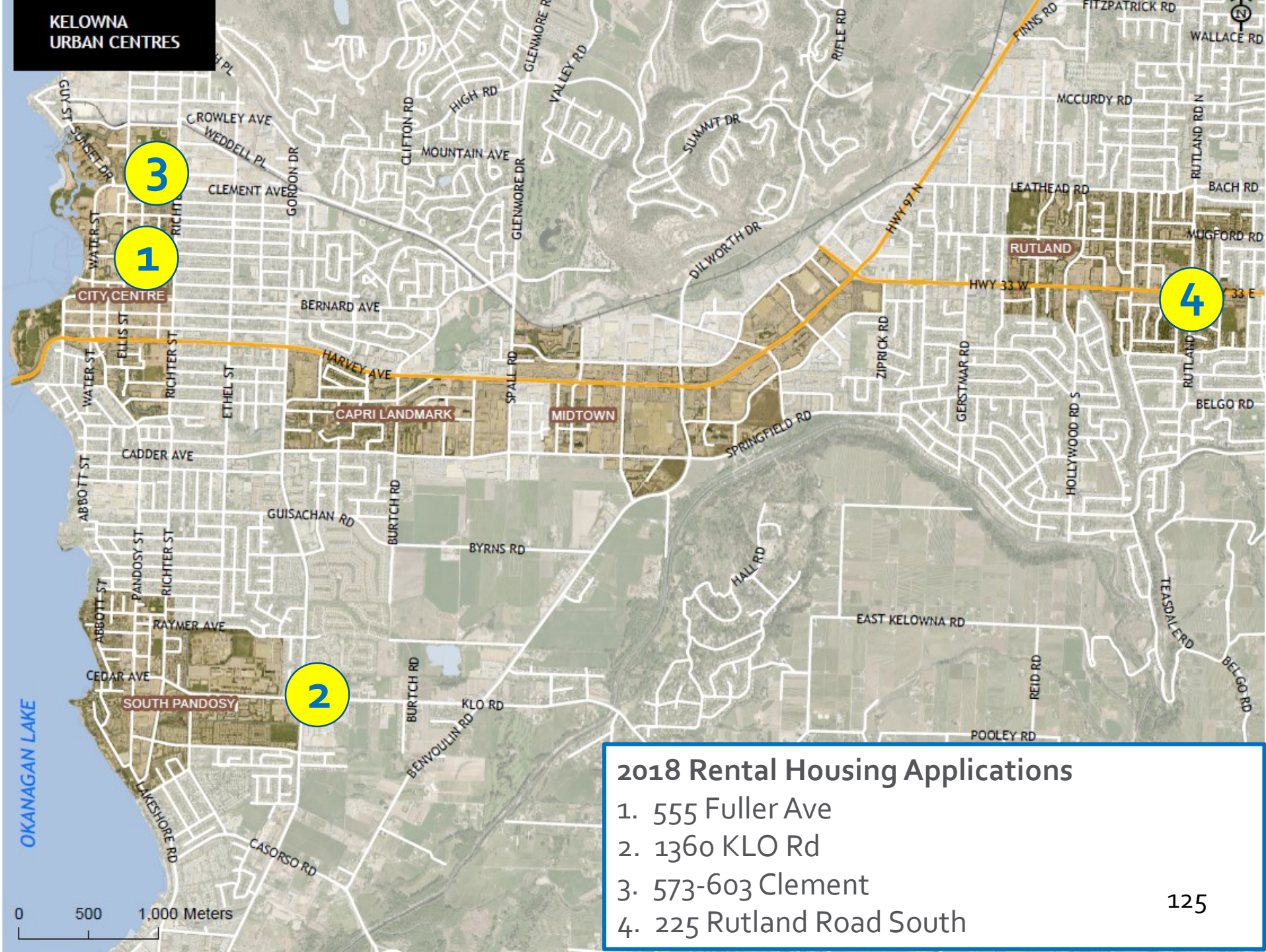
- ▶ Program update in 2016
- ▶ Increased funding for three bedroom units
- ▶ Increased funding pool for 2017 & 2018 funding cycles

Rental Housing Grant Applications



2018 Applications

- ▶ 302 units applied
- ▶ Below revised target of 400 purpose-built rental units
- ▶ Mix of market and affordable rental
- ▶ 10 per cent of units are three bedrooms
- ▶ Projects within Urban Centres



2018 Rental Housing Applications

1. 555 Fuller Ave
2. 1360 KLO Rd
3. 573-603 Clement
4. 225 Rutland Road South

2018 Grant Applicants

| Project | Number of Units | Type of Project |
|-----------------------|-----------------|-------------------------------|
| 555 Fuller Ave. | 68 | Affordable Rental |
| 1360 KLO Rd. | 154 | Seniors Market Rental |
| 573-603 Clement Ave. | 58 | Affordable Rental |
| 225 Rutland Rd. South | 22 | Student Housing Market Rental |

- ▶ Total funds available for 2018 is \$420,000 disbursed among 302 units
- ▶ Grant amounts: \$3,307 /3+ bedroom,\$1,653 /2 bedroom,& \$826 /1 bedroom

Recommended Grants

| Project | Number of Units | Recommended Grant |
|-----------------------|-----------------|-------------------|
| 555 Fuller Ave. | 68 | \$76,062 |
| 1360 KLO Rd. | 154 | \$159,566 |
| 573-603 Clement Ave. | 58 | \$115,748 |
| 225 Rutland Rd. South | 22 | \$68,622 |
| Totals | 302 | \$420,000 |

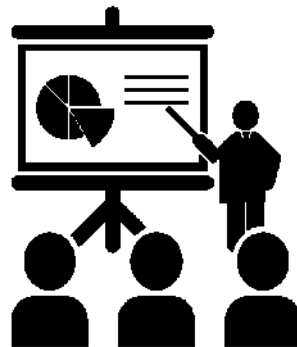
Next Steps & Timeline

- ▶ Letters to successful applicants (subject to council approval)
- ▶ Secure Housing Agreements
- ▶ Credit on DCC payment with building permit
- ▶ Next intake Fall 2018

Applications
Received



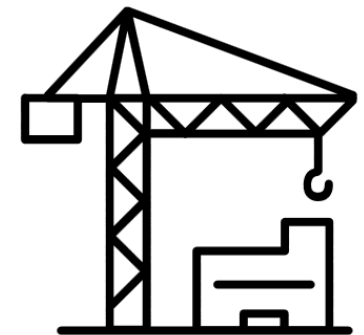
Council Approves
Grant Amounts



DCC Credits at
Building Permit



Construction Of
New Rental Units



Conclusion

- ▶ Population growth fueling demand for rental housing
- ▶ Large number of units to be completed in 2018/2019
- ▶ Continued need for rental supply
 - ▶ Affordable rental and family friendly units
- ▶ City continues to work with key partners
- ▶ Rental incentives to be reviewed through Healthy Housing strategy

Report to Council



Date: January 22, 2017

File: 1880-06

To: City Manager

From: Divisional Director, Infrastructure

Subject: Budget Adjustment Required for Highway 97 Sanitary Manhole Upgrades

Report Prepared by: Brian Beach, Infrastructure Delivery Department Manager

Recommendation:

THAT Council receives for information, the report from the Infrastructure Division Director dated January 22, 2018 regarding the budget adjustment required for the Highway 97 Sanitary Manhole Upgrades;

AND THAT the 2017 Financial Plan be amended to reflect the transfer of \$35,000 from the Bay Avenue Sewer project to the Highway 97 Sanitary Manhole Upgrades project;

AND FURTHER THAT Council authorize the additional expenditure totaling \$35,000 for the Highway 97 Sanitary Manhole Upgrades project.

Purpose:

To transfer budget from the identified 2017 capital project, to meet financial commitments for the Highway 97 Sanitary Manhole Upgrades projects.

Background:

As part of the Highway 97 Six Laning Project, the Ministry of Transportation and Infrastructure is assisting the City with upgrading of manhole entrances as part of their work. The estimated cost for all the manholes is \$100,000. The City typically provides the new manhole frames and covers and they are installed by the Ministry's contractor. Some of the manhole cover materials were purchased by the City in 2017, therefore a budget item is required for this \$35,000 expense. The 2018 Budget includes \$65,000 for the completion of these works that will be invoiced to the City later this year.

Surplus budget is available for transfer from the Bay Avenue Sanitary Project- Guy St Lift Station to Ellis #3260, as it is being re-scoped for a future budget year.

The following budget transfers are required to meet current commitments.

Financial/Budgetary Considerations:

Summary of Budget Transfer Requests

| Capital Project | 2017 Current Budget | Addition Required |
|---|------------------------------------|------------------------------|
| Hwy. 97 Sanitary Manhole Upgrades #3342 | \$ 0 | \$ 35,000 |
| Estimated Total Transfers | | \$ 35,000 |

Internal Circulation:

Divisional Director, Infrastructure
Financial Planning Manager
Department Manager, Utilities Planning
Department Manager, Infrastructure Administration

Considerations not applicable to this report:

Personnel Implications:
Legal/Statutory Authority:
Legal/Statutory Procedural Requirements:
Existing Policy:
External Agency/Public Comments:
Communications Comments:
Alternate Recommendation:

Submitted by:

Brian Beach
Department Manager, Infrastructure Delivery

Approved for inclusion:



A. Newcombe, Divisional Director, Infrastructure

cc: Divisional Director, Infrastructure
Director, Financial Services
Financial Planning Manager
Department Manager, Transportation Planning
Department Manager, Utilities Planning
Department Manager, Infrastructure Administration

Report to Council



Date: January 22, 2018

File: 1850-30

To: City Manager

From: Jerry Dombowsky, Transit and Programs Manager

Subject: Kelowna Bikeshare Proposal

Report Prepared by: Matt Worona, Active Transportation Coordinator

Recommendation:

THAT Council receives for information, the report from the Transit and Programs Manager, dated January 22, with respect to the Kelowna Bikeshare Proposal;

AND THAT Council authorize staff to negotiate an agreement with Dropbike Inc. for the purpose of licensing use of the public right-of-way for a bikeshare service in 2018 as described in the report of the Active Transportation Coordinator dated January 22, 2018;

AND THAT Council direct staff to bring forward an agreement with Dropbike Inc. for Council's consideration to a future Council meeting.

Purpose:

To present Council with a proposal for an 18-month bikeshare pilot in Kelowna.

Background:

Bikeshare is a service where bicycles are made available for shared use to individuals on a short term basis. These systems allow users the ability to pick up a bicycle in one location and return it to another located within a defined service area.

The City of Kelowna is a bike friendly city and the establishment of a bikeshare system that allows residents and visitors to access affordable and convenient bicycles for short distance trips would provide a number of community benefits that support the City's mobility, carbon reduction and economic development goals.

A new type of bikeshare, commonly referred to as dockless bikeshare, has enabled operators to shed costs and, in doing so, positions bikeshare as a profitable business.

Staff presented a framework of requirements for a bikeshare pilot to Council in December. This framework was used to evaluate operators. ***Staff are recommending Dropbike be selected as the operator for the pilot.***

Attachments to this report include presentation slides, and background reports.

Next Steps

1. Pending Council authorization to proceed, staff will bring forward a bikeshare license agreement to Council for consideration.
2. Staff will work to fulfill responsibilities outlined in the Implementation section of the background report, attached.
3. The proposed launch of the Dropbike pilot is Spring 2018.
4. Staff will report back to Council at the end of the pilot in Fall 2019 with results of the pilot and future recommendations.

Internal Circulation:

Traffic Operations and Technical Services Supervisor
Communications Advisor
Divisional Director, Corporate Strategic Services
Community Communications Manager
Community Engagement Manager
Transportation Engineering Manager
Enterprise Kelowna
By-Law Services
Integrated Transportation Manager
Transit Service Coordinator
Transportation Planner
Planner Specialist
Long Range Policy Planning Manager
Senior Buyer
Divisional Director, Infrastructure
Park and Landscape Planner
Infrastructure Operations Department Manager
Director Strategic Investments

Existing Policy:

The Kelowna On The Move: Pedestrian and Bicycle Master Plan recommends staff “research and develop a strategy to demonstrate which programs would be most effective in achieving behavioural change to grow the share of residents selecting active modes of transportation.”

The council-endorsed *2012 Community Climate Action Plan* recommended the City plan a public bike share system.

In December 2017, Council directed staff to “bring forward a proposal in 2018 for a potential pilot bikeshare program.”

External Agency/Public Comments:

Pandosy Village Business Association
Downtown Kelowna Business Association
Braintrust Canada
Tourism Kelowna

Considerations not applicable to this report:

Personnel Implications
Financial/Budgetary Considerations
Legal/Statutory Authority
Legal/Statutory Procedural Requirements
Communications Comments
Alternate Recommendation
External Agency/Public Comments

Submitted by:

J. Dombowsky, Transit and Programs Manager

Approved by:

R. Villarreal, Manager, Integrated Transportation

Approved for inclusion:



A. Newcombe, Divisional Director, Infrastructure

Attachment A - Kelowna Bikeshare Systems Pilot Opportunity Presentation

Attachment B - Background Report Kelowna Bikeshare Proposal

Attachment C - Project Risk Register Kelowna Bikeshare Proposal

Attachment D - Dropbike Pitch Document

Attachment E - Dropbike Operations Document

cc: Deputy City Manager
Divisional Director, Community Planning & Strategic Investments
Divisional Director, Corporate & Protective Services
Divisional Director, Corporate Strategic Services
Divisional Director, Infrastructure
Infrastructure Operations Department Manager

Kelowna Bikeshare Proposal

January 22nd 2018

What is Bikeshare?

- ▶ Bikes available at locations throughout the city for short term rental
 - ▶ Accessed through digital technology
 - ▶ Grab a bike from one location and drop it off in another
- ▶ Significant bikeshare systems exist in Canadian cities including:
 - ▶ Vancouver, Montreal, Toronto, Hamilton, and Ottawa

A man wearing glasses and an orange safety vest is seated on a blue motorized bicycle carrier. The carrier is loaded with several orange bicycles, each with a black basket. The scene is outdoors, with a building and other people visible in the background. The text "Pilot Requirements and Proponent Selection" is overlaid in white on the image.

Pilot Requirements and Proponent Selection

Requirements for Consideration

- ▶ Requirements for a pilot were delivered in the Kelowna Bikeshare System Options Report on Dec 11th 2017
- ▶ It includes specifications regarding:
 - ▶ minimum fleet size
 - ▶ launch date
 - ▶ deployment
 - ▶ rebalancing
 - ▶ repairs
 - ▶ theft
 - ▶ liability
 - ▶ insurance
 - ▶ data collection



The Basics

- ▶ Single operator, exclusive agreement
- ▶ No cost to the municipality
- ▶ Launch in Spring 2018
- ▶ 18 month pilot
- ▶ 500-1500 bikes





1. Scan.

Find a dropbike through your Dropbike smartphone application. Scan the QR code, the bike will unlock automatically, and you're free to go!



2. Ride.

It's all yours! You're encouraged to wear a helmet, and you must follow all local laws while riding a bicycle. Dropbike provides instructions both on our bikes and in-app.





3. Park.

the bike.

Third-generation bikes with LTE and GPS.



Havens

- ▶ Havens are virtual spaces where users can start or end their trip
 - ▶ Havens can be moved easily and created at the drop of a hat for special events
 - ▶ Haven costs are negligible



A row of orange dropbike bicycles parked on a sidewalk. The bicycles are parked in a line, facing right. They have black frames with orange accents and the word "dropbike" written on the frame. The background shows green trees and a building. The text "Implementation Plan" is overlaid in white.

Implementation Plan

Staff finalizes haven siting criteria

- ▶ Havens are locations where Dropbike users can drop-off bikes at a discount.
- ▶ Staff are developing criteria for locating havens.
- ▶ Criteria have been created to ensure bikeshare doesn't affect circulation or accessibility within the public right-of-way.
- ▶ Dropbike will use the City's siting criteria
- ▶ The City will review haven locations submitted

Dropbike installs haven and hires staff

- ▶ Dropbike uses city-approved materials and installs havens at approved locations
- ▶ This work will be approved by Traffic Operations and Integrated Transportation staff
- ▶ Approved materials will be temporary and facilitate easy removal at the end of
- ▶ Dropbike will procure a local repair partner and hire staff including a general manager and field staff

Dropbike Launch, Spring 2018

- ▶ With a pilot launch in Spring 2018 the 18-month pilot will end in Fall 2019.
- ▶ System expansion will happen over first few months of pilot.
- ▶ Staff will manage feedback from the public, oversee the operation and collect data about trips.

Engagement of Residents and Bikeshare users

- ▶ Haven Siting Engagement
- ▶ Staff will deliver surveys to close out the pilot.
 - ▶ One for bikeshare users
 - ▶ One for Kelowna residents
- ▶ Surveys will help better define both benefits and challenges the service experienced.



Measuring Success

Determining Success

- ▶ Typical bikeshare metrics define value for money
 - ▶ Therefore, not applicable
- ▶ Do we see benefits?
- ▶ What are the detriments?
- ▶ Main inputs
 - ▶ Bikeshare data
 - ▶ Bikeshare user survey
 - ▶ Public survey to residents
 - ▶ Feedback received



Bikeshare Benefits

- ▶ Access to Transit
- ▶ Transportation & Land Use Planning
- ▶ Support Climate Change Goals
- ▶ Active Transportation
- ▶ Road Safety
- ▶ Transportation Choice
- ▶ Traffic Congestion
- ▶ Tourism





Stakeholder Comments

Internal

- ▶ Consulted with multiple city departments including:
 - ▶ *Policy and Planning, Traffic Operations, City Clerks, By-Law Enforcement, Parks Planning, Economic Development, and Communications*

External

- ▶ Consulted with multiple external partners including:
 - ▶ *Downtown Kelowna, Braintrust Canada, Tourism Kelowna, Pandosy Village Business Association*

Next Steps

- ▶ 1. Staff will bring forward a bikeshare license agreement to Council for consideration.
- ▶ 2. Proceed with implementation plan outlined in the Background Report.
- ▶ 3. Launch in Spring 2018.
- ▶ 4. Staff report back to Council at the end of the pilot in Fall 2019.

Questions?

For more information, visit kelowna.ca.

Matt Worona
Active Transportation Coordinator
(250)407-0652
mworona@kelowna.ca

Chan-Norris, Jesse. "20120620D_0091." Flickr, Yahoo!, 21 June 2012,
www.flickr.com/photos/jcn/7412020066.

Dc, Ddot. "Capital Bikeshare Launch Event." Flickr, Yahoo!, 21 Sept. 2010,
www.flickr.com/photos/ddotphotos/5011380837.

Kelowna Bikeshare Proposal

January 22nd 2018



City of
Kelowna



Contents

| | |
|---|---|
| Executive Summary | 1 |
| Requirements for Consideration of a Dockless Bikeshare Pilot..... | 1 |
| Proponent Selection | 2 |
| Risk Register | 3 |
| Implementation | 4 |
| Measuring Success..... | 7 |
| After the Pilot | 8 |
| Engagement Summary | 8 |

Title page note: The Dropbike bicycle shown on the title page of the background report is not the bike Kelowna will receive during the pilot. Photos of the bike from Kelowna Dropbike fleet are incorporated into this report.

Executive Summary

Staff are currently working on the delivery of the Pedestrian and Bicycle Master Plan. The plan recommends that staff “research and develop a strategy to demonstrate which programs would ... grow the share of residents selecting active modes of transportation”. Through this research, staff have discovered an evolution in the bikeshare industry with regards to cost and operating models available.

This report builds on the Kelowna Bikeshare System Options report to council on Dec 11th, 2017. That report describes the benefits of bikeshare, governance models, types of bikeshare, and issue areas for a potential pilot.

In December 2017, Council directed staff to “bring forward a proposal in 2018 for a potential pilot bikeshare program.” This report describes that potential bikeshare program. The pilot would last 18 months, start in Spring 2018, and operate at no cost to the municipality.

Requirements for Consideration of a Dockless Bikeshare Pilot

Staff presented the framework below as requirements for a bikeshare pilot to council in December. This framework has been used to evaluate operators for the pilot.

In order to facilitate the pilot test, the City would allow a provider to use approved locations within the City’s right-of-way network at no cost, through an exclusive license agreement that requires as a minimum the following:

- 1. Selection of bikeshare havens (parking areas for unrented bikes) to be approved by the Manager of Integrated Transportation;*
- 2. A minimum of 500 bikes deployed as early as April 1st 2018;*

- 3. Bikeshare deployment within the coverage area approved by the Manager of Integrated Transportation;*
- 4. Contact information for the bikeshare service provider to be clearly shown on all deployed bikes and user interface point of sale applications;*
- 5. Commitment of bikeshare provider to:*
 - a. maintain bikes in a safe and fully functional state of operation;*
 - b. remove any bike that is unsafe, damaged or otherwise in need of repair or replacement;*
 - c. provide regular balancing of the bike fleet and remove bikes that have been left at improper locations;*
 - d. discourage the use of public bike racks within the right-of-way for bikeshare havens;*
 - e. operate the system so that it does not create a hazard or restrict access to, from or through any portion of the municipal right-of-way, especially with respect to sidewalks and persons with disabilities;*
 - f. take steps to avoid parked bikes being easily tipped over;*
 - g. promote the bikeshare service to the entire community;*
 - h. respond to inquiries and complaints in a timely and businesslike manner;*
 - i. maintain operating data and provide data to the City of Kelowna upon completion of the pilot test;*
 - j. remove all bikes and any appurtenances from the right-of-way upon completion of the pilot test; and*
 - k. return deposits provided by system users at the end of the pilot or end of user's membership.*
- 6. Insurance and indemnifications to be provided to the satisfaction of the City Solicitor;*
- 7. Refundable financial securities to be provided to the city that are sufficient to pay for the gathering up and disposal of deployed bikeshare bikes and any appurtenances from the city right-of-way;*
- 8. Provisions to end the pilot test early and cancel the license agreement should commitments not be kept or conditions become undesirable to the City;*
- 9. Require the operator to share system-wide trip, user, network data sets with the City as requested; and*
- 10. Require the operator to, at a minimum, alert users to BC's mandatory bike helmet law on all bikes and in all user interface point-of-sale applications.*

Proponent Selection

Staff evaluated many bikeshare industry offerings, while focusing on dockless bikeshare companies. The dockless model's low costs were most likely to deliver the requirements defined earlier.

One operator met all the requirements identified in our framework. There are a dozen or so owner and/or operators in North America delivering dockless bikeshare systems. Many of them showed interest in the Kelowna market.

Staff are recommending Dropbike as the operator for the pilot. Dropbike met requirements and has many advantages beyond our framework's scope. Their core principles focus on working hand-in-hand with municipalities. They have operated in winter cities and within many Canadian jurisdictions. Dropbike is aligned with the City of Kelowna's goals for a bikeshare system because their success is directly linked to bike usage.



Figure 1 Dropbike Haven across the street from a Dockless Bikeshare Station in Montreal, QC Note: these are not the bikes Kelowna will receive during the pilot.

Risk Register

Staff have evaluated risks to the City of a bikeshare service and have outlined the approach with regards to these risks. For each risk, staff identified the approach to avoid, mitigate, transfer, and/or accept the risk. Staff rated each risk on a scale of 1-5 for likelihood of occurrence and the severity of consequences. To determine risks borne by the City, the responsible parties are also defined for each risk.

Staff believe the addition of a bikeshare system will improve road safety in Kelowna. This is explained in detail in the [Kelowna Bikeshare System Options](#) report to council dated Dec 11th, 2017.

¹ "Montréal voit d'un mauvais oeil le vélopartage sans bornes." *Le Devoir*, Le Devior, 11 Sept. 2017, www.ledevoir.com/societe/actualites-en-societe/507697/montreal-voit-d-un-mauvais-oeil-le-veloportage-sans-bornes.

Implementation

Dropbike has provided details as to their operations standards and the service they deliver in their overview documents which are included as "Attachment D" and "Attachment E" to this report.

To supplement these details, this report will delve further into the roll-out of the pilot from present day until the end of the pilot. The focus will be upon the City's responsibilities in relation to the implementation of the bikeshare system to provide a more complete picture of what to expect and the touchpoints in the process.

Within the roll-out and implementation of the bikeshare system it's important to understand that much of the City's role is in oversight and supervision.

1. Staff finalizes haven siting criteria

Havens are locations where bikeshare users can drop-off bikes at a discount. Staff are currently developing criteria for locating bikeshare havens in Kelowna. This process clarifies where and how havens can be located within the municipally owned public right-of-way as well as other city-owned facilities like parks, community centres and other properties. The criteria have been created to ensure bikeshare doesn't affect circulation or accessibility within sidewalks travel lanes parking stalls and other considerations.

Staff have developed the criteria so Dropbike can easily propose haven locations. The City then can act as a regulator, approving and rejecting haven locations.

2. Dropbike moves the bikeshare fleet to Kelowna and stores locally until launch



Figure 2 Kelowna's Dropbike's fleet features front and rear lights, fenders, and a basket.

Dropbike will ship and store their fleet in Kelowna in advance of the launch of their bikeshare system.

3. Staff develops a helmet discount program for Bikeshare users

Staff will develop a helmet discount program for Dropbike users. Dropbike users will be able to access discounts on helmets from local retailers. Any local retailer who sells bicycle helmets will be able to participate. Users will likely need to show the app on their phone or something similar to get the discount.

4. Dropbike plans coverage areas, haven locations, and expansion areas

Using data provided by the City and their own bikesharing network design expertise Dropbike will define their initial service area, propose haven locations to the City for approval and plan expansion areas for the future.



5. Dropbike submits haven locations, City approves those that meet criteria

Dropbike sends their haven siting locations to the

City's Transit and Programs Manager to review the locations, contacting any relevant staff. Staff will expedite the approvals and give alternatives for rejected havens.

Figure 3 QR codes are located at various points on the bike to make unlocking simple and easy.

6. Dropbike sets up repair partners and hires local staff

Dropbike will procure local repair partner(s) and hire staff including a General Manager and field staff.

7. Dropbike installs permitted haven locations, installation is assessed by Traffic Operations and Integrated Transportation

The Traffic Operations and Technical Services Supervisor will define materials to be used to establish haven locations. With haven locations approved by the City and the materials that the City approved, Dropbike can install markings for their havens within the public right-of-way. This installation process will be inspected by Traffic Operations and Integrated Transportation

staff. This will take place at the earliest a few weeks before the pilot launches. Havens will be installed as the bikeshare system is expanded and to close gaps in the existing service area. Haven markings will utilize temporary materials that are easily removed at completion of the pilot.

8. Dropbike Launch, Spring 2018

Dropbike will launch their pilot in Spring 2018. With that launch date the pilot will end in Fall 2019. Rebalancing, repairs, and gradual system expansion will be handled by their staff. System expansion will happen over first few month of pilot guided by uptake from the general public.

9. Ongoing complaint resolution, oversight, and data collection

Staff will need to manage complaints that come in and ensure the clauses of our agreement are being upheld. We will also receive weekly data releases from Dropbike. Bikeshare data could be used to develop both the upcoming Transportation Master Plan and Official Community Plan update.



Figure 4 Kelowna's Dropbike fleet is outfitted with internal gears and a drum brake for low maintenance operation.

10. By-law to track # of Dropbike riders stopped without helmets

As a way to gather more layers of data for the pilot, By-law Enforcement will keep count and report on how many Dropbike users they stop who aren't wearing helmets.

11. Survey of Public and Bikeshare Users

Towards the end of the pilot, staff will deliver two surveys to define the impact bikeshare had on our community. One will survey Dropbike members and another the general public. This will help us define whether we saw benefits and what challenges the service experienced.

12. Deliver Pilot Report and Permanent Bikeshare Proposal

In fall 2019, Staff will submit a report to Council outlining the results of the bikeshare pilot and our recommended approach going forward. That recommendation could be to dismantle the bikeshare system, choose a new provider, choose a different style of bikeshare or continue with the current provider.

Measuring Success

Many commonly used success metrics for bikeshare help define value for money. With this pilot there is no financial impact to the municipality. Without relevant success metrics we created our own. Staff have defined the benefits of bikeshare in the [Kelowna Bikeshare System Options](#) report to council on Dec 11th, 2017. Measuring those potential benefits as metrics can help to understand the pilot's impact. In this case, as no financial impact exists to the City the interest is in a bikeshare system that provides more benefit than conflict.

To assess these two elements, staff will use the information and data provided by Dropbike. During the pilot bikeshare data will be shared with us. At the end of the pilot, a bikeshare user survey as well as a public survey to residents will be conducted.

The surveys will supplement user data to help assess the bikeshare service.

Access to Transit

Use Bikeshare user survey to assess use and impact upon transit service.

Better Evidence-Based Decision Making

Identify how data collected can be used in the future and what questions it helps answer today.

Reducing Greenhouse Gas (GHG) Emissions

Use bikeshare data and the bikeshare user survey to assess GHG reduction.

Supporting Active Transportation

Use bikeshare data and the City's pedestrian and bike counter network to assess overall Active Transportation goal achievement.

Improving Road Safety

Collisions data citywide and on-bikeshare to assess road safety.

Improving Transportation Choice and Lowering Cost of Living

No measurement metric identified.

Reduction in Car Use and Congestion

No measurement metric identified.

Supporting Tourism

Bikeshare user profiles and Tourism Kelowna feedback to assess impacts upon tourism.



Figure 5 Kelowna's Dropbike's fleet is sized for everyone, just adjust the seat and ride away.

After the Pilot

A recommendation to Council will be presented - that measures success based on anticipated benefits and compares those benefits against conflicts. The end of the pilot marks multiple decision points. Staff need to decide whether a bikeshare service has more benefit than detriment in the Kelowna context. Staff need to decide whether dockless bikeshare worked or whether another bikeshare model should be used. Staff will also be looking at whether Dropbike's offering is best or another bikeshare operator is offering a service that is more attractive.

After the pilot, if it is decided to award a bikeshare provider with a license agreement a competitive RFP process will be employed with requirements informed by lessons learned in the pilot. This RFP process will award a longer-term contract and also marks the time to decide whether advertising should be permitted on the bikes and within what parameters.

Dockless bikeshare is currently growing at an alarming pace and more companies are entering the market. An 18-month pilot allows the city to fully understand the tradeoffs involved in bikeshare operations and gives the bikeshare market time to mature before awarding a possible long-term contract.

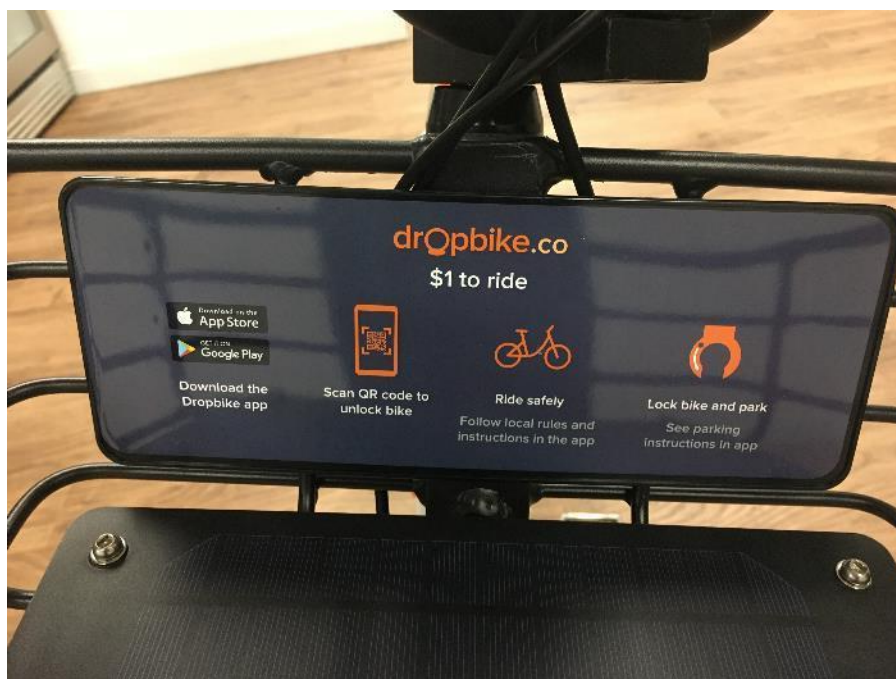


Figure 6 Kelowna's Dropbike fleet will have information about helmet use in BC on the bike as well as info about how to use the service.

Engagement Summary

Engagement with other departments and the public will continue for many bikeshare activities including, determining haven locations and surveying the public towards the end of the pilot.

Staff have consulted with multiple City departments including Communications, Traffic Operations, Parks, City Clerks, Economic Development, By-law Enforcement, Property Management, and Purchasing. We have also consulted with external stakeholders including Okanagan College, Downtown Kelowna, Pandosy Village Business Association, and Braintrust Canada. Staff have integrated the comments of those stakeholders into the terms of the license agreement by which we

will permit Dropbike Inc. to operate within the public right-of-way. A selection of their comments and concerns is provided below.

Internal

Policy and Planning

Policy and Planning see bikeshare as fully aligning with the City's goals for multi-modal transportation outlined in the OCP and Urban Centres Roadmap.

Traffic Operations

Road marking concerns regarding location of "havens" that could be resolved by requiring the proponent to use temporary road tape. Favour an oversight role vs. installation as this can be left to the proponent. General agreement with helmet approach of proponent and the City. Agree with need for re-balancing bikes as a requirement for the proponent.

City Clerks

Agree with this direction as it is referenced in the adopted Pedestrian and Bicycle Master Plan. Suggest consideration of the impact upon the City's internal staff bikeshare system.

By-Law Enforcement

By-Law concern focused on bikes ridden on sidewalks and helmet use. Agree with approach with regards to helmets by alerting users on the app and on the bike re the mandatory helmet law. Suggest the proponent include information for users re bylaws regarding bikes on sidewalks.

Agreed to capture data on offences (both from bikeshare system users and general public) as observed for program review.

Parks Planning

No issues with integration of bikeshare locations at key parks. Parks facilities typically have unusable space that could be re-purposed for bikeshare "havens".

Economic Development



Figure 7 Kelowna's Dropbike fleet is attractive and easily recognizable with an orange and white colour scheme..

Economic Development is fully supportive. It is believed adoption of a dockless bikeshare service further positions Kelowna as a tech leader.

Communications

Would like proponent to have a requirement to share ridership and other utilization data with the City. Concern about accessibility and need to ensure it is maintained on all public right of ways i.e., un-hindered by parked bikes. Concern that any existing bike rental operations are aware of City's intentions.



Figure 8 Kelowna's Dropbike fleet attaches the front basket directly to the frame resulting in fewer bikes falling over and improved handling for riders.

External

Downtown Kelowna Association

Provided a strong letter of support. Prefers a longer pilot and sees the needs for an extendable or permanent option. Recommends a helmet discount business program.

Braintrust Canada

Some concern with risk but appreciate the effort to have users wear helmets. Want to see a helmet discount business program. Cautioned against free helmets for adults who need it based on experience with a similar program. Agreed with approach that delivers information about helmet use on-bike and in-app. Discussion about incentivizing users and giving rewards for wearing helmets similar to a Kelowna RCMP program.

Tourism Kelowna

Strongly supportive. Will link to tourism partners if approved by Council.

Pandosy Village Business Association

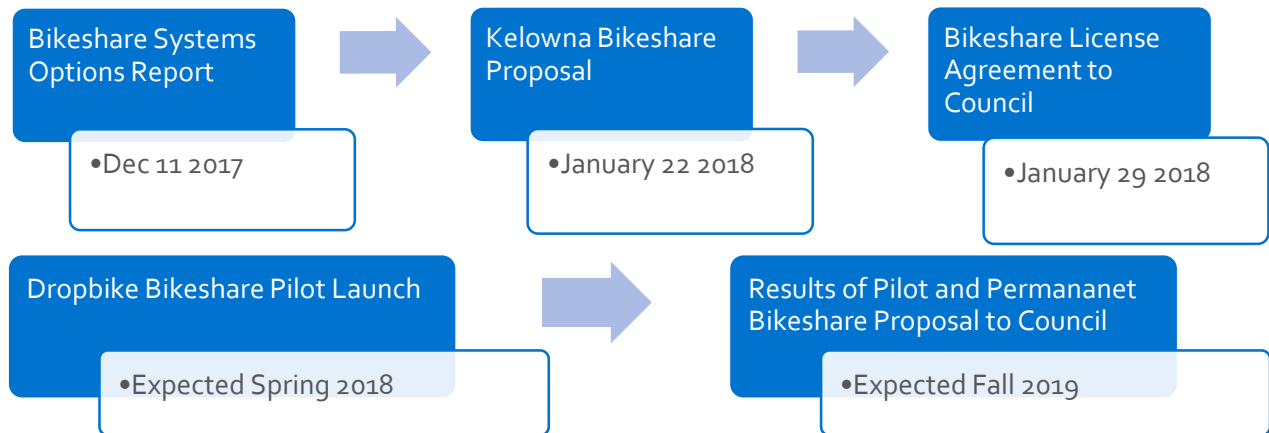
Supportive of the proposal. Wish to review "haven" siting in the area if proposal is approved.

Next Steps

1. Pending Council authorization to proceed, staff will bring forward a bikeshare license agreement to Council for consideration.
2. Staff will work to fulfill responsibilities outlined in the Implementation section of the background report, above.
3. The proposed target date of Dropbike launch is Spring 2018.

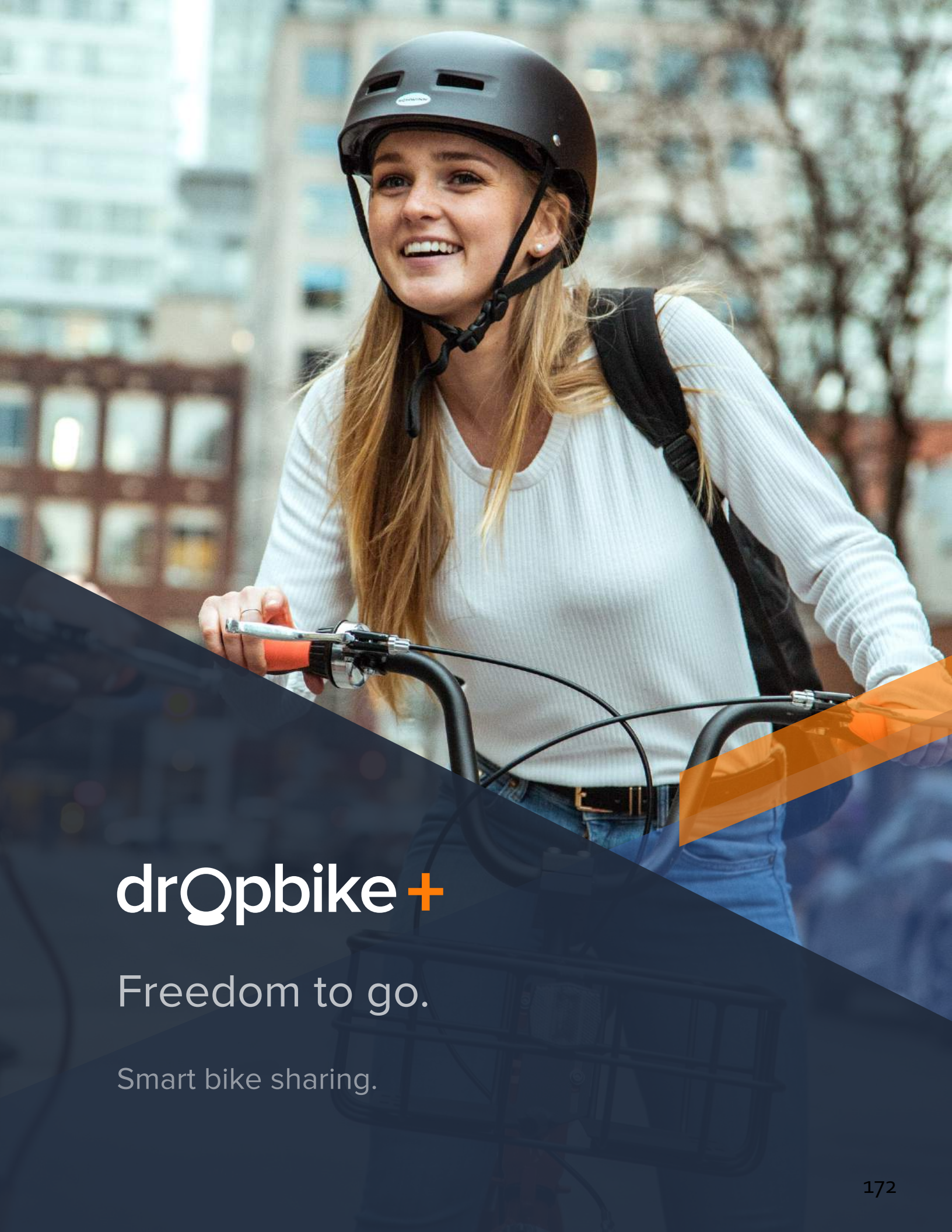
4. Staff will report back to Council at the end of the pilot in Fall 2019 with results of the pilot and future recommendations.

Proposed Process timeline



| Risk (including triggers) | Consequences of | Likelihood | Risk Strategy | Risk Category | Mitigation Actions | Responsible Parties |
|---|-----------------|------------|----------------------------|---|--|----------------------------|
| Helmet law compliance | 3 | 2 | Transfer, Mitigate, Accept | Operational, Political, & Institutional | Staff worked with Braintrust Canada to understand get more people on bikes wearing helmets and lower the incidence of head trauma while promoting more physical activity. Staff believe it would be unsafe to provide shared helmets. Additionally, the likelihood of shared helmets being worn by riders is low. | City of Kelowna |
| Not enough of the potential target market has cell, credit card and data plan | 1 | 3 | Mitigate, Accept, Transfer | Operational | There is significant uptake in wireless service especially with younger household's province-wide. It's important to eliminate the perception of this system being exclusively for the wealthy. | City of Kelowna |
| Good uptake, then pilot ends, backlash from removal | 2 | 2 | Mitigate, Transfer | Operational | Ensure the agreement has a caveat for interim extension if both parties agree. This give us time to plan without removing a service that works. | City of Kelowna & Dropbike |
| Someone is injured or dies | 5 | 1 | Transfer, Accept | Operational | Bikeshare is significantly safer when compared against regular cycling for a variety of reasons. We believe this program will overall have a positive safety impact on road safety in Kelowna. With a new program we will face additional scrutiny about performance and likelihood of non-compliance with the helmet law. Even though the City does not operate the service, Dropbike still carries liability insurance on our behalf to guard against this. (http://transweb.sjsu.edu/PDFs/research/1204-bikesharing-and-bicycle-safety.pdf). | City of Kelowna & Dropbike |
| A bike breaks | 1 | 5 | Transfer | Operational | Providers have maintenance plans where users can flag damaged or broken bikes that are then automatically taken out of service and sent to a repair facility. The City of Kelowna does not get involved with any piece of this process, it's all done in-app. | Dropbike |
| Bikes are stolen | 1 | 4 | Transfer | Operational | All bikes are equipped with GPS, LTE, and Bluetooth connectivity. Bikes that are stolen are immediately flagged and easily trackable. In previous pilots Dropbike had no bikes lost to theft over a multi-month pilot. | Dropbike |
| Blocks sidewalk, impedes accessibility or is unsightly | 2 | 3 | Transfer | Operational | Staff will add clauses in the agreement to ensure we discourage impediments to accessibility. We will work with Downtown Kelowna to address any impacts to the right-of-way and help the public efficiently report this so we can then pass it onto Dropbike. | Dropbike |
| Provider not fulfilling their contractual obligations | 4 | 1 | Mitigate | Operational | City of Kelowna will retain the ability to exit the agreement with sufficient notice or review the service. | City of Kelowna |
| Contractual obligations | 3 | 2 | Mitigate | Operational | Keep an open dialogue through agreement negotiations. Think about possible | City of |

| | | | | | | |
|---|---|---|----------|---------------|---|-----------------|
| are too loose and provider poses unforeseen consequences | | | | | loopholes that we are missing. Talk to other departments about what they are concerned about and try and address that in the pilot structure. | Kelowna |
| Contractual obligations too stringent and provider pulls out of the market | 2 | 1 | Mitigate | Operational | Ensure Dropbike is comfortable with any deviation from their desired agreement structure. | City of Kelowna |
| Provider insolvent, right of way cleanup implications | 4 | 1 | Accept | Operational | Within the agreement there is a clause where refundable financial securities can be provided to the city that are sufficient to pay for the gathering up and disposal of deployed DropBike bicycles and other materials from the City's right-of-way. | City of Kelowna |
| Not meeting use targets, this might impact the perceived validity of our projections | 1 | 1 | Mitigate | Institutional | Staff will not set targets and instead propose a range of expected benefits. Make it clear the pilot project will give us more data to structure our cycling investments and a possible permanent bikeshare system in the future. | City of Kelowna |
| Liability of municipality | 2 | 1 | Transfer | Institutional | Dropbike covers the municipality in their general liability coverage. We need to manage safety in the right-of-way with regards to bikeshare parking and any possible injuries. | City of Kelowna |
| Perception that City of Kelowna is the service provider instead of a private provider | 1 | 4 | Mitigate | Institutional | Make sure it's well communicated that this is a company coming in instead of the City launching a program. | City of Kelowna |
| Internal support but challenge comes w/ timeline | 2 | 1 | Accept | Institutional | Timeline should not be a problem because there is very little workload for other groups at the City with this project. Highlighting this lack of inputs should help reduce the risk. | City of Kelowna |
| Little internal support | 2 | 1 | Accept | Institutional | At the moment this does not seem to be a problem but as we move forward and solidify details, it may occur. Looping key internal stakeholders in early helps with building buy-in. | City of Kelowna |
| Other work gets delayed due to the extra workload | 2 | 2 | Accept | Institutional | This is possible. Within the integrated transportation team the bikeshare pilot will ensure we have better data for the planning work we conduct which will enable to move through upcoming projects like the Regional Active Transportation Master Plan with better and more complete information. | City of Kelowna |



drOpbike +

Freedom to go.

Smart bike sharing.



Table of contents.

| | |
|----------------------------------|----|
| Summary. This is the future. | 3 |
| Overview. How it works. | 4 |
| Hardware. Here's the bike. | 6 |
| Innovation. Here are the Havens. | 7 |
| Plan. Why it works. | 8 |
| Our vision. Everyone wins. | 10 |
| Our roots. Proudly Canadian. | 11 |
| Contact Us. Let's talk. | 12 |

This is the future.

Dropbike is smart bike sharing. We are the first and only Canadian dockless bike sharing company, and the first North American company ever to partner with a city to launch dockless bike sharing. Dropbike is a sustainable company that requires **no subsidies** to get started, keeping costs low because it's **unencumbered by costly docks**.



How it works.

Third-generation bikes.



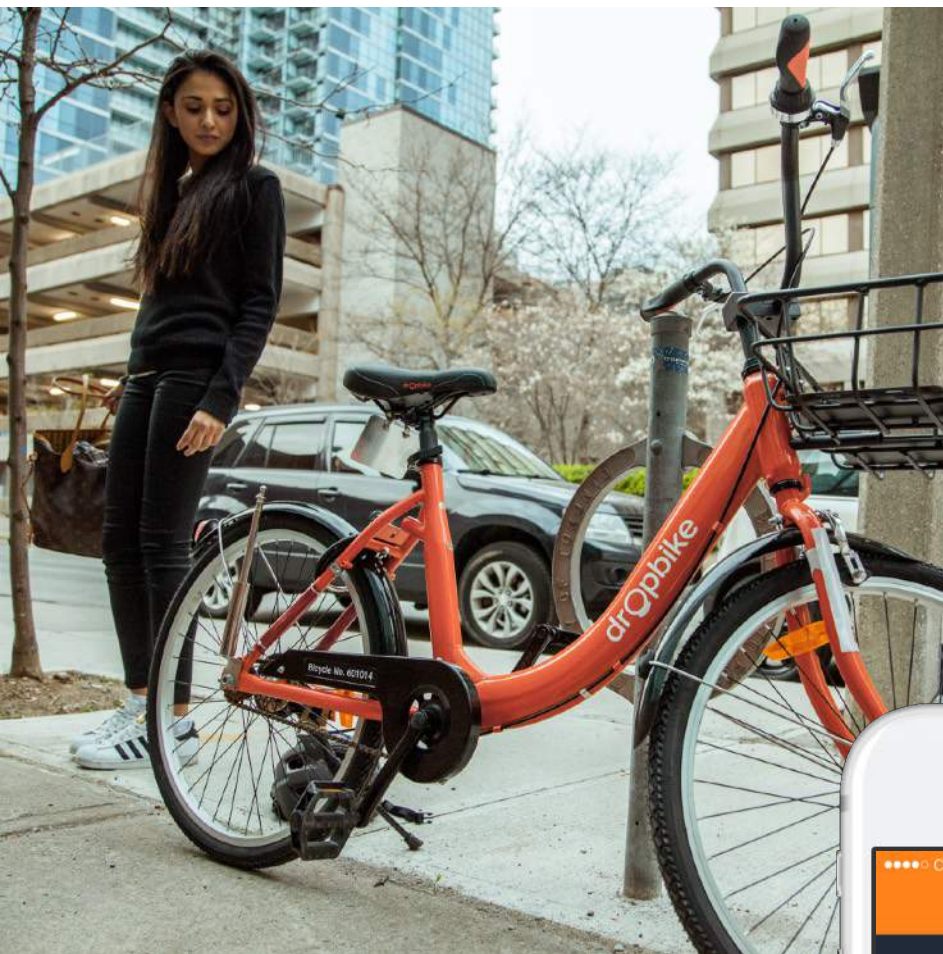
1. Scan.

Find a dropbike through your Dropbike smartphone application. Scan the QR code, the bike will unlock automatically, and you're free to go!



2. Ride.

It's all yours! You're encouraged to wear a helmet, and you must follow all local laws while riding a bicycle. Dropbike provides instructions both on our bikes and in-app.



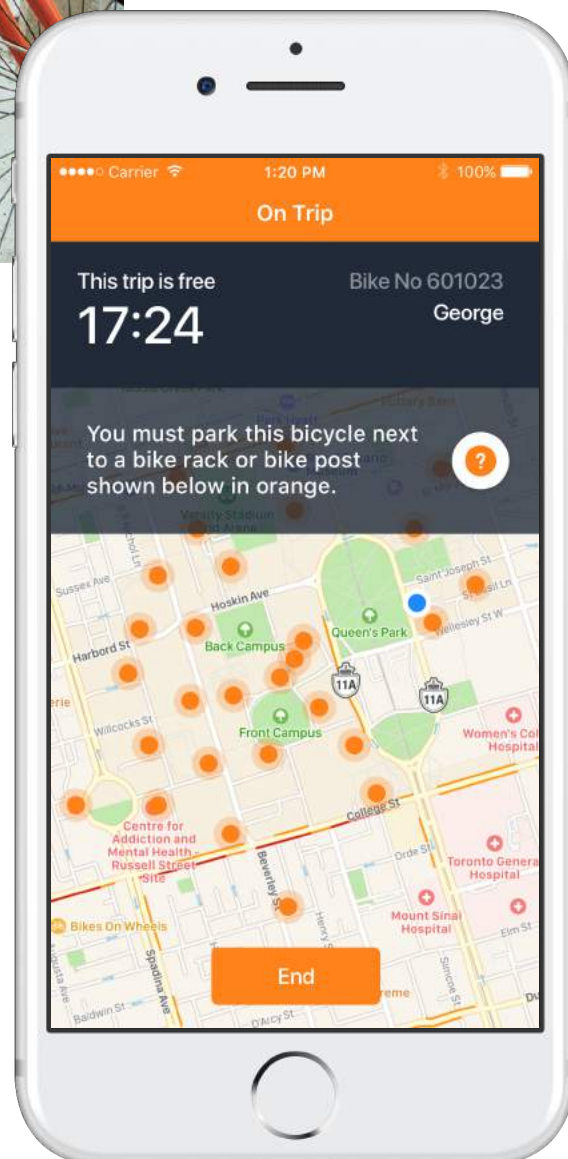
3. Park.

Dropbikes can be parked at any 'Haven' location. These are bike posts/bike racks and any other bike parking infrastructure, which we clearly mark on the app and even physically.

That's it!

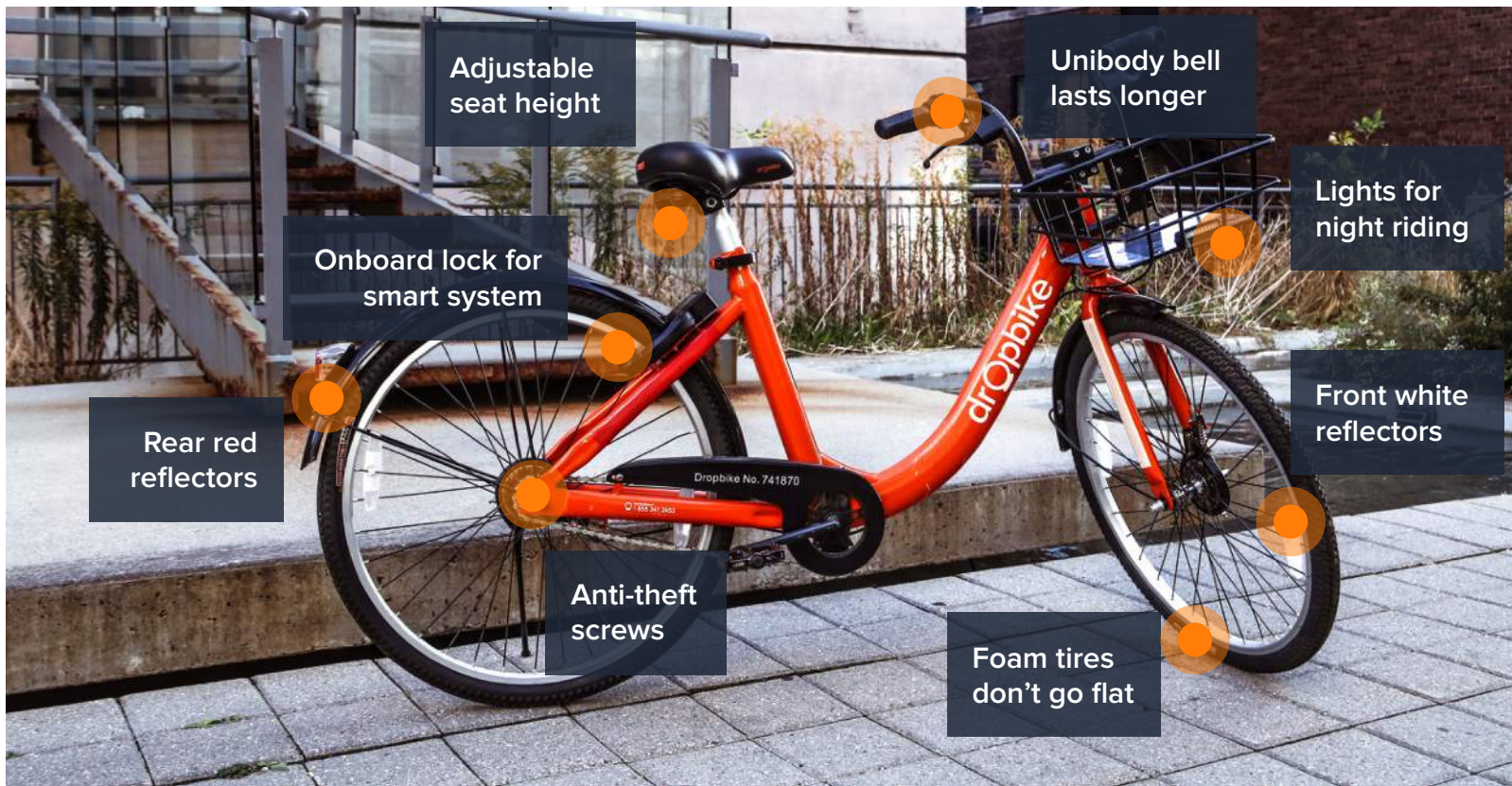
Dropbike starts at \$1/hour.

Dropbike manages all payments seamlessly and securely in-app, using an industry-leading platform known as Stripe. Dropbike also offers in-app messaging, email support lines and a phone support line to users.



Here's the bike.

Third-generation bikes with LTE and GPS.



Here are the Havens.

A world of difference for users.



Why it works.

Our long-term plan is to work with you.

Public transit is a critical part of how a city works. As biking becomes a more crucial part of daily commutes, we intend to work with you to grow and develop a bike share program that matches your city's needs.

Start by taking away the biggest limitation.

Remove the largest cost—docks.

Docks are the most expensive part of a bike sharing system: up to \$3,500/bike. Appreciating the organization that they provide, Dropbike can do everything docks can with software and our Havens system, without the physical cost and issues.

Jumpstart the network.

Dropbike creates hundreds of stations.

The value of bike share increases with the number of stations—just 100 or 200 stations aren't enough to be useful. Residents don't always live near a station and they're not always going to a station.

Scale a smarter network.

Build the right infrastructure.

Dropbike's analytics system knows where residents park bikes the most. With this information, we've designed a robust and secure system. In the future, our data will help our partners build better infrastructure and smarter cities.

Become adaptive.

Adapt to events. Dropbike helps cities come alive.

Dropbike is a software company. When physical locations are not the constraint, adaptive Havens can be introduced for sporting events, music festivals, parades, and other large movements of residents. These are GPS-based spots where you can park the bike, combined with temporary posts on the ground. Cities come truly alive with Dropbike.

Everyone wins.

Biking is good for everyone.

If just 1% of drivers switch to cycling, traffic congestion can be reduced by up to 18%.

In dense areas, cycling can be as much as 30% faster than car travel for distances up to 5 kilometres.



Residents win.

An affordable, accessible, and healthy transit option—at \$1 per hour and with monthly pricing options, residents can take advantage of a low-cost system.



Cities win.

Reduced congestion, insights on transportation data, local jobs...plus, our powerful software has the ability to give cities true insight into how people move, providing essential data to help with transportation and land-use planning activities.



Environment wins.

Bike sharing leads to lower emissions and a greener world. Dropbike encourages sustainable transportation even in winter months.

Proudly Canadian.

Started here. Grown here. And here to stick around.

Dropbike is based in downtown Toronto. We believe in local managers, local operators and local expertise. Our team includes tech entrepreneurs, municipal and federal government experience, and supply chain and operational experience across continents.



Let's talk.

At Dropbike, our goals are to work with like-minded partners and remain flexible. We're here for the long haul—contact us to learn more.

Afraj Gill

VP of Business Development and Government Affairs

afraj@dropbike.ca

(613) 583-1678

Emmett Meacher

Manager of Business Development and Government Affairs

emmett@dropbike.ca

(613) 214-0872

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www.dropbike.ca



dr**Q**pbike

Freedom to go.

Smart bike sharing.



Table of contents.

| | |
|-----------------------------------|---|
| Summary. Operational plan. | 3 |
| Partners. Working with care. | 4 |
| Commitment. Creating local jobs. | 5 |
| Zero subsidies. Business model. | 6 |
| Appendix. Details and operations. | 7 |

Operational plan.

The purpose of this document is to provide details on a number of operational elements of a Dropbike program.

If you have any questions, please email Emmett Meacher at **emmett@dropbike.ca**.



Working with care.

Robust system planning.

To launch in a new region, the Dropbike team takes an expert approach. Dropbike researches the needs of community members specific to that region. We look at topography, demographics, neighbourhood density, and much more. We combine this knowledge with information about:

- The most popular destinations in the region for commuters.
- The gaps left by existing transit infrastructure.
- Conversations with locals (commuters, tourists, shop owners, etc.) and government officials.

With a comprehensive view of how to adapt to best serve the community, where Dropbike Havens would be most useful and how many bikes to start with, our team then reaches out for a discussion and future partnerships with local stakeholders.

Best-in-class service.

Dropbike's operational team monitors the real-time locations of each bicycle. There is a fast response team to pick up any bicycles left in inappropriate locations. Dropbike also works on in-app safety features and signage reminders to allow more people to learn about cycling rules and safety. In the Dropbike app, real-time user data and feedback will allow cities unprecedented insight into travel behaviour.

Local commitment.

Engaging local authorities, being accountable to the concerns of governments and adapting our system to the needs of residents allow us to incorporate Dropbike into the fabric of a community and its transit system, creating valuable infrastructure that's focused on serving residents in the long term.

In our many current markets in and around Toronto, Kingston and Montreal—including partnerships with cities, universities, local groups and private partners—we have hired and maintain a team of local operators.

Creating local jobs.

At Dropbike, we believe in local managers, operators and manufacturing. Our HQ team includes tech entrepreneurs, municipal and federal government experience, and supply chain and operational experience across continents.

HQ team.

Canadian crew.

Our HQ team consists of business development, government relations, software engineering, design, operations, warehouse management, communications and talent acquisition experts. As an all-Canadian and locally-based crew, we take pride in giving our own citizens first-class service, understanding the nuances of our cities, and creating excellence on our soil. We put Canada first.

General Managers.

Expert city operators.

General Managers are operations experts, responsible for launching Dropbike in a city. General Managers are Dropbike brand leaders who can grow, manage and motivate a team, implement creative growth marketing strategies and execute on them, and are not afraid to be hands on. At its core, a General Manager's responsibility is to run a world-class operation system that makes cities proud to partner with us.

Dropbike Warriors.

Knowledge experts.

Bike Warriors are brand ambassadors, knowledge experts, Dropbike's rebalancing team and the true lifeblood of the Dropbike ecosystem.

The primary responsibility of a Bike Warrior is to ensure that all dropbikes are appropriately parked at Dropbike Havens (as previously defined) while not in use, repaired or checked for quality, and kept within appropriate boundaries.

Business model.

How is Dropbike able to operate with **zero subsidies**?

More than 10x more convenient.

Most bike sharing systems are limited to fixed docks. By creating Havens (practically virtual posts), Dropbike saves money and time. Docked systems have 5-500 stations—Dropbike can turn 17,500 bike posts into Havens. Shared bicycles are a network that's only as valuable as the number of its nodes—with every new bike and added Haven, the convenience of bike sharing makes it a better transit option for residents. Research has consistently backed up this simple fact: the more bikes and the more places to leave bikes, the higher the growth of ridership in a city.

More than 10x more affordable.

Physical docks are the most expensive part of a shared bicycle system: up to \$3,500 of a \$5,000/bike cost. Dropbikes are over 10x cheaper (including all tariffs, shipping, etc.) than traditional bike share systems—making Dropbike a more affordable option for riders, as well. Currently, we offer our service for only \$1/hour.

Aligned incentives.

Dropbike does not make money by selling bicycles to cities. Dropbike only wins when more people ride bikes more often, which is perfectly aligned with government objectives. This is why cities can trust Dropbike with their best interests.

Venture-backed & unit costs.

For more details on Dropbike's operational costs, unit economics, sustainable business model or venture success, please contact us directly (contact information is on the final page).

Details and operations.

We thought you'd never ask. A breakdown of everything else—the elephants in the room.

Deployment.

Once an agreement is made between a city and Dropbike to permit usage of our product within a certain geography, dropbikes will be privately transported from our main Dropbike warehouse to a local storage locker until the agreed date of deployment.

At the date of deployment, dropbikes will be released gradually into the agreed upon location.

Rebalancing.

Since dropbikes aren't tied to docks, our app encourages riders to park all dropbikes at Havens when they are finished with their ride.

Dropbike HQ knows exactly where each dropbike is located before and after a ride. Bike Warriors are responsible for moving dropbikes from inappropriate locations or overcrowded Havens to areas where there are fewer bikes and assigned Havens. Dropbikes are maintained by Bike Warriors 12 hours/day.

In the event that a dropbike is not parked by the previous user at a Haven, our Bike Warriors are notified to move the bike back to an appropriate location.

We make an effort to encourage users to park their dropbike at a Haven by providing the ultimate level of clarity in our app (where users can see all surrounding Havens on the map), and penalizing riders for inappropriate parking by charging an extra fee, called out-of-Haven pricing.

Our city General Manager is responsible for collecting all bikes that have been taken outside of designated boundaries.

Misplacement.

Thanks to a combination of Bluetooth, LTE and GPS technology, Dropbike HQ knows exactly where each dropbike is located. However, if a dropbike appears to have been stolen, is missing or is parked at an inappropriate location, we encourage the finder to call Dropbike directly at 1-855-341-2453 so that we can dispatch a General Manager or Bike Warrior to immediately collect the stolen bike. This number is provided on each bike and in our app.

Recovery.

If (in case of inclement weather or an agreement with the city) dropbikes need to be transported off the street, the

Dropbike operations team will take care of this move.

The dropbikes may then be held at our local storage locker, or may be privately transported to our main Dropbike warehouse.

Incidents.

In the case of incidents involving either our staff or users, we urge that 911 (or an applicable local non-emergency number) be called for two reasons. Though the affected party might not be visibly injured, a medical professional should confirm that no injuries were sustained during the incident. As our app does not currently allow for riders under the age of 18, no incidents will involve a minor. It is also important for incidents to be officially documented by local authorities.

We also encourage our staff and users to call Dropbike directly at 1-855-341-2453 in case of an incident. This number is provided on each bike and in our app. This number provides a direct line to the General Manager who will walk the involved parties through the process of calling local authorities.

Dropbike has the appropriate insurance to deal with incidents (including commercial general liability and full third-party liability).

Inclement Weather.

Unless under threat of extreme weather conditions, all dropbikes will remain in the designated geographic area. In the case of extreme weather conditions, all dropbikes will be moved by the local General Manager to our local storage locker.

Theft.

Dropbike is 100% liable for all hardware that is stolen. It is important to note that all bikes are equipped with a hard back lock, and anti-theft screws (which can't be unscrewed by regular tools, and require a specific Dropbike screwdriver). However, in the event that a bike is stolen, Dropbike will ensure that a new bike is placed in the geography.

We believe that with proper market saturation, there will be no marketplace to buy a stolen dropbike. The bike will be easily identifiable for use and will re-enter the market. Finally, as dropbike parts are non-standard, there will be no marketplace for them.

Broken Bikes.

As with any bike, dropbikes will need regular repair and maintenance.

In our app, riders will have the ability to mark a bike in need of repair. This dropbike will immediately be decommissioned for riding to the next rider seeking a dropbike. Bike Warriors are immediately notified to move the bike back to our repair shop or our General Manager is notified to collect the bike and transport it to our local storage locker. As mentioned, Bike Warriors also have the ability to mark a bike in need of repair.

Additionally, Dropbike will have a bike repair professional routinely check a portion of the dropbikes to ensure that they are in good condition for use.

Bike Repairs.

Dropbike believes in promoting local people and businesses. In addition to

employing local General Managers and Bike Warriors, Dropbike would love to create a mutually beneficial working relationship with a student repair group and/or local bike repair shops for the duration of our program.

Liability.

Users sign a waiver during registration. Also, Dropbike indemnifies the city so users deal exclusively with the company.

24/7 operations.

Dropbike's operational team monitors the real-time locations of each bicycle. There is a fast response team to pick up any bicycles left in inappropriate places. Based on the chain of custody, Dropbike is able to effectively and efficiently identify the individual responsible.

GPS monitoring.

Dropbike's third-generation bikes are equipped with onboard GPS units. An operational backend monitors these units to ensure bikes are not placed in inappropriate locations. If they are, our on-the-ground operations team is on-call to ensure that bicycles are left appropriately.

Chain of custody.

For security reasons, Dropbike tracks which users were using which bicycles at a given time throughout the day, week or month. This allows Dropbike to pinpoint any negative behaviour and community misconduct. We will then assign responsibility, warn and/or potentially remove users engaging in misconduct.

Sharing user data with the city.

Dropbike offers an unprecedented opportunity for governments to better understand their cycling infrastructure. In a formal partnership, Dropbike shares relevant (anonymized) data that helps cities make better decisions.

Engineering care.

Dropbike is a software company in an industry traditionally slowed by expensive, physical docks. In a software-driven bike share model, it is imperative to incentivize positive user behaviour.

Dropbike uses a combination of rewards, Havens, negative reinforcements and other user experience strategies to nudge user behaviour in a positive direction. We strongly believe in leveraging technology to help build a community that respects the bikes and the city in which they are used.

Report to Council



Date: January 15, 2018
File: 1110-61-009
To: City Manager
From: J. Säufferer, Manager, Real Estate Services
Subject: Road Closure and Sale – Adjacent to 1969 Cross Road
Report Prepared by: A. Warrender, Property Officer Specialist

Recommendation:

THAT Council receives, for information, the Report from the Manager, Real Estate Services dated January 15, 2018, recommending that Council adopt the proposed road closure adjacent to 1969 Cross Road;

AND THAT Bylaw No.11534, being the proposed road closure adjacent to 1969 Cross Road, be given reading consideration.

Purpose:

To close a 0.148-hectare portion of excess laneway adjacent to 1969 Cross Road.

Background:

The proposed road closure area is an unconstructed road right of way left over from previous plans for a Glenmore Bypass. City staff have been working with the adjacent owners to consolidate the remnant road area with the adjacent properties to create a comprehensive development site.

Legal/Statutory Authority:

Section 26 and 40, Community Charter

Considerations not applicable to this report:

Legal/Statutory Procedural Requirements:

Existing Policy:

Financial/Budgetary Considerations:

Internal Circulation:

Personnel Implications:

External Agency/Public Comments:
Communications Comments:
Alternate Recommendation:

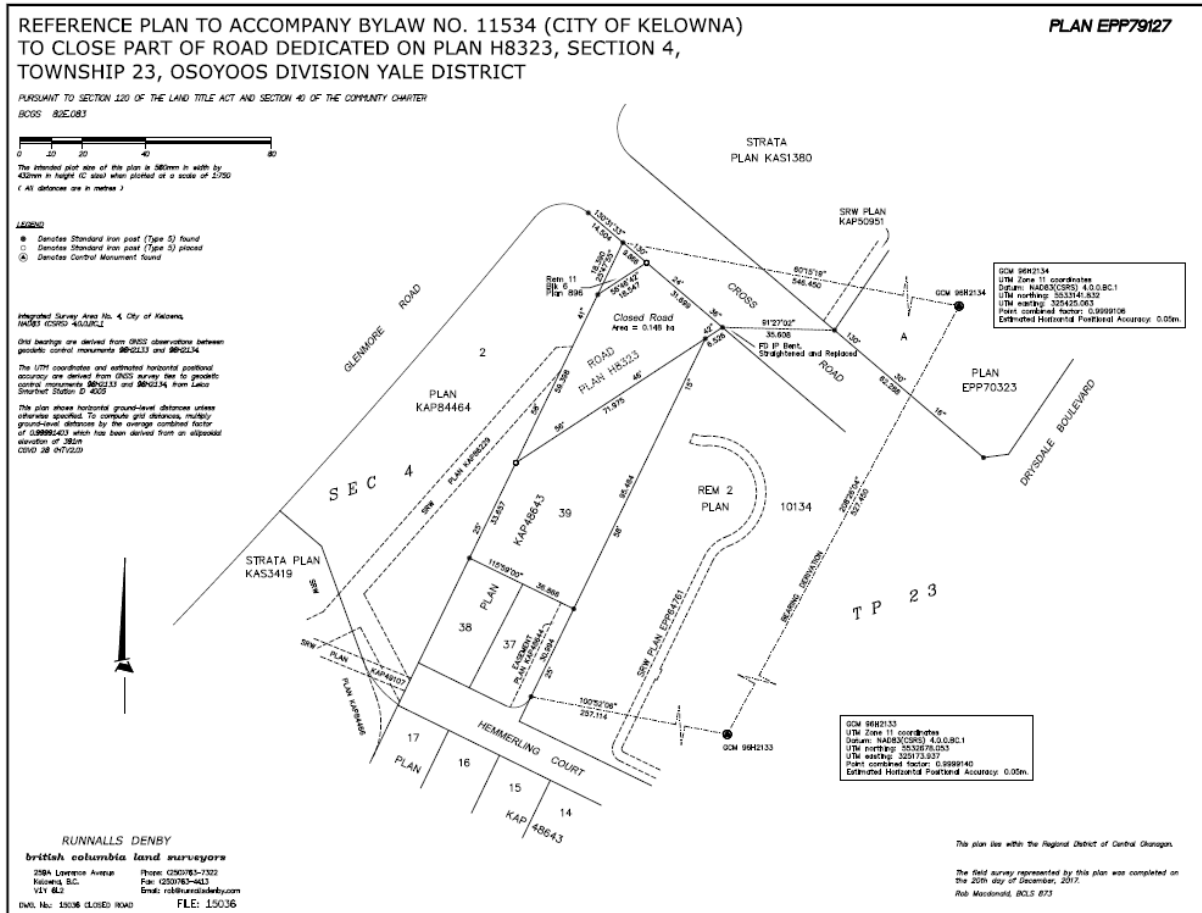
Submitted by: J. Säufferer, Manager, Real Estate Services

Approved for inclusion: D. Edstrom, Director, Real Estate

Attachments: 1. Schedule A – Survey Plan

cc: G. Foy, Manager, Transportation & Mobility
J. Kay, Manager, Development Engineering
T. Barton, Manager, Urban Planning

Schedule A



CITY OF KELOWNA

BYLAW NO. 11534

Road Closure and Removal of Highway Dedication Bylaw (Portion of Cross Road)

A bylaw pursuant to Section 40 of the Community Charter to authorize the City to permanently close and remove the highway dedication of a portion of highway on Cross Road

NOW THEREFORE, the Municipal Council of the City of Kelowna, in open meeting assembled, hereby enacts as follows:

1. That portion of highway attached as Schedule “A” comprising 0.148 ha shown in bold black as Closed Road on the Reference Plan EPP79127, Rob prepared by Rob Macdonald, B.C.L.S., is hereby stopped up and closed to traffic and the highway dedication removed.
2. The Mayor and City Clerk of the City of Kelowna are hereby authorized to execute such conveyances, titles, survey plans, forms and other documents on behalf of the said City as may be necessary for the purposes aforesaid.

Read a first, second and third time by the Municipal Council this

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

CITY OF KELOWNA

BYLAW NO. 11516

Road Closure and Removal of Highway Dedication Bylaw **(Portion of Queensway Avenue)**

A bylaw pursuant to Section 40 of the Community Charter to authorize the City to permanently close and remove the highway dedication of a portion of highway on Queensway Avenue

NOW THEREFORE, the Municipal Council of the City of Kelowna, in open meeting assembled, hereby enacts as follows:

1. That portion of highway attached as Schedule "A" comprising 115m² shown in bold black as Closed Road on the Reference Plan EPP77919 prepared by Mark Cahill, B.C.L.S., is hereby stopped up and closed to traffic and the highway dedication removed.
2. The Mayor and City Clerk of the City of Kelowna are hereby authorized to execute such conveyances, titles, survey plans, forms and other documents on behalf of the said City as may be necessary for the purposes aforesaid.

Read a first, second and third time by the Municipal Council this 8th day of January, 2018.

Approved Pursuant to Section 41(3) of the Community Charter this 9th day of January, 2018.

Audrie Henry
(Approving Officer-Ministry of Transportation)

Adopted by the Municipal Council of the City of Kelowna this

Mayor

City Clerk

Schedule "A"

